ALTERNATIVES ANALYSIS

FOR

LANDING ROAD BRIDGE OVER NJ TRANSIT AND MORRIS CANAL

MORRIS COUNTY, NEW JERSEY

Prepared for

Morris County Department of Public Works Morristown, New Jersey

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Alternatives Analysis Executive Summary

EXECUTIVE SUMMARY

This Alternatives Analysis report documents the selection of the preferred alternative for the proposed Landing Road Bridge Project, providing information on the alternatives considered for the project and their effectiveness in addressing the issues generating the need for the project. The report includes a summary of the anticipated effects of the alternatives on socioeconomic resources, right-of-way, natural resources, cultural resources, and hazardous substances, as well as a summary of estimated project costs. This project, while utilizing both state and federal funding in its early stages, will now use only state funding as it moves forward into design and through construction.

The historic significance of the Landing Road Bridge (County Bridge No. 73) requires that preliminary cultural resources information be a focus of this report. The bridge is a contributing element within the Morris Canal Historic District, which was listed on the New Jersey Register of Historic Places on November 16th, 1973 and on the National Register of Historic Places on October 1st, 1974. The Landing Road Bridge was also determined individually eligible for listing on the New Jersey and National Registers of Historic Places in a SHPO Opinion on February 25, 1994. The bridge is also considered a key contributing resource to the Old Main, Delaware, Lackawanna& Western Railroad Historic District. The New Jersey Register of Historic Places Act requires that any undertaking that will "encroach upon, damage or destroy" a resource listed on the New Jersey Register is subject to review. This review commences with the submission of an Application for Project Authorization to the New Jersey Historic Preservation Office, with a determination of Encroachment or No Encroachment to result. In the event that the project is ruled to be an Encroachment, a further review by the New Jersey Historic Sites Council will be required. The process concludes with final action being taken by the Commissioner of the Department of Environmental Protection based on recommendations provided by the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council.

The bridge was constructed in 1907 and carries Landing Road over the NJ TRANSIT Railroad and the former Morris Canal in Roxbury Township, Morris County, New Jersey. The project is located near the town of Landing in the western portion of Morris County in north-central New Jersey. Landing lies at the southwestern tip of Lake Hopatcong, a portion of which is bisected by the Morris-Sussex county line. Landing Road extends from Interstate Route 80 to the northwest approximately 0.8 miles to the southern edge of Lake Hopatcong. Landing Road travels over the former Delaware, Lackawanna and Western Railroad (currently the NJ TRANSIT Montclair-Boonton Line) via the Landing Road Bridge (County Bridge No. 73) and intersects with Lakeside Boulevard (County Route 602) and Mount Arlington Boulevard. The Landing Road Bridge provides one of the few major access points for residents on both the east and west shores of Lake Hopatcong. Lakeside Boulevard provides access to the Landing Road Bridge from the north and to Mount Arlington Boulevard from the south.

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A July 2003 engineering evaluation of the bridge concluded that the bridge is structurally deficient due to the "poor" condition of the superstructure and the "serious" condition of the substructure. In addition, the bridge was determined to be functionally obsolete due to inadequate deck geometry. Subsequent evaluations, including one conducted in July 2009 & October 2011, have resulted in the same conclusions. Although the latest inspection report dated September 2013 has upgraded the bridge to "fair" condition for both the superstructure and substructure due to recent concrete repairs that have been made, the inadequate deck geometry of the bridge continues to be classified as functionally obsolete (see Appendix B for the September 2013 evaluation). To respond to these overall needs, Morris County began evaluating options for replacing or rehabilitating the existing bridge.

The results of the 2003 evaluation were presented to the Roxbury Township Council at a Workshop Meeting on April 14, 2003, and the Council issued a resolution supporting the Replacement Alternative (Alternative No. 7-C) for the project. A copy of the resolution of support issued by the Roxbury Township Council is included in Appendix D of this report. The project was again presented on June 9, 2015 to the Roxbury Township Council updating them on the status of the project with the various alternates studied. On August 12, 2015 a Public Information was held at the Roxbury Township Municipal Building with Morris County and the RBA Group and alternate 7-C was again the recommended alternate presented and received support from the public and Township, Resolution 2015-310 was adopted on September 29, 2015 by the Township of Roxbury supporting this alternate and is included in Appendix D of this report.

The Township of Roxbury has requested that the sidewalk be extended on the south eastern side of the bridge to the end of the project limits and across the bridge on both sides. Additionally on Lakeside Boulevard the sidewalk will be widened and extended in front of the wall at Lake Hopatcong and the current fence replaced with new aesthetical pleasing fence. A schematic of Alternate 7-C with the sidewalk improvements is included in Appendix I.

The Replacement Alternative (Alignment No. 7-C) was selected as the Initially Preferred Alternative (IPA) because it would best address the project needs and would have a limited number of environmental impacts, at a comparatively low cost. The Replacement Alternative would improve safety and traffic operations in the project area, provide a new widened bridge meeting current design standards, and allow maintenance of traffic on the existing bridge while the new bridge is being constructed. The Replacement Alternative is recommended for the following reasons:

- 1. The alternative would satisfy the traffic operations needs of the project area.
- 2. The alternative is compatible with the proposed widening of Landing Road to I-80.
- 3. The alternative would improve the safety of the project area.
- 4. The new bridge would meet current structural design standards.
- 5. The proposed construction operations would not impact existing wetlands or regulated hazardous and solid waste sites in the project area.
- 6. The construction cost of the Replacement Alternative is significantly lower than that of the Bypass Alternative.
- 7. Traffic could be maintained during construction of the Replacement Alternative, although delays and increased travel time and distance are likely.

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8. The alternative is compatible with the streetscape improvements project being undertaken by the Roxbury Township.

The IPA involves the demolition of the existing historic bridge. Consultation with the New Jersey Historic Preservation Office has preliminarily determined that the IPA will represent in an Encroachment under the New Jersey Register of Historic Places Act as the Landing Road Bridge is a contributing resource within the Morris Canal Historic District. In addition, archaeological features contributing to the eligibility of the Morris Canal Historic District may be impacted during construction, and this would also represent an Encroachment under the New Jersey Register of Historic Places Act.

The project team anticipates continuing consultation with the New Jersey Historic Preservation Office and with the New Jersey Historic Sites Council to ensure that the design of the new structure reflects the shape and appearance of the existing bridge to the greatest extent possible and that other roadway and landscape design elements are consistent with the character of the historic district. In addition, the project team will develop an archaeological monitoring plan as the initial phase of work necessary to address the above noted potential for archaeological resources associated with the district, again through continuing consultation with the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council.

Morris County will also consult with the Roxbury Township Historic Advisory Committee and the Landing Gateway Committee during design of the project to comply with their recommendations of a two span arch structure, the proposed railing design, concrete texture of the surfaces to mimic the historic look of the original bridge, lighting, plantings, pedestrian crossing, bollards, sidewalks, parking and the fence details at the lake.

This report was revised by The RBA Group in February of 2015 to reflect the project's planned use of only state funding for design and construction and to provide an updated list (with an associated graphic) of property owners located within 200 feet of the project area. Morris County further revised the report in October 2015 to reflect SHPO comments in their letter dated October 9, 2015.

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SECTION 1 - INTRODUCTION / SPONSOR

1.1 SPONSOR

The Landing Road Bridge Project considered various alternatives for the replacement or rehabilitation of existing County Bridge No. 73. This bridge was constructed in 1907 and carries Landing Road over the New Jersey TRANSIT (NJ TRANSIT) Montclair-Boonton Line railroad and the former Morris Canal in Roxbury Township, Morris County, New Jersey.

The sponsor of the Landing Road Bridge Project and this alternatives analysis is the Morris County Department of Planning & Public Works in the State of New Jersey. The work under this project also includes coordination with Morris County, the New Jersey Department of Transportation (NJDOT), the North Jersey Transportation Planning Authority (NJTPA), the Federal Highway Administration (FHWA), and other affected local groups, officials, and residents. Because the Landing Road Bridge continues to deteriorate, the County of Morris requested and obtained federal funding through NJTPA to complete this alternatives analysis, which examines alternatives considered throughout the course of the project and documents the selection of Alternative 7-C as the Initially Preferred Alternative (IPA).

1.2 PURPOSE OF THE ALTERNATIVES ANALYSIS

The purpose of this report is to document the evaluation of alternatives considered for the Landing Road Bridge Project. The evaluation focuses on those alternatives that were considered reasonable from a cost, engineering, and environmental perspective. For the evaluation, each alternative was assessed to determine how well it met the project purpose and need, its potential impacts on the natural and built environments, and its anticipated costs and affordability. Based on this information, input from the local community and other interested parties, and Resolutions of Support from the Roxbury Township Council, Alternative 7-C was selected as the IPA. Alternative 7-C involves the replacement of County Bridge No. 1400-073 along a similar alignment.

Federal funds for future phases of the project were formerly to be provided through NJTPA in partnership with NJDOT – it has now been determined that these future phases will be financed using only State funding. State regulations require that the lead agencies take into account the effects of the project on any recognized historic properties during the planning stages of the project.

The New Jersey Register of Historic Places Act of 1970 (Chapter 268 and Procedures for Project Authorization) outlines the procedures to follow if a proposed project will encroach on a property listed in the New Jersey Register of Historic Places. These procedures include consultation with the New Jersey Historic Preservation Office and other interested parties regarding the Area of Potential Effect (APE) (i.e., the area within which the project undertaking may have direct or indirect impacts on listed or eligible properties). If the project will encroach on historic properties, then the Act outlines the procedures to be followed, including:

- (1) Preparation of documentation to assess project impacts (encroachment) on the property;
- (2) Attendance at public meetings;
- (3) Consultation with local governments, property owners, historic preservation groups, and others; and
- (4) Preparation of an application for project authorization for review by the SHPO and the New Jersey Historic Sites Council. Based on the recommendations of the Historic Sites Council, the Commissioner of the Department of Environmental Protection may authorize the encroachment with or without conditions or deny the application with specific reasons.

While a number of historically significant cultural resources have been identified within the APE for the Landing Road Bridge Project, only one – the Morris Canal Historic District – is listed on the New Jersey Register of Historic Places. A *Phase 1a Archaeological Investigation* completed for the project in July of 2003 identified areas of archaeological potential associated with the Morris Canal Historic District and recommended that these areas be monitored during construction of the project. The three areas noted are area 1, around the eastern abutment, area 2, off the south-eastern corner of the bridge at the location of the former post office and area 3, the western abutment and to the south of the western abutment. The New Jersey Historic Preservation Office concurred with the findings of this report and the proposed strategy for addressing these potential archaeological issues in a letter dated February 7, 2006.

1.3 CONTENTS OF THE ALTERNATIVES ANALYSIS

This report documents the selection of the preferred alternative for the proposed project, providing information on the alternatives considered for the project and their effectiveness in addressing the issues generating the need for the project. The results of these evaluations and comparative analyses of the alternatives considered are compiled in the report's remaining sections:

Section 2 - Project Location

Section 3 - Detailed Description of Structure

Section 4 - Purpose and Need

Section 5 - Project Alternatives

Section 6 - Environmental Impacts

Section 7 - Evaluation of Alternatives

Section 8 - Recommendation

Section 9 - References

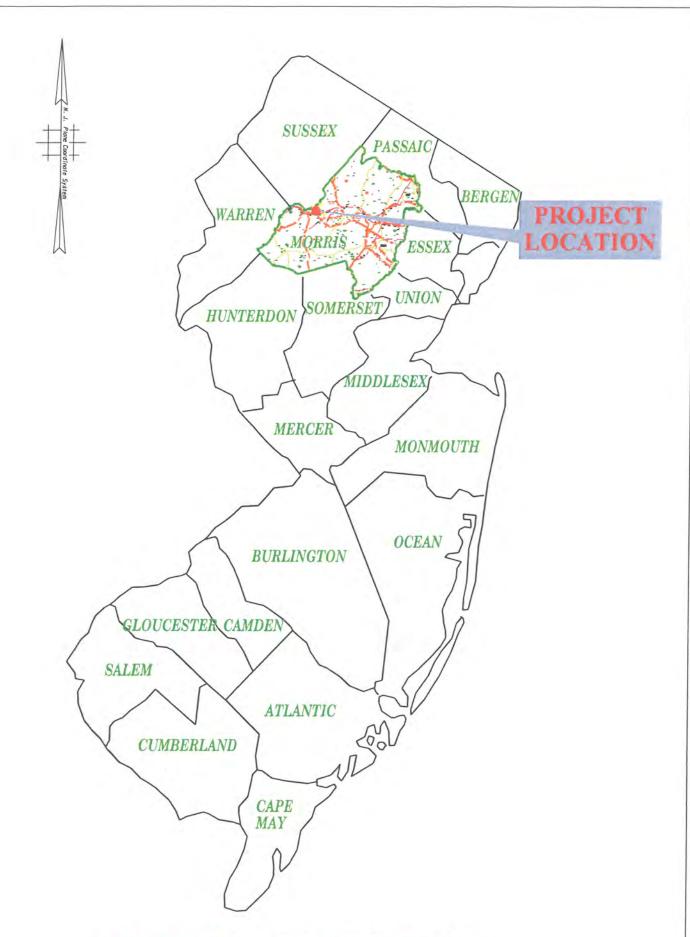
SECTION 2 - PROJECT LOCATION

The Landing Road Bridge (County Bridge No. 73) is located in Roxbury Township near the town of Landing, in the western portion of Morris County in north central New Jersey. Landing lies at the southwestern tip of Lake Hopatcong, a portion of which is bisected by the Morris-Sussex county line.

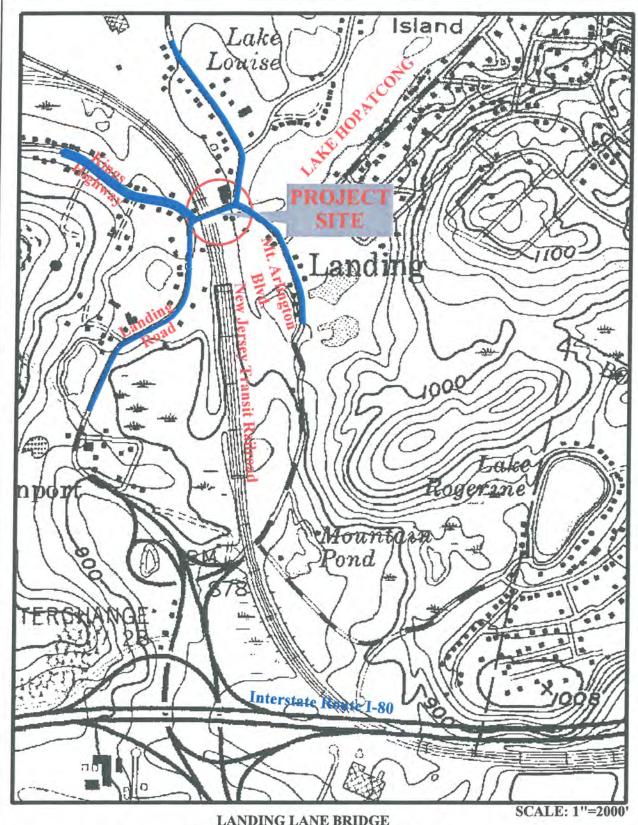
The Landing Road Bridge carries Landing Road, also known as Morris County Route 631, over the dual-track NJ TRANSIT Boonton Line and the former Morris Canal. Before crossing on the bridge, Landing Road extends approximately 0.8 miles in a northerly direction from its interchange with Route I-80 in the south to its intersection with Kings Highway at the southerly approach to the bridge. Landing Road then crosses the bridge in a general west-to-east direction before terminating at a signalized intersection with Lakeside Boulevard (County Route 602) and Mount Arlington Boulevard near the southern tip of Lake Hopatcong. Lakeside Boulevard runs to the north from that intersection while Mount Arlington Boulevard runs to the south.

Canal Street, which is accessed from its intersection with Landing Road immediately east of the bridge, runs southeast to northwest under the Landing Road Bridge on fill along the former alignment of the Morris Canal. Also located under the bridge along the former Morris Canal is a paved parking area serving the track-level Lake Hopatcong NJ TRANSIT station.

A general location map is shown in Figure 2-1 and a more detailed project location map is provided in Figure 2-2.



GENERAL LOCATION MAP



LANDING LANE BRIDGE PROJECT AREA TOPOGRAPHIC MAP TOWNSHIP OF ROXBURY, MORRIS COUNTY



PARSONS TRANSPORTATION GROUP, INC.

FIGURE 2-2 Source: Stanhope, NJ USGS 7.5 Minute Series Map, Quad #26

SECTION 3 - DETAILED DESCRIPTION OF STRUCTURE

The existing bridge, constructed in 1907, is approximately 136 feet long. The superstructure consists of twin reinforced concrete arches and a reinforced concrete deck with an asphalt overlay. The bridge has a curb-to-curb width of 29.6 feet and an overall width of 38.8 feet, including a 6-foot wide sidewalk on the north side and concrete parapets on both sides. The substructure is comprised of concrete abutments, which are at a 90-degree angle with the bridge deck and wingwalls. The Landing Road Bridge provides one of the few major access points for residents on both the east and west shores of Lake Hopatcong. Landing Road, Lakeside Boulevard and Mount Arlington Boulevard all meet at the eastern end of Landing Road Bridge; connections between the bridge and the west shore of Lake Hopatcong are provided via Lakeside Boulevard, while connections between the bridge and the east shore are provided via Mount Arlington Boulevard.

Photographs of the bridge are provided at the end of this section.

3.1 TECHNICAL BACKGROUND

Arch bridges are one of the oldest types of bridges and have great natural strength. They rely on the concept that the arch displaces half the weight of the structure diagonally to either side, rather than the entire weight pulling straight down on the supports. The most commonly used material for arch bridges is reinforced concrete, due to its ability to resist compression, tension, and torsion stresses.

The existing Landing Road Bridge is a twin reinforced concrete spandrel arch. The riding surface consists of a 5-inch base course with a bituminous concrete wearing surface over it. The base course and riding surface rest on earthen fill of various thicknesses. The spandrel area (i.e., the area between the arch and the roadway) is filled with earth fill material and retained by the vertical spandrel walls. The arch member is called a ring or a barrel and is continuous between the spandrel walls. The arch and its associated members supported by the arch are the primary superstructure elements. The arch itself is the primary load-carrying element of the superstructure. The primary reinforcing steel in the arch ring follows the shape of the arch from support to support and consists of a mat of reinforcing steel on both the top and bottom surfaces of the arch. The spandrel walls are designed to retain the back fill material. The primary tension steel for the walls is at the back, or unexposed, face of the wall. The front, or outside, face wall is reinforced in both directions with temperature and shrinkage steel.

The arch is supported at the ends by reinforced concrete abutments. The purpose of the abutments and pier is to receive the loads transmitted through the arch ring from the riding surface and to fill and redistribute these loads to the ground below. The Landing Road Bridge has two single abutments at each end of the bridge and a common center pier shared by both arches. The abutments and pier are at 90-degree angles with the bridge deck and wingwalls.

The arches carry Landing Road over the NJ TRANSIT Montclair-Boonton Line and a parking area (on the site of the former Morris Canal) and provide a 64-foot clear span from face of abutment to face of pier in both spans. The arches provide an 11-foot 5-inch rise from the spring line to the crown.

Figures 3-1 and 3-2 show a typical section and the general elevation of the existing bridge.

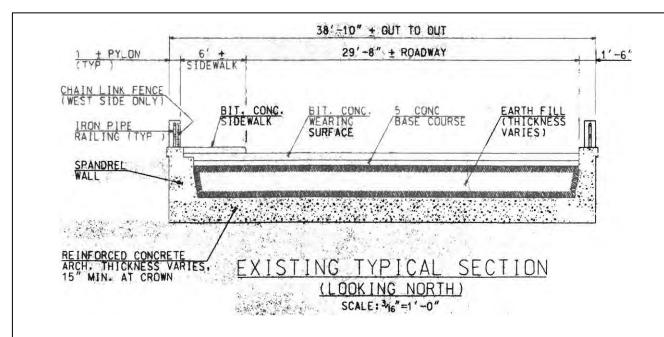


Figure 3-1: General Nomenclature of Existing Landing Road Bridge

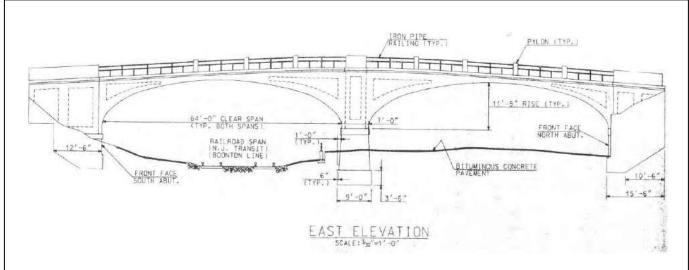


Figure 3-2: General Elevation of the Existing Landing Road Bridge

3.2 TECHNICAL INFORMATION

The existing Landing Road Bridge has remained largely unchanged since its construction in 1907. The undersides of the arch rings and fascias were treated with gunite in 1972 as a maintenance measure. In January 2000, the underside of the arch ring, spandrel wall fascias, pier, and abutments had the gunite treatment removed; the exposed concrete surface sounded using chipping hammers; areas of hollow sounding, delaminated, or spalled concrete removed to sound concrete; deteriorated reinforcing bars spliced with new bars; and areas patched with epoxy mortar to arrest falling concrete and gunite material from the underside of the arch.

Utility lines are present both above and below the deck of the bridge. Electrical lines are present along the east side of the center pier, and overhead electrical, telephone and cable TV lines run along the north fascia of the bridge.

3.3 FIELD INSPECTION

3.3.1 Superstructure

As previously mentioned, the superstructure for both spans of the existing Landing Road Bridge is a reinforced concrete spandrel arch. The arch thickness varies, with a 15-inch deep minimum at the crown and increasing thickness towards the abutments and pier.

Based on the County's maintenance records, the underside of the arch was treated with a gunite material (maintenance measures) in 1972, and major repairs (by contract) were made to the underside of the arch, abutment breastwalls, pier walls and spandrel walls in 2000. Since 2000, the following maintenance work has been performed by the County:

Work Description	Start Date	Stop Date	Equipment Cost	Labor Cost	Material Cost
Place 6 - 6' water filled barriers in front of the damaged section of the rail ontop of sidewalk. The rail was damaged by a vehicle accident on 11-13-2004.	11/16/2004	11/16/2004	\$160.00	\$1,080.24	\$0.00
Cut plates, pipe, guide rail post in preparation for the rail repair. Prefabricate sections of pipe rail and posts in the yard. Replace the damaged sections of rail with prefabricated section. Repair the concrete balistrades. Install galvanized guide rail in front of the entire existing pipe rail.	11/17/2004	12/13/2004	\$5,160.00	\$11,882.64	\$4,317.90
Remove all unsound sections of concrete from the Arch over the railroad tracks. Remove the fallen material from the tracks.	6/5/2009	6/5/2009	\$180.00	\$665.72	\$0.00
Remove the asphalt and deteriorated concrete from a 3' x 3' area over the arch under direction of an engineering consultant. He wants to see the extent of the deteriorated concrete over the arch. Mix and pour concrete to fill the hole to the existing sidewalk height. Place a road plate over the concrete for the night. Remove the road plate in the morning.	12/11/2009	12/12/2009	\$840.00	\$1,860.58	\$61.35
Clean the debris from the sidewalks and curb line. Asphalt patch the sidewalk where needed.	4/9/2010	4/9/2010	\$180.00	\$270.06	\$0.00

In September 1990, a bridge evaluation study report prepared by Maitra Associates, Inc. gave the bridge an overall condition rating of fair, with severe spalling and cracking in the underside of the arches.

In June 1991, four core samples were extracted from the underside of each arch at midspan. Five out of the eight cores crumbled during the coring drilling and extraction. The remaining three cores were tested for compression strength, and one of the cores failed before a load was registered on the compression machine. The other two cores obtained compression strengths of 4,000 and 6,000 psi. Chloride tests performed on the all samples revealed a very high level of chloride ion in the concrete, indicating electrochemical corrosion of the embedded reinforcing steel.

In December 1995, the seventh cycle inspection and evaluation report, prepared by Bettigole, found the superstructure in fair condition due to severe spalling, with exposed rusted reinforcement and medium to wide cracks with efflorescence and leaching. Rating calculations were performed under this cycle report using the Load Factor method. The mid-span crown was given an inventory rating of 65 tons for an HS truck, which exceeds the current design load of 36 tons for this configuration. The bridge also had a sufficiency rating of 20.0.

In May 1997, Storch Engineers performed a rehabilitation design study. This study found that replacement of the existing structure is necessary due to the deteriorated condition of the arches and the spandrel walls. The findings were based on core samples extracted from the arches, spandrel walls, abutments, pier and wingwalls, and once again several of the cores broke into several pieces after extraction. The remaining intact portions were tested for compressive strength with results from 2,700 to 5,150 psi. Two of the cores had chloride levels below the accepted threshold of two lb/cycle, and the four remaining cores exceeded the threshold with the highest level recorded at 7.56 lb/cycle. Freeze-thaw test showed that after 15 out of 25 freeze-thaw cycles, the cement paste in the tested core samples had completely separated from the course aggregate and broke into small pieces. This is an indication of very poor durability of the existing concrete.

The eleventh cycle report prepared in July 2003 by KS Engineers, P.C., rates the condition of the superstructure as poor due to cracking, scaling, and efflorescence throughout the arch intrados and spandrel walls. The bridge load rating of 65 tons for inventory and the sufficiency rating of 20.0 has not changed from the seventh cycle report.

The fourteenth cycle report prepared in July 2009 by S&R Engineers, P.C., rates the condition of the superstructure as poor due to cracking, scaling, leakage, and efflorescence throughout the arch intrados and spandrel walls. The intrados is reported to be contaminated up to 60% of the area. The bridge load rating of 65 tons for inventory has not changed from the eleventh cycle report. The sufficiency rating is now 19.0 compared to 20.0 in the eleventh cycle report.

The fifteenth cycle report prepared in October 2011 by Stantec Consulting Services, Inc., has upgraded the condition of the superstructure to fair due to the concrete repairs throughout both intrados and in the spandrel wall and coping at both spandrel walls. The bridge load rating of 65 tons for inventory has not changed from the fourteenth cycle report, but the sufficiency rating has improved to 58.5 compared to 19.0 in the fourteenth cycle report, presumably due to the number of interim repairs that have been made since that time.

The latest inspection, the sixteenth cycle report prepared in September 2013 by Stantec Consulting Services, Inc. rates the condition of the overall structure as fair due to the superstructure and substructure. The sufficiency rating still remains 58.3 and load rating of 65 tons.

3.3.2 Substructures

The existing center pier and both abutment and wingwalls are constructed of reinforced concrete.

Based on the County's maintenance records, the abutment breastwalls and pier stem walls were treated with a gunite material (maintenance measures) in 1972 and major concrete repairs (by contract) were made to the breastwall and pier walls in 2000 in conjunction with the superstructure repairs.

In December 1995, the seventh cycle inspection and evaluation report, prepared by Bettigole, gave the overall condition of the substructure as serious due to severe spalling and medium to wide cracks with large areas of efflorescence and leaching.

The eleventh cycle report prepared in July 2003 by KS Engineers, P.C., rates the condition of the substructure as serious due to medium to wide cracks and large areas of leaching, efflorescence, spalls, and hollow concrete.

The fourteenth cycle report prepared in July 2009 by S&R Engineers, P.C., rates the condition of the substructure as serious due to medium to wide cracks and large areas of leaching, efflorescence, spalls and hollow concrete on the pier and west abutment.

The fifteenth cycle report prepared in October 2011 by Stantec Consulting Services, Inc., has upgraded the substructure condition from serious to fair due to the concrete repairs in the coping and wall at the northwest edge and south face of the pier.

The latest inspection, the sixteenth cycle report prepared in September 2013 by Stantec Consulting Services, Inc., the substructure condition remains fair due to the large concrete patches and the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments.

3.3.3 Structure Geometry and Safety

Landing Road is classified as a minor urban arterial with an ADT of 37,079 vehicles per day. The existing Landing Road Bridge is considered functionally obsolete due to the substandard deck geometry. The curb-to-curb width of 29 feet 7 inches is less than the minimum NJDOT design width of 32 feet, which accommodates two 12-foot lanes and two 4-foot shoulders. The minimum width of traveled way and shoulders given by AASHTO for local bridges carrying more than 2,000 vehicles per day are 24 feet and 8 feet, respectively.

The Rehabilitation Design Study conducted by Storch Engineers in May 1997

recommended the structure be widened to accommodate an additional lane in the northbound direction. The study did not investigate the required number of lanes necessary to eliminate the existing traffic problem in the project area.

The bridge carrying capacity for inventory rating was calculated to be 65 tons for a HS20 truck at the crown of the arch in the seventh cycle report bridge inspection report. However, these ratings may be theoretical due to the condition of the concrete of the arch found during the past coring extraction operations, where core samples crumbled during the extraction process. There is currently no load limitation posted on the bridge.

3.4 GEOTECHNICAL (SUBSURFACE MATERIAL)

Geotechnical research information was collected from the NJDOT Bureau of Geotechnical Engineering, the New Jersey Soil Survey (Rutgers), and the Township of Roxbury. The bridge site is situated within the Appalachian Highlands subdivisions of the Piedmont Physiographic Province, which is one of six subdivisions covering southern New York and New Jersey. The bedrock in the Piedmont area consists of red shale and sandstones, interceded sandstones and siltstone, Lockatong argillites, and basalt (Wolf 1977).

Based on information obtained from the *Soil Survey of Morris County, New Jersey*, the soils at the project site consist of very stony sandy loams and Hibernia stony loam. The site is also covered with a thick mantle of glacial terminal moraine. The till can be reasonably classified as silty sand with gravel.

According to *Ground-Water Resources of Morris County, New Jersey* (Special Report No. 25, 1965), the ground water in Morris County occurs under unconfined or water table conditions. However, the lowland areas generally consist of consolidated rocks covered with unconsolidated deposits, which contain impermeable clay and silt beds. The capacity of wells in Morris County is classified as moderate to large. The water is generally acceptable for most uses with minimal to no treatment; however, there are hardness-forming constituents present.

3.5 TRAFFIC

Traffic volume data was collected at four locations in March 2011 (see Appendix H). This information consisted of both manual classification turning movement and Automatic Traffic Recorder (ATR) counts. Each count was collected in 15-minute intervals and summarized hourly. Morning and evening peak hour periods were identified from the combination of manual traffic counts and from the data collected by the ATRs installed on the bridge and on Mount Arlington Boulevard north of Shippenport Road. The morning peak was noted to occur between 7:00 AM and 8:00 AM, while the evening peak hour occurred between 5:00 PM and 6:00 PM. Listed below are the four locations that were counted:

Location # 1 - Landing Road and Kings Highway

Location # 2 – Landing Road and Lakeside Boulevard/Mt. Arlington Boulevard

Location #3 - Mt. Arlington Boulevard and Shippenport Road

Location #4 - Lakeside Boulevard and Kingsland Road

Utilizing the classification traffic volume data, capacity analyses were performed for existing conditions at the above locations for each of the peak periods. The method of analysis employed conforms to the Highway Capacity Manual 2000 Edition and was facilitated with computer software. Level of service (LOS) and delays were calculated to evaluate the operating characteristics for the four intersections under 2011 traffic demands. These results are presented in Table 3-1.

TABLE 3-1: 2011 Existing Condition Delay and LOS												
Intersection Number			A	AM Pea	k			I	PM Peal	k		
INT. #1	<u>E</u> :	<u>B</u>	NB	<u>LT</u>			<u>EB</u>		<u>NB</u>	<u>LT</u>		
	Delay	LOS	Delay	LOS			Delay	LOS	Delay	LOS		
	37.0"	Е	13.1"	В			52.3"	F	9.1"	A		
INT. #2	WB		<u>NB</u>		<u>SB</u>		$\underline{\mathrm{WB}}$		<u>NB</u>		<u>SB</u>	
	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
	23.8"	C	12.0"	В	30.4"	C	25.8"	C	19.9"	В	16.1"	В
INT. #3	WB		<u>NB</u>				W	В	<u>N</u>	<u>B</u>		
	Delay	LOS	Delay	LOS			Delay	LOS	Delay	LOS		
	8.0"	A	12.5"	В			7.9"	A	15.4"	C		
INT. #4	WB		NB LT		SB LT		W	В	<u>NB</u>	<u>LT</u>	SB	LT
	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
	23.8"	D	13.7"	В	7.9"	A	39.2"	E	9.0"	A	12.6"	В

As presented in the table, left turning movements onto Landing Road Bridge from Kings Highway (i.e., EB movement at Intersection #1) experience a Level of Service "E" during the morning peak travel period and Level of Service "F" during the evening peak. In addition, the westbound approach leg of Kingsland Road at its intersection with Lakeside Boulevard (i.e., WB movement at Intersection #4) experiences a Level of Service "E" during the evening peak travel period.

Each of the four intersections was also evaluated in the No-Build condition for the project horizon year of 2031 (see Table 3-2). Under this scenario, three out of the four intersections studied will experience extraordinarily long delays, at least on certain approaches. At the signalized intersection of Landing Road and Lakeside Boulevard/Mt Arlington Boulevard, southbound approaches along Lakeside Boulevard (i.e., SB movement at Intersection #2) during the morning peak hour would exceed the maximum saturated flow rate of 1,900 vehicles per hour (vph) and will require capacity increase in order to improve from a Level of Service "F". In addition, the left turning movements onto Landing Road Bridge from Kings Highway (i.e., EB movement at Intersection #1) and onto Lakeside Boulevard from Kingsland Road (i.e., WB movement at Intersection #4) would be Level of Service "F" during both the morning and evening peak periods. These left turning vehicles would require substantial gaps in the through traffic in order to perform these movements safely during these periods.

TABLE 3-2: 2031 No-Build Condition Delay and LOS										
Intersection Number		AM Pea	k	PM Peak						
INT. #1	EB	NB LT		EB	NB LT					
	Delay LOS	Delay LOS		Delay LOS	Delay LOS					
	102.9" F	18.0" C		663.2" F	10.0" A					
INT. #2	<u>WB</u>	<u>NB</u>	<u>SB</u>	<u>WB</u>	<u>NB</u>	<u>SB</u>				

	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
	25.6"	C	12.3"	В	161.5"	F	34.9"	C	34.6"	C	24.0"	В
INT. #3	W	<u>B</u>	<u>N</u>	<u>B</u>			W	<u>B</u>	<u>N</u>	<u>B</u>		
	Delay	LOS	Delay	LOS			Delay	LOS	Delay	LOS		
	8.3"	A	16.5"	C			8.2"	A	31.7"	E		
INT. #4	W	<u>B</u>	NB	<u>LT</u>	SB	<u>LT</u>	W	<u>B</u>	NB	<u>LT</u>	<u>S</u>	<u>B</u>
	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
	65.5"	F	19.5"	C	8.3"	A	177.0	, F	10.0"	Α	17.1"	C

It should also be noted that the existing layout of the signalized intersection of Landing Road and Lakeside Boulevard / Mt. Arlington Boulevard does not have dedicated lanes for left turning vehicles. Increasing traffic volumes combined with the delay from left turning vehicles has made this intersection a major congestion point for the roadway network. It is also believed that many of the accidents experienced at this intersection occur because the signal layout does not accurately reflect the existing traffic needs.

Overall, the proposed project (assuming the Replacement Alternative) affords a design that is expected to provide acceptable levels of service at the study area intersections through the design year of 2031.

Accident data collected by the Roxbury Township Police Department for this segment of Landing Road is included in Appendix E. This data has been collected for Landing Intersection (which is the three-roadway intersection of Lakeside Blvd., Mt. Arlington Blvd. and Landing Rd.) and Landing Rd. (from the Landing Intersection to the Landing Rd. Bridge).

A review of the data indicates the following:

- At Landing Intersection most accidents occur during the months of June, July and August, while at Landing Rd. most accidents occur during the months of May and June.
- At Landing Intersection most accidents occur between the hours of 8 AM and 9 AM, while at Landing Rd. most accidents occur between the hours of 5 PM and 6 PM.
- At both locations most accidents involved vehicles struck from behind while stopped in traffic.

In April of 2015 Morris County took traffic counts at the three intersections and the following counts were documented:

Mount Arlington Blvd (616) - N of Shippenport Rd

Dates: 5/26 - 5/29/2015

AADT: 10,464

AM Peak (8-9am): 672 PM Peak (5-6pm): 900

Lakeside Blvd (631) - N of Landing Rd

Dates: 4/28 - 5/1/2015

AADT: 23,702

AM Peak (7-8am): 1,751 PM Peak (5-6pm): 1,915 Dates: 4/28 - 5/1/2015

AADT: 577

AM Peak (6-7am): 47 PM Peak (5-6pm): 47

Landing Rd (631) - N of Shippenport Rd

Dates: 10/30 - 11/1/2013 (traffic counts for this intersection was done in 2013 not 2015)

AADT: 21,350

AM Peak (7-8am): 1,466 PM Peak (5-6pm): 1,553

3.6 HISTORY / SIGNIFICANCE

3.6.1 Bridge Significance

The Landing Road Bridge is a two-span reinforced concrete deck arch bridge with a simple metal railing. The bridge was designed by the Delaware, Lackawanna and Western (DL&W) Railroad Company and was constructed in 1907 by Salmon Brothers Construction Company of Netcong, New Jersey. The bridge was built to replace an earlier iron frame bridge with a wooden plank deck that had carried traffic over the DL&W tracks and the adjacent Morris Canal. The bridge was made necessary by the crossing of a trolley to the end of Lake Hopatcong and King's Store.

The Landing Road Bridge represents the earliest example of an earth-filled concrete deck arch bridge in Morris County and is the only two-span example of the type sited within the county. In 1994 the bridge received a State Historic Preservation Officer Opinion of Eligibility as an individual resource that met the requirements of both the New Jersey and National Registers of Historic Places under Criterion C. It was also recognized at this time as a contributing resource within the Morris Canal Historic District, which was listed on the New Jersey Register of Historic Places in 1973 and the National Register of Historic Places in 1974 (see Hall/Zerbe to Hajtovik, February 25th, 1994). It is the designation of the bridge as a contributing resource within the New Jersey Register-listed Morris Canal Historic District that necessitates project compliance with the requirements of the New Jersey Register of Historic Places Act.

The Landing Road Bridge is over 100 years old, and the superstructure and substructure have not been altered or modified from its conception in 1907. The longevity of the bridge can be contributed to the workmanship and materials of the era. The integrity of the bridge has survived years of environmental and weather exposure; however, with age comes wear and tear on the components of the bridge, as is the case with the existing Landing Road Bridge.

3.6.2. Project History

In the 1970s and 1980s, the County of Morris constructed major improvements to Lakeside Boulevard from the Sussex-Morris county line to its intersection with Landing Road. In addition, substantial improvements to Mount Arlington Boulevard and Howard Boulevard were made using federal funding, State Aid Road System grants, and County resources. Proposals to realign Landing Road to the west of its present location in the late 1970s and early 1980s failed due to lack of sufficient federal funding, environmental problems, and lack of

community support. In 1983, NJDOT retained Elam & Popoff, P.A. to investigate alternative alignments for the Landing Road Bridge. This investigation was concluded in May of 1985 with the submission of a report that studied six alternative alignments.

In 1992, the County retained Storch Engineers to complete the Elam & Popoff study and to investigate other alternatives as required. The Storch study, which was completed in March 1996, developed and studied eight new improvement alternatives, and also evaluated two of the six alignments studied in the 1985 Elam & Popoff report. The Storch study recommended the construction of a new bridge as an extension of Lakeside Boulevard joining Landing Road south of the existing bridge. The existing bridge was to remain for local access to Landing Road and King's Highway. However, as with previous studies, the Storch recommendations were never implemented.

Because the Landing Road Bridge continues to deteriorate, the County of Morris requested and obtained federal funding through NJTPA to complete a new alternatives analysis for the project, including the development of alternatives not considered in any of the previous studies. Steinman Boynton Gronquist & Birdsall (now Parsons) was selected in April 1998 to complete the study and select an alternative for implementation; this report is part of this ongoing effort. The work under this project also includes coordination with Morris County, NJDOT, NJTPA, FHWA and other affected local groups, officials, and residents to ensure support for the final alignment selected.

In the interim, the serious condition of the existing bridge necessitated immediate rehabilitation measures to prevent further deterioration and to extend the useful life of the bridge until a replacement structure is constructed. A *Rehabilitation Design Study* (May 1997) was performed by Storch Engineers under a separate agreement. The *Rehabilitation and Design Study* concluded that replacement of the existing structure is necessary due to the deteriorated conditions of the arches and spandrel walls. These findings were based on core samples taken from the arches and spandrel walls, which revealed high levels of chloride contamination causing corrosion of the embedded reinforcing steel. It is to be noted that five of the eight cores taken crumbled during the extraction operations, supporting the classification that the existing structure is severely deteriorated. The study also found that the structure should be wider to accommodate an additional lane in the northbound direction. The rehabilitation study did not investigate the required number of lanes that the replacement bridge would need to eliminate existing traffic problems in the project area.

Due to the different and distinct proposed work items between this contract and the rehabilitation work recommended by Storch Engineers, the County decided to proceed with each of the two contracts separately.

In coordination with the New Jersey Historic Preservation Office (NJHPO), two (2) interim rehabilitation contracts were completed on the Landing Road Bridge using County funds:

• From April 2000 to June 2000 unsound concrete on the underside of the arches was removed and repairs were performed at a cost of \$289,050.00.

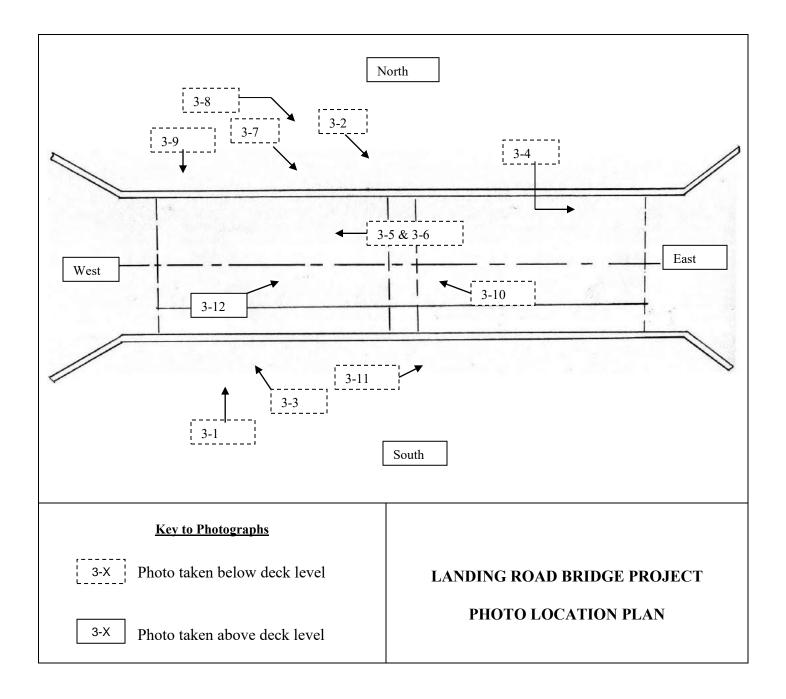
From September 2010 to November 2012 all unsound concrete was removed in the fascias and repairs were performed; the asphalt across the deck was milled to a depth of approximately 8" and a spray waterproof membrane was applied prior to resurfacing; cracks were routed and sealed in the roadway and concrete cracks were repaired by pressure injection; approach guide rails were replaced; and asphalt sidewalk was replaced with a tinted concrete sidewalk. All of these repairs were performed at a cost of \$661,707.24.

With the completion of the 2010 interim rehabilitation contract, it is anticipated that the Landing Road Bridge will require primarily routine maintenance repairs (performed by County personnel) until such time when the bridge is completely replaced.

The coordination with NJHPO associated with the Landing Road Bridge Replacement Project will be concerned with the new replacement bridge that would be constructed under the IPA, Alternative 7-C.

3.7 LANDING ROAD BRIDGE PHOTOS

Photos that depict Landing Road Bridge conditions prior to repairs undertaken in 2000 are provided on the next several pages, along with a plan view indicating the locations of each photo. More recent photos of Landing Road Bridge conditions are provided in the September 2013 *Bridge Re-Evaluation Survey Report* that is included in Appendix B.



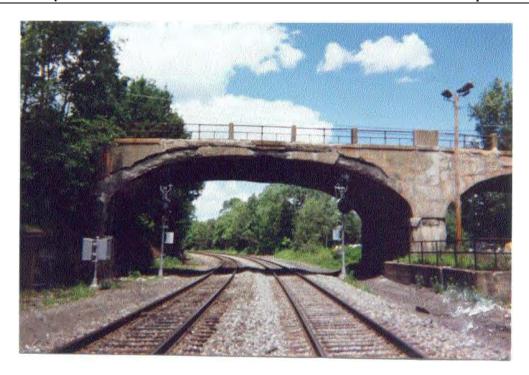


Photo 3-1: Looking north at south fascia of the west arch over the NJ TRANSIT Montclair-Boonton Line (formerly the DL&W Railroad). Note: Large area of the spandrel wall and edge of arch ring heavily spall (Photo taken prior to repairs in 2000).

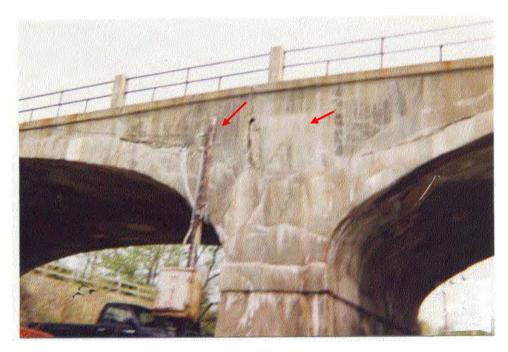


Photo 3-2: Looking south at north face of east and west arches at center pier. Note: Heavy medium cracking with efflorescence and leaching in arches, spandrel walls and piers and spalls in spandrel wall (arrows). Photo taken prior to repairs in 2000.



Photo 3-3: Looking northwest and up at the edge of the underside of the west span arch ring over NJ TRANSIT tracks. Note large spalls in underside and edge of arch ring. Photo taken prior to repairs in 2000.



Photo 3-4: Looking east at north edge of east span arch ring underside. Note: Spalled gunite treatments and large spall in underside of arch ring (arrow). Photo taken prior to repairs in 2000.



Photo 3-5: Looking west at large spall with exposed corroded reinforcing bars in north half of the west span arch ring underside (arrow). Photo taken prior to repairs in 2000.

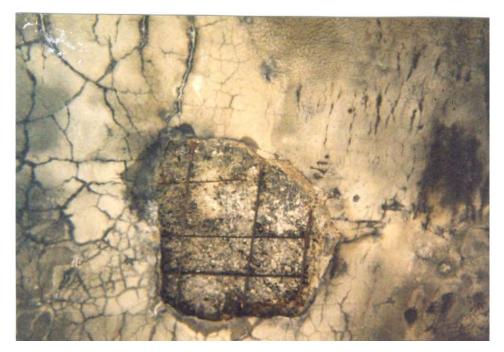


Photo 3-6: Looking up and west at underside of the west span arch ring. Note: Large spall with deteriorated reinforcing bars and medium to wide cracks in the surrounding concrete. Photo taken prior to repairs in 2000.



Photo 3-7: Looking southwest at spandrel at center pier for east and west spans. Note: Large concrete repair areas (light gray color in photo) done under repair contract in 2000.



Photo 3-8: Looking northwest at the south face spandrel wall, center pier and the edge of the west span arch ring. Note: Large areas of concrete repair done under repair contract in 2000.



Photo 3-9: Looking south at the north face of the east span arch ring and spandrel wall. Note: Large areas of concrete repair work to the spandrel wall (light color in photo) under repair contract in 2000.

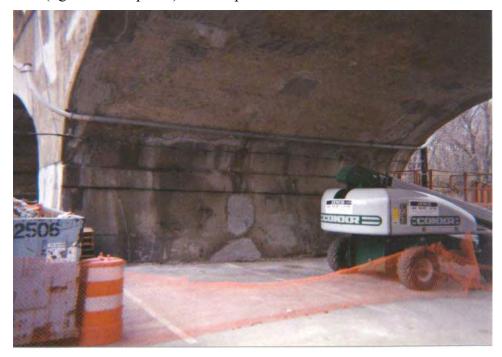


Photo 3-10: Looking northwest at the east face of the center pier. Note: Concrete repair work done under repair contract in 2000.

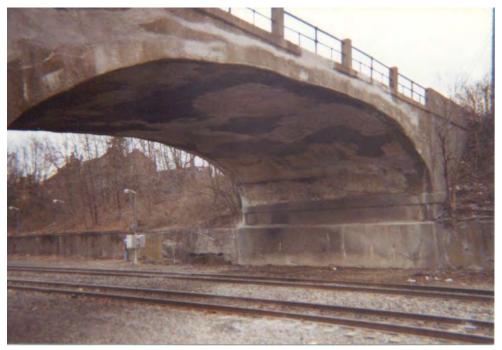


Photo 3-11: Looking southwest at the north face of the west span arch and spandrel wall. Note: Large area of concrete repairs to spandrel wall and arch ring under repair contract in 2000.

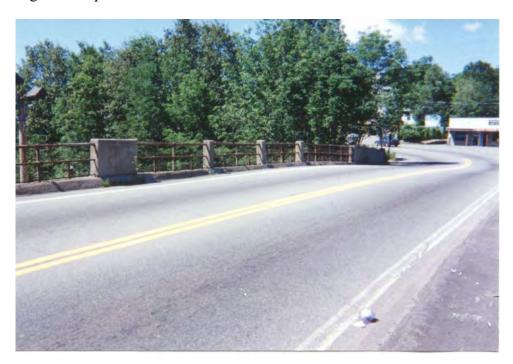


Photo 3-12: Looking southeast at the west span roadway over NJ TRANSIT Montclair-Boonton Line. Note: Bridge deck geometry has an inadequate to curb-to-curb width of 29 feet 7 inches (present design standards require 34-foot minimum).

SECTION 4 – PURPOSE AND NEED

4.1 PURPOSE

The purpose of the Landing Road Bridge Project is to improve safety and traffic operations through the rehabilitation or replacement of the Landing Road Bridge and improvements to the intersecting roadways on both approaches. Improvements are needed to address structural and functional deficiencies in the bridge and the approach roadways. Prior to the latest engineer's inspection report that was prepared in September 2013 (see Appendix B), the existing bridge was determined to be structurally deficient, due to the condition of the superstructure and substructure, and functionally obsolete, due to inadequate deck geometry. However, due to recent interim repairs to the superstructure and substructure that have been completed, the latest inspection report only cites that the bridge is functionally obsolete due to inadequate deck geometry, while the condition of the superstructure and substructure is considered to be fair. In addition, several intersection movements along the approach roadways experience Level of Service (LOS) "E" or "F".

4.2 BRIDGE CONDITION

The Landing Road Bridge is over 100 years old and has far exceeded its structural design life. The design life is considered the safe duration a structure can remain in service without significant rehabilitation and is dependent upon the materials used in the bridge, the load history of the crossing, the maintenance record, and other factors. A 50-year design life is common for most of today's modern bridge designs.

The most recent Bridge Re-Evaluation Survey report dated September 2013, which is included in Appendix B, shows that the bridge's superstructure (comprised of the reinforced concrete arches) has been given a Condition Rating of 5 (fair condition) due to patching covering 60% of both arch intrados, as well as of the cracking, scaling, leakage and efflorescence throughout both intrados and spandrel walls. This condition was upgraded to fair due to the concrete repairs performed since the previous Bridge Re-Evaluation Survey report dated October 2011. The substructure units (the abutments and pier) were also given a Condition Rating of 5 (fair condition) due to the large concrete patches, the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments. This condition was upgraded to fair due to the concrete repairs in the coping and wall at the northwest edge and south face of the pier since the previous Bridge Re-Evaluation Survey report dated October 2011.

The previous Bridge Re-Evaluation Survey report dated October 2011, which is also included in Appendix B, shows that the bridge's superstructure had been given a Condition Rating of 5 (fair condition) due to cracking, scaling, leakage, and efflorescence throughout the arch intrados and spandrel walls. The intrados was reported to have moderate scaling and efflorescence up to 60% of the area. The substructure units were given a Condition Rating of 5 (fair condition) due to medium to wide cracks and large areas of leaching, efflorescence, spalls and hollow concrete on the pier and west abutment. Per the Federal Highway Administration (FHWA), bridge elements that have a Condition Rating of 4 (poor condition) are characterized by advanced section loss, deterioration, spalling or scour. Bridge elements that have a Condition Rating of 3 (serious condition) are characterized by loss of section, deterioration, spalling or scour. Fatigue cracks in steel or shear cracks in concrete may also be present. This level of deterioration can seriously affect primary structural components, and local failures are possible.

The top of the bridge deck has exhibited deterioration due to direct traffic loading and salt intrusions because of snow and ice removal operations. Based upon the results of the concrete coring and testing program, it has been determined that the overall quality of concrete is poor. Compressive strength tests indicated that the available concrete strength is approximately 20 percent less than the current standard values, and petrographic analysis performed on the cores indicated that the composition of the existing concrete lacks integrity. Extensive cracking and spalling of concrete has occurred, and chunks of loose concrete have fallen from the bridge onto the railroad right-of-way and the parking area beneath the bridge.

The concrete is not air entrained. This has contributed to the severe spalling, which could be attributed in part to the freeze/thaw cycles of the concrete over its life. The concrete exhibits high levels of chloride (several times higher than the threshold level established by NJDOT to indicate the need for replacement), and active alkalai-silica reaction appears to be ongoing.

In 2000, Morris County undertook a project costing about \$300,000 to repair the bridge. This project consisted primarily of removing loose concrete, patching spalls, and sealing cracks. The primary purpose of the project was to repair the bridge in order to eliminate the safety hazard to the public and the railroad due to falling concrete and was not intended to significantly improve the structural integrity of the bridge. Furthermore, it did not eliminate the causes of the deterioration (i.e., lack of air entrainment, freeze/thaw cycles, and high chloride levels in the concrete). Miscellaneous repair and maintenance projects (including costs) related to the bridge that have occurred since that time are identified on the table on page 3-3.

The bridge load carrying capacity was rated using the Load Factor Method under the seventh cycle inspection dated December 1995. The ratings found the mid-span crown to an inventory rating of 65 tons for an HS truck configuration, which exceeds the current design of 36 tons for this truck. However, these ratings may be theoretical due to the condition of the concrete of the arch found during past coring extraction operations, where core samples crumbled during the extraction process. As such, it is likely that the bridge does not meet the current load rating requirements.

The bridge is also considered functionally obsolete due to the inadequate deck geometry. The curb-to-curb width is 29 feet 7 inches, which is less than the minimum NJDOT design width of 32 feet, as well as the AASHTO standard for design width of 34 feet (two 12-foot lanes and two 5-foot shoulders).

4.3 ROADWAY AND TRAFFIC

The Landing Road Bridge provides one of the few major access points for residents on both the east and west shores of Lake Hopatcong. The existing two-lane roadway on the bridge is not adequate to handle the design year traffic (as discussed in Section 3.5), and the approach roadways on both sides of the bridge are becoming increasingly congested.

As discussed in Section 3.5 and presented in Appendix H, traffic counts were taken in March 2011 at four intersections in proximity to the Landing Road Bridge. These counts indicate that left-turning movements onto Landing Road Bridge from Kings Highway experience a Level of Service "E" during the morning peak travel period and Level of Service "F" during the evening peak. In addition, the westbound approach leg of Kingsland Road at its intersection with Lakeside Boulevard experiences a Level of Service "E" during the evening peak travel period. No turning movements at either the Landing Road and Lakeside Boulevard/Mt. Arlington Boulevard or the Mt. Arlington Boulevard and Shippenport Road intersections experience levels of service worse than "C" during either the morning or evening peak periods.

Each of the four intersections was also evaluated in the No-Build condition for the project horizon year of 2031. Under this scenario, three out of the four intersections will experience extraordinarily long delays, at least on certain approaches. At the signalized intersection of Landing Road and Lakeside Boulevard/Mt Arlington Boulevard, southbound approaches along Lakeside Boulevard during the morning peak hour would exceed the maximum saturated flow rate of 1,900 vehicles per hour (vph) and will require capacity increase in order to improve from a Level of Service "F". In addition, the left turning movements onto Landing Road Bridge from Kings Highway and onto Lakeside Boulevard from Kingsland Road would be Level of Service "F" during both the morning and evening peak periods. These left turning vehicles would require substantial gaps in the through traffic in order to perform these movements safely during these periods.

Finally, the existing layout of the signalized intersection of Landing Road, Lakeside Boulevard and Mt. Arlington Boulevard does not have provisions for left turning vehicles. Increasing traffic volumes combined with the delay from left turning vehicles has made this intersection a major congestion point for the roadway network. It is also believed that many of the accidents experienced at this intersection occur because the signal layout does not accurately reflect the existing traffic needs.

Additionally in Morris County took traffic counts in April of 2015 at the three intersections and the following counts were documented:

Mount Arlington Blvd (616) - N of Shippenport Rd

Dates: 5/26 - 5/29/2015

AADT: 10,464

AM Peak (8-9am): 672 PM Peak (5-6pm): 900

Lakeside Blvd (631) - N of Landing Rd

Dates: 4/28 - 5/1/2015

AADT: 23,702

AM Peak (7-8am): 1,751 PM Peak (5-6pm): 1,915

Kings Hwy - W of Landing Rd

Dates: 4/28 - 5/1/2015

AADT: 577

AM Peak (6-7am): 47 PM Peak (5-6pm): 47

Landing Rd (631) - N of Shippenport Rd

Dates: 10/30 - 11/1/2013 (Traffic counts were not taken at this intersection in 2015 only in 2013)

AADT: 21,350

AM Peak (7-8am): 1,466 PM Peak (5-6pm): 1,553

SECTION 5 - PROJECT ALTERNATIVES

This section provides a description of the alternatives considered for the Landing Road Bridge Project and an evaluation of how well each alternative satisfies the project needs of improving the structural capacity of the bridge and providing adequate traffic operations and safety. Each alternative was designed to incorporate good engineering practices and minimize the environmental and community impacts to the maximum extent possible. A comparative review of these alternatives is presented in Section 6 of this alternatives analysis.

During earlier stages of project development, eight preliminary conceptual alignment alternatives were developed and studied (see Figure 5-1). During the final scoping process, **Alternative 7** was selected for further development and study. Alternative 7 consists of replacing the existing bridge along roughly the same alignment and would provide for two lanes of traffic in each direction. Six different replacement alignments (Alternatives 7-A through 7-F) were developed and evaluated, with Alternative 7-C found to be the most feasible.

Subsequent to the conclusion of scoping, the SHPO requested that the **Bypass Alternative** also be advanced for comparison purposes. Alternative 1 consists of providing a new bridge on a bypass alignment as an extension of Lakeside Boulevard joining Landing Road south of the existing bridge.

Under both Alternative 1 and Alternative 7-C, the existing Landing Road Bridge would be demolished. The possibility of retaining the existing structure for alternate uses (such as a pedestrian overpass) was considered. However, if left standing, it is likely that the bridge would also be required to be available for emergency vehicle use. Regardless of its ultimate use, it would be imperative that the bridge structure be maintained for use. The cost of basic maintenance of the structure for some sort of alternate use over the next 20 years is estimated to be approximately \$1.0 million (in 2011 dollars), based on an assumed \$50,000 per year. Given the "fair" rating of the bridge condition in the most recent inspection report (September 2013), it is further assumed that the bridge would not require any major rehabilitation beyond basic maintenance during that 20-year period. However, if major rehabilitation should become necessary, that would result in a major additional cost. Due to the need for regular maintenance and the possible need for future rehabilitation, as well as the associated costs for each, alternatives that include maintaining the existing structure for alternate uses were not carried forward for further evaluation.

Thus, in accordance with the requirements of the Alternatives Analysis and as agreed upon with the SHPO, the following alternatives were considered and studied for this report:

- No-Build Alternative;
- Rehabilitation Alternative;
- Bypass Alternative (Alternative 1); and
- Replacement Alternative (Alternative 7-C).

5.1 NO-BUILD ALTERNATIVE

The No-Build Alternative would maintain the existing bridge in its current location without rehabilitation or replacement. It would include basic measures to stabilize the structure and maintain it until the end of its useful life, although without providing structural or associated roadway improvements, rehabilitation, or preventative measures. The alternative would not require any right-of-way taking of residential or business properties.

The existing concrete arch bridge is over 100 years old and has experienced extensive cracking, spalling of concrete, and rusting of steel reinforcement over the years. Chunks of loose concrete have fallen from the bridge onto the railroad right-of-way and the parking area beneath the bridge, endangering the public and presenting a safety hazard. Under the No-Build Alternative, the bridge would continue to deteriorate over time (even with recent interim repairs that have been made) and would eventually reach a point that would require limited weight usage and possibly complete restriction of vehicle use. There would also continue to be operational deficiencies associated with the bridge due to existing geometric and capacity limitations.

To continue use of the existing structure beyond its design life, a continuous program of maintenance would be required during the next 20 years, simply to extend its life to the end of that 20-year period. Although an initial outlay of capital costs to improve the bridge would not be required, the cost of continuous maintenance that would be required throughout the 20-year period could still be significant. As discussed above, the total maintenance cost during the 20-year period is estimated to be approximately \$1.0 million. This cost would not result in significant long-term benefit, but only the ability to extend the life of the bridge for an additional 20 years. There would also be no improvements to operational conditions on or near the bridge. Therefore, the return on the investment would be limited.

5.2 REHABILITATION ALTERNATIVE

The Rehabilitation Alternative would rehabilitate the bridge to the *Secretary of the Interior's Standards*, which would not alter the existing historic features or further modernize the bridge. The bridge would not be widened under this alternative, and some supporting structures may not be repaired or replaced. Existing bridge maintenance requirements will continue to increase as rehabilitation within the parameters of historic integrity will result in repairs of failing and severely deteriorating structural elements with materials to match the old in design, color, and texture. Replacement of historic features will need to be substantiated with documentary and physical evidence.

Under this alternative, rehabilitation of the existing bridge would be performed to increase its load carrying capacity. There are two options that fall under the general category of rehabilitation, both of which would have a similar footprint to the No-Build Alternative, although they would involve a more extensive level of repairs and possible widening of the bridge.

One rehabilitation option would be to replace the existing bridge's concrete arch in kind. The existing concrete arch, together with the asphalt pavement, would be demolished and replaced with a new concrete arch and pavement in sections. No widening of the existing bridge deck would occur. The construction duration for this rehabilitation alternative would be approximately 1 to 2 years.

The second rehabilitation option would be to replace the concrete arch in kind, replace the deck, and improve the integrity of the substructure along with possible widening of the bridge. This could result in the bridge being out of service for a longer duration; the approximate construction duration would be on the order of 3 to 4 years. This construction duration could possibly be shortened by using more modern construction methods such as pre-fabrication. The life expectancy under this rehabilitation alternative would be 75 years, which is in conformance with the latest AASHTO bridge design standards.

In either case, any rehabilitation effort would fall short in terms of increasing the strength of the bridge. Deterioration would continue for any sections that are not replaced. In addition, the same potential for long-term vehicle restrictions as the No-Build Alternative would result as the bridge nears the end of its useful life. Rehabilitation also would not increase capacity to better address the operational deficiencies that would exist on the bridge and its approaches.

Any rehabilitation alternative would conform to the Secretary of the Interior's Standards for the Treatment of Historic Properties in regards to retaining and preserving the historic character of the bridge. In this case, keeping the same structure type (i.e., a two-span reinforced concrete deck arch bridge) would comply with this requirement. In addition, where the severity of deterioration requires replacement of a distinctive feature, the new feature would match the old in design, color, texture and materials.

5.3 BYPASS ALTERNATIVE

The Bypass Alternative, shown in Figure 5-2, would involve the construction of a new bridge on a new alignment south of the existing bridge. The portion of Landing Road from its intersection with Lakeside Boulevard to the property at 151 Landing Road would be relocated to the south, crossing over the NJ TRANSIT Boonton Line on a new structure south of the existing bridge. The new curvilinear, two-span bridge would provide two lanes in each direction and would be approximately 340 feet long and 57 feet wide, with a 55 degree skew.

The intersection of Lakeside Boulevard, Mt. Arlington Boulevard, Landing Road, and the access ramp to the NJ TRANSIT station parking area would be reconstructed into a four-legged intersection providing channelization for the right turning vehicles. The intersection would be improved with a new signalized intersection and a new lane configuration. Westbound Mt. Arlington Boulevard and southbound Lakeside Boulevard would have shared through lanes with left turns; eastbound Landing Road over the new bridge and westbound Mt. Arlington Boulevard would have exclusive right turn lanes. At the southern end of the proposed bridge, access would be provided to Kings Highway via a new perpendicular intersection. The portion of Landing Road west of this intersection would become a dead-end terminating with a cul-de-sac.

Under this alternative, the existing bridge would be demolished. The possibility of retaining the existing structure for alternate uses (such as a pedestrian overpass) was considered. However, if left standing, it is likely that the bridge would also be required to be available for emergency vehicle use. Regardless of its ultimate use, it would be imperative that the bridge structure be maintained for use. The cost of basic maintenance of the structure for some sort of alternate use over the next 20 years is estimated to be approximately \$1.0 million (in 2011 dollars), based on an assumed \$50,000 per year. Given the "fair" rating of the bridge condition in the most recent inspection report (September 2013), it is further assumed that the bridge would not require any major rehabilitation beyond basic maintenance during that 20-year period. However, if major rehabilitation should become necessary, that would result in a major additional cost. Due to the need for regular maintenance and the possible need for future rehabilitation, as well as the associated costs for each, alternatives that include maintaining the existing structure for alternate uses were not carried forward for further evaluation.

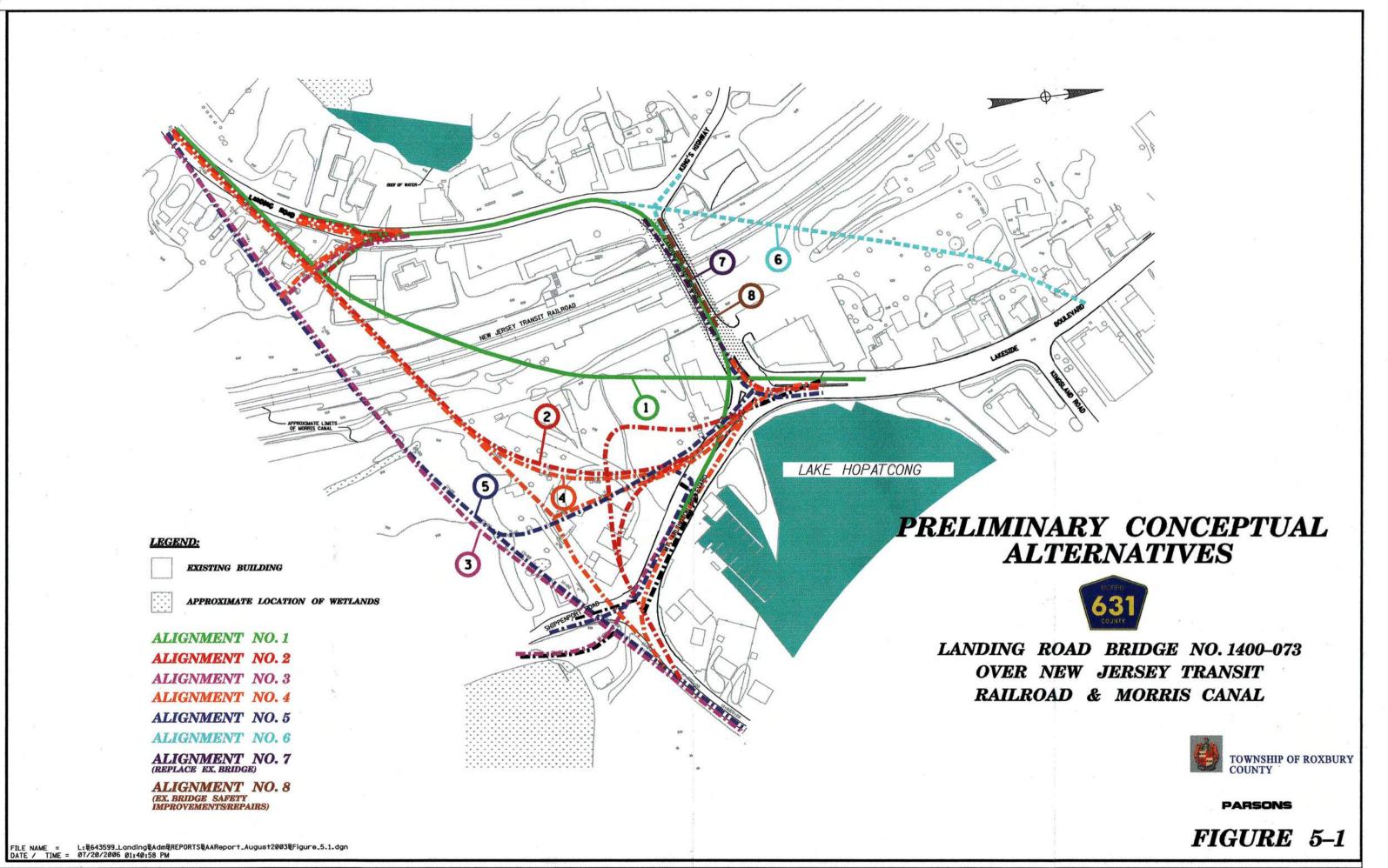
Additionally the Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter mile segment of Landing Road involving acquisitions of 29 properties with 6 complete takes.

As described under the No-Build Alternative, the costs associated with regular maintenance and periodic minor rehabilitation of the existing bridge would result in limited overall benefit and would not be financially feasible for the County. Therefore, the Bypass Alternative is only being considered in conjunction with the removal of the existing bridge.

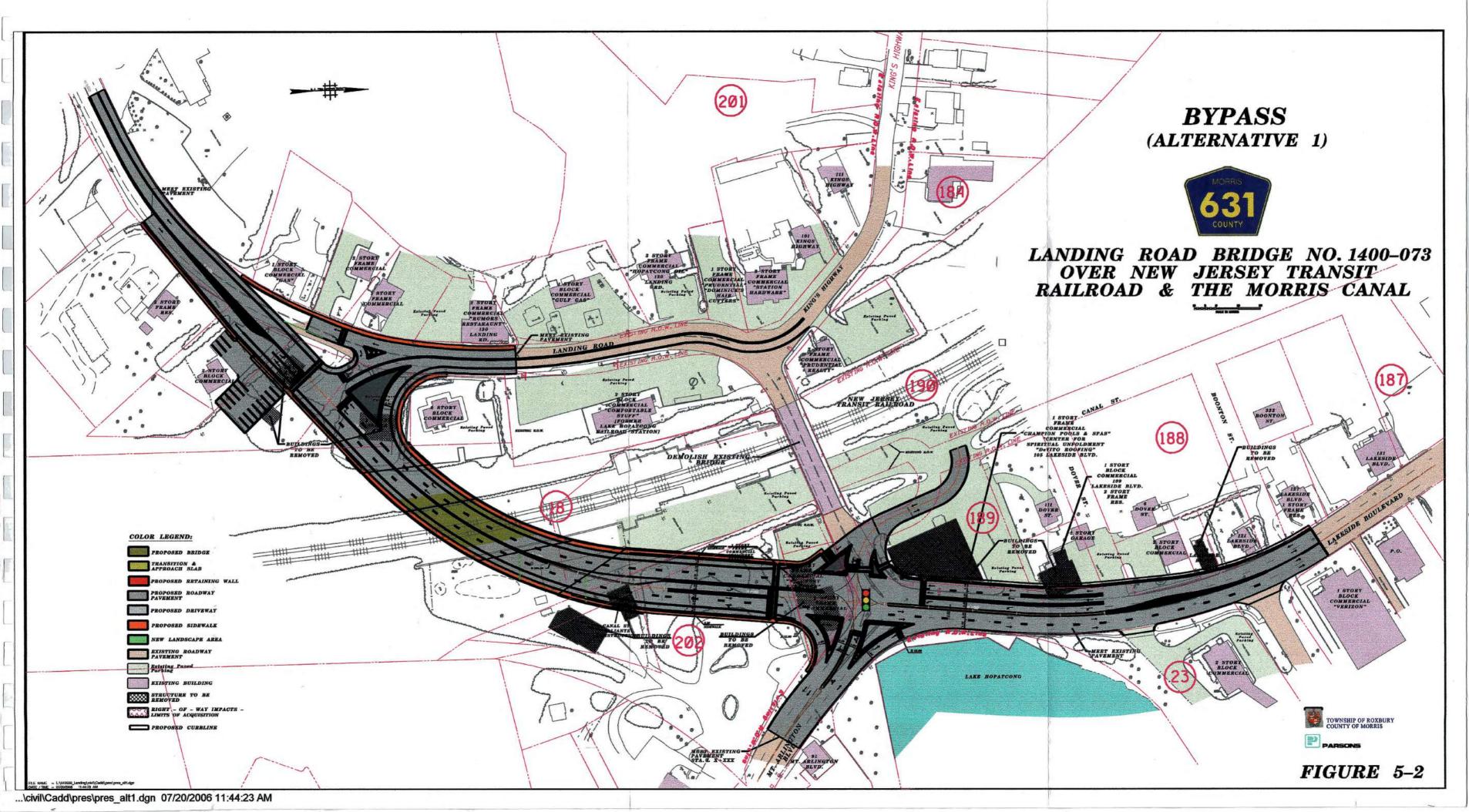
5.4 REPLACEMENT ALTERNATIVE (ALTERNATIVE 7-C)

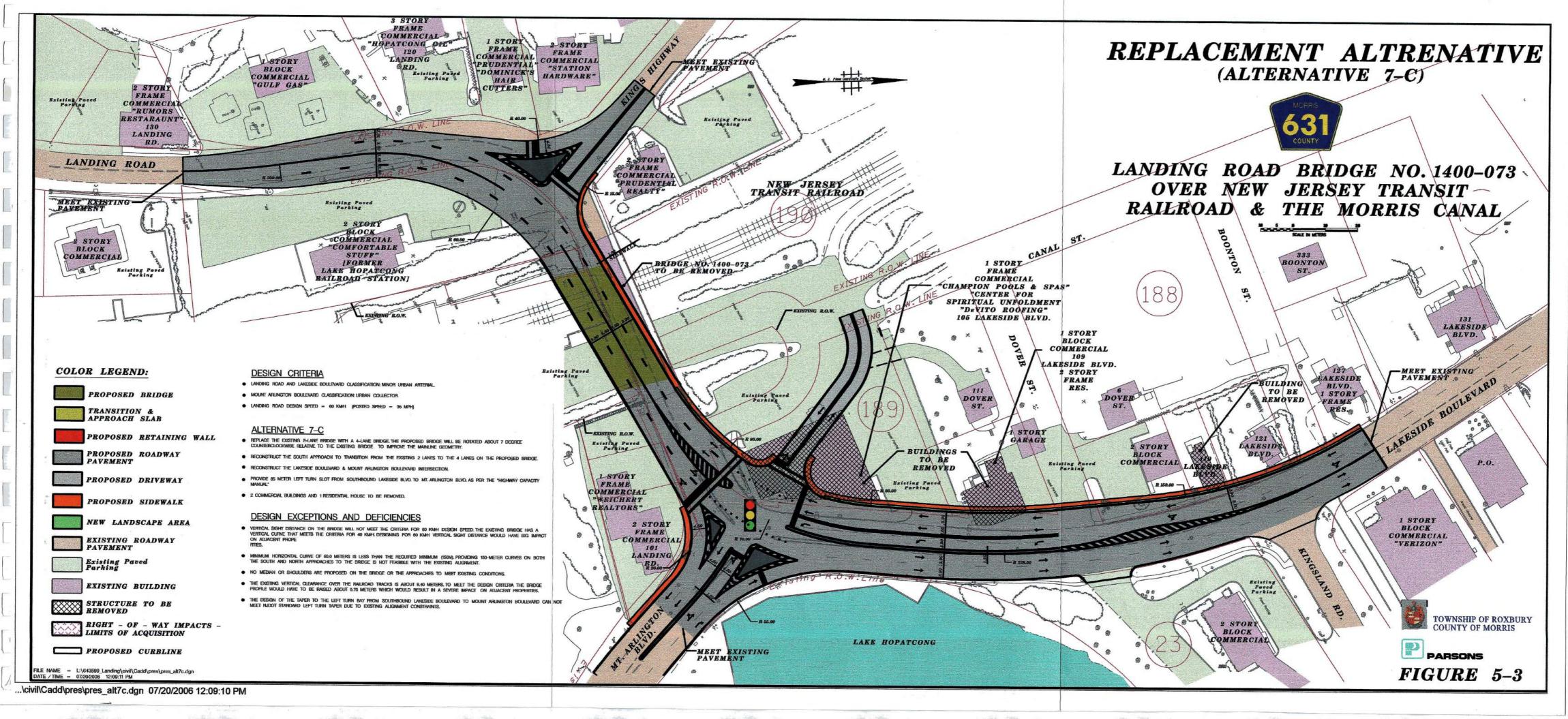
The Replacement Alternative (Alternative 7-C), shown in Figure 5-3, is the Preferred Alternative for this project. This alternative would provide a replacement bridge in the same general location as the existing bridge, with some adjustments of the bridge alignment and approaches for safety improvements and upgrading.

This alternative would replace the existing two-lane bridge with a new four-lane bridge and would improve the horizontal alignment of the bridge by rotating it slightly relative to the existing bridge. The new bridge would provide two 12-foot lanes in each direction and a 6-foot wide sidewalk on its west side. The south approach of Landing Road would be reconstructed to transition from the existing two lanes to four lanes on the bridge. The intersection of Landing Road/Mt. Arlington Boulevard and Lakeside Boulevard would be reconstructed to incorporate a fourth leg providing access to and from Canal Street and the NJ TRANSIT station parking area. The existing Canal Street access from Landing Road and Lakeside Boulevard to the parking area would be eliminated, improving traffic operations and safety on these roads. Southbound Lakeside Boulevard would be widened to provide an exclusive left turn lane, an exclusive through lane, and a shared through and right turn lane.



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SECTION 6 - ENVIRONMENTAL IMPACTS

This section provides an evaluation of the potential environmental and community impacts associated with each of the four alternatives introduced in Section 5: the No-Build Alternative, the Rehabilitation Alternative, the Bypass Alternative (Alternative 1), and the Replacement Alternative (Alternative 7-C). Alternatives considered earlier in the screening process that were determined to not be practicable or feasible were not carried through to this stage of the analysis.

Each of the four alternatives is evaluated in this section with reference to environmental, economic, and engineering attributes. The potential effects of each alternative are analyzed relative to other alternatives and relative to existing baseline environmental conditions. Impacts that preserve the integrity of an attribute are considered beneficial, whereas impacts that degrade the integrity of the attribute are considered adverse.

The impacts are identified as short-term or long-term and direct or indirect. Short-term impacts are usually associated with construction activities and are expected to diminish or end at the close of construction or shortly thereafter. Long-term impacts occur or continue after construction of the project is complete. Direct impacts occur as a primary result of the construction and/or operation of the project, whereas indirect, or secondary, impacts are reasonably foreseeable impacts that are caused by the action, but are later in time or farther removed in distance. Mitigation measures may be utilized to minimize non-significant impacts to an acceptable level of effect on the environment and/or community.

The attributes evaluated include the following:

- socioeconomic resources and right-of-way;
- physical resources;
- water resources;
- biological resources;
- cultural resources; and
- hazardous substances.

The narrative discussions are limited to those impacts that are shown to be adverse or beneficial. Discussions are not provided for areas where the attribute is not present in the project area and where there is no reasonably identifiable effect by the implementation of either the nobuild or an action alternative. The following subsections provide discussions regarding the specific nature and extent of impacts associated with each of the four alternatives.

6.1 SOCIOECONOMIC RESOURCES AND RIGHT-OF-WAY

The project area is primarily within the town of Landing in Roxbury Township, which has a Limited Business Zoning District that is slated for service and retail businesses according to the Roxbury Land Development Ordinance. The properties located along the roadways within the project area include residences and small businesses, some of which have living quarters in the buildings, and some of which are currently vacant. The properties, land use, businesses, local economy, and people represent the socioeconomic resources of the project area. This section also includes a general assessment of each alternative's consistency with regional, county, and local planning goals.

The number and size of residential and business parcels affected by each alternative are included in the cost estimate tables found in Appendix C of this report. The property owner list for Alternative 7C was updated in February of 2015 and is also included (along with an accompanying graphic) in Appendix C.

6.1.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, but would require regular maintenance and minor rehabilitation over the next 20 years in order to extend the life of the bridge during that period. The No-Build footprint would be consistent with the existing bridge and roadways, and would not be perceived as any change to the current condition. The No-Build Alternative would not involve any taking of additional right-of-way or construction outside of the existing footprint of the bridge, and no residential or business properties or utilities would be impacted. Secondary indirect impacts that could potentially result from the deteriorating condition of the bridge include the loss of NJ TRANSIT station parking under the bridge due to hazards from spalling concrete, and reduced usage (load limits or single lane) or closing of the bridge due to long-term deterioration. The long-term lack of traffic improvements and the continued deterioration of the existing bridge could potentially result in a reduction in any planned business development, as well as an increase in local traffic congestion.

The No-Build Alternative is not in compliance with regional, county, and local planning goals. Because the alternative does not include improvements that would address the poor traffic conditions in the study area, conditions would be expected to deteriorate further as traffic levels increase in the future. Planned economic growth could be impaired as the congestion intensifies and motorists avoid the area. These would be viewed as long-term indirect adverse impacts.

6.1.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build (i.e., the same as the existing footprint) but it would require periodic construction activities in order to extend the useful life of the bridge for as long as possible. Existing bridge maintenance requirements will continue to increase as rehabilitation within the parameters of historic integrity will result in repairs of failing and severely deteriorating structural elements with materials to match the old in design, color, and texture. Replacement of the historical features will be substantiated with documentary and physical evidence.

Although this alternative may extend the life of the bridge beyond that expected under the No-Build Alternative, overall deterioration of the bridge would continue to exist, and the operational deficiencies of the bridge and its approaches would not be improved. This alternative would not involve any taking of additional right-of-way, and no direct impact to residential or business properties would occur; the potential for disruption to utilities would also be limited. However, this alternative would involve short-term adverse impacts during the construction

period due to increased congestion and traffic disruption. During the construction period, local businesses may have some loss of business due to users avoiding the area or being delayed by the construction. Because engineering studies have indicated the substandard condition of the bridge, the Rehabilitation Alternative would be envisioned to have the same long-term secondary impacts associated with bridge deterioration or restricted usage.

The Rehabilitation Alternative is not in compliance with regional, county, and local planning goals. Because the alternative does not include improvements that would address the poor traffic conditions in the study area, conditions would be expected to deteriorate further as traffic levels increase in the future. Planned economic growth could be impaired as the congestion intensifies and motorists avoid the area. These would be viewed as long-term indirect adverse impacts.

6.1.3 Bypass Alternative

The Bypass Alternative would involve extensive acquisition of right-of-way (29 partial or whole property takings) for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. It is anticipated that demolition of at least four residences and four businesses locations would be required. One of the buildings to be removed houses two businesses on the lower level and three residential units that provide lower-cost housing; this is consistent with a judicial mandate requirement for Roxbury Township. The removal of the needed properties from the tax base would have an impact on the community as a whole. Extensive utility impact and relocation would also be required.

This alternative would allow traffic to bypass the last quarter mile of Landing Road, which includes as many as 11 business locations with up to five business offices within them. This would eliminate the roadside exposure and would increase the effort required to access these business locations, resulting in a long-term adverse impact on the businesses similar to that of changing the business locations from a thoroughfare to a side street.

The short-term construction disruptions of the businesses would be more extensive than the Rehabilitation Alternative due to the length of construction and multiple locations.

The Bypass Alternative is also not in compliance with regional, county, and local planning goals. The regional and local land-use plans provide for business development in the Landing area. Planned economic growth could be impaired as the bypass reroutes motorists away from this portion of the Landing area. This would be viewed as a long-term, indirect, adverse impact.

6.1.4 Replacement Alternative

The Replacement Alternative incorporates safety improvements and advanced engineering factors that would require the acquisition of some additional right-of-way, although not to the same extent as with the Bypass Alternative (20 partial or whole property takings in comparison to 29). Specifically, two commercial buildings and at least one residence, all located along Lakeside Boulevard, would be acquired. Utility relocations would also be required. Replacement of the bridge would have similar short-term construction disruptions to those associated with the Rehabilitation Alternative, with a somewhat larger area and a longer duration.

The replacement of the bridge with upgraded engineering and a minor modification of the alignment would not have the long-term adverse impacts on the businesses from traffic diversion or potential restrictions or closure of bridge traffic. In fact, this alternative is generally

compatible with the planned business development, and would provide long-term roadway enhancements that would decrease congestion and improve access, although short-term traffic and business disruptions would occur during construction. The addition of turning lanes would reduce the congestion related to the signal light operation. The turning lanes and widening would allow for a greater ease of access and egress for the businesses.

The Replacement Alternative is in compliance with regional, county, and local planning goals by meeting the long-term transportation needs associated with business development in the Landing area. The Replacement Alternative would slightly modify the original alignment and retain the usage of Landing Road. Because the Replacement Alternative would retain existing traffic patterns and improve roadway operations in the Landing Road vicinity, it would have long-term benefits for the Landing Road businesses and community and would support planned economic growth in the Landing area.

6.2 PHYSICAL RESOURCES

The physical resources within the project area include geologic and topographic features, soils, air quality, and noise levels. The project area lies within the Highlands physiographic province that includes northeast trending ridges and valleys gradually dropping in elevation from west to east. The topography varies, as the landforms have been reworked as the area developed. The elevations range from 900 feet above mean sea level (msl) where the NJ TRANSIT tracks leave to the south, to the hillside east of Landing Road where the slopes pass 950 feet above msl. The Lake Hopatcong spillway holds the lake at approximately 924 feet. The reported elevation of the Landing Road Bridge is also 924 feet.

No unique geologic formations or outcrops have been identified within the project area. The soils on both sides of Landing Road were formed in young glacial till. In general, these soils belong to the Netcong-Rockaway associations. The soils throughout the project area are unsuited to cultivation. Many of the adjacent properties have had the soils modified through cut and fill operations. Much of the area adjacent to Landing Road and the bridge has been leveled and filled to support commercial structures or cutout to provide access. These soils are not associated with special agricultural designations, nor are they acid producing soils.

The project area is primarily within the town of Landing in Roxbury Township, which consists of service and retail businesses. The community roadways are two or four lanes with posted speed limits of 25 and 30 miles per hour. The properties located along the roadways within the project area include residences and a variety of small businesses, including offices, food service, and fuel service stations. Landing is adjacent to the recreational community associated with Lake Hopatcong. Other than the residences and Lake Hopatcong (a recreational lake), there are no other sensitive air or noise receptors such as motels, hotels, schools, churches, hospitals, nursing homes, or libraries within the project area. There are no major sources of air pollution or noise identified near the area.

The entire state of New Jersey, including Morris County, is classified as non-attainment for the 1997 (0.08 ppm) and 2008 (0.075 ppm) 8-hour ozone National Ambient Air Quality Standards (NAAQS). Morris County is also classified as non-attainment for the 1997 annual PM 2.5 standard (15 μ g/m³) and 2006 24-hour PM2.5 standard (35 μ g/m³).

6.2.1 No-Build Alternative

The No-Build footprint would be the same as the existing bridge and roadways. The No-Build Alternative would not involve any taking of additional right-of-way and therefore would

not alter the existing physical resources over the short term. The No-Build Alternative would not improve the peak-hour traffic congestion, which contributes slightly to the degraded air quality and noise levels in the project area.

6.2.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative, but would involve short-term adverse impacts during the construction period due to increased congestion and traffic disruption. Short-term air quality impacts could be expected during the construction associated with the bridge rehabilitation. During the construction period, increased amounts of dust or particulate matter would be generated during excavation, grinding, hauling, and other construction operations. The operation of heavy equipment during construction would increase dust emissions and vehicle exhaust emissions. Dust emissions can be mitigated through the implementation of dust control measures such as wetting exposed soils, covering material stockpiles, and cleaning trucks prior to their leaving the construction site.

During the construction period, short-term noise levels would also increase. Mitigation measures and controls could be incorporated into the project to reduce these to acceptable levels.

6.2.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and new bridge that would bypass a quarter-mile segment of Landing Road. The construction of the bypass would involve the reworking of the soil in the area to be developed. Short-term adverse impacts during the construction period would include increased congestion and traffic disruption. During the construction period, fugitive dust and noise levels would increase. Short-term air quality impacts would be expected during the construction of the bridge and associated highway improvements. During the construction period, increased amounts of dust or particulate matter would be generated during excavation, grading, hauling, and other construction operations. The operation of heavy equipment during construction would increase dust emissions and vehicle exhaust emissions. Dust emissions can be mitigated through the implementation of dust control measures such as wetting exposed soils, covering soil stockpiles, and cleaning trucks prior to their leaving the construction site.

The placement of the bypass on an elevated structure would create long-term impacts associated with the adverse increases in the noise levels around the bypass, some of which is currently undeveloped land. The removal of the trees in the bypass route would also accentuate the noise impacts by diminishing the noise-buffering effect of these resources. Further study may be required during design to determine if additional mitigation measures will be required to address the increases in noise levels.

6.2.4 Replacement Alternative

The Replacement Alternative consists of bridge replacement and widening with approach improvements. The safety improvements include lane widening, addition of turning lanes, and modification of existing intersections. The improvements would not involve a significant increase in traffic volumes or speed increase; therefore, detailed noise studies are not required for this project.

The Replacement Alternative would likely have more impacts on the physical resources than the No-Build and Rehabilitation alternatives, but notably less than the Bypass Alternative. Replacement of the bridge would have similar short-term construction disruptions to those

associated with the Rehabilitation Alternative, with a somewhat larger area and a longer duration. The incorporation of safety improvements and advanced engineering factors would require the acquisition of additional right-of-way involving the demolition of three structures.

Short-term air quality impacts would be expected during the demolition and reconstruction of the bridge and associated highway improvements. During the construction period, increased amounts of dust or particulate matter would be generated during excavation, grading, hauling, and other construction operations. The operation of heavy equipment during construction would increase dust emissions and vehicle exhaust emissions. Dust emissions can be mitigated through the implementation of dust control measures such as wetting exposed soils, covering soil stockpiles, and cleaning trucks prior to their leaving the construction site.

The addition of turning lanes would bring traffic noise closer to the adjacent buildings, although any noise level increase would likely not be significant. Unlike the Bypass Alternative, this alternative would not create a new noise corridor. The reduction in peak hour congestion would provide a secondary long-term beneficial reduction in the associated noise and air quality concerns. This alternative would not have an impact on regional emissions since it does not provide for additional capacity or new access. Further study may be required during design to determine if additional mitigation measures will be required to address the increases in noise levels.

6.3 WATER RESOURCES

Water resources include surface waters (ponds, lakes, and streams), associated flood plains, and ground water resources. The Landing area lies on an upland portion between the Delaware River and the Raritan River drainage basins. Lake Hopatcong drains to the upper reaches of the Musconetcong River basin that leads to the Delaware River. The spillway for the lake is approximately one mile northwest of Landing. A small pond named Riggs Lake (1.47 acres) is located behind the properties on the west-side of Landing Road just outside the project area. The project area does not include any running streams, but it does include the properties adjacent to the south end of Lake Hopatcong. A small wetland is located just outside of the project area at the south end of the Canal Street properties. Environmental mapping and the visual site inspection did not identify any ponds or wetlands within the immediate project area.

Because the Landing area is in the upper portions of the drainage basin, natural drainages have not developed to transport the runoff from the small drainage areas. No flood plains are identified within the project area other than the back portions of the properties adjacent to the southeast side of the lake and fronting on Mt. Arlington Boulevard (FEMA, 1982). These potential flood areas are outside of any construction areas.

The Landing area is located over glacial deposits with moderate permeability material and silty sand mixtures as identified in the county resource mapping (MCDPD, 2000). In the vicinity of the project area, ground water wells provide the water supply for many residences and businesses.

6.3.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. The No-Build Alternative would not impact water resources within the project area.

6.3.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build. This alternative may involve some minor short-term adverse impacts during the construction period due to runoff from the construction area.

6.3.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and new bridge that would bypass a quarter-mile segment of Landing Road. The Bypass Alternative involves construction over undeveloped areas and may generate short-term adverse effects from construction runoff and sedimentation. A long-term adverse impact would be the need for management of stormwater runoff from the additional paved areas associated with this alternative. The new pavement would cover approximately 2.5 acres that would include the new roadway and shoulders. This would require the management of approximately 68,000 gallons of runoff for each 1-inch of rainfall. Additional right-of-way acquisition would not be required for management of stormwater runoff. Construction and operational runoff would need to be properly managed to prevent impacts on the lake within the project area and the ponds and wetland that lie just outside the project area.

6.3.4 Replacement Alternative

The Replacement Alternative would result in more surface water resource impacts than the No-Build and Rehabilitation alternatives, but much less than the Bypass Alternative. Replacement of the bridge would have similar short-term construction disruptions to those associated with the Rehabilitation Alternative, with a somewhat larger area and a longer duration. The incorporation of safety improvements and advanced engineering factors would require the acquisition of additional right-of-way involving previously developed areas, most of which are paved road shoulders, paved parking or roadside lawns. The expanded construction area would create short-term adverse impacts through the sedimentation associated with runoff. Over the long-term, this alternative would not create a substantial amount of additional paved area and is not expected to create adverse impact on surface water resources. The new pavement would cover less than 0.1 acres. This would require the management of approximately 2,700 gallons of runoff for each 1-inch of rainfall.

6.4 BIOLOGICAL RESOURCES

Biological resources include fish, wildlife, vegetation, wetlands, and potential for threatened and endangered species. The land surface of the entire project area has been reworked, in some cases several times, as historical development occurred. The only portion of the project area that is considered vacant or undeveloped is a 1.27-acre lot between Canal Street and Mt. Arlington Boulevard next to the NJ TRANSIT parking area. This area is often used for overflow parking from the NJ TRANSIT lots. It is covered with grasses with some herbaceous vegetation and young deciduous hardwood species such as maple, ash, oak, and hickory.

The wildlife in the area is typical of those occupying developed areas, including small mammals (mice, shrews, bats, rabbits, skunk, opossum, raccoon, woodchuck, and squirrel), and occasional large mammals such as fox and deer. A variety of reptiles and amphibians also inhabit the area. Most of the project area consists of developed yards or paved areas that have minimal value as wildlife habitat. Lake Hopatcong is classified as Trout Maintenance, indicating that it is capable of supporting trout throughout the year. The developed nature of the project area precludes the existence of habitat for threatened and endangered species, and none were noted during the site inspection or in the literature reviewed.

The project area does not include any running streams providing aquatic wildlife habitat, but it does include the properties adjacent to the south end of Lake Hopatcong. A small wetland is located just outside of the project area at the south end of the Canal Street properties. Environmental mapping and the visual site inspection did not identify any ponds or wetlands within the immediate project area.

6.4.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. The No-Build Alternative would not involve any taking of additional property and therefore would not be expected to have any impacts on the biological resources in the project area.

6.4.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative. The Rehabilitation Alternative would not involve any taking of additional property and therefore would not be expected to have any impacts on the biological resources in the project area.

6.4.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. The new bypass roadway and bridge would be approximately one-quarter mile in length. The noise and construction activities would cause short-term impacts on the animals as they try to avoid the construction zone. However, approximately half of the length would be constructed in vacant field or semi-wooded areas that would have some wildlife value. This alternative would add another break in the limited undeveloped tree covered areas that remain in this area. This terrestrial habitat and the associated wildlife would have long-term adverse impacts associated with the loss of habitat through the clearing for construction and maintenance of the new bypass roadway and bridge. The location of the bypass also threatens to have indirect short-term and long-term impacts on the wetland just outside of the project area. Land-clearing activities, construction, and operational runoff may encroach on the associated transition area or disrupt the ecology of this small wetland. These indirect impacts would be considered short- and long-term adverse impacts on the wetland.

6.4.4 Replacement Alternative

The Replacement Alternative would have more biological impacts than the No-Build and Rehabilitation alternatives, but much less than the Bypass Alternative. Replacement of the bridge would have similar short-term construction disruptions to those associated with the rehabilitation alternative with a somewhat larger area and a longer duration. Short-term construction adverse impacts include the potential for wildlife inhabiting nearby properties to leave the area in order to avoid the noise and disruption. Some field areas may be used as temporary areas for stockpiling materials and equipment.

The incorporation of safety improvements and advanced engineering factors will require the acquisition of additional right-of-way involving previously developed and paved areas. The acquired right-of-way for this alternative has negligible wildlife habitat value and should not result in long-term impacts on the biological resources in the area. Some incidental tree loss may occur adjacent to the reworked roadway and bridge; however, none of the trees appear to be of specimen quality.

6.5 CULTURAL RESOURCES

Several cultural resource investigations of the Area of Potential Effect (APE) were conducted as part of this study, including a *Historic Architectural Sites Survey* completed in September 2001 and revised in October 2004 and a *Phase 1A Archaeological Investigation* completed in July 2003. The results of these studies, as well as the prehistoric and historic context of the Landing area, are described below.

6.5.1 Identification of Resources

During most of the prehistoric period (ca. 10,000 B.C. to A.D. 1650), northern New Jersey was occupied by various groups of nomadic hunters and gatherers. However, during the last 1,000 to 1,500 years, prehistoric populations became more numerous and more sedentary. The diffusion and subsequent adoption of agricultural practices, supplemented with hunting, fishing, and intensive plant collecting, permitted the production of food surpluses as well as the development of sedentary populations. Prehistoric campsites (and later hamlets or villages) were generally located in proximity to potable water and in locations that provided access to diverse resources. The APE is located in a shallow valley south of Lake Hopatcong, a natural glacial lake. Prehistoric sites in New Jersey are frequently associated with level to moderately level terrain, well drained or moderately well drained soils such as those associated with stream terraces, and proximity to potable water. Although much of the APE is relatively level, the soils are steep to very steep, stony or gravely, and moderately well drained to poorly drained.

Based on the diverse land use history within the town of Landing and in the vicinity of the bridge, railroad station, and canal, it is possible that a variety of historical archaeological sites may be located beneath fill deposits within the project area. In addition to the Morris Canal, other possible sites in the project area include artifacts or features associated with the canal, the railroad, the railroad station (both the original station and the current station), commercial businesses, and historical residences.

The Village of Landing developed in the third quarter of the nineteenth century as a result of the Morris Canal and the Delaware, Lackawanna & Western (DL&W) Railroad. The Morris Canal (described in detail below), opened in 1831 and in operation until the 1920s, was designed to transfer coal, iron ore, and other goods from eastern Pennsylvania and northwestern New Jersey to market centers like Jersey City, Paterson, Newark, and New York. Within decades, however, the concern for faster and cheaper year-round transportation of coal and iron ore, and the desire to increase tonnage, resulted in the formation and construction of a railroad. The DL&W Railroad was constructed in the 1850s and followed a circuitous route between Phillipsburg on the Delaware River to Jersey City on the east, the same terminus points as used by the Morris Canal. The Morris Canal was abandoned in 1924 and subsequently filled.

No archaeological sites that are eligible for listing in the NRHP were identified within the APE for archaeology, which corresponds to the footprint of anticipated ground disturbance directly associated with the project actions. The July 2003 *Phase 1A Archaeological Investigation* identified three areas of moderate historic archaeological potential within the APE for archaeology. One of these locations, designated as Area 1, is located at the eastern abutment of the Landing Road Bridge and may possess archaeological deposits associated with the eastern bank and towpath of the Morris Canal. It was recommended that no further investigation of this area be conducted until site preparation work for the bridge rehabilitation/replacement begins. In

the event that potentially significant canal-related archaeological resources are identified, consultation between the New Jersey Department of Transportation and the New Jersey Historic

Preservation Office will define the procedures necessary to proceed with the handling of said resources.

With state funding to be utilized for all forthcoming project actions, only a single cultural resource – **the Morris Canal Historic District** – that requires consideration as a consequence of its listing on the New Jersey Register of Historic Places on November 26th, 1973. The New Jersey Register of Historic Places Act requires that any undertaking that will "encroach upon, damage or destroy" a resource listed on the New Jersey Register is subject to review. This review commences with the submission of an Application for Project Authorization to the New Jersey Historic Preservation Office, with a determination of Encroachment or No Encroachment to result. In the event that the project is ruled to be an Encroachment, a further review by the New Jersey Historic Sites Council will be required. The process concludes with final action being taken by the Commissioner of the Department of Environmental Protection based on recommendations provided by the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council.

The Morris Canal was instrumental in the development of the nineteenth-century village of Landing, now part of the Township of Roxbury. The waterway, which operated from 1831 to 1924, was designed to transfer coal, iron ore, and other goods from eastern Pennsylvania and northwestern New Jersey to markets such as Jersey City, Paterson, Newark, and New York City. The summit of the canal was Lake Hopatcong, and a 0.65-mile navigable feeder connected Lake Hopatcong with the main canal at Landing. A series of locks and dams as well as a system of inclined planes was constructed to negotiate the mountainous terrain of northwestern New Jersey. With the opening of the Morris and Essex Railroad in 1853, which included stations at Landing and Port Morris, passengers and freight were transferred from boat to railroad car at the Landing (Lake Hopatcong) Station. A detailed history of the Morris Canal can be found in the October 2004 Historic Architectural Sites Survey. Within the project's APE, the site of the Morris Canal has been filled, with the sections adjacent to the Lake Hopatcong Station now serving as an asphalt-paved parking lot and a gravel service road associated with the station.

6.5.2 Evaluation of Effects

The potential effects on of the four alternatives on the Morris Canal Historic District can by summarized as follows:

- No-Build Alternative no effect
- Rehabilitation Alternative no adverse effect
- Bypass Alternative **Adverse Effect** resulting from the demolition of the Landing Road Bridge, a contributing resource within the district and **Potential Adverse Effect** resulting from impacts to potential canal-related archeological resources that may be disturbed during construction
- Replacement Alternative **Adverse effect** resulting from the demolition of the Landing Road Bridge, a contributing resource within the district and **Potential Adverse Effect** resulting from impacts to potential canal-related archeological resources that may be disturbed during construction

No-Build Alternative

The No-Build Alternative would not involve any taking of additional right-of-way and therefore not involve excavation or modification of existing structures. This alternative would have no effect on the Morris Canal Historic District.

Rehabilitation Alternative

The Rehabilitation Alternative would not involve any taking of additional right-of-way and would have a similar footprint to the No-Build Alternative, but it would involve short-term adverse impacts during the construction period due to increased congestion and traffic disruption. During the construction period, the bridge and the Morris Canal Historic District would be disturbed by the rehabilitation activities. Assuming that the rehabilitation of the bridge was completed in a historically appropriate manner, this alternative would have no adverse effect on the Morris Canal Historic District.

Bypass Alternative

The Bypass Alternative would have an adverse effect on the Morris Canal Historic District as the bridge would be demolished to the expense of continued maintenance during the next 20 years and beyond. The New Jersey Historic Preservation Office has determined that the construction of the project as proposed in this alternative will have an adverse effect on the Morris Canal Historic District due to removal of the Landing Road Bridge, a contributing resource to this historic district. It is also possible that archaeological features contributing to the eligibility of the Morris Canal Historic District could be discovered during construction, which may also result in an adverse effect. An archaeological monitoring plan will be developed through consultation between the New Jersey Department of Transportation and the New Jersey Historic Preservation Office as an initial step in dealing with these potential archaeological issues.

Replacement Alternative

The Replacement Alternative would also have adverse effect on Morris Canal Historic District as it will involve the demolition of the existing bridge. The New Jersey Historic Preservation Office has determined that the construction of the project as proposed in this alternative will have an adverse effect on the Morris Canal Historic District due to removal of the Landing Road Bridge, a contributing resource to this historic district. It is also possible that archaeological features contributing to the eligibility of the Morris Canal Historic District could be discovered during construction, which may also result in an adverse effect. An archaeological monitoring plan will be developed through consultation between the New Jersey Department of Transportation and the New Jersey Historic Preservation Office as an initial step in dealing with these potential archaeological issues.

6.5.3 Mitigation

To mitigate the adverse effect of the project on the Morris Canal Historic District the project team will coordinate with the New Jersey Department of Transportation and the New Jersey Historic Preservation Office to ensure that the design of the new structure reflects the shape and appearance of the existing bridge to the greatest extent possible, and that other

roadway and landscape design elements are consistent with the character of the said historic district. In addition, to minimize potential effects on unknown archeological resources within the district an archaeological monitoring plan will be developed by the project team for the approval of the New Jersey Department of Transportation and the New Jersey Historic Preservation Office.

6.6 HAZARDOUS SUBSTANCES

Two of the project alternatives would involve the taking of properties that include buildings that were constructed prior to 1970. These buildings may have asbestos containing materials (ACM) in the insulation, fireproofing, flooring, or other construction materials. This concern needs to be addressed on a building-by-building basis and is beyond the scope of this screening study. Site-specific surveys need to be conducted for lead-based paint and other household hazardous substances that also need to be properly managed. Older heating oil USTs also represent a potential for contamination that need to be checked on a specific property basis when the selected alternative is chosen and the preliminary design is complete. Water lines and other utilities may also need verification for the absence of ACM and PCB containing transformers and capacitors. These items need to be identified during the final design and provisions developed to provide for their proper management.

Properties considered to have potential concerns during the hazardous substance contamination screening are identified in the *Hazardous Substance Screening Report* prepared by Parsons in July 2002. Further investigations such as reviewing site-specific studies, remediation, and monitoring records will be required if the selected routing involves the taking of a specific property or subsurface excavation near a site of potential contamination. Soil and groundwater sampling may also be required depending on the involvement with each potentially contaminated property. Resurfacing of an existing roadway within an existing right-of-way represents limited involvement with the property and would most likely not cause exposure or release of contaminated areas. Acquisition of and/or excavation within properties that have been identified as having a potential for contamination would require special precautions and proper classification and management of contaminated media (soil, ground water, and rock).

Specific properties of concern identified during the hazardous substance contamination screening include the following:

Kingsland Road

• 20 Kingsland Road, Residence - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the leaking heating oil UST has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination.

Lakeside Boulevard

• 105 Lakeside Boulevard, Commercial, Multiple Businesses - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of reports and remediation records may provide sufficient

information that the threat of contamination from the septic system has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The building would also need to be checked for ACM, lead-based paint, PCB-containing transformers and capacitors, and any potential heating oil tanks. The property has a long record of commercial use (since the late 1800s) and excavators should be aware of potential concerns.

• 130 Lakeside Boulevard, U.S. Postal Service, Landing Post Office - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of the reports, remediation records, and monitoring records may provide sufficient information that the threat of contamination from the leaking heating oil UST has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination.

Landing Road

- 101 Landing Road, 101 Landing Company LLC/Weichert Realty The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, leaded gasoline, and waste oil has been adequately addressed (the site was a former gas station). Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The building would also need to be checked for ACM, lead-based paint, PCB-containing transformers and capacitors, and any potential heating oil tanks.
- 124 Landing Road, Gulf Service Station The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of the reports, remediation records, and monitoring records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, leaded gasoline, and waste oil has been adequately addressed. The site is reported to have free product recovery continuing. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system and floor drain fields may also be of concern if in the path of the improvements.
- 130 Landing Road, Rumors Restaurant The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the leaking heating oil UST has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination.

• 146 Landing Road, Speedway (formerly Sunoco) Service Station - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of the reports, remediation records, and monitoring records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, leaded gasoline, and waste oil has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system and floor drain fields may also be of concern if in the path of the improvements. The property has a record of other oil and fuel spills and excavators should be aware of potential concerns.

Mt. Arlington Boulevard

- 91 Mt. Arlington Boulevard, Lakes End Marina The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of the reports and remediation records may provide sufficient information that the threat of contamination from the former USTs holding heating oil and gasoline has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system may also be of concern if in the path of the improvements. The property has a record of other oil and fuel spills and excavators should be aware of potential concerns.
- The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, gasoline, and waste oil has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system and floor drain fields may also be of concern if in the path of the improvements.

6.6.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. The No-Build Alternative would not involve any taking of additional right-of-way and therefore would not involve properties that have potential hazardous substance concerns.

6.6.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative, but would involve short-term adverse impacts during the construction period due to construction work associated with bridge rehabilitation. The properties at 105 Lakeside Boulevard, 101 Landing Road and 124 Landing Road are in close proximity to the bridge and any excavation plans should be reviewed for the potential to involve hazardous substances.

Generation of contaminated media would have short-term adverse impacts involved with its management or remediation.

6.6.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. This may avoid potential involvement with several of the properties of concern along Landing Road except for those between 130 and 146 Landing Road, where the bypass roadway would split off. On the north end of the bypass, properties on Canal Street, 101 Landing Road, and 91 Mt. Arlington Boulevard represent potential short- and long-term impacts associated with the potential management and remediation of any remaining residues.

Any excavation plans should be reviewed for the potential to involve hazardous substances. Generation of contaminated media would have short-term adverse impacts involved with its management or remediation. Long-term impacts would result if the hazardous substance findings warrant long-term remediation activities.

6.6.4 Replacement Alternative

The Replacement Alternative would have more potential interference impacts than the No-Build and Rehabilitation alternatives. Replacement of the bridge would have similar short-term construction potential impacts to those associated with the rehabilitation alternative with a somewhat larger area and a longer duration. The incorporation of safety improvements and advanced engineering factors would require the acquisition of additional right-of-way involving additional properties.

The properties at 105 Lakeside Boulevard, 101 Landing Road, and 124 Landing Road are in close proximity to the bridge and any excavation plans should be reviewed for the potential to involve hazardous substances. The additional work upgrading Landing Road and Lakeside Boulevard could also have associated impacts with properties between 101 and 130 Landing Road and between 105 and 130 Lakeside Boulevard. Generation of contaminated media would have short-term adverse impacts involved with its management or remediation. Long-term impacts would result if the hazardous substance findings warrant long-term remediation activities.

SECTION 7 – EVALUATION OF ALTERNATIVES

This section provides a comparative evaluation of the alternatives under consideration for the Landing Road Bridge Project. The evaluation is intended to demonstrate the relative effectiveness of each alternative in addressing the project's stated needs and to highlight the relative costs, benefits, and impacts of each alternative.

The evaluation framework addresses the following key questions for each alternative:

- How well does the alternative address the project purpose and need?
- What are the alternative's effects on the natural and built environments?
- What is the alternative cost and would it be financially feasible for the County?
- What degree of community support does the alternative have?

7.1 CONFORMANCE WITH PROJECT PURPOSE AND NEED

The purpose and need for this project is articulated in Section 4 of this Alternatives Analysis. The purpose of the Landing Road Bridge Project is to improve safety and traffic operations through the rehabilitation or replacement of the Landing Road Bridge and improvements to the intersecting roadways on both approaches. Improvements are needed to address structural and functional deficiencies in the bridge and the approach roadways. As shown in previous Bridge Re-Evaluation Survey Reports, the existing bridge has been considered to be structurally deficient due to the condition of the superstructure and substructure, although the most recent report (September 2013) does not specifically mention "structurally deficient" due to recent interim repairs that have been made (see Appendix B). These reports, including the most recent report, also identify the bridge as being functionally obsolete, due to inadequate deck geometry. In addition, several intersections along the approach roadways experience a Level of Service (LOS) "F" and have substandard geometry.

7.1.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. Normal maintenance would stabilize the structure, preserving the structural integrity of the bridge and extending its useful life. However, these repairs would not address the structural deficiencies of the bridge. Moreover, the bridge would continue to remain functionally obsolete for both the existing and projected future level of daily traffic. The failing levels of service at intersections approaching the bridge would be expected to continue, and as traffic increases, the number of accidents on Landing Road would be expected to increase. Traffic operations would also be negatively affected by the increased need for maintenance and stabilization repairs as the existing bridge continues to age and deteriorate. In the future, without comprehensive rehabilitation, restrictions will likely need to be placed on traffic loads on the bridge, ultimately culminating in its closure to traffic.

Overall, the No-Build Alternative would not meet the purpose and need for the project.

7.1.2 Rehabilitation Alternative

The Rehabilitation Alternative would partially address the structural deficiencies of the bridge. In the near term, the proposed rehabilitation would increase the bridge's load carrying capacity; however, the repairs would not increase the overall strength of the bridge for the long term. Because the bridge has far exceeded its design life and its structural elements have experienced substantial deterioration, such repairs would only be possible by completely replacing the bridge.

The Rehabilitation Alternative would not include widening of the bridge; therefore, under this alternative, the bridge would continue to remain functionally obsolete for both the existing and projected future level of daily traffic. The failing levels of service at intersections approaching the bridge would be expected to continue, and as traffic increases, the number of accidents on Landing Road would be expected to increase.

Similar to the No-Build Alternative, traffic operations under the Rehabilitation Alternative would be negatively affected in the long term by the increased need for maintenance and stabilization repairs as the existing bridge continues to age and deteriorate. In the future, restrictions would likely need to be placed on traffic loads on the bridge, ultimately culminating in its closure to traffic. Due to the structural improvements associated with the rehabilitation work, such a scenario would likely occur at a later time than expected under the No-Build Alternative.

Overall, the Rehabilitation Alternative would partially address the project purpose and need, but most needs would remain unmet.

7.1.3 Bypass Alternative

Under the Bypass Alternative a new bridge would be constructed on a new alignment south of the existing Landing Road Bridge. The new bridge would be designed to meet NJDOT and AASHTO design standards for the current and projected levels of daily traffic in the Landing area. As such, the alternative would address both the structural and functional needs for the project. Moreover, the alternative would include roadway modifications that would improve traffic operations in the Landing area. The associated reduction in traffic congestion would be expected to reduce accident rates in the study area.

Overall, the Bypass Alternative meets the project purpose and need.

7.1.4 Replacement Alternative

Under the Replacement Alternative a new bridge would be constructed to replace the existing Landing Road Bridge in the same general location. The new bridge would be designed to meet NJDOT and AASHTO design standards for the current and projected levels of daily traffic in the Landing area. As such, the alternative would address both the structural and functional needs for the project. Moreover, the alternative would include roadway modifications that would improve traffic operations. The associated reduction in traffic congestion would be expected to reduce accident rates in the study area.

Overall, the Replacement Alternative meets the project purpose and need.

7.2 PROJECT COSTS

Project costs include the cost of relocation of utilities, cost of right-of-way acquisition, and the initial construction cost. Please see Appendix C for detailed cost estimates for the various alternatives. Right-of-way acquisition costs do not include relocation costs.

7.2.1 No-Build Alternative

The No-Build Alternative would involve neither utility relocation nor right-of-way takings. Because no initial construction is required under this alternative, there would be no initial construction cost. However, the need for regular maintenance of the bridge would be required during the next 20 years, simply to extend its life to the end of that 20-year period. This cost would not result in significant long-term benefit, but only the ability to extend the life of the bridge for an additional 20 years.

7.2.2 Rehabilitation Alternative

The cost of utility relocations under the Rehabilitation Alternative would be \$0.12 million (in 2011 dollars), with no right-of-way cost. The initial construction cost of this alternative would be \$1.8 million (in 2011 dollars). These costs total \$1.92 million (in 2011 dollars).

7.2.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of new right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. This new right-of-way would involve acquisition from 29 properties, with six complete takes. The cost of right-of-way acquisition and utility relocations for this alternative would be \$3.8 million and \$1.9 million, respectively, with an initial construction cost of \$9.7 million (in 2011 dollars). These costs total \$15.4 million (in 2011 dollars).

7.2.4 Replacement Alternative

Total capital costs associated with the Replacement Alternative (approximately \$8.7 million in 2011 dollars) would be higher than those of the No-Build and Rehabilitation alternatives but substantially lower than the capital costs associated with the Bypass Alternative (\$15.4 million in 2011 dollars). The Replacement Alternative would also provide for the incorporation of safety improvements and advanced engineering factors, which require the acquisition of additional right-of-way involving 20 properties, with three complete takes. The cost of right-of-way acquisition and utility relocations for this alternative would be \$1.6 million and \$1.0 million, respectively, with an initial construction cost of \$6.1 million (in 2011 dollars).

7.3 COMMUNITY SUPPORT

After considering the results of this alternatives analysis, the Roxbury Township Council issued a resolution supporting the Replacement Alternative (Alternative No. 7-C) on April 14, 2003 and also on September 29, 2015 (see Appendix D). The Roxbury Township Historic Advisory Committee had previously confirmed its support for the Replacement Alternative in a letter dated September 23, 1998 (see Appendix F).

The Canal Society of New Jersey submitted a letter in support of repairing and reusing the bridge on its original site, stating that the loss of the historic bridge would be a serious negative impact to the Morris Canal Historic District and the planned Morris Canal Greenway (see Appendix F). Efforts will be made to meet with the Canal Society to alleviate their concerns regarding the Replacement Alternative.

7.4 SUMMARY EVALUATION

The evaluation of alternatives is summarized in Table 7-1 included on the next two pages. This table indicates the results / impacts of each alternative studied in terms of a variety of factors including Traffic Operations and Safety Improvements, Engineering / Construction Impacts, Costs, Socioeconomic Resources, Physical Resources, Water Resources, Biological Resources, Cultural Resources, Hazardous Substances and Ability to Achieve Project Needs. A further description of the evaluation of each alternative follows.

7.4.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. With the No-Build Alternative, the long-term lack of traffic improvements and the continued deterioration of the existing bridge would result in no benefit to any planned business development or benefit to traffic operations and safety conditions.

The No-Build Alternative would not involve any taking of additional right-of-way or construction outside of the existing footprint of the bridge. Water, biological and cultural resources and potential hazardous substance sites would not be directly impacted.

The following are the advantages of this alternative:

- 1. No right-of-way impact.
- 2. No utility impact.
- 3. No initial outlay of cost, although overall increase of maintenance cost would be spread out during the next 20 years.
- 4. The No-Build Alternative would not directly affect any environmental resources.
- 5. No short-term traffic impact or associated air quality / noise impact during construction.

The following are the disadvantages of this alternative:

- 1. It would not address the documented deterioration of the bridge.
- 2. It would not improve traffic operations and safety.
- 3. Minimal, if any, construction employment benefit.
- 4. Potential long-term adverse impacts to the Landing Road Bridge and the Morris Canal Historic District (of which the bridge is a contributing feature) due to continued deterioration of the bridge.
- 5. Does not meet the overall needs of the community.

TABLE 7-1: ALTERNATIVES EVALUATION MATRIX

DESCRIPTION	No-Build Alternative	Rehabilitation Alternative (Maintain existing bridge geometry)	Bypass Alternative (Alt. No. 1)(New bridge on a new alignment)	Replacement Alternative (Alt. No. 7C) (New bridge on existing alignment)
Traffic Operations And Safety Improvements				
Improve Access to RR Station Parking Area	NO	NO	YES	YES
Increased Width of Landing Rd. and Bridge	NO	NO	YES	YES
Improve Vertical Sight Dist. Over Bridge	NO	NO	YES	YES
Reconst. Int. of Landing Rd & Lakeside Blvd	NO	NO	YES	YES
Other Adverse Impacts				
Utility Relocation	NO	MINOR	YES	YES
Right of Way	NO	NO	YES	YES
Traffic During Const.	NO	YES	YES	YES
Costs				
Utility Cost (Millions in 2011 Dollars)	\$0.00	\$0.12	\$1.90	\$1.00
ROW cost (Millions in 2011 Dollars)	\$0.00	\$0.00	\$3.80	\$1.60
Initial Const. Cost (Millions in 2011 Dollars)	\$0.00	\$1.80	\$9.70	\$6.10
Total Cost (Millions in 2011 Dollars)	\$0.00	\$1.92	\$15.40	\$8.70
Socioeconomic Resources				
Residential Properties	NONE	NONE	HIGH IMPACT	MODERATE IMPACT
Businesses	NONE	SLIGHT IMPACT	HIGH IMPACT	MODERATE IMPACT
Local Economy	NONE	MODERATE IMPACT	MODERATE IMPACT	MODERATE IMPACT
Compliance with Regional, County & Local Plans	NO	NO	NO	YES
Construction Employment	NO BENEFIT	SLIGHT BENEFIT	HIGH BENEFIT	MODERATE BENEFIT
Physical Resources				
Geologic / Topographic	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Soils	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Air Quality	NONE	SLIGHT IMPACT	HIGH IMPACT	MODERATE IMPACT
Noise Levels	NONE	SLIGHT IMPACT	MODERATE IMPACT	SLIGHT IMPACT

TABLE 7-1: ALTERNATIVES EVALUATION MATRIX (CONT'D)

DESCRIPTION	No-Build Alternative	Rehabilitation Alternative (Maintain existing bridge geometry)	Bypass Alternative (Alt. No. 1)(New bridge on a new alignment)	Replacement Alternative (Alt. No. 7C) (New bridge on existing alignment)
Water Resources				
Surface Waters	NONE	SLIGHT IMPACT	MODERATE IMPACT	SLIGHT IMPACT
Ground Water	NONE	NONE	MODERATE IMPACT	NONE
Biological Resources				
Fish / Wildlife	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Vegetation	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Wetlands	NONE	NONE	MODERATE IMPACT	NONE
Cultural Resources				
Morris Canal Historic District	POTENTIAL FUTURE IMPACT	POTENTIAL FUTURE IMPACT	POTENTIAL ARCHAEOLOGICAL IMPACT	POTENTIAL ARCHEOLOGICAL IMPACT
Hazardous Substances				
Kingsland Road	NONE	NONE	NONE	NONE
Lakeside Boulevard	NONE	SLIGHT IMPACT (POTENTIAL)	NONE	MODERATE IMPACT (POTENTIAL)
Landing Road	NONE	SLIGHT IMPACT (POTENTIAL)	MODERATE IMPACT (POTENTIAL)	MODERATE IMPACT (POTENTIAL)
Mt. Arlington Boulevard	NONE	NONE	MODERATE IMPACT (POTENTIAL)	NONE
Conformance with Project Needs	DOES NOT MEET PURPOSE AND NEED	DOES NOT MEET PROJECT PURPOSE AND NEED	PARTIALLY MEETS PROJECT PURPOSE AND NEED	MEETS PROJECT PURPOSE AND NEED

The No-Build Alternative overall does not meet the needs of the community or the purpose and need for the project. Even with stabilization measures, the bridge would continue to deteriorate, resulting in a continued decline in transportation services and of the integrity of the bridge as a resource. Although capital costs are the lowest, maintenance costs would continue to increase and would result in less improvement for the investments made.

7.4.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative but would require some construction activities to rehabilitate the bridge to the *Secretary of the Interior's Standards*, which would not alter the existing historic features or further modernize the bridge. Existing bridge maintenance requirements will continue to increase as rehabilitation within the parameters of historic integrity will result in repairs of failing and severely deteriorating structural elements with materials to match the old in design, color, and texture. Replacement of missing features will be substantiated with documentary and physical evidence.

Similar to the No-Build, the Rehabilitation Alternative footprint would be consistent with the existing bridge and roadways. Also, the long-term lack of traffic improvements and the continued long-term deterioration of the existing bridge would result in no benefit to any planned Short-term impacts associated with traffic disruptions, noise, and dust during construction would be likely, while some minor adverse impacts related to socioeconomic, physical, water, and cultural resources may also occur. If the rehabilitation involves any excavation, some properties with potential hazardous substance concerns represent potential impacts.

The following are the advantages of this alternative:

- 1. The historic bridge would be retained and rehabilitated, although long-term deterioration would continue.
- 2. No right-of-way impact.
- 3. Minimal utility impact.
- 4. Relatively low initial outlay of cost, although need for maintenance would continue in the future.
- 5. Relatively limited direct impact to environmental resources.
- 6. Minor construction employment benefit.

The following are the disadvantages of this alternative:

- 1. The long-term historic nature of the bridge could be compromised due to continued long-term deterioration.
- 2. It would not improve traffic operations and safety.
- 3. Impact on traffic during construction.
- 4. Impact on noise and air quality during construction.
- 5. Potential impact to businesses during construction.

- 6. Potential long-term adverse impact to the Landing Road Bridge and Morris Canal Historic District (of which the bridge is a contributing feature) due to continued deterioration of the bridge in the long term.
- 7. Does not meet the overall needs of the community.

Similar to the No-Build Alternative, the Rehabilitation Alternative overall does not meet the needs of the community or the purpose and need for the project. Due to the deterioration and substandard condition of the bridge, the Rehabilitation Alternative would have the same long-term adverse impacts on not meeting the transportation and infrastructure needs of the area.. The capital costs of the Rehabilitation Alternative are more than the No-Build Alternative, but substantially less than the Bypass or Replacement alternatives. Maintenance costs would also continue to increase, resulting in less improvement for the investments made in the long term.

7.4.3 Bypass Alternative

The Bypass Alternative would involve extensive acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. The removal of the needed property from the tax base and the bypassing of the portion of Landing would have adverse impacts on the businesses along that portion of Landing Road. This would be adverse to the long-term planned business development in Landing. These adverse socioeconomic impacts, as well as other environmental impacts associated with the alternative, are substantial.

In addition, the long-term commitment of additional lands to this alternative was viewed as an adverse impact to the physical, water, and biological resources of the area. Construction activities would also cause short-term adverse impacts to these resources. A wetland on an adjacent property could also be impacted by construction runoff. This alternative cuts through some of the last remaining undeveloped portions of the Landing area. Also, this alternative, which includes the demolition of the subject bridge, represents an adverse impact to the Morris Canal Historic District. The various socioeconomic and environmental impacts associated with this alternative are considered to be the worst in comparison to all of the other alternatives.

The following are the advantages of this alternative:

- 1. This alternative would improve traffic operations and safety.
- 2. It would provide improved access to and from the NJ TRANSIT station parking area.
- 3. No design exceptions are anticipated.
- 4. Major construction employment benefit.

The following are the disadvantages of this alternative:

- 1. Extensive utility impact and relocation.
- 2. Requires extensive right-of-way acquisition.
- 3. Requires demolition of at least four residential buildings and four commercial buildings.

- 4. Adverse impact to the Morris Canal Historic District as the Landing Road Bridge (a contributing resource within the said district) would be demolished as part of this alternative.
- 5. Potential adverse impact on archaeological resources contained within the Morris Canal Historic District.
- 6. It would have greater overall long-term and short-term environmental impacts than the Replacement Alternative or either of the other alternatives.
- 7. Highest construction cost of all alternatives.
- 8. Bypassing of existing businesses could have an adverse economic effect.
- 9. Does not meet the overall needs of the community.

Although traffic and safety conditions would be improved, the Bypass Alternative does not meet the overall needs of the community given the extent of the impacts associated with it. The Bypass Alternative also has the highest construction cost of any of the alternatives, including the costs associated with right-of-way acquisition and utility relocations.

7.4.4 Replacement Alternative

The Replacement Alternative incorporates safety improvements and advanced engineering factors that would require the acquisition of some additional right-of-way. This alternative is compatible with the planned business development with roadway enhancements that would decrease congestion and improve access. Under the Replacement Alternative, the socioeconomic resources would have some loss of right-of-way and several buildings, but the Landing business district would be maintained.

Long-term and short-term impacts on physical, water, and biological resources would occur, but at a lesser level than with the Bypass Alternative. Similar to the Bypass Alternative, the Replacement Alternative requires the demolition of the Landing Road Bridge, although a new bridge would be constructed at the same location as the existing bridge instead of on new alignment. As a result, the adverse impact of this alternative on the Morris Canal Historic District is somewhat less than that of the Bypass Alternative as said impact can be reduced through the recordation of the existing bridge and the incorporation of historic design elements in the new structure.

The following are the advantages of this alternative:

- 1. This alternative would improve traffic operations and safety.
- 2. It would provide improved access to and from the NJ TRANSIT station parking area under the bridge.
- 3. Both the load capacity and life expectancy of the bridge would be increased.
- 4. Improved vertical sight distance over the bridge.
- 5. No detour would be required during construction.
- 6. Moderate construction employment benefit.
- 7. Meets the overall needs of the community.

The following are the disadvantages of this alternative:

- 1. Adverse impact to the Morris Canal Historic District as the Landing Road Bridge (a contributing resource within the said district) would be demolished as part of this alternative.
- 2. Potential adverse impact on archaeological resources contained within the Morris Canal Historic District.
- 3. It would require demolition of two commercial buildings and one residential building.
- 4. It would require utility relocations, but fewer than the Bypass Alternative.
- 5. It would require right-of-way acquisition, but fewer properties than the Bypass Alternative.
- 6. It would have greater overall environmental impacts than the Rehabilitation Alternative, but less than the Bypass Alternative.
- 7. It would have a higher construction cost than the Rehabilitation Alternative, but substantially less than the Bypass Alternative.

7.4.5 Summary

Based on the above comparative analysis and the summary in Table 7-1, it has been determined that the Replacement Alternative best meets the needs of the community and provides the best combination of overall benefits and impacts. It adequately addresses the long-term transportation needs without encroaching on the community's plans and provides a long-term solution with the least disruption. The capital costs of the Replacement Alternative would also be much lower than those for the Bypass Alternative, which involves greater right-of-way acquisition and roadway construction, and the maintenance costs of the Replacement Alternative would be less than for either the No-Build or the Rehabilitation Alternatives. For these reasons, the Replacement Alternative (Alternative 7-C) was selected as the Initially Preferred Alternative. Further discussion of this recommendation is provided in the next section.

SECTION 8 - RECOMMENDATION

The preceding section presents the comparative analysis used by the County of Morris in selecting an Initially Preferred Alternative (IPA) for the Landing Road Bridge Project. The reader is referred to the various sections of this report for detailed discussions of each category and overall impact of each alternative.

The results of this alternatives analysis were presented to the Roxbury Township Council at a Workshop Meeting on April 14, 2003, and the Council issued a resolution supporting the Replacement Alternative (Alternative No. 7-C) for the project. A copy of the resolution of support issued by the Roxbury Township Council is included in Appendix D of this report.

The project was again presented on June 9, 2015 to the Roxbury Township Council updating them on the status of the project with the various alternates studied. On August 12, 2015 a Public Information was held at the Roxbury Township Municipal Building with Morris County and the RBA Group and alternate 7-C was again the recommended alternate presented and received support from the public and Township, Resolution 2015-310 was adopted on September 29, 2015 by the Township of Roxbury supporting this alternate and is also included in Appendix D of this report.

The Replacement Alternative (Alignment No. 7-C) was selected as the IPA because it best addresses the project needs. The Replacement Alternative would improve safety and traffic operations in the project area, provide a new widened bridge meeting current design standards, and allow maintenance of traffic on the existing bridge while the new bridge is being constructed. The Replacement Alternative is recommended for the following reasons:

- 1. The alternative would satisfy the traffic operations needs of the project area.
- 2. The alternative is compatible with the proposed widening of Landing Road to I-80.
- 3. The alternative would improve the safety of the project area.
- 4. The new bridge would meet current structural design standards.
- 5. The proposed construction operations would not impact existing wetlands or regulated hazardous and solid waste sites in the project area.
- 6. The construction cost of the Replacement Alternative is significantly lower than that of the Bypass Alternative.
- 7. Traffic could be maintained during construction of the Replacement Alternative, although delays and increased travel time and distance are likely.
- 8. The alternative is compatible with the streetscape improvements project being undertaken by the Township of Roxbury.

The Replacement Alternative requires the replacement of the existing Landing Road Bridge, which is a contributing resource within the Morris Canal Historic District. The project team will coordinate with the New Jersey Historic Preservation Office to mitigate the adverse impact created by the demolition of the bridge by ensuring that the design of the new structure reflects the appearance of the existing bridge and that other roadway and landscape design elements are consistent with the character of bridge and the district. In addition, the project team will develop an archaeological monitoring plan as the initial phase of work necessary to address the above noted potential for archaeological resources associated with the district, again through continuing consultation with the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council

Morris County will also consult with the Roxbury Township Historic Advisory Committee and the Landing Gateway Committee during design of the project to comply with their recommendations of a two span arch structure, the proposed railing design, concrete texture of the surfaces to mimic the historic look of the original bridge, lighting, plantings, pedestrian crossing, bollards, sidewalks, parking and the fence details at the lake.

SECTION 9 - REFERENCES

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Steinman, 1998

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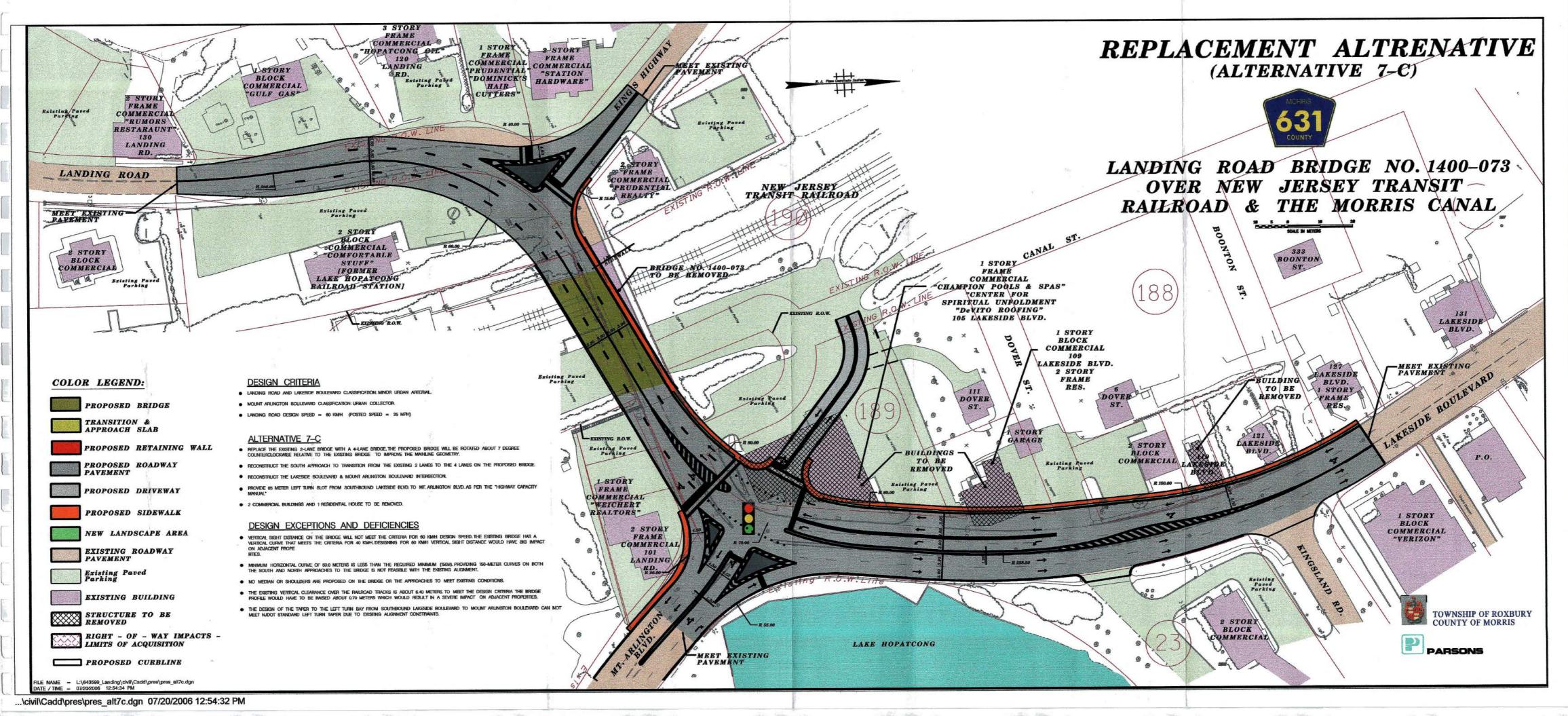
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APPENDIX – A

REPLACEMENT ALTERNATIVE FIGURE

ALTERNATIVE 7-C



APPENDIX – B BRIDGE RE-EVALUATION REPORTS

CYCLE NO. 15 (OCTOBER 2011)

&

CYCLE NO. 18 (UGRVGO DGT '4235)



Morris County DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION COURT STREET

PO Box 900 MORRISTOWN, NEW JERSEY 07963-0900

BRIDGE RE-EVALUATION SURVEY REPORT

STRUCTURE NO. 1400-073
COUNTY ROUTE 631 (LANDING ROAD)
OVER NJ TRANSIT-MORRISTOWN LINE
TOWNSHIP OF ROXBURY
MORRIS COUNTY

15TH CYCLE

OCTOBER 17, 2011

NOTE: This Bridge Re-evaluation Report shall be filed immediately after the 14TH Cycle Inspection Report.

Prepared By



Stantec Consulting Services Inc. 365 West Passaic Street Rochelle Park NJ 07662

TABLE OF CONTENTS

		Page No.
1	Structural Data	15-1
2	Conclusions and Recommendations	15-2
3	Structural Inventory & Appraisal and Pontis Sheets	15-3
4	Load Rating Summary Sheet (LRSS)	15-6
5	Drawings, and Photographs	15-8
6	Field Notes	15-20
7	Memorandum	15-34

N.J.D.O.T. - STRUCTURAL EVALUATION RE-EVALUATION BRIDGE SURVEY REPORT

CYCLE NO. 15

Year Built:

1907

Widened/Rehab:

1972

STRUCTURAL DATA:

1400-073

Bridge No.:

Date:

Route No.:	9014		Length: 136.0'		136.0'	Wic	dth:	38.8'		
Mile Point:	1.31		Date	of this Evalu	ation:	10/1	17/2011 & 01/04	1/2012		
Name:	County F	Route 631 (Landing	By: Stantec Consulting Services, Inc.							
	Road) Over NJ Transit- Morristown Line		Date of Previous Evaluation: 07/21/2009							
	Wionisto	wii Ellie	By:	S & R Eng	ineers, PC					
			•							
			Speci	al Equipmen	t Used:	No	one			
Structure Type:	•	n, Closed Spandrel, ed Concrete Arch	Unde	rwater Inspe	ction:	Not I	Required			
			Scou	Critical:	N/A					
WORK DONE: New deck waterproofing (see attached email and photos from Morris County) with new asphalt overlay on top that extends over both approaches (Photo 15-15). New concrete sidewalk along north side (Photo 15-16). New concrete curbs and guide rails along both sides (Photo 15-15). Concrete repairs throughout both arch intrados (Photos 15-05 and 15-06). Concrete repairs in the north parapet/headwall along the west span (Photo 15-17). Concrete repairs in the spandrel wall and coping at both spandrel walls of both spans (Photos 15-17 and 15-18). Concrete repairs at the south wingwall (Photo 15-19) and in the coping and vertical portion below the arch at north end of the west skewback (Photo 15-20). Concrete repairs in coping and wall at the northwest edge (Photo 15-22) and the south face of the pier stem (Photo 15-21). OVERALL PHYSICAL CONDITION: Fair due to the superstructure and substructure. Fair due to the superstructure and substructure.							v concrete along both 15-05 and oto 15-17). In the coping to 15-20.			
Inspection Team	Leader:	Thane Syverson, P.I	E		Initial	s: _	TJS			
Certifying Engine	eer:	Abdel A. Denho, P.	Е							
N.J. P.E. Number:		GE 39572								
I certify that this report is an accurate description of subject structure, to the extent determinable by visinspection and testing performed. Signature:							Original Sigr Seale			

Structure No.:	1400-073	Route:	9014	Cycle No.:	15	
	County Route 631 (La	nding Road) over NJ Transit-	_	10/17/2011 &	
Name:	Morristown Line			Insp. Date:	01/04/2012	

CONCLUSIONS AND RECOMMENDATIONS:

The overall condition of the structure is fair due to the superstructure and substructure.

The superstructure is in fair condition due to the numerous patching covering 60% of both arch intrados and the cracking, scaling, leakage and efflorescence throughout both arch intrados and spandrel walls.

The substructure is in fair condition due to the large concrete patches and the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments.

Since the previous inspection, the superstructure has been upgraded from poor to fair condition due to the concrete repairs throughout both intrados and in the spandrel wall and coping at both spandrel walls. The substructure has been upgraded from serious to fair condition due to the concrete repairs in the coping and wall at the northwest edge and south face of the pier. The approach roadway has been upgraded from satisfactory to good condition due to the new bituminous concrete overlay in the top of roadway at both approaches.

The structure is classified as functionally obsolete due to the inadequate deck geometry (Item 68 = 2). Therefore, we recommend the following remedial action:

1. Increase the roadway width from 28.8' to 32.0' by widening the bridge 3.2'.

a.	Demolition of existing south bridge railing:	\$25,000
	(Lump Sum)	
b.	Bridge Widening:	\$130,560
	3.2' x 136' = 435.2 SF @ \$300/SF	
c.	Associated approach work:	<u>\$50,000</u>
	(Lump Sum)	
	Subtotal	\$205,560
	Preliminary Engineering (15%)	\$30,834
	Traffic Maintenance (10%)	<u>\$20,556</u>
	Total	\$256,950
	SAY	\$260,000

In the interim until the bridge is widened, we recommend that the following Emergency/Priority repairs be made to retard further deterioration, preserve the structural integrity of the bridge, improve safety and extend its useful life:

None.

10/17/2013

StructNum: 1400073

NJDOT SI and A Sheet

Name: CO RT631(LANDING RD) / NJT-MORRISTOWN LINE S.R.: 58.5 SD/FO- 2 -Functionally Obsolete

24 months

91 Frequency:

IDENTIFICATION							
1 State:	34 New Jersey	8 Struc Num:	1400073				
7 Facility Carried:	CR631 (Landing Rd)	9 Location: At	Kings Highway Junction				
5A Rte.(On/Under):	Route On Structure	5B Rte. Signing Pre	efix: 4 -County Hwy				
5C Level of Service:	1 -Mainline	5D Rte. Number:	00631				
5E Directional Suffix:	0- Not Applicable	% Responsibility :	NA				
2 SHD District:	01- North	3 County Code:	Morris				
4 Place Code:	Roxbury (Township of), M	11 Mile Post:	1.310 mi				
6 Feature Intersected :	NJT-Morristown Line						
16 Latitude:	40d 54' 17.00"	17 Longitude:	074d 39' 57.42"				
l							

STRUCTURE TYPE AND MATERIALS 46 Number of Approach Spans: 0 45 Number of Spans Main Unit: 2

-2 Not Applicable (P)

43A/B Main Span Material/Design:

99 Border Bridge Number: Unknown

98 Border Bridge Code:

11 -Arch-Deck

107 Deck Type: 1- Conc..-Cast-in-Place 108A Wearing Surface: 6 -Bituminous 108B Membrane: 3- Epoxy 108C Deck Protection: 8- Unknown

AGE AND SERVICE

27 Year Built: 1907 106 Year Reconstructed: 1972

42A Type of Service On: 5 -Highway-pedestrian

42B Type of Service Under: 2 -Railroad

28A Lanes on: 2 28B Lanes Under: 0 19 Detour Length: 2.0 mi 22,606 109 Truck ADT: 4 % 30 Year of ADT: 2011

GEOMETRIC DATA

48 Length Max Span: 64.0 ft 49 Structure Length: 136.0 ft 50A Curb/Sdwlk Wdth L: 0.0 ft 50B Curb/Sidewalk Width R: 5.7 ft Width Curb to Curb 51: 28.8 ft 52 Width Out to Out: 38.8 ft 32 Approach Roadway Width: 28 ft (w/ shoulders) 33 Median: 0 No median

Deck Area: 5,277. sq. ft

34 Skew: 0.00 ° 35 Structure Flared: 0 -No flare 53 Minimum Vertical Clearance Over Bridge: 9999

54A Minimum Vertical Underclearance Reference: R -Railroad beneath str

54B Minimum Vertical Underclearance: 20.91 ft

55A Minimum Lateral Underclearance Reference R: R -Railroad beneath str

20.50 ft 55B Minimum Lateral Undrclearance R: 56 Minimum Lateral Undrclearance L: 0.00 ft

INSPECTION 90 Inspection Date: 10/17/2011 Next Inspection:

92A FC Frequency: NA 93A FC Inspection Date: Next FC Inspection: NA 92B UW Frequency: NA 93B UW Inspection Date: NA Next UW Inspection: NA 93C SI Date: 92C SI Frequency: NA NA Next SI: NA

Element Frequency: 24 months Element Inspection Date: 10/17/2011 Next Elem. Insp. Due: 10/17/2013

CLASSIFICATION

100 STRAHNET Highway: 0 -Not a STRAHNET hwy 101 Parallel Structure: N -No II bridge exists 102 Direction of Traffic: 2 -2-way traffic 103 Temporary Structure:

104 Highway System: 0 -Not on NHS 112 NBIS Length: Y - Long Enough 20 Toll Facility: 16- Urban Minor Arterial 3 -On free road 26 Functional Class:

37 Historical Significance: 2 -Eligible for NRHP 22 Owner: 02 County Hwy Agency 21 Custodian: 02 County Hwy Agency

CONDITION

58 Deck: 7 -Good 59 Super: 5 -Fair 60 Sub: 5 -Fair 62 Culvert: N -Not applicable 61 Channel/Channel Protection: N -Not applicable

LOAD RATING AND POSTING

65 Inventory Rating Method: 1-LF Load Factor 63 Operating Rating Method: 1-LF Load Factor

66 Inventory Rating: 64 Operating Rating:

31 Design Load: 0 -Other or Unknown 70 Postina: 5 At/Above Legal Loads

41 Posting status: A -Open, no restriction

APPRAISAL

36A Bridge Rail: 0 -Substandard 36C Approach Rail: 0 -Substandard 36B Transition: 0 -Substandard 36D Approach Rail Ends: 0 -Substandard 67 Str. Evaluation: 68 Deck Geometry: 2 Intolerable - Replace 69 Underclearance, Vertical and Horizontal: 4 -Tolerable 71 Waterway Adequacy: N -Not applicable 72 Approach Alignment: 5 -Above Tolerable

113 Scour Critical: N -Not Over Waterway

PROPOSED IMPROVEMENTS

94 Bridge Cost: \$130,560 75 Type of Work: 33 95 Roadway Cost: 76 Length of Improvment: 136 ft \$50,000 96 Total Cost: \$260,000 114 Future ADT: 30.450 97 Year of Cost Estimate: 2011 2031 115 Year of Future ADT:

NAVIGATION DATA

38 Navigation Control: N N -N/A-No waterway

39 Vertical Clearance: 0.0 ft 40 Horizontal Clearance: 0 0 ft 111 Pier Protection: 116 Lift Bridge Vertical Clearance:

ELEMENT CONDITION STATE DATA

Str Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
0	14/3	P Conc Deck/AC Ovly	(SF)	5,277	100 %	5,277	0 %	0	0 %	0	0 %	0	0 %	0
0	144/3	R/Conc Arch	(LF)	136	0 %	0	0 %	0	100 %	136	0 %	0	0 %	0
0	210/3	R/Conc Pier Wall	(LF)	39	0 %	0	100 %	39	0 %	0	0 %	0	0 %	0
0	215/3	R/Conc Abutment	(LF)	78	0 %	0	100 %	78	0 %	0	0 %	0	0 %	0
0	334/3	Metal Rail Coated	(LF)	272	100 %	272	0 %	0	0 %	0	0 %	0	0 %	0
0	503/3	Curbs/Walks - Conc	(LF)	136	100 %	136	0 %	0	0 %	0	0 %	0	0 %	0

NJDOT Inspection_SIA_English_No

Wed 2/29/2012 11:23:04

(v1.1)

StructNum: 1400073

NJDOT SI and A Sheet

S	tr Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
	0	506/3	Wingwalls - Abut	(LF)	32	50 %	16	50 %	16	0 %	0	0 %	0	0 %	0
	0	507/3	Headwalls - Other	(LF)	272	50 %	136	50 %	136	0 %	0	0 %	0	0 %	0

NJDOT BRIDGE FIELDS

NJDOT BRIDGE Brkey: 1400073	_		(V1.1)
GE	NERAL	COUNTER MEASURES	TEMPORARY STRUCTURES
(A) Town:	Roxbury Twp.	(FI) Recommended:	(GV) Bridge: N
(AA) Route:	9014		(GW) Shoring: N
(AD) Admin:	-1		(GX) Reports: N
(AE) Alt Agency:	=	J	(GY) Measures: N
STRUCTURE AN	ND APPURTENANCES	(FJ) Cost: 0 (FK) Installed: 1 _ 2 _ 3 _	(GZ) Cond Desc:
(AC) Non-Inv Feature: (AF) Alt Struct #:	RR Rdwy and/or RR ovr RR	(FL) Monitoring Reqd:	Д
(AG) Rail Type:	30	PROJECT PROGRAMMING	
(AH) Rail Height::	2.25	(HB) Bridge List ID: 14C1_	LOAD RATINGS
(AJ) Slope Protection:	-1	(HC) Consultant A47	Type Inv Opr
(AK) Abutment:	03 Full Height	(HD) 2-Cy Insp Contr: Y	H (BQ) 46 (CA) 77
(AL) Pier:	02 Solid Wall Pier		HS (BR) 65 (CB) 99
(AM) Depth of Fill:	0.00	(HE) Proceed Date: I 5/8/2003	3 (BS) 58 (CC) 98
(AP) Fender System:	-1	II 6/6/2005	3S2 (BT) 37 (CD) 63
(AT) Special Material 1:	_	(HF) State Proj Mgr: MAK	3-3 (BU) 59 (CE) 99
(AT) Special Material 2:	_		M (BV) (CF)
(AU) Add Struct: 1	_	BRIDGE NOISE BARRIER (HA	Misc (CH1) L
2	-	Material 1: _	(CH2) 0
, , , , , , , , , , , , , , , , , , , ,	aterial Design	2: _	
1st Widen _	-1	Height 1:	MISCELLANEOUS
2nd Widen _	-1	2:	(CQ) Bridge List:
FUNDIN	G AND COST	DAINTING	(BK) Overstress %: 0
	071112 0001	PAINTING	(FV) Route Milepost: .
(BL) Discretionary:	=	(GB) Environment: -1	(FM) Incident Reprtd:
(BM) Fed Job #:	BR NBIS755	(GR) Last Painting:	II
(BN) State Job #:	2205215	(GA) Painting Req: N	(AO) Utilities: 1: _
(BO) St Maint Cost:	0		2: _
(BP) Brdg Demo	N) FENCING	3: _
SCOUR	EVALUATION	(AQ) Chain Link:	4. –
(FA) FHWA Category:	-1	(FN) Warranted: Y	(CR) Off-Route Bridge: N
(FB) Stage 1 Date:	•	SIGN STRUCTURES	(BB) Orphan Bridge: N
(FC) Stage 1 Consultant:	-1	SIGN STRUCTURES	(AN) Plan Available: N
(FD) Prioritization:		(GS) Overhead: 0	(FX) Fed Error Uncorr N
/	-	(GT) Captilovor:	(FW) Estimated ADT N

(GT) Cantilever:

(CG) Load Type:

(CG) Tons

(AI) Speed:

(GU) Fascia Mounted:

POSTINGS

0

20

NJDOT Inspection_SIA_English_No

(FE) Sufficiency:

(FF)Date Stage II

(FH) Crit. Element:

(FG) Stage II Consultant: -1

(BC) USRA Code:

(BE) Rail Milepost:

(BD 1) Rail On/Under:

(BD2)RRs intersected:

0

2

45.53

RAILROAD

StructNum: 1400073

NJDOT SI and A Sheet

NJDOT INSPECTION FIELDS

(V1.2)

GENERAL	MISCELLANEOUS	REMARKS	PAINTING
(CI) Cycle Number: 15	(B) Deletion Code: N	(BF) Deck: 1: _	(GD) Fascia Beam: -1
(CJ) Insp Type: S	(BA) Apr Rdwy Cond: 7	2: _	(GE) Fascia B. Flange: -1
(CK) Inspection Crew: R		3: _	(GF) Interior Beam: -1
(CM) Consultant: V08	FATIGUE DETAIL (AZ)	4: _	(GH) Interior B. Flange: -1
(CO) Prev Consultant: A47	Location1: -1	5: _	(GI) Beam Ends: -1
(CP) Federal Report:	-1	(BG) Super: 1: 9	(GJ) Connections: -1
	Location 2: -1	2: Z	(GK) Bracings: -1
INSPECTION DATES	-1	3: _	(GL) Bearings: -1
(AW) Mech/Electrical:	Location 3: -1		(GM) Substructure: -1
(AX) Deck:	-1	(BH) Sub: 1: A	(GN) Above Deck Super: -1
(AY) Special Testing:		2: C	(GO) Railings/Fence: -1
(GC) Paint:	I IN-DEPTH PIN-HANGER	3: B	(GP) Remarks 1:
(AR) Equipment: 1: _	(FR) Consultant: -1	(BI) Channel _	
2: _	(FS)	(BJ) Culvert	
3: _			
(AS) Testing: 1: _		FENCING	(GQ) Remarks 2:
2: _		(FO) Pedestrian Traffic: 0	
3: _	(FT) Combo: N	(FP) Improvement Cost: \$ 20 Thousands	

ROADWAY DATA

Bridge Id: 1400073 SRI 14000631_ (V1.1)

(FQ) FCM/Pin-Hngr Insp Date:

	ROADWAY	/ IDENTIF	ICATION	
NBI Roadway?:	1			
Roadway Name:	CR 631 (L/	ANDING ROAD)		
Item 5A:	1			
Item 5B:	4			
Item 5C:	1			
Item 5D:	00631	Item 5E	0	
(

HWY NETWORKS AND SERVICE CLASSIFICATION

11 Milepost:	1.310 mi		
12 Base Hwy Network:	0		
20 Toll Facility:	3		
13A LRS Inventory Route		Subroute No.:	
26 Functional Class:	16	102 TraffDirection:	2

AI TERNATE	CLAS	SIFICA	LIUN

100 Strahnet Hwy:	0	Transit Rt:	0
104 NHS System:	0	Emergency	0
105 Fed. Lands Hwy:	0		
110 Truck Hwy Net:	0		
School Bus:	0		

TRAFFIC AND ACCIDENTS

28 Lanes :	2
Num Median:	0
Road Speed:	35 mph
ADTclass:	04
29 ADTtotal:	22606
30 Year of ADT:	2011
114 Future ADT:	30450
115 Year of Future ADT:	2031
109 Truck ADTT %:	4
19 Bypass Length:	2.00 mi
Detour Speed:	30 mph

CLEARANCES

10 Vertical Clearance:	99.99 ft
47 Invent Route Horiz Clear:	28.80 ft
(DJ) Min Vert Undrclr:	00.00 ft

WIDTHS

32 Appr Rdwy Width:	281
51 Brdg Rdwy With Curb-Curb:	28.8

Structure No.:		Cycle No.:	15
Name:	County Route 631 (Landing Road) over NJ Transit- Morristown Line	Insp. Date:	10/17/2011 & 01/04/2012
	LOAD RATING SUMMARY SHEET	(LRSS)	
	(Form NJ-BI-101 Created 1/25/2011)		
Project Inform	nation:		
Group: <u>140</u>	Agreement No.: 2011BI840C Contract ID: _	12-50808 Agre	e/Mod No.: 00
Rating Inform	nation:		
Method: L	RFR: No LFR: Yes ASR:	No Other (Spec	ify): N/A
Rating Date:	05/05/95 Computer Software Used: STAAD-II	II Versio	on: Rev. 20.0W
Load Testing:	No Cycle Rating Performed: 6 th	Design Loa	nd: <u>Unknown</u>
Structure Info	ormation:		
Plans Available			
Overlay?	Yes Considered in Rating? Yes T	ype/Thickness: Bit	. Conc./Unknown
Section Losses	? No Considered in Rating? N/A	Item 59 Cond.: Poo	or
For LRFR Us	e Only:		
Surface Rough	ness Factor: Condition Factor:	System Factor	r:
ADTT (one dir	rection): Resistance Factor:	FCM	I: Yes/No
Load Rating I	Engineer (LRE):		
Name: <u>Unkn</u>	own Firm: Unknown	In	itial: K.P.L.
Load Rating I	Reviewer (LRR) certification as per the NBIS Title 23 (CFR Section 650.309	(c):
Name: T.J.	•	E. No.: Unknown	. ,
Firm: Unkn			
considering all extent determ performed. I an	is rating is an accurate representation of the subject structured deterioration and/or changes to loading conditions, to sinable by research and visual inspection and test mechanged with the overall responsibility for bridge capacithe above mentioned structure.	the ing Sign are Rating 1	nd Seal if Performed s Cycle
	Sign Date		

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 6	31 (Landing Road	l) over NJ Transit-		10/17/2011 &
Name:	Morristown Lin	ie		Insp. Date:	01/04/2012

LOAD RATING SUMMARY SHEET (LRSS) (cont.)

Rating Comments:

None.

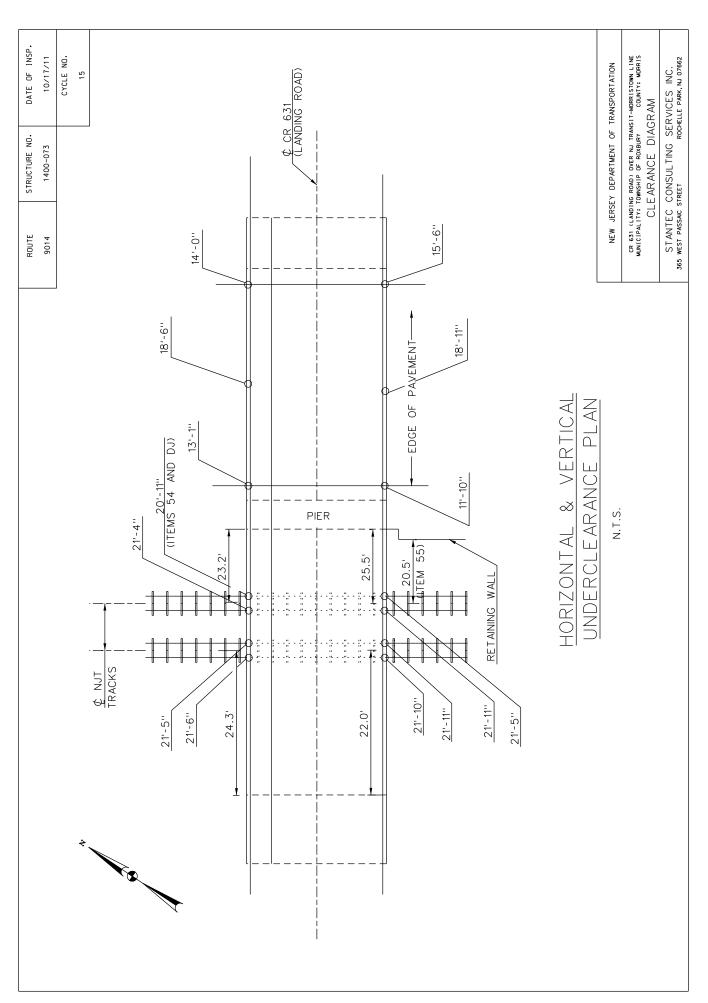
The Load Factor ratings, computed in the 6th Cycle report in accordance with the FHWA directive dated November 1993 and AASHTO Manual for Condition Evaluation of Bridges, 1994, as modified by Division 4 of the New Jersey Department of Transportation Design Manual, Bridges and Structures, are as follows:

	Allowable Stresses (Psi)					
<u>Material</u>	<u>Compressive</u> <u>Strength f'c</u>	Yield	<u>Inventory</u>	Operating		
Concrete	2,500	N/A	800	1,200		
Reinforcing Steel	N/A	33,000	18,000	25,000		

Rating (Tons)

Load Factor

<u>Member</u>	Truck Type (Tons)	<u>Inventory</u>	Operating
	H15 (15T)	46	77
Arch at Crown Cond. Rating = 5	HS20 (36T)	65	109
	3 (25T)	58	98
	3S2(40T)	37	63
	3-3 (40T)	59	99
	Type HL-93		



Structure No.: Cycle No.: 1400-073 Route: 9014

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-01

Location: North elevation, looking south

General view. Note the leaning utility pole on the spandrel wall between the two spans. **Description:**



Photo No: 15-02

Location: South elevation, looking north

Description: General view.

14<u>00-073</u> Cycle No.: Structure No.: Route: 9014

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-03

Location: Roadway, looking east

Description: General view.



Photo No: 15-04

Location: Roadway, looking west

Description: General view.

Cycle No.: Structure No.: 1400-073 Route:

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Morristown Line 01/04/2012 Name: Insp. Date:



Photo No: 15-05

Location: Underside of intrados, west span, looking west

Description: General view. Work Done: Concrete repairs throughout intrados. Note the light to moderate scaling with fine to medium cracks, efflorescence and numerous large concrete patches throughout.



Photo No: 15-06

Location: Underside of intrados, east span, looking west

Description: General view. Work Done: Concrete repairs throughout intrados. Note the moderate scaling,

efflorescence and numerous large concrete patches throughout.

Structure No.: 1400-073 Route: Cycle No.:

County Route 631 (Landing Road) over NJ Transit-

Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-07

South pipe railing, 3rd section from west, west span, looking southwest **Location:**

Description: Moderate rusting with holes in upper and lower rails.



Photo No: 15-08

South pipe railing, 5th section from west, looking northwest **Location:**

Severe impact damage with fractured middle rail and middle post. Note the spall along the cold joint in **Description:**

the southwest approach parapet outside face near the bottom west end.

Cycle No.: Structure No.: 1400-073 Route:

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-09

West skewback, looking northwest **Location:**

Description: Fine vertical and map cracks with efflorescence and spalls/incipient spalls throughout.



Photo No: 15-10

Location: Northeast wingwall, east end, looking southeast

Description: Full height wide vertical crack with spall in the gunite coating. Structure No.: 1400-073 Route: 9014 Cycle No.: 15

County Route 631 (Landing Road) over NJ Transit-

Name: Morristown Line Insp. Date: 01/04/2012

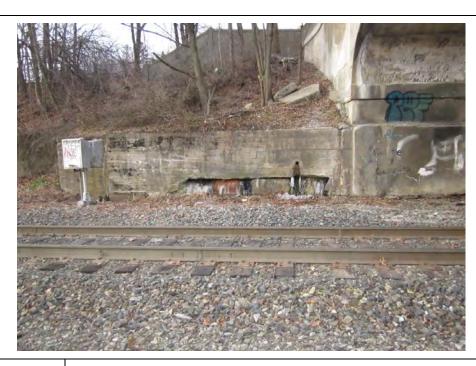


Photo No: 15-11

Location: Southwest retaining wall, looking

Description: Spalls with active leakage at bottom.



Photo No: 15-12

Location: Southeast retaining wall, looking east

Description: Voids and missing pointing throughout.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15

County Route 631 (Landing Road) over NJ Transit-

Name: Morristown Line Insp. Date: 01/04/2012



Photo No: 15-13

Location: West face of pier, looking southeast

Description: Efflorescence and seepage stain at middle below coping.



Photo No: 15-14

Location: Northeast approach roadway, looking west

Description: 20 MPH speed advisory posting.

Structure No.: 1400-073 Route: 9014 Cycle No.:

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-15

Location: Top of roadway, looking southeast

Description: Work Done: New asphalt overlay and new concrete curb and guide rail along south side.



Photo No: 15-16

Location: Roadway, looking northeast

Description: Work Done: New concrete sidewalk and curb and new guide rail installed at north side

Cycle No.: Structure No.: 1400-073 Route:

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Morristown Line 01/04/2012 Name: Insp. Date:



Photo No: 15-17

Location: North spandrel wall, west span, looking southwest

Description: Work Done: Concrete repairs to spandrel wall and coping. Note the fine map cracking with efflorescence and moisture staining.



Photo No: 15-18

Location: South spandrel wall, east span, looking north

Description: Work Done: Concrete repairs to spandrel wall and coping. Note the fine map cracks with efflorescence

at east end and the broken section of conduit along the east face of the pier.

Structure No.: Route: 1400-073 9014 Cycle No.:

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-19

Location: Southwest wingwall and south end of west skewback

Description: Work Done: Patched with concrete.



Photo No: 15-20

Location: North end of west skewback and northwest corner of arch intrados

Description: Work Done: Patched with concrete.

Cycle No.: 1400-073 Structure No.: Route: 9014

County Route 631 (Landing Road) over NJ Transit-

10/17/2011 & Name: Morristown Line 01/04/2012 Insp. Date:



Photo No: 15-21

South face of pier, looking north **Location:**

Description: Work Done: Concrete repairs in coping and wall. Note the medium horizontal and vertical cracks with efflorescence.



Photo No: 15-22

Location: North face of pier, looking southeast

Description: Work Done: Concrete repairs in coping and wall at northwest edge. Note the rust/water stains on

delaminated gunite.

Structure No.:	1400-073	Route:	9014		Cycle No.:	15	
Name:	County Route 631 (Landing Road) over NJ Transit- Morristown Line				Insp. Date:	10/17/20 01/04/2	
	NEW JER	FI	TMENT OF T RAL EVALU ELD NOTES RRIS COUNTY	ATION	ATION		
Inspectors:	M. Ramirez, P.E.	Name:	County Route	e 631 (Landing Ro	oad) over NJ	Transit-Morris	stown
	A. La Bianca		Line				
Crew Chief:	Thane Syverson, P.E.	Weather:		Cloudy / Cle	ar		
Temperature:	55°F / 10°F	Special Eq	uipment Used:	None			
RATINGS:							
 7 Good Cor 6 Satisfacto 5 Fair Con 4 Poor Cor 3 Serious C 2 Critical C 1 Imminent 	od Condition – no probabilition – some minor pory Condition – some minor section dition – minor section dition – advanced section dition – seriously decondition – facility shots Failure Condition – facility is closed to Condition – facility is	oroblems. In inor deterioration I loss to primary I ion loss to primary I eteriorated primary I losed until I colosed Studies of the second of the sec	y structural elements structural elements structural elements are made and you of repairs is repair.	nents. nents. nents. de. feasible.		COORDINAT Southwest corner 54' 17.00" 39' 57.42"	
Year Built: 19	907		Vear of Wi	idening / Major R	enairs: 19		
No. of Lanes:			•			12	
Vertical Clearan	ces: O	ver Deck: <u>Unli</u>	imited				
Minimum U	Jnder: 20	0.91' from the top	of east rail of th	e east track unde	r north fascia	ì.	
Maximum U	Under (Item 10): N	//A					
Horizontal Unde	erclearance:	Total Horizo	ontal Clearance:	N/A			
Right 20.	.50' from the centerline	e of the east track	to the pier retain	ning wall at the so	outh fascia.		

Overall Physical Condition of Structure: Fair due to the superstructure and substructure.

Left

N/A

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (La	nding Road) over NJ Transit-		10/17/2011 &
Name:	Morristown Line			Insp. Date:	01/04/2012

DECK SI&A Item 58 Condition Rating: ______**7**

SPAN # West

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	No significant defects. Work Done: New deck waterproofing (see attached email and photos from Morris County) with new asphalt overlay on top (Photo 15-15).
N	Underside of Deck	Not visible.
N	Median	N/A
8	Curbs Concrete	Integral with sidewalk at north side. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
8	Sidewalks/ Safetywalk Concrete @ North only	No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
7	Parapets/ Balustrades	No significant defects. Work Done: Concrete repairs in north parapet/headwall (Photo 15-17).
7	Railings W-beam guide rail and pipe railing with concrete pylons	The original deteriorated pipe rails on both sides are now protected by newly installed bridge mounted w-beam guide rail (Photos 15-07, 15-15 and 15-16). Work Done: New w-beam guide rail installed along both sides (Photos 15-15 and 15-16).
N	Deck Joints / Filler Material	N/A
N	Drains and Scuppers	N/A
N	Light Stands	N/A
N	Utilities	None. Overhead wires along north side.
N	Others	N/A

1400-073	Route:	9014	Cycle No.:	15
County Route 63	1 (Landing Road) over NJ Transit-		10/17/2011 &
Morristown Line	<u>-</u>		Insp. Date:	01/04/2012
	County Route 63		County Route 631 (Landing Road) over NJ Transit-	County Route 631 (Landing Road) over NJ Transit-

DECKSI&A Item 58 Condition Rating: 7

SPAN # East

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	No significant defects. Work Done: New deck waterproofing (see attached email and photos from Morris County) with new asphalt overlay on top (Photo 15-15).
N	Underside of Deck	Not visible.
N	Median	N/A
8	Curbs Concrete	Integral with sidewalk at north side. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
8	Sidewalks/ Safetywalk Concrete @ North only	No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
7	Parapets/ Balustrades	No significant defects.
7	Railings W-beam guide rail and pipe railing with concrete pylons	The original deteriorated pipe rails on both sides are now protected by newly installed bridge mounted w-beam guide rail (Photos 15-08, 15-15 and 15-16). Work Done: New w-beam guide rail installed along both sides (Photos 15-15 and 15-16).
N	Deck Joints / Filler Material	N/A
N	Drains and Scuppers	N/A
N	Light Stands	N/A
N	Utilities	None. Overhead wires along north side.
N	Others	N/A

APPROA	CHES			SI&A Item BA Rati		7
Name:	Morristown Lin	e		Insp. Da	te: 01/	04/2012
	County Route 6	31 (Landing Road)	over NJ Trans	it-	10/1	7/2011 &
Structure No.:	1400-073	Route:	9014	Cycle No	o.:	15

SI&A Item 72 Rating: 5

APPROACH West

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement Bituminous Concrete	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
7	Approach Shoulder	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from the bridge. Horizontal: Sharp (90°) turn south.
N	Guide Rail Condition	N/A
8	Sidewalks Concrete @ North only	Continuous. No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
8	Curbs Concrete	Integral with sidewalk at north side. Continuous. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
7	Utilities	Overhead wires crossing over roadway.
7	Approach Roadway Embankment	No significant defects.
7	Others/ Parapet	No significant defects.

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 63	1 (Landing Road) over NJ Transit-		10/17/2011 &
Name:	Morristown Line			Insp. Date:	01/04/2012

API	PRO	ACF	IES

SI&A Item BA Rating:	7

SI&A Item 72 Rating: 5

APPROACH East

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement Bituminous Concrete	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
7	Approach Shoulder	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from bridge. Horizontal: T-intersection 100' to east. Speed reduction to 20 MPH.
7	Guide Rail Condition W-beam @ NE only	No significant defects.
8	Sidewalks Concrete @ North only	Continuous. No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
8	Curbs Concrete	Integral with sidewalk at north side. Continuous. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
7	Utilities	Overhead wires along north side.
7	Approach Roadway Embankment	No significant defects.
6	Others/ Parapet	Small spall along cold joint in southwest approach parapet outside face near bottom west end (Photo 15-08).

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 6	31 (Landing Road) over NJ Transit-		10/17/2011 &
Name:	Morristown Lin	e		Insp. Date:	01/04/2012
SUPERST (ARCH)	RUCTURI	<u>C</u>	SI&A Item 5	9 Condition Rating: _	5

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Light to moderate scaling with fine to medium random cracks with efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 15-05). Work Done: Concrete repairs throughout intrados (Photo 15-05).
N	Spandrel Columns/ Extrados	N/A
5	Spandrel Walls	North: Fine map cracks with efflorescence and moisture staining (Photo 15-17). South: Scattered fine map cracks and random cracks. Work Done: Spandrel wall and coping has been patched with concrete at both openings (Photo 15-17).
N	Others	N/A

Additional Remarks:

SPAN#

West

Structure No.:	1400-073	Route:	9014	Cycle No.: _	15
	County Route 63	1 (Landing Road	l) over NJ Transit-		10/17/2011 &
Name:	Morristown Line	•		Insp. Date:	01/04/2012
SUPERST (ARCH)	RUCTURE		SI&A Item 5	9 Condition Rating: _	5

RATING	COMPONENT	REMARKS
	Intrados of Arch (Soffit) Arch Ribs	Moderate scaling and efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 15-06).
		Work Done: Concrete repairs throughout intrados (Photo 15-06).
5		
	Spandrel Columns/	N/A
N	Extrados	
N		
	Spandrel Walls	Fine map cracks with efflorescence throughout north wall and east end of south wall (Photo 15-18).
		North: Wide vertical crack at the east end from the top of the pylon, 15 LF.
5		Work Done: Spandrel wall and coping has been patched with concrete at both openings (Photo 15-18).
	Others	Utility pole is leaning onto north spandrel wall between the two spans (Photo 15-01).
5		

Additional Remarks:

SPAN#

East

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (La	nding Road	d) over NJ Transit-		10/17/2011 &
Name:	Morristown Line	-		Insp. Date:	01/04/2012

<u>SUBSTRUCTURE</u>
(Arch/Frame)

SI&A Item 60 Condition Rating: _____5

ABUTMENT West

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Bottom Portion-Crashwall: Fine vertical and map cracks with efflorescence and spalls/incipient spalls along the full length of the horizontal face (shotcrete) (Photo 15-09). Work Done: Concrete repair at coping and vertical portion below the arch at the north end (Photo 15-20).
5	Wingwalls/ Retaining Walls	South: Fine map cracks and large concrete patch at east end. North: 50 LF of medium cracks with efflorescence. Work Done: Concrete repair at south wingwall, 120 SF (Photo 15-19).
4	Others Retaining Walls	Spalls, 40 SF, with active leakage along bottom at southwest wall (Photo 15-11).

Additional Remarks:

ABUTMENT East

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Area of thick and heavy efflorescence and a few fine vertical cracks at few locations. Hollow (5%) areas of gunite throughout.
5	Wingwalls/ Retaining Walls	North: Full height wide vertical crack with a spall (2 SF) in the gunite coating (Photo 15-10). Medium horizontal cracks (30 LF) with efflorescence. South: Scattered fine map cracks with efflorescence.
5	Others Retaining Walls for Parking Area	North: Several loose stones protruding up to 6"; missing and loose mortar (80 LF). South: Half covered with gunite; scattered areas of discoloration; remaining half has missing pointing, 150 SF (Photo 15-12).

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 63	1 (Landing Road) over NJ Transit-		10/17/2011 &
Name:	Morristown Line	;		Insp. Date:	01/04/2012

SUBSTRUCTURE

SI&A Item 60 Condition Rating: 5

PIER Center

RATING	COMPONENT	REMARKS
5	Columns/ Stem Crashwall Gunite cover	West Face: Spalled/delaminated bottom along exposed footing (20 SF) full width. Heavy efflorescence and water seepage stain at middle below coping (Photo 15-13). Hollow (30%) areas throughout gunite cover. East Face: Heavy efflorescence and stains on delaminated gunite cover at north end (2' x 8' high). Incipient spalled coping top at several locations (5 SF). Hollow (30%) areas throughout gunite cover. North Face: Heavy rust/water stains on delaminated gunite, 20 SF (Photo 15-22). South Face: Medium horizontal and vertical cracks with efflorescence (Photo 15-21). Work Done: Concrete repairs in coping and wall at northwest edge (Photo 15-22) and at south face (Photo 15-21).
N	Pier Cap	N/A
N	Bridge Seat	N/A
5	Others/Fender	Broken section of conduit hanging from cable at south end of pier along east face (Photo 15-18).

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631	(Landing Road	d) over NJ Transit-		10/17/2011 &
Name:	Morristown Line			Insp. Date:	01/04/2012

HIGHWAY SAFETY

Coding of SI&A Item 36: 0000
1: Good

0: Not Good N: Not Applicable

RAT	ΓING	COMPONENT	REMARKS
0		Bridge Railing	W-beam guide rail mounted on top of short concrete parapet/headwall (2" to 8" high from grade). There is a pipe railing behind guide rail (3'-4" high). Chain link fence between guide rail and steel railing.
	0	Transition to Bridge Railing	Northeast corner: Leading end within clear zone. Substandard single rail w-beam, posting 6'-3" spacing, no spacer blocks. No guide rails on southeast, southwest and northwest due to urban built-up areas.
0	1	Curb / Sidewalk Terminations	Continuous sidewalks and curbs. No sidewalk at southeast and southwest.
0		Approach Guide Rails	Northeast only: Very short and no spacer blocks. 6'-3" post spacing. No guide rail at other three corners.
0		Approach Guide Rail End Terminals	Northeast only: Buffer end treatment only.

DECK GEOMETRY

SI&A Item 68 Rating: 2

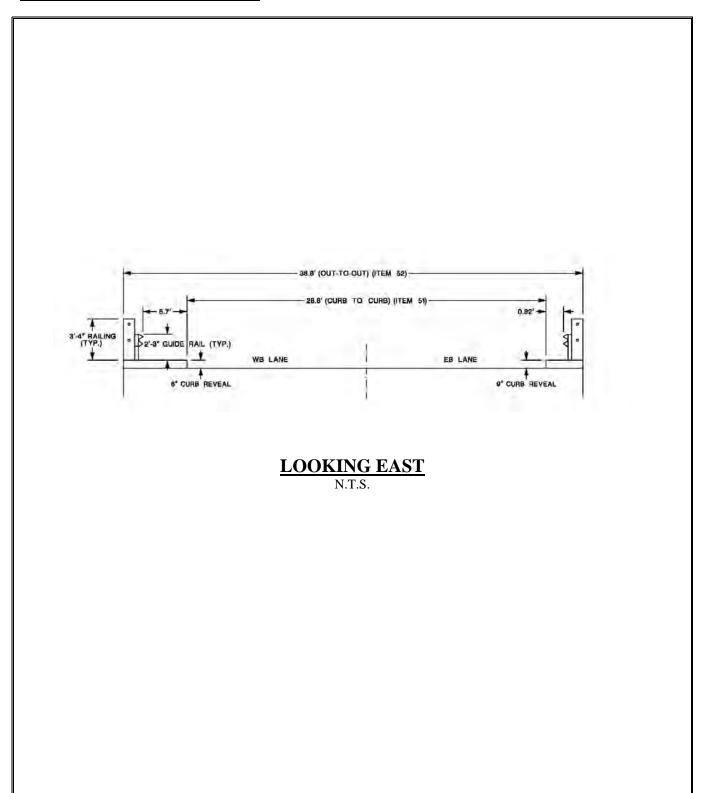
COMPONENT	REMARKS
Bridge Cross Section	Approaches are wider than bridge cross section. See sketch on following page.
Adequacy of Lane / Shoulder Widths	Two-way traffic, 2 lanes, 1' wide shoulders (typical). ADT = 22,606 (2011 projection from Traffic Count Summary by Morris County). Curb to curb width = 28.8', Table 2A.
Vertical Clearance over Deck	Unlimited.

*Posting for Load /	20 MPH speed posting (Photo 15-14).
Speed / Clearance	
Restrictions	

 Structure No.:
 1400-073
 Route:
 9014
 Cycle No.:
 15

 County Route 631 (Landing Road) over NJ Transit-Name:
 Morristown Line
 Insp. Date:
 01/04/2012

DECK CROSS SECTION



Structure No.:	1400-073	Route: 9014	Cycle No.:	15	
	County Route 63	1 (Landing Road) over NJ Transit-		10/17/20	11 &
Name:	Morristown Line		Insp. Date:	01/04/2	2012
			<u>-</u>		
CIEADAR	MOTO				
<u>CLEARA</u>	NCES				
FEATURE ON	STRUCTURE:	County Route 631 (Landing Road)	SI&A SHEE	.Т	1
Minimum Verti	cal	Unlimited.			
Clearance (SI&	A Item 10)				
Total Horizonta		28.80' curb to curb.			
Clearances (SI&		20.00 tare to tare.			
Cicarances (Bio	C/1 (CIII +/)				
CONTROLLIN	G UNDERCLEA	RANCE DATA:			
Minimum Vertic	cal	20.91' from the top of east rail of the east tr	ack at the north f	ascia.	
Underclearance	(SI&A Item 54)	,			
Minimum Vertic	·	N/A			
	(incl. shoulders)				
(SI&A Item DJ)	'				
Lateral Right	'	20.50' from the centerline of the east track t	to the nier retaini	ng wall at	the
(SI&A Item 55)		south fascia.	to the pier retaining	ng wan at	tiic
Lateral Left		N/A			
		N/A			
(SI&A Item 56)					
		D 37/4	GIO A GIVEE		
FEATURE UNI	DER STRUCTUR	E: N/A	SI&A SHEE	T = 20	or A
.					
*Minimum Vert	tical	N/A			
Clearance (SI&	A Item 10)				
Total Horizonta	l Clearance	N/A			
(SI&A Item 47)					
Minimum Verti		N/A			
	(incl. shoulders)				
(SI&A Item DJ)	·				
(SIGH ICHI DJ)	•				
EEATHDE LIMI	TED CTDIICTID	E. N/A	SI&A SHEE	'T	В
FEATURE UNI	DER STRUCTUR	E: <u>N/A</u>	SI&A SHEE	.1	D
*Minimum Vert		N/A			
Clearance (SI&	A Item 10)				
Total Horizonta	l Clearance	N/A			·
(SI&A Item 47)					
Minimum Verti		N/A			
	(incl. shoulders)				
(SI&A Item DJ)	`				
(Sich ittill DJ)	•	1			

* Minimum clearance for a 10 foot width of the pavement or traveled part of the roadway where the clearance is greatest shall be coded in feet and inches.

Structure No.:	1400-073 Route: 9014	Cycle No.: _	15
	County Route 631 (Landing Road) over NJ Transit-		10/17/2011 &
Name:	Morristown Line	Insp. Date:	01/04/2012

FENCINGCoding of SI&A Item FN:YCoding of SI&A Item FO:0Coding of SI&A Item FP (in thousands):20

Warranted (Per Design Manual Section 23): Yes

If Yes: (#1) Description: Highway carrying, grade separation or high level bridges with facility for pedestrian traffic.

 Current Status of Fence & Sidewalk:
 Left Side
 Right Side

 a. Fence:
 No
 No

 b. Sidewalk Width:
 5.67 FT
 0.00 FT

 c. Total Height of fence above curb/sidewalk:
 N/A
 N/A

 d. Type of Fence (per Design Manual Section 23):
 N/A
 N/A

Action Recommended: Install a curved top, chain link fence at the north fascia.

Estimated Cost: \$19,176

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (La	nding Road	d) over NJ Transit-		10/17/2011 &
Name:	Morristown Line	-		Insp. Date:	01/04/2012

WORK DONE HISTORICAL DATA

CYCLE NO.	YEAR	WORK DONE SUMMARY
15	2011	New deck waterproofing with new asphalt overlay on top that extends over both approaches. New concrete sidewalk along north side. New concrete curbs and guide rails along both sides. Concrete repairs throughout both arch intrados. Concrete repairs in the north parapet/headwall along the west span. Concrete repairs in the spandrel wall and coping at both spandrel walls of both spans. Concrete repairs at the south wingwall and in the coping and vertical portion below the arch at north end of the west skewback. Concrete repairs in coping and wall at the northwest edge and the south face of the pier stem.
14	2009	Concrete repair to the southwest abutment wingwall.
		Concrete repair patches to both intrados.
13	2007	None.

La Bianca, Anthony

From: Denho, Abdel

Sent: Tuesday, January 24, 2012 2:13 PM To: La Bianca, Anthony; Ramirez, Mark

Cc: Lambrinos, Laura

Subject: FW: MC Bridge N0. 1400-073 Repair Photos

Attachments: Deck Waterproofing 011.jpg; Deck Waterproofing 008.jpg; Deck Waterproofing 009.jpg; Deck

Waterproofing 010.jpg

From: Caruso, Brian [mailto:BCaruso@co.morris.nj.us]

Sent: Tuesday, January 24, 2012 1:51 PM

To: Denho, Abdel

Subject: MC Bridge NO. 1400-073 Repair Photos

Attached are some photos of the condition of the top of the arch and the sprayed-on / painted membrane sealant applied before it was overlaid with asphalt. I am mailing you the plans and supplemental specs. I spoke to Roslyn and we agreed that the County does not have a problem with the rating going up to a 5. The Feds are still looking to fund the replacement project; so, it shouldn't be an issue.

Brian M. Caruso, P.E. Senior Engineer Morris County Engineering bcaruso@co.morris.nj.us Phone# 973-285-6986 Fax# 973-539-3141

Photos received from Morris County



Photo No. 1



Photo No. 2



Photo No. 3



Photo No. 4



Morris County DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION COURT STREET PO Box 900 MORRISTOWN, NEW JERSEY 07963-0900

BRIDGE RE-EVALUATION SURVEY REPORT

STRUCTURE NO. 1400-073 COUNTY ROUTE 631 (LANDING ROAD) OVER NJ TRANSIT-MORRISTOWN LINE TOWNSHIP OF ROXBURY MORRIS COUNTY

16TH CYCLE

SEPTEMBER 13, 2013

NOTE: This Bridge Re-evaluation Report shall be filed immediately after the 15TH Cycle Inspection Report.

Prepared By



TABLE OF CONTENTS

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2	Conclusions and Recommendations	16-2
3	Structural Inventory & Appraisal and Pontis Sheets	16-3
4	Load Rating Summary Sheet (LRSS)	16-6
5	Drawings, Soundings and Photographs	16-8
6	Field Notes	16-16

N.J.D.O.T. - STRUCTURAL EVALUATION AND BRIDGE MANAGEMENT RE-EVALUATION BRIDGE SURVEY REPORT

CYCLE NO. 16

STRUCTURAL DATA:

Bridge No.:	1400-073	3	Year	Built:	1907	Widened/Rehab:	1972
Route No.:	9014		Lengt	h:	136.0'	Width:	38.8'
Mile Point:	1.31		Date of	of this Evalu	ation:	09/13/2013 & 11	/08/2013
Name:	County R		By:	Stantec Con	nsulting Serv	ices Inc.	
		Road) over NJ Jorristown Line	Date of	of Previous I	Evaluation:	10/17/2011 & 01	/04/2012
			By:	Stantec Con	nsulting Serv	ices Inc.	
Special Equipment Used: None Structure Type: Two Span, Closed Spandrel, Reinforced Concrete Arch							
WORK DONE: OVERALL PHYS	None.	<u>ONDITION:</u> Fair o	due to t	he superstru	cture and sub	structure.	
OVERALL CON	DITION ((ITEM 67): Fair of	due to t	he superstru	cture and sub	structure.	
Inspection Team		Mark V. Ramirez, P. Abdel A. Denho, P.F.			Initials	s: MVR	
N.J. P.E. Number: GE 39572							
I certify that this report is an accurate description of subject structure, to the extent determinable by vis inspection and testing performed. Signature:					_	_	l Signed Sealed
Date:							

Structure No.:	1400-073	Route:	9014	Сус	cle No.:	15	
	County Route 631 (Landing Road	l) over			09/13/2013 &	
Name:	NJ Transit-Morristo	wn Line		Inst	p. Date:	11/08/2013	

CONCLUSIONS AND RECOMMENDATIONS:

The overall condition of the structure is fair due to the superstructure and substructure.

The superstructure is in fair condition due to the numerous patching covering 60% of both arch intrados and the cracking, scaling, leakage and efflorescence throughout both arch intrados and spandrel walls.

The substructure is in fair condition due to the large concrete patches and the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments.

Since the previous inspection, there has been no change in the overall condition of the structure.

The structure is classified as functionally obsolete due to the inadequate deck geometry (Item 68 = 2). Therefore, we recommend the following remedial action:

1. Increase the roadway width from 28.8' to 32.0' by widening the bridge 3.2'.

a.	Demolition of existing south bridge railing:	\$25,000
	(Lump Sum)	
		A400 = 40

b. Bridge Widening: \$130,560 3.2' x 136' = 435.2 SF @ \$300/SF

c. Associated approach work: \$50,000 (Lump Sum)

Subtotal	\$205,560
Preliminary Engineering (15%)	\$30,834
Traffic Maintenance (10%)	<u>\$20,556</u>
Total	\$256,950
SAY	\$260,000

In the interim until the bridge is widened, we recommend that the following Emergency/Priority repairs be made to retard further deterioration, preserve the structural integrity of the bridge, improve safety and extend its useful life:

None.

StructNum: 1400073

NJDOT SI and A Sheet

Name: CO RT631(LANDING RD) / NJT-MORRISTOWN LINE S.R.: 58.3 SD/FO- 2 -Functionally Obsolete

IDENTIFICATION					
1 State:	34 New Jersey	8 Struc Num:	1400073		
7 Facility Carried:	CR631 (Landing Rd)	9 Location: At Ki	ngs Highway Junction		
5A Rte.(On/Under):	Route On Structure	5B Rte. Signing Prefix	: 4 -County Hwy		
5C Level of Service:	1 -Mainline	5D Rte. Number:	00631		
5E Directional Suffix:	0- Not Applicable	% Responsibility :	NA		
2 SHD District:	01- North	3 County Code:	Morris		
4 Place Code:	Roxbury (Township of), M	11 Mile Post:	1.310 mi		
6 Feature Intersected :	NJT-Morristown Line				
16 Latitude:	40d 54' 17.00"	17 Longitude:	074d 39' 57.42"		
98 Border Bridge Code	-2 Not Applicable	(P)			

STRUCTURE :	TYPE AND MATERIALS
46 Number of Approach Spans: 0	45 Number of Spans Main Unit: 2
43A/B Main Span Material/Design:	

1 -Concrete 11 -Arch-Deck

99 Border Bridge Number: NA

 107 Deck Type:
 1- Conc..-Cast-in-Place

 108A Wearing Surface:
 6-Bituminous

 108B Membrane:
 3- Epoxy

 108C Deck Protection:
 8- Unknown

 AGE AND SERVICE

 27 Year Built:
 1907
 106 Year Reconstructed: 1972

 42A Type of Service On:
 5 -Highway-pedestrian

 42B Type of Service Under:
 2 -Railroad

 28A Lanes on:
 2
 28B Lanes Under:
 0
 19 Detour Length:
 2.0 mi

 29 ADT:
 23,061
 109 Truck ADT:
 4 %
 30 Year of ADT:
 2013

GEOMETRIC DATA 48 Length Max Span: 64.0 ft 49 Structure Length: 136.0 ft 50A Curb/Sdwlk Wdth L: 0.0 ft 50B Curb/Sidewalk Width R: 5.7 ft Width Curb to Curb 51: 28.8 ft 52 Width Out to Out: 38.8 ft 32 Approach Roadway Width: 28 ft (w/ shoulders) 33 Median: 0 No median Deck Area: 5,277. sq. ft 34 Skew: 0.00 ° 35 Structure Flared: 0 -No flare 53 Minimum Vertical Clearance Over Bridge: 9999 54A Minimum Vertical Underclearance Reference: R -Railroad beneath str. 54B Minimum Vertical Underclearance: 20.92 ft 55A Minimum Lateral Underclearance Reference R: R -Railroad beneath str.

55B Minimum Lateral Undrclearance R: 20.50 ft
56 Minimum Lateral Undrclearance L: 0.00 ft

INSPECTION					
91 Frequency:	24 months	90 Inspection Date:	9/13/2013	Next Inspection:	09/13/2015
92A FC Frequency:	NA	93A FC Inspection Date:	NA	Next FC Inspection:	NA
92B UW Frequency:	NA	93B UW Inspection Date:	NA	Next UW Inspection:	NA
92C SI Frequency:	NA	93C SI Date:	NA	Next SI:	NA
Element Frequency:	24 months	Element Inspection Date:	09/13/2013	Next Elem. Insp. Due	: 09/13/2015

CLASSIFICATION					
100 STRAHNET Highway: 0 -Not a STRAHNET hwy 101 Parallel Structure: N -No bridge exists					
102 Direction of Traffic:	2 -2-way traffic	103 Temporary Structure:	-1		
104 Highway System:	0 -Not on NHS	112 NBIS Length:	Y - Long Enough		
20 Toll Facility:	3 -On free road	26 Functional Class:	16- Urban Minor Arterial		
37 Historical	Significance: 2 -Eligible	e for NRHP			
22 Owner:	02 County	Hwy Agency			
21 Custodian	: 02 County	Hwy Agency			

•			CC	NDITION		
	58 Deck:	7 -Good	59 Super:	5 -Fair	60 Sub:	5 -Fair
	62 Culvert:	N -Not applicable	61 Cha	annel/Channel Protection:	N -No	t applicable

	LOAD RATING	AND POSTING	
65 Inventory Rating Metho	od: 1- LF Load Factor	63 Operating Rating Method:	1 -LF Load Factor
66 Inventory Rating:	HS65.0	64 Operating Rating:	HS99.9
31 Design Load:	0 -Unknown	70 Posting:	5 At/Above Legal Loads
41 Posting status:	A -Open, no restriction		

\bigcap	APPRAISAL					
	36A Bridge Rail:	0 -Substandard	36C Approach Rail:	0 -Substandard		
	36B Transition:	0 -Substandard	36D Approach Rail Ends:	0 -Substandard		
	67 Str. Evaluation:	5	68 Deck Geometry:	2 Intolerable - Replace		
	69 Underclearance, Vertica	al and Horizontal: 4 -T	olerable			
	71 Waterway Adequacy:	N -Not applicable	72 Approach Alignment:	5 -Above Tolerable		
	113 Scour Critical:	N -Not Over Waterway				
ı						

PROPOSED IMPROVEMENTS								
94 Bridge Cost:	\$130,560	75 Type of Work:	33					
95 Roadway Cost:	\$50,000	76 Length of Improvment:	136 ft					
96 Total Cost:	\$260,000	114 Future ADT:	31,060					
97 Year of Cost Estimate:	2013	115 Year of Future ADT:	2033					

NAVIGATION DATA								
38 Navigation Control:	N N -N/A-No waterv	vay						
39 Vertical Clearance:	0.0 ft	40 Horizontal Clearance:	0.0 ft					
111 Pier Protection:	-1	116 Lift Bridge Vertical Clearance:						

ELEMENT CONDITION STATE DATA

Str Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
0	14/3	P Conc Deck/AC Ovly	(SF)	5,277	100 %	5,277	0 %	0	0 %	0	0 %	0	0 %	0
0	144/3	R/Conc Arch	(LF)	136	0 %	0	0 %	0	100 %	136	0 %	0	0 %	0
0	210/3	R/Conc Pier Wall	(LF)	39	0 %	0	100 %	39	0 %	0	0 %	0	0 %	0
0	215/3	R/Conc Abutment	(LF)	78	0 %	0	100 %	78	0 %	0	0 %	0	0 %	0
0	334/3	Metal Rail Coated	(LF)	272	100 %	272	0 %	0	0 %	0	0 %	0	0 %	0
0	503/3	Curbs/Walks - Conc	(LF)	136	100 %	136	0 %	0	0 %	0	0 %	0	0 %	0

 ${\bf NJDOT\ Inspection_SIA_English_No}$

Wed 12/11/2013 10:16:16

(v1.1)

StructNum: 1400073

NJDOT SI and A Sheet

0

2

45.36

RAILROAD

(BC) USRA Code:

(BE) Rail Milepost:

(BD 1) Rail On/Under:

(BD2)RRs intersected:

St	r Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
	0	506/3	Wingwalls - Abut	(LF)	32	50 %	16	50 %	16	0 %	0	0 %	0	0 %	0
	0	507/3	Headwalls - Other	(LF)	272	50 %	136	50 %	136	0 %	0	0 %	0	0 %	0

NJDOT BRIDGE FIELDS

NJDOT BRIDGE Brkey: 1400073			(V1.2)
	NERAL	COUNTER MEASURES	TEMPORARY STRUCTURES
(A) Town:	Roxbury Twp.	(FI) Recommended:	(GV) Bridge: N
(AA) Route:	9014	-1	(GW) Shoring: N
(AD) Admin:	-1		(GX) Reports: N
(AE) Alt Agency:	-1 _	JI	(GY) Measures: N
\succeq		(FJ) Cost: 0	(GZ) Cond Desc:
STRUCTURE AN	ID APPURTENANCES	(FK) Installed: 1 _ 2 _ 3 _	-1
(AC) Non-Inv Feature:	RR Rdwy and/or RR ovr RR	(FL) Monitoring Regd:	Ш
(AF) Alt Struct #:	-1		(
(AG) Rail Type:	30 Concrete Metal Combo	PROJECT PROGRAMMING	
(AH) Rail Height::	2.25	(HB) Bridge List ID: 14C1	LOAD RATINGS
(AJ) Slope Protection:	-1	(HC) Consultant A47	Type Inv Opr
(AK) Abutment:	03 Full Height	(HD) 2-Cy Insp Contr: Y	H (BQ) 46 (CA) 77
(AL) Pier:	02 Solid Wall Pier		HS (BR) 65 (CB) 109
(AM) Depth of Fill:	0.00	(HE) Proceed Date: 1 5/8/2003	3 (BS) 58 (CC) 98
(AP) Fender System:	-1	II 6/6/2005	3S2 (BT) 37 (CD) 63
(AT) Special Material 1:	=	(HF) State Proj Mgr: MAK	3-3 (BU) 59 (CE) 99
(AT) Special Material 2:	_		M (BV) (CF)
(AU) Add Struct: 1	=	BRIDGE NOISE BARRIER (HA	Misc (CH1) L
2	=	Material 1:	(CH2) 0
(AV) Widen Stru: Ma	aterial Design	2:	
1st Widen _	-1	Height 1:	MISCELLANEOUS
2nd Widen _	-1	2:	(CQ) Bridge List:
FUNDIN	C AND COCT		(BK) Overstress %: 0
FUNDIN	G AND COST	PAINTING	(FV) Route Milepost:
(BL) Discretionary:	=	(GB) Environment: -1	(FM) Incident Reprtd:
(BM) Fed Job #:	STPNBIS760	(GR) Last Painting: 01/1901	
(BN) State Job #:	2205569	(GA) Painting Reg: N	(AO) Utilities: 1: _
(BO) St Maint Cost:	0		2: _
(BP) Brdg Demo	N	∬ FENCING	3: _
SCOLIB	EVALUATION	(AQ) Chain Link:	4: _
	LVALUATION	(FN) Warranted: Y	(CR) Off-Route Bridge: N
(FA) FHWA Category:	-1		(BB) Orphan Bridge: N
(FB) Stage 1 Date:	1/1/1901	SIGN STRUCTURES	(AN) Plan Available: N
(FC) Stage 1 Consultant:	-1	(GS) Overhead: 0	(FX) Fed Error Uncorr N
(FD) Prioritization:	=.	(GT) Cantilever:	(FW) Estimated ADT N

(GT) Cantilever:

(CG) Load Type:

(CG) Tons

(AI) Speed:

(GU) Fascia Mounted:

POSTINGS

0

20

(FE) Sufficiency:

(FF)Date Stage II

(FH) Crit. Element:

(FG) Stage II Consultant: -1

0

1/1/1901

StructNum: 1400073

NJDOT SI and A Sheet

NJDOT INSPECTION FIELDS

(V1.2)

GEN	IERAL) MISCELLA	ANEOUS	REM	REMARKS		PAINTING	
(CI) Cycle Number: (CJ) Insp Type: (CK) Inspection Crew:	16 S M	(B) Deletion Code: (BA) Apr Rdwy Cond:	N 7		1: _ 2: _ 3: _		(GD) Fascia Beam: (GE) Fascia B. Flange: (GF) Interior Beam:	-1 -1 -1
(CM) Consultant: (CO) Prev Consultant (CP) Federal Report:	V08 : V08 -	FATIGUE DI	ETAIL (AZ) -1 -1		4: _ 5: _ 1: 9		(GH) Interior B. Flange: (GI) Beam Ends: (GJ) Connections:	-1 -1 -1
INSPECT (AW) Mech/Electrical: (AX) Deck: (AY) Special Testing:	1/1/1901 1/1/1901 1/1/1901	Location 2:	-1 -1 -1 -1	(BH) Sub:	2: Z 3: _ 1: A 2: C		(GK) Bracings: (GL) Bearings: (GM) Substructure: (GN) Above Deck Super: (GO) Railings/Fence:	-1 -1 -1 -1 -1
(GC) Paint: (AR) Equipment:	1/1/1901 1: _ 2: _	IN-DEPTH PI (FR) Consultant: (FS)	N-HANGER	(BI) Channel (BJ) Culvert	3: B - -		(GP) Remarks 1: -1	
(AS) Testing:	3: _ 1: _ 2: _	-1	N	FEN (FO) Pedestrian Traffic: (FP) Improvement Cost		Thousands	(GQ) Remarks 2:	
	3: _	(FT) Combo: (FQ) FCM/Pin-Hngr Insp Date:	1/1/1901	(FF) improvement Cost	. \$20	modsands		

ROADWAY DATA

Bridge Id: 1400073 SRI 14000631__ (V1.2)

ROADWAY IDENTIFICATION								
NBI Roadway?:	1							
Roadway Name:	CR 631 (L	ANDING ROAD)					
Item 5A:	1							
Item 5B:	4							
Item 5C:	1							
Item 5D:	00631	Item 5E	0					

HWY NETWORKS AND SERVICE CLASSIFICATION

 11 Milepost:
 1.310 mi

 12 Base Hwy Network:
 0

 20 Toll Facility:
 3

 13A LRS Inventory Route
 Subroute No.:

 26 Functional Class:
 16
 102 TraffDirection:
 2

ALTERNATE	CL	ASSIF	ICAT	ION

 100 Strahnet Hwy:
 0
 Transit Rt:
 0

 104 NHS System:
 0
 Emergency
 0

 105 Fed. Lands Hwy:
 0

 110 Truck Hwy Net:
 0

 School Bus:
 0

TRAFFIC AND ACCIDENTS

28 Lanes : Num Median: Road Speed: 35 mph ADTclass: 04 29 ADTtotal: 23061 30 Year of ADT: 2013 114 Future ADT: 31060 115 Year of Future ADT: 2033 109 Truck ADTT %: 19 Bypass Length: 2.00 mi 30 mph Detour Speed:

CLEARANCES

 10 Vertical Clearance:
 99.99 ft

 47 Invent Route Horiz Clear:
 28.80 ft

 (DJ) Min Vert Undrcir:
 00.00 ft

WIDTHS

32 Appr Rdwy Width: 28 ft 51 Brdg Rdwy With Curb-Curb: 28.8 ft

Structure No.:	1400-073		9014		-	15
Name:	•	e 631 (Landing Road) o Iorristown Line	ver		Insp. Date:	09/13/2013 &
rame.	113 Transic-ivi	OHISTOWII LINE			msp. Date.	11/00/2013
		LOAD RATING SUI			<u>S)</u>	
		(Form NJ-BI-10	01 Created 1/25/201	.1)		
Project Inform						
Group:	Agreemer	nt No.:	_ Contract ID):	- Agree/N	Iod No.:
Rating Informa	ation:					
Method: LI	RFR: No	LFR: Yes	ASR:	No	Other (Specify): <u>N/A</u>
Rating Date:	05/05/95	Computer Softw	are Used: S7	ΓAAD-III	Version:	Rev. 20.0W
Load Testing:	No	Design Load:	Unknown			
Structure Info	rmation:					
Plans Available	? <u>No</u>	Contract Designation	on: <u>Unknown</u>	1		
Overlay?	Yes	Considered in Ratin	g? Yes	Type/Tł	nickness: Bit. C	Conc./Unknown
Section Losses?		Considered in Ratin				
bection Losses.	110		g. <u>1771</u>	_ 101113	y cond Tun	
For LRFR Use	Only:					
Surface Roughn	ess Factor:	Conditio	n Factor:		System Factor:	
ADTT (one dire	ection):				FCM:	
Lood Doting E	- circon (I DE)					
Load Rating E					*	** 1
Name: Unkno	own	Firm: Unk	known		Initial:	Unknown
Load Rating R	eviewer (LRR) certification as per th	he NBIS Title	23 CFR Se	ection 650.309(c):	:
Name: T.J.		-	NI I		Unknown	
Firm: Unkno						
Timi. Chikile	7W11					
		ccurate representation of and/or changes to load				
extent determi	nable by res	No Load	Rating			
performed. I am evaluation for the		perform				
Craidation for th	io accive menti	snoa siracturo.			Cyc	
			-			
	Sign		Da	ate		

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (Lan		09/13/2013 &		
Name:	NJ Transit-Morristown	Line		Insp. Date:	11/08/2013

LOAD RATING SUMMARY SHEET (LRSS) (cont.)

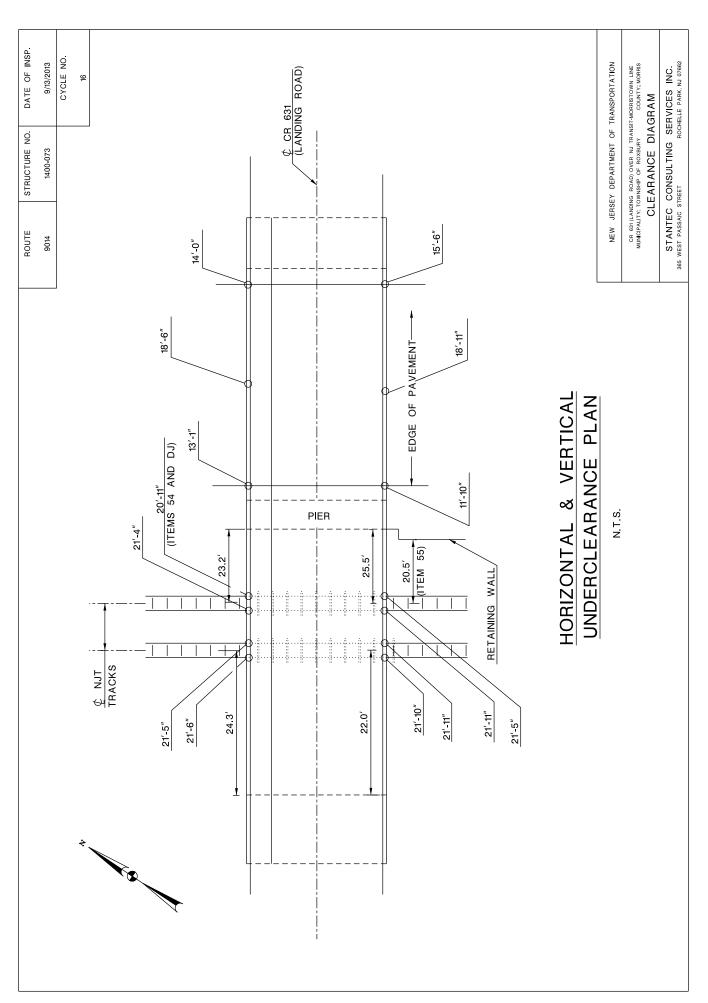
Rating Comments:		
None.		
		·
		 -,

The Load Factor ratings, computed in the 6^{th} Cycle report in accordance with the FHWA directive dated November 1993 and AASHTO Manual for Condition Evaluation of Bridges, 1994, as modified by Division 4 of the New Jersey Department of Transportation Design Manual, Bridges and Structures, are as follows:

		<u>Allowable</u>	Allowable Stresses (Psi)				
<u>Material</u>	<u>Compressive</u> Strength f'c	Yield	<u>Inventory</u>	Operating			
Concrete	2,500	N/A	800	1,200			
Reinforcing Steel	N/A	33,000	18,000	25,000			

						Rating	(Tons)			
				LI	F <u>R</u>			LR	<u>FR</u>	
Mambar	Truck	<u>Туре</u>	As-I	<u>Built</u>	As-I	nsp.	As-I	<u>Built</u>	As-I	nsp.
<u>Member</u>	(To	ons)	<u>Inv.</u>	<u>Op.</u>	<u>Inv.</u>	<u>Op.</u>	<u>Inv.</u>	<u>Op.</u>	<u>Inv.</u>	<u>Op.</u>
	H15	(15T)	46	77						
	HL-93	(NL)								
	HS-20	(36T)	65	109						
	3	(25T)	58	98						
Arch at Crown	3S2	(40T)	37	63						
Cond. Rating = 5	3-3	(40T)	59	99						
	SU4	(27T)								
	SU5	(31T)								
	SU6	(35T)								
	SU7	(39T)								

(NL) = Notional Load



1400<u>-0</u>73 Structure No.: 9014 Cycle No.: Route:

County Route 631 (Landing Road) over

09/13/2013 & Name: NJ Transit-Morristown Line 11/08/2013 Insp. Date:



Photo No: 16-01

Location: North elevation, looking south

General view. Note the leaning utility pole on the spandrel wall between the two spans. **Description:**



Photo No: 16-02

Location: South elevation, looking north

Description: General view. Structure No.: 1400-073 9014 Route: Cycle No.:

County Route 631 (Landing Road) over

09/13/2013 & Name: NJ Transit-Morristown Line 11/08/2013 Insp. Date:



Photo No: 16-03

Location: Roadway, looking east

Description: General view.



Photo No: 16-04

Location: Roadway, looking west

Description: General view. Note speed advisory posting. Structure No.: 1400-073 Cycle No.: 9014 Route:

County Route 631 (Landing Road) over

09/13/2013 & NJ Transit-Morristown Line 11/08/2013 Name: Insp. Date:



Photo No: 16-05

Location: Underside of intrados, west span, looking west

Description: General view. Note the light to moderate scaling with fine to medium cracks, efflorescence and numerous large concrete patches throughout.



Photo No: 16-06

South pipe railing, 3rd section from west, west span, looking southeast **Location:**

Description: Moderate rusting with holes in upper and lower rails.

Cycle No.: Structure No.: 1400-073 9014 Route:

County Route 631 (Landing Road) over

09/13/2013 & NJ Transit-Morristown Line 11/08/2013 Name: Insp. Date:



Photo No: 16-07

Location: South pipe railing, east end of the east span, looking northwest

Description: Damaged pipe railing with fractured middle rail and middle post is now protected by bridge-mounted guide rail. Note the spall along the cold joint in the outside face of the southwest approach parapet.



Photo No: 16-08

Location: South spandrel wall, east end of east span, looking northwest

Description: Fine map cracking with light efflorescence. Note the broken section of conduit along the east face of the

pier.

 Structure No.:
 1400-073
 Route:
 9014
 Cycle No.:
 15

 County Route 631 (Landing Road) over
 09/13/2013 &

Name: NJ Transit-Morristown Line Insp. Date: 11/08/2013



Photo No: 16-09

Location: East skewback, north end, looking southeast

Description: Fine vertical cracks with efflorescence.



Photo No: 16-10

Location: Northeast wingwall, east end, looking south

Description: Full height wide vertical crack with spall in the gunite coating.

Structure No.: 1400-<u>073</u> Route: 9014 Cycle No.:

County Route 631 (Landing Road) over

09/13/2013 & Name: NJ Transit-Morristown Line 11/08/2013 Insp. Date:



Photo No: 16-11

Location: Southwest retaining wall, looking west

Description: Spalls with active leakage at bottom.



Photo No: 16-12

Location: Southeast retaining wall, looking east

Description: Voids and missing pointing throughout. Structure No.: Cycle No.: 1400-073 Route: 9014

County Route 631 (Landing Road) over

09/13/2013 & Name: NJ Transit-Morristown Line 11/08/2013 Insp. Date:



Photo No: 16-13

Location: West face of pier, looking northeast

Description: Efflorescence and seepage stain at middle below coping.



Photo No: 16-14

Location: Southwest wingwall, looking northeast

Description: Fine map cracking with efflorescence.

Structure No.:	1400-073	_ Route:	9014	(Cycle No.:	15	
Name:	County Route 631 (La NJ Transit-Morristow		l) over	1	nsp. Date:	09/13/201	
	NEW JERSE STRUCTURAL E	EVALUATI FII	TMENT OF TA ION AND BRI ELD NOTES RRIS COUNTY				
Inspectors:	Mark V. Ramirez, P.E.	Name:	County Route 6	531 (Landing Ro	oad) over NJ 7	Γransit-Morris	stown
	A. La Bianca	_	Line				
Crew Chief:	Mark V. Ramirez, P.E.	Weather:		Clear			
Temperature:	40°F	_ Special Eq	uipment Used:	None			
RATINGS:							
 8 Very Goo 7 Good Cor 6 Satisfacto 5 Fair Cone 4 Poor Cone 3 Serious C 2 Critical C 1 Imminent 	Condition. d Condition – no problems ndition – some minor proble ry Condition – some mino dition – minor section los dition – advanced section l ondition – seriously deterion ondition – facility should l Failure Condition – facilit ndition – facility is closed Two Span, Closed Span	ems. r deterioratio s to primary oss to primar orated primar oe closed unti- y closed. Stu and beyond r	y structural elementy structural elementy structural elementy structural element repairs are made and y of repairs is ferepair.	ents. ents. ents. e. easible.		OORDINAT outhwest corner 54' 17.00" 39' 57.42"	
Year Built: 19	907		Year of Wid	ening / Major R	epairs: <u>1972</u>	2	
No. of Lanes:	On <u>2</u>		Under None				
Vertical Clearand	ces: Over	Deck: Unli	mited				
Minimum U	Inder: 20.91	from the top	of east rail of the	east track unde	r north fascia.		
Maximum U	Under (Item 10): N/A						
Horizontal Unde	rclearance:	Total Horizo	ontal Clearance:	N/A			
Right _20.	50' from the centerline of t	the east track	to the pier retaini	ng wall at the so	uth fascia.		

Overall Physical Condition of Structure: Fair due to the superstructure and substructure.

Left

N/A

1400-073	Route:	9014	Cycle No.:	15	
County Route 63	31 (Landing Road	l) over		09/13/2013 &	•
NJ Transit-Morn	istown Line		Insp. Date:	11/08/2013	
		SI&A It	em 58 Condition Rating: _	7	-
	County Route 63		County Route 631 (Landing Road) over NJ Transit-Morristown Line	County Route 631 (Landing Road) over	County Route 631 (Landing Road) over NJ Transit-Morristown Line O9/13/2013 & 11/08/2013

SPAN # West

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	No significant defects.
N	Underside of Deck	Not visible.
N	Median	
8	Curbs Concrete	Integral with sidewalk at north side. No apparent defects.
8	Sidewalks/ Safetywalk Concrete @ North only	No apparent defects.
7	Parapets/ Balustrades/ Pedestals	No significant defects.
7	Railings W-beam guide rail and pipe railing with concrete pylons	The original deteriorated pipe rails on both sides are protected by bridge mounted w-beam guide rail (Photo 16-06). No significant defects.
N	Deck Joints / Filler Material	
N	Drains and Scuppers	
N	Light Stands	
N	Utilities	
N	Others	

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 63	1 (Landing Road) over		09/13/2013 &
Name:	NJ Transit-Morr	stown Line		Insp. Date:	11/08/2013
DECK			SI&A	Item 58 Condition Rating:	7

SPAN # East

RATING	COMPONENT	REMARKS
	Wearing Surface / Top of Deck	Medium to wide longitudinal crack in the centerline of the westbound lane over the pier. No other significant defects.
7		
N	Underside of Deck	Not visible.
N	Median	
	Curbs	Integral with sidewalk at north side. No apparent defects.
8	Concrete	
8	Sidewalks/ Safetywalk Concrete @ North only	No apparent defects.
7	Parapets/ Balustrades	No significant defects.
7	Railings W-beam guide rail and pipe railing with concrete pylons	The original deteriorated pipe rails on both sides are protected by bridge mounted w-beam guide rail (Photo 16-07). No significant defects.
N	Deck Joints / Filler Material	
N	Drains and Scuppers	
N	Light Stands	
N	Utilities	
N	Others	

Structure No.:	1400-073	Route: 9014	Cycle No.:	15
	County Route 6.	31 (Landing Road) over		09/13/2013 &
Name:	NJ Transit-Morr	ristown Line	Insp. Date:	11/08/2013
APPROA (CHES		SI&A Item BA Rating: _	7
			SI&A Item 72 Rating: _	5

APPROACH West

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement Bituminous Concrete	No significant defects.
7	Approach Shoulder	No significant defects.
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from the bridge. Horizontal: Sharp (90°) turn south.
N	Guide Rail Condition	
8	Sidewalks Concrete @ North only	Continuous. No apparent defects.
8	Curbs Concrete	Integral with sidewalk at north side. Continuous. No apparent defects.
N	Utilities	
7	Approach Roadway Embankment	South and north: Moderate slope, well vegetated; stable.
7	Others/ Parapet	No significant defects.

Structure No.:	1400-073	Route:	9014	Cycle No.: _	15
	County Route 631	(Landing Road	l) over		09/13/2013 &
Name:	NJ Transit-Morris	town Line		Insp. Date:	11/08/2013

APPROACHES

SI&A Item BA Rating:	7

SI&A Item 72 Rating: 5

APPROACH East

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement	No significant defects.
	Bituminous Concrete	
7	Approach Shoulder	No significant defects.
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from bridge. Horizontal: T-intersection 100' to east. Speed reduction to 20 MPH.
7	Guide Rail Condition W-beam @ NE only	No significant defects.
8	Sidewalks Concrete @ North only	Continuous. No apparent defects.
7	Curbs Concrete	Integral with sidewalk at north side. Continuous. No significant defects.
N	Utilities	
7	Approach Roadway Embankment	South: Flat, parking lot. North: Moderate slope, vegetated, entrance to parking lot.
6	Others/ Parapet	Small spall along cold joint in southwest approach parapet outside face near bottom west end (Photo 16-07).

Structure No.	: <u>1400-073</u>	Route:	9014	Cycle No.: _	15
	County Route 6	31 (Landing Road)	over		09/13/2013 &
Name:	NJ Transit-Mor	ristown Line		Insp. Date:	11/08/2013
SUPERS (ARCH)	TRUCTURE	2	SI&A Ite	em 59 Condition Rating: _	5
SPAN#	West				

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Light to moderate scaling with fine to medium random cracks with efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 16-05).
N	Spandrel Columns/ Extrados	
5	Spandrel Walls	North: Fine map cracks with efflorescence and moisture staining (typical Photo 16-08) (60 SF total). South: Scattered fine map cracks and random cracks.
N	Others	

Structure No.:	1400-073	Route:	9014	Cycle	No.: _	15
	County Route 6	31 (Landing Road) over			09/13/2013 &
Name:	NJ Transit-Mor	ristown Line		Insp. I	Date:	11/08/2013
SUPERST (ARCH)	RUCTURE	2		SI&A Item 59 Condition R	ating: _	5

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Moderate scaling and efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 16-05). Medium longitudinal and transverse cracks with light to moderate efflorescence (180 LF total), mostly near the north and south ends of arch intrados.
N	Spandrel Columns/ Extrados	
5	Spandrel Walls	Fine map cracks with efflorescence throughout north wall and east end of south wall (Photo 16-08) (20 SF).
5	Others	Utility pole is leaning onto north spandrel wall between the two spans (wires are disconnected) (Photo 16-01).

Additional Remarks:

SPAN#

East

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (I	Landing Road	l) over		09/13/2013 &
Name:	NJ Transit-Morristov	wn Line		Insp. Date:	11/08/2013

SUBSTRUCTURE (Arch/Frame)

SI&A Item 60 Condition Rating: _____5

ABUTMENT West

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Bottom Portion-Crashwall: Fine vertical and map cracks with efflorescence and spalls/incipient spalls along the full length of the horizontal face (shotcrete) (typical Photo 16-09).
5	Wingwalls/ Retaining Walls	South: Fine map cracks and large concrete patch at east end (Photo 16-14). North: Medium cracks (50 LF) with efflorescence.
4	Others Retaining Walls	Spalls (40 SF) with active leakage along bottom at southwest wall (Photo 16-11).

Additional Remarks:

ABUTMENT East

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Few fine vertical cracks with moderate efflorescence at few locations (16 LF total) (Photo 16-09). Hollow (5%) areas of gunite throughout.
5	Wingwalls/ Retaining Walls	South: Scattered fine map cracks with efflorescence. North: Full height wide vertical crack with a spall (2 SF) in the gunite coating (Photo 16-10). Medium horizontal cracks (30 LF) with efflorescence.
5	Others Retaining Walls for Parking Area	North: Several loose stones protruding up to 6"; missing and loose mortar (80 LF). South: Half covered with gunite; scattered areas of discoloration; remaining half exhibits voids up to 1'-4" deep (6 SF) and missing mortar pointing (150 SF) (Photo 16-12).

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (I	Landing Road	l) over		09/13/2013 &
Name:	NJ Transit-Morristov	wn Line		Insp. Date:	11/08/2013

SUB	STR	UCT	URE

SI&A Item 60 Condition Rating: 5

PIER Center

RATING	COMPONENT	REMARKS
5	Columns/ Stem Crashwall Gunite cover	West Face: Spalled and delaminated bottom along exposed footing (20 SF) full width. Heavy efflorescence and water seepage stain at middle below coping (Photo 16-13). Hollow (30%) areas throughout gunite cover. East Face: Heavy efflorescence and stains on delaminated gunite cover at north end (2' x 8' high). Incipient spalled coping top at several locations (5 SF). Hollow (30%) areas throughout gunite cover. North Face: Heavy rust and water stains on delaminated gunite (20 SF). South Face: Fine to medium horizontal and vertical cracks throughout face.
N	Pier Cap	
N	Bridge Seat	
5	Others/Fender	Broken section of conduit hanging from cable at south end of pier along east face (Photo 16-08).

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631	(Landing Road	l) over		09/13/2013 &
Name:	NJ Transit-Morris	town Line		Insp. Date:	11/08/2013

HIGHWAY SAFETY

Coding of SI&A Item 36: 0000
1: Good

0: Not Good N: Not Applicable

RAT	ΓING	COMPONENT	REMARKS
	0	Bridge Railing	W-beam guide rail mounted on top of short concrete parapet/headwall (2" to 8" high from grade). There is a pipe railing behind guide rail (3'-4" high). Chain link fence between guide rail and steel railing.
	0	Transition to Bridge Railing	Northeast corner: Leading end within clear zone. Substandard single rail w-beam, posting 6'-3" spacing, no spacer blocks. No guide rails on southeast, southwest and northwest due to urban built-up areas.
0	1	Curb / Sidewalk Terminations	Continuous sidewalks and curbs. No sidewalk at southeast and southwest.
(0	Approach Guide Rails	Northeast only: Very short and no spacer blocks. 6'-3" post spacing. No guide rail at other three corners.
(0	Approach Guide Rail End Terminals	Northeast only: Buffer end treatment only.

DECK GEOMETRY

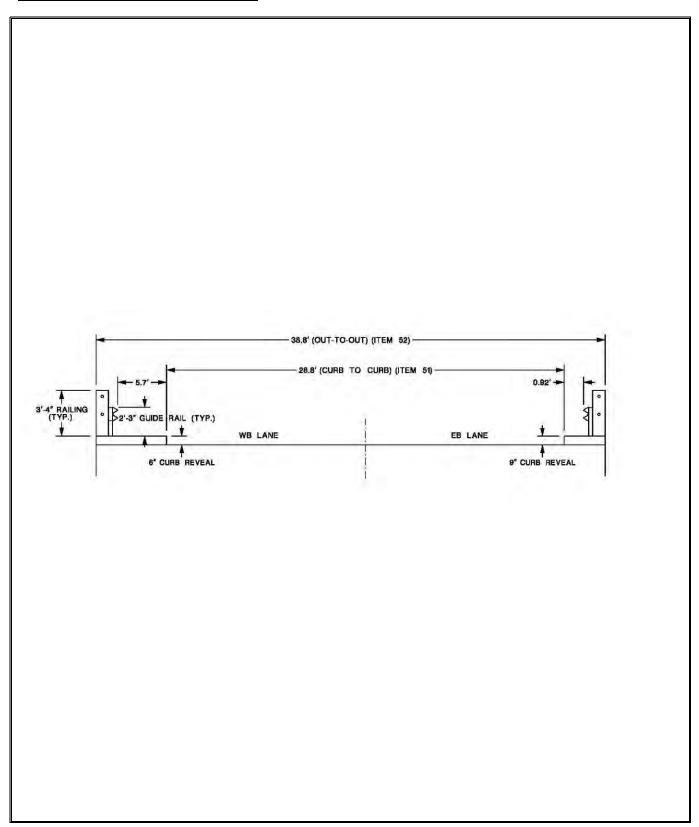
SI&A Item 68 Rating: 2

COMPONENT	REMARKS
Bridge Cross Section	Approaches are wider than bridge cross section. See sketch on following page.
Adequacy of Lane / Shoulder Widths	Two-way traffic, 2 lanes, 1' wide shoulders (typical). ADT = 23,061 (2013 projection from Traffic Count Summary by Morris County). Curb to curb width = 28.8', Table 2A.
Vertical Clearance over Deck	Unlimited.

*Posting for Load /	20 MPH speed posting (Photo 16-04).
Speed / Clearance	
Restrictions	

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631	Landing Road) over	·	09/13/2013 &
Name:	NJ Transit-Morristo	own Line		Insp. Date:	11/08/2013

DECK CROSS SECTION



Structure No.: 1400-073	Route: 9014	Cycle No.:	15
County Route	e 631 (Landing Road) over		09/13/2013 &
	forristown Line	Insp. Date:	11/08/2013
CLEADANCES			
<u>CLEARANCES</u>			
EFATURE ON CTRUCTURE	Country Design (21 (Leasting Design)	CLO A CLIET	P.T. 1
FEATURE ON STRUCTURE	County Route 631 (Landing Road)	SI&A SHEE	ET1
Minimum Vertical	Unlimited.		
Clearance (SI&A Item 10)			
Total Horizontal	28.80' curb to curb.		
Clearances (SI&A Item 47)			
,	<u>'</u>		
CONTROLLING UNDERCLI	EARANCE DATA:		
Minimum Vertical	20.92' from the top of east rail of the ea	ast track at the north f	ascia.
Underclearance (SI&A Item 54	-		
Minimum Vertical	N/A		
Underclearance (incl. shoulder			
(SI&A Item DJ)	3)		
Lateral Right	20.50' from the centerline of the east tra	ack to the pier retaini	ng wall at the
<u> </u>	south fascia.	ack to the pier retains	ng wan at the
(SI&A Item 55)			
Lateral Left	N/A		
(SI&A Item 56)			
FEATURE UNDER STRUCT	URE: N/A	SI&A SHEE	ET 2 or A
*Minimum Vertical	N/A		
Clearance (SI&A Item 10)			
Total Horizontal Clearance	N/A		
(SI&A Item 47)			
Minimum Vertical	N/A		
Underclearance (incl. shoulder			
•	S)		
(SI&A Item DJ)			
EEATIDE INDED OFFICE	LIDE. NI/A	OTO A CITED	err D
FEATURE UNDER STRUCT	URE: N/A	SI&A SHEE	ET <u>B</u>
*Minimum Vertical	N/A		
Clearance (SI&A Item 10)			
Total Horizontal Clearance	N/A		
(SI&A Item 47)			
Minimum Vertical	N/A		
Underclearance (incl. shoulder			
(SI&A Item DJ)	9/		
(DICA ICIII DJ)			

* Minimum clearance for a 10 foot width of the pavement or traveled part of the roadway where the clearance is greatest shall be coded in feet and inches.

FENCING	ι <u>Γ</u>			Coding of SI&A Item FN:	Y	
Name:	NJ Transit-Mo	`	,	Insp. Date:	11/08/2013	
	County Route	531 (Landing Road	l) over		09/13/2013 &	
Structure No.:	1400-073	Route:	9014	Cycle No.:	15	

Coding of SI&A Item FO: 0

Coding of SI&A Item FP (in thousands): 20

Warranted (Per Design Manual Section 23): Yes

If Yes: (#1) Description: Highway carrying, grade separation or high level bridges with facility for pedestrian traffic.

 Current Status of Fence & Sidewalk:
 Left Side
 Right Side

 a. Fence:
 No
 No

 b. Sidewalk Width:
 5.67 FT
 0.00 FT

 c. Total Height of fence above curb/sidewalk:
 N/A
 N/A

 d. Type of Fence (per Design Manual Section 23):
 N/A
 N/A

Action Recommended: Install a curved top, chain link fence at the north fascia.

Estimated Cost: \$19,176

Structure No.:	1400-073	Route:	9014	Cycle No.:	15
	County Route 631 (La	nding Road	d) over	 _	09/13/2013 &
Name:	NJ Transit-Morristowi	n Line		Insp. Date:	11/08/2013

WORK DONE HISTORICAL DATA

CYCLE NO.	YEAR	WORK DONE SUMMARY
16	2013	None.
15	2011	New deck waterproofing with new asphalt overlay on top that extends over both approaches. New concrete sidewalk along north side. New concrete curbs and guide rails along both sides. Concrete repairs throughout both arch intrados. Concrete repairs in the north parapet/headwall along the west span. Concrete repairs in the spandrel wall and coping at both spandrel walls of both spans. Concrete repairs at the south wingwall and in the coping and vertical portion below the arch at north end of the west skewback. Concrete repairs in coping and wall at the northwest edge and the south face of the pier stem.
14	2009	Concrete repair to the southwest abutment wingwall. Concrete repair patches to both intrados.
13	2007	None.

APPENDIX – C COST ESTIMATES

SUMMARY

Route	Landing Road Bridge	Section/Contract #	Alternative #1	
PM	0	UPC No.		0

			Totals from other
Work Type			pages
Earthwork			40000
Pavement			518520
Context Sensitive Design			200000
Culverts			0
Bridges			4175671.5
Drainage			219830
Incidental Items			186400
Landscape			71565
Noise Abatement			0
General Items			55575
Retaining Walls	582.8	1000	582800
PROJECT SUBTOTAL			6050361.5

Other Items	Proj. Subtotal Range	Choice	Amount
Lighting, Traffic Stripes, Signs		3% of Proj.	
and Delineators		Subtotal	181510.845
		1.5% of Proj.	
Maintenance of Traffic		Subtotal	90755
		1% of Proj.	
Training		Subtotal	60503.615
Mobilization			605036.15
		9% of Proj.	
	Project Cost < 5.0 (Mil.)	Subtotal	
	,	10% of Proj.	1
	Project Cost 5.0 & above	Subtotal	
Progress Schedule	Project Cost(Mil.)	\$	8000
ŭ	Less than 2.0	0	
	2.0 to 5.0	6,000	1
	5.0 to 10.0	8,000	
	10.0 to 20.0	15,000	
	20.0 to 30.0	30,000	
	30.0 to 40.0	40,000	
	40.0 & above	58,000	
Clearing Site	Project Cost (Mil.)	\$	115000
	Less than 1.0	15,000	
	1.0 to 2.0	30,000	
	2.0 to 5.0	45,000	
	5.0 to 10.0	115,000	
	10.0 to 20.0	220,000	
	20.0 to 30.0	240,000	
	30.0 to 40.0	250,000	
	40.0 & above	490,000	
Construction Layout	Project Cost(Mil.)	\$	87000
	Less than 1.0	7,000	
	1.0 to 2.0	20,000	
	2.0 to 5.0	42,000	
	5.0 to 10.0	87,000	
	10.0 to 20.0	160,000	
	20.0 to 30.0	220,000	
	30.0 to 40.0	490,000	
	40.0 & above	890,000	
	1.0.0 & 4.0010	PROJECT TOTAL	7198168

Υ **CONTINGENCIES & ESCALATION** 3.00 Y = Number of Years until midpoint of construction duration plus number of 3.00 years until construction start. If midpoint is less than 2 years from the date of this estimate, no escalation is required. Maximum value = 10% 7198167.533 1.030 1.04 7710677 Project Total Contingencies (1+C) 1 + [0.01 (Y+1) (Y- Construction Estimate for Initial 2)] Average Construction Project Cost(Mil.) Contingencies (C) Percent **Duration in Years** 3% 0.030 0-10 2 10-20 2.50% 0.000 3 20-50 0.000 1.50% 4 0.000 Over 50 CONSTRUCTION CHANGE ORDER CONTINGENCIES Total Federal Participating Items in Millions of \$ Construction Change Order Contingency Amount \$6,000 0 \$0 to 0.1 25,000 0.1 to 0.5 0 0.5 to 5.0 25,000 + 4% of amount in excess of \$500,000 0 5.0 to 10.0 205,000 + 3% of amount in excess of \$5,000,000 286300 10.0 to 15.0 355,000 + 2% of amount in excess of \$10,000,000 O 455,000 + 1.5% of amount in excess of \$15,000,000 - \$500,000 max 15.0 and above 0 0 For State Funded Projects, Contingencies for Change orders = 0 CHANGE ORDER CONTINGENC 286300 UTILITIES RELOCATIONS BY COMPANIES/OWNERS 0 0 7710677.061 x % or + Estimate **Utility Relocation** Construction Cost for Initial Use % or utilities detailed Cost for Initial Estimate estimate Estimate If there are no utility relocations on the project indicate "No Utilities" in the box above. RIGHT OF WAY COST If there is no ROW cost on the project indicate "No ROW" the box **SUMMARY** Construction Estimate for Initial 7,710,677 Construction Engineering (CE) Contingencies 286,300 **Utilities Relocations Total Construction Cost** 7.996.977

Right of Way Cost

0

Project: Landing Bridge Road Utility: Summary Estimate

Alternative: 1

Date: 1/14/03 AmerCom Job #2003

TOTAL
\$808,573
\$433,550
\$433,550
\$150,980

Total \$1,826,653

Project: Landing Bridge Road Utility: Electric Alternative: 1

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
E1	Electric	Remove old wooden pole	16	UNIT(S)	\$598	\$9,568
E2	Electric	New Pole, 12.2 m high	16	UNIT(S)	\$1,093	\$17,488
E3	Electric	Foundation for line poles, excavation in earth	16	UNIT(S)	\$59	\$944
E4	Electric	Guys, anchors and hardware for pole, in earth	16	UNIT(S)	\$1,323	\$21,168
E5	Electric	Cross arms with hardware and insulators, 1.5 m long	16	UNIT(S)	\$380	\$6,080
E6	Electric	Transformers, Oil Filled, 1000kVA	3	UNIT(S)	\$25,070	\$75,210
E7	Electric	Light Braket Arm	3	UNIT(S)	\$150	\$450
E8	Electric	Manhole	0	UNIT(S)	\$23,000	\$0
E9	Electric	3 Phase , Distrib. 34kV High Line, Primary	769	L.M.	\$566	\$435,254
E10	Electric	3 Phase , Distrib. 12kV	0	L.M.	\$566	\$0
E11	Electric	2 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E12	Electric	1 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E13	Electric	120/240V Secondary Electrical Overhead	643	L.M.	\$377	\$242,411

Total	 \$808,573

Project: Landing Road Bridge Utility: Telephone Alternative: 1

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
T1	Telephone	Remove old wooden pole	0	UNIT(S)	\$598	\$0
T2	Telephone	New Pole, 12.2 m high	0	UNIT(S)	\$1,093	\$0
Т3	Telephone	Foundation for line poles, excavation in earth	0	UNIT(S)	\$59	\$0
T4	Telephone	Guys, anchors and hardware for pole, in earth	0	UNIT(S)	\$1,323	\$0
T5	Telephone	Cross arms with hardware and insulators, 1.5 m long	0	UNIT(S)	\$380	\$0
T6	Telephone	Telephone Wire, 300 Pair	754	L.M.	\$575	\$433,550
T7	Telephone	Fiber Optic Cable	0	L.F.	\$575	\$0
Т8	Telephone	Manhole	0	UNIT(S)	\$23,000	\$0
						·
						·

Total	 \$433,550

Project: Landing Road Bridge Utility: Telephone Alternative: 1

Date: 4/04/03 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
C1	Cable	Cable Wire	754	L.M.	\$575	\$433,550

Total \$433,550

Project: Landing Road Bridge Utility: Water Alternative: 1

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
W1	Water	300MM Water Main	242	L.M.	\$426	\$103,092
W2	Water	Reset Water Valve Box	4	UNIT(S)	\$472	\$1,888
W3	Water	300 MM Gate Valves and Boxes	10	UNIT(S)	\$4,600	\$46,000

Total	 \$150,980

LANDING ROAD BRIDGE - AMERCOM #1011

Bypass Alternative (Alternative 1)

Right-Of-Way Acquisitions

	TABLE 2 - SUMMARY MATRIX											
PARCEL							RATABLE		TAKING AREA (ap		\$ BASED ON	
#	LOCATION / OWNER NAME	STREET	CITY, NJ	ZIP	LOT	BLOCK	\$ / Ac.	SQ. M	SQ. FT.	ACRE		BLDG. (TYPE)
1	DARYL D. & KIM W. REIGEL	120 LANDING RD.	ROXBURY TWP., NJ	07850	5	10020	\$ 930,000.00	44.00	473.61	0.011	\$ 10,111.55	Y (COMMERCIAL)
2	MARZANO ASSOCIATES	101 KINGS HIGHWAY	ROXBURY TWP., NJ	07850	7	10020	\$ 930,000.00	26.00	279.86	0.006	\$ 5,975.01	Y (COMMERCIAL)
3	D & B SHICKEN SHACK	118 LANDING RD.	ROXBURY TWP., NJ	07850	8	10020	\$ 930,000.00	28.40	305.70	0.007	\$ 6,526.55	Y (COMMERCIAL)
4	CUMBERLAND FARMS, INC.	124 LANDING RD.	ROXBURY TWP., NJ	07850	9	10020	\$ 930,000.00	140.70	1,514.48	0.035	\$ 32,333.99	Y (COMMERCIAL)
5	130 LANDING CORP., & RUMORS, INC.	130 LANDING RD.	ROXBURY TWP., NJ	07850	10	10020	\$ 930,000.00	23.50	252.95	0.006	\$ 5,400.49	Y (COMMERCIAL)
6	JOHN R. & JANICE CHAPLIN	132 LANDING RD.	ROXBURY TWP., NJ	07850	11	10020	\$ 930,000.00	21.90	235.73	0.005	\$ 5,032.80	Y (COMMERCIAL)
7	DEAN & RICHARD CURTIS	134 LANDING RD.	ROXBURY TWP., NJ	07850	12	10020	\$ 930,000.00	8.30	89.34	0.002	\$ 1,907.41	Y (COMMERCIAL)
8	RICHARD B. TREACY	146 LANDING RD.	ROXBURY TWP., NJ	07850	13	10020	\$ 930,000.00	19.40	208.82	0.005	\$ 4,458.28	Y (COMMERCIAL)
9	LAKESIDE REALTY ASSOC. C/O J. CINOT	150 LANDING RD.	ROXBURY TWP., NJ	07850	16	10020	\$ 930,000.00	311.70	3,355.11	0.077	\$ 71,631.15	Y (COMMERCIAL)
10	MATTHEW & EDNA VESEL	147 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	25	10101	\$ 460,000.00	230.30	2,478.93	0.057	\$ 26,177.85	Y (RESIDENTIAL)
11	DOMINICK DELEFAVE III	143 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	26	10101	\$ 930,000.00	739.80	7,963.14	0.183	\$ 170,011.96	Y (COMMERCIAL)
12	CHARLES T & HILDE KLINE	139 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	27*	10101	\$ 460,000.00	1,156.10	12,444.16	0.286	\$ 158,600.00	Y (RESIDENTIAL)
13	ERIC FEIGELSON	133 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	28*	10101	\$ 460,000.00	1,396.90	15,036.11	0.345	\$ 405,000.00	Y (RESIDENTIAL)
14	LANDING INVESTMENTS LLC	131 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	29	10101	\$ 930,000.00	120.20	1,293.82	0.030	\$ 27,622.92	Y (COMMERCIAL)
15	THEODORE W. & JOAN R. MILLER	125 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	30	10101	\$ 930,000.00	71.90	773.93	0.018	\$ 16,523.19	Y (COMMERCIAL)
16	NJ TRANSIT		ROXBURY TWP., NJ	07850	31	10101	\$ 930,000.00	4,598.30	49,495.69	1.136	\$ 1,056,726.11	N (RAILROAD)
17	THOMAS VALIANTE	CANAL STREET	ROXBURY TWP., NJ	07850	47*	10101	\$ 930,000.00	1,048.40	11,284.88	0.259	\$ 225,000.00	Y
18	RAYMOND & LINDA VAN HOREBEKE	LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	48	10101	\$ 250,000.00	2,037.40	21,930.39	0.503	\$ 125,863.12	N
19	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49*	10101	\$ 930,000.00	1,427.60	15,366.56	0.353	\$ 390,000.00	Y (COMMERCIAL)
20	LILLIAN DELASANDRO	119 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	2*	10903	\$ 930,000.00	332.95	3,583.83	0.082	\$ 114,200.00	Y
21	THOMAS & JUNE VALIANTE	117 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3	10903	\$ 930,000.00	208.30	2,242.12	0.051	\$ 47,869.01	Y
22	ABRAM LOYD III SNYDER	6 DOVER ST.	ROXBURY TWP., NJ	07850	4	10903	\$ 930,000.00	70.80	762.08	0.017	\$ 16,270.41	Y
23	CYNTHIA & BIAGIO BERDARDO	127 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	6	10903	\$ 930,000.00	27.50	296.01	0.007	\$ 6,319.72	Y
24	B & E LEE ASSOCIATES	109 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1*	10904	\$ 930,000.00	836.72	9,006.41	0.207	\$ 280,000.00	Y
25	DIMDAN ASSOCIATES	105 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3*	10904	\$ 930,000.00	2,419.10	26,038.97	0.598	\$ 616,600.00	Υ
26	KIMDAN ASSOICATES C/O SHORTINO	104 LANDING RD.	ROXBURY TWP., NJ	07850	1	10905	\$ 250,000.00	128.80	1,386.39	0.032	\$ 7,956.79	N
27	FIRST FIDELITYc/o FIRST UNION CRE281094	118 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1	11002	\$ 930,000.00	305.90	3,292.68	0.076	\$ 70,298.27	Y (COMMERCIAL)
28	STATE OF NEW JERSEY		ROXBURY TWP., NJ	07850	1	11101	\$ 250,000.00	268.50	2,890.11	0.066	\$ 16,586.95	N
29	NEW JERSEY, STATE OF LABOR/INDUSTRY BLDG.		ROXBURY TWP., NJ	07850	2	11101	\$ 930,000.00	58.50		0.014	<u> </u>	Y (COMMERCCIAL)
	* DENOTES ENTIRE PARCEL TAKING OR BUILDING DEMO	LITION (\$ BASED ON TAX RECORD ASSES	SSMENT)						TOTAL R.O.W. TA	AKING COST =	\$ 3,947,188.43	

SUMMARY

			ALTERNATIVE
Route	Landing Road Bridge	Section/Contract #	NO. 7-C
PM	0	UPC No.	0

	Totals from other
Work Type	
Work Type	pages
Earthwork	35000
Pavement	371320
Context Sensitive Design	150000
Culverts	0
Bridges	2577708.9
Drainage	161190
Incidental Items	142900
Landscape	52095
Noise Abatement	0
General Items	38475
PROJECT SUBTOTAL	3528688.9

Other Items	Proj. Subtotal Range	Choice	Amount	
Lighting, Traffic Stripes, Signs		3% of Proj.		
and Delineators		Subtotal	105860.667	
		1.5% of Proj.		
Maintenance of Traffic		Subtotal	52930	
		1% of Proj.		
Training		Subtotal	35286.889	
Mobilization			317582.001	
		9% of Proj.		
	Project Cost < 5.0 (Mil.)	Subtotal		317582
		10% of Proj.		
	Project Cost 5.0 & above	Subtotal		0
Progress Schedule	Project Cost(Mil.)	\$	6000	
	Less than 2.0	0		0
	2.0 to 5.0	6,000		6000
	5.0 to 10.0	8,000		0
	10.0 to 20.0	15,000		0
	20.0 to 30.0	30,000		0
	30.0 to 40.0	40,000	-	0
	40.0 & above	58,000		0
Clearing Site	Project Cost (Mil.)	\$	45000	
	Less than 1.0	15,000		0
	1.0 to 2.0	30,000		0
	2.0 to 5.0	45,000		45000
	5.0 to 10.0	115,000		0
	10.0 to 20.0	220,000		0
	20.0 to 30.0	240,000		0
	30.0 to 40.0	250,000		0
	40.0 & above	490,000		0
Construction Layout	Project Cost(Mil.)	\$	42000	
	Less than 1.0	7,000		0
	1.0 to 2.0	20,000		0
	2.0 to 5.0	42,000		42000
	5.0 to 10.0	87,000		0
	10.0 to 20.0	160,000		0
	20.0 to 30.0	220,000		0
	30.0 to 40.0	490,000		0
	40.0 & above	890,000	-1	0
	•	PROJECT TOTAL	4133349	

CONTINGENCIES & ESCALATION Υ 3.00 Y = Number of Years until midpoint of construction duration plus number of 3.00 years until construction start. If midpoint is less than 2 years from the date of this estimate, no escalation is required. Maximum value = 10% 1.030 4133348.791 1.04 4427643 Project Total Contingencies (1+C) 1 + [0.01 (Y+1) (Y- Construction Estimate for Initial 2)] Average Construction Project Cost(Mil.) Contingencies (C) Percent **Duration in Years** 0-10 3% 0.030 1 2 10-20 2.50% 0.000 3 20-50 2% 0.000 Over 50 1.50% 4 0.000 CONSTRUCTION CHANGE ORDER CONTINGENCIES Total Federal Participating Items Construction Change Order Contingency Amount in Millions of \$ \$0 to 0.1 \$6,000 0 0.1 to 0.5 25,000 0 0.5 to 5.0 25,000 + 4% of amount in excess of \$500,000 182100 205,000 + 3% of amount in excess of \$5,000,000 5.0 to 10.0 0 10.0 to 15.0 355,000 + 2% of amount in excess of \$10,000,000 0 15.0 and above 455,000 + 1.5% of amount in excess of \$15,000,000 - \$500,000 max 0 0 For State Funded Projects, Contingencies for Change orders = 0 CHANGE ORDER CONTINGENC 182100 UTILITIES RELOCATIONS BY COMPANIES/OWNERS 4427643.224 0.09 0 x % or + Estimate **Utility Relocation** Construction Cost for Initial Use % or utilities detailed Cost for Initial **Estimate Estimate** estimate If there are no utility relocations on the project indicate "No Utilities" in the box above. RIGHT OF WAY COST If there is no ROW cost on the project indicate "No ROW" the box SUMMARY Construction Estimate for Initial 4,427,643 Construction Engineering (CE) Contingencies 182,100 **Utilities Relocations Total Construction Cost** 4,609,743

0

Right of Way Cost

Project: Landing Bridge Road Utility: Summary Estimate

Alternative: 7-C

Date: 1/14/03

AmerCom Job #2003

UTILITY	COMPANY	DESCRIPTION	WORK SUMMARY	TOTAL
Electric	GPU	Preliminary Estimated Utility Impacts	5 Relocated Poles, wire	\$230,294
Telephone	Verizon/BA	Preliminary Estimated Utility Impacts	1 Additional Pole, wire	\$265,078
Cable	Comcast	Preliminary Estimated Utility Impacts	415 LM of cable wire	\$238,625
Water	Roxbury Water Co.	Preliminary Estimated Utility Impacts	417 LM of new line	\$202,530

Total \$936,527

Project: Landing Bridge Road Utility: Electric Alternative: 7-C

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
	<u> </u>					
E1	Electric	Remove old wooden pole	5	UNIT(S)	\$598	\$2,990
E2	Electric	New Pole, 12.2 m high	5	UNIT(S)	\$1,093	\$5,465
E3	Electric	Foundation for line poles, excavation in earth	5	UNIT(S)	\$59	\$295
E4	Electric	Guys, anchors and hardware for pole, in earth	5	UNIT(S)	\$1,323	\$6,615
E5	Electric	Cross arms with hardware and insulators, 1.5 m long	5	UNIT(S)	\$380	\$1,900
E6	Electric	Transformers, Oil Filled, 1000kVA	2	UNIT(S)	\$25,070	\$50,140
E7	Electric	Light Braket Arm	2	UNIT(S)	\$150	\$300
E8	Electric	Manhole	0	UNIT(S)	\$23,000	\$0
E9	Electric	3 Phase , Distrib. 34kV High Line, Primary	204	L.M.	\$566	\$115,464
E10	Electric	3 Phase , Distrib. 12kV	0	L.M.	\$566	\$0
E11	Electric	2 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E12	Electric	1 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E13	Electric	120/240V Secondary Electrical Overhead	125	L.M.	\$377	\$47,125

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Project: Landing Road Bridge Utility: Telephone Alternative: 7-C

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
T1	Telephone	Remove old wooden pole	1	UNIT(S)	\$598	\$598
T2	Telephone	New Pole, 12.2 m high	1	UNIT(S)	\$1,093	\$1,093
T3	Telephone	Foundation for line poles, excavation in earth	1	UNIT(S)	\$59	\$59
T4	Telephone	Guys, anchors and hardware for pole, in earth	1	UNIT(S)	\$1,323	\$1,323
T5	Telephone	Cross arms with hardware and insulators, 1.5 m long	1	UNIT(S)	\$380	\$380
T6	Telephone	Telephone Wire, 300 Pair	415	L.M.	\$575	\$238,625
T7	Telephone	Fiber Optic Cable	0	L.F.	\$575	\$0
T8	Telephone	Manhole	1	UNIT(S)	\$23,000	\$23,000

Total	 \$265,078

Project: Landing Road Bridge Utility: Telephone Alternative: 7-C

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
C1	Cable	Cable Wire	415	L.M.	\$575	\$238,625

Total	 \$238,625

Project: Landing Road Bridge Utility: Water Alternative: 7-C

Date: 4/04/03 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
W1	Water	300MM Water Main	417	L.M.	\$426	\$177,642
W2	Water	Reset Water Valve Box	4	UNIT(S)	\$472	\$1,888
W3	Water	300 MM Gate Valves and Boxes	5	UNIT(S)	\$4,600	\$23,000

Total \$202,530

LANDING ROAD BRIDGE - AMERCOM #1011

Replacement Alternative (Alternative 7-C)

Right-Of-Way Acquisitions

	TABLE 1 - SUMMARY MATRIX												
PARCEL							RATA	ABLE	ROW	TAKING AREA (ap	px.)	\$ BASED ON	
#	LOCATION / OWNER NAME	STREET	CITY, NJ	ZIP	LOT	BLOCK	\$/.		SQ. M	SQ. FT.	ACRE		BLDG. (TYPE)
Α	MARZANO ASSOCIATES	101 KINGS HIGHWAY	ROXBURY TWP., NJ	07850	7	10020	\$ 930	0,000.00	19.00	204.51	0.005	4,366.35	Y (COMMERCIAL)
	D a D OLHOVEN OLIVOX	440 LANDING DD	DOVDUDY TWO ALL	07050		40000	Φ 00	0.000.00	0.40	20.07	0.004	740.40	V (OOMMEDOIAL)
В	D & B SHICKEN SHACK	118 LANDING RD.	ROXBURY TWP., NJ	07850	8	10020	\$ 930	0,000.00	3.10	33.37	0.001	5 712.40	Y (COMMERCIAL)
С	CUMBERLAND FARMS, INC.	124 LANDING RD.	ROXBURY TWP., NJ	07850	9	10020	\$ 930	0,000.00	173.30	1,865.39	0.043	39,825.73	Y (COMMERCIAL)
D	EMEDIN & MARY RIVERA	100 KINGS HIGHWAY	ROXBURY TWP., NJ	07850	6	10021	\$ 030	0,000.00	10.80	116.25	0.003	2,481.93	Y (COMMERCIAL)
	LIVEDING WART RIVERA	TOO KINGO TIIGI WAT	ROADORT TWI ., NO	07030		10021	ψ 330	0,000.00	10.00	110.23	0.003	2,401.93	1 (COMMERCIAE)
Е	THEODORE W. & JOAN R. MILLER	125 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	30	10101	\$ 460	0,000.00	924.70	9,953.39	0.228	105,109.24	Y (VACANT)
F	NJ TRANSIT		ROXBURY TWP., NJ	07850	31	10101	\$ 930	0,000.00	488.00	5,252.79	0.121	112,146.30	N (RAILROAD)
		_											
G	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49*	10101	\$ 930	0,000.00	211.80	2,279.80	0.052	48,673.33	Y (COMMERCIAL)
Н	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49	10101	\$ 250	0,000.00	0.00	0.00	0.000	-	N
	WEIGHERT REALTORS	404 LANDING DD	DOVDLIDY TWO ALL	07050	40	40404	Φ 05/	0.000.00	0.00	0.00	0.000	h	N
	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49	10101	\$ 250	0,000.00	0.00	0.00	0.000	-	N
J	CYNTHIA & BIAGIO BERDARDO	127 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	6	10902	\$ 460	0,000.00	27.50	296.01	0.007	3,125.88	Y (RESIDENTIAL)
K	LILLIAN DELASANDRO	119 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	2*	10903	\$ 230	0,000.00	332.95	3,583.83	0.082	114,200.00	Y (RESIDENTIAL)
L	THOMAS & JUNE VALIANTE	117 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3	10903	\$ 460	0,000.00	206.70	2,224.90	0.051	23,495.27	Y (RESIDENTIAL)
M	ABRAM LOYD III SNYDER	6 DOVER ST.	ROXBURY TWP., NJ	07850	4	10903	\$ 230	0,000.00	70.80	762.08	0.017	4,023.86	Y (RESIDENTIAL)
NI	B & E LEE ASSOCIATES	109 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1*	10904	ф 02 <i>/</i>	0,000.00	020.70	0.000.44	0.207	280,000.00	Y (COMMERCIAL)
N	B & E LEE ASSOCIATES	109 LAKESIDE BLVD.	ROABURT TWP., NJ	07850	<u> </u>	10904	\$ 930	0,000.00	836.72	9,006.41	0.207 3	280,000.00	Y (COIVIIVIERCIAL)
0	KIMDAN ASSOCIATES	105 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3*	10904	\$ 930	0,000.00	2419.10	26,038.97	0.598	616,600.00	Y (COMMERCIAL)
Р	KIMDAN ASSOCIATES	104 LANDING RD.	ROXBURY TWP., NJ	07850	1	10905	\$ 250	0,000.00	73.50	791.15	0.018	4,540.56	N
Q	BELL ATLANTIC CORP/PROPERTY TAX # 3137	128 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	7	11001	\$ 930	0,000.00	43.60	469.31	0.011	10,019.63	Y (COMMERCIAL)
R	FIRST FIDELITYc/o FIRST UNION CRE281094	118 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1	11002	\$ 930	0,000.00	277.50	2,986.99	0.069	63,771.72	Y (COMMERCIAL)
0	OTATE OF NEW JEDOEV			07050	4	44404	Φ 05.	0.000.00	0.40.00	0.050.50	0.004	45.046.40	, ,
S	STATE OF NEW JERSEY		ROXBURY TWP., NJ	07850	1	11101	\$ 250	0,000.00	246.80	2,656.53	0.061	15,246.40	N
T	NEW JERSEY, STATE OF LABOR/INDUSTRY BLDG.		ROXBURY TWP., NJ	07850	2	11101	\$ 930	0,000.00	20.10	216.35	0.005	4,619.14	Y (COMMERCIAL)
	* DENOTES ENTIRE PARCEL TAKING (\$ BASED ON TAX R	ECORD ASSESSMENT)								TOTAL R.O.W. TA	AKING COST = S	1,461,288.99	

Township of Roxbury L

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_edgewood, New Jers	sey 07852

Planning/Zoning	973-448-2008
Public Works	973-448-2069
Recreation	973-448-2015
Tax Assessor	973-448-2021
Tax Collector	973-448-2022
Technology	973-448-2099
Township Clerk	973-448-2001
Treasurer	973-448-2006
Wastewater Treatment Plant	973-398-2818
Zoning Officer	973-448-2013

Number-15-6 Page 1 of 4 **Property Location:** Route 631/Landing Rd

Date: February 6, 2015 To: RBA Group Inc

7 Campus Dr., Suite 300 Parsippany, NJ 07054-4495

NOTICE: Every property owner listed, as well as all utility companies listed (see attached) must be served either by personal service, registered or certified mail. If an adjoining municipality is included within the search area, that Township Clerk must be notified. These notices must be mailed or delivered at least ten days before the meeting. Affidavit of Service must be notarized by a Notary Public of New Jersey and submitted to the Clerk of the Board at least five days before the meeting.

Name/Address (as indicated on accompanying lists) Block/Lot

In addition, the agencies checked (X) below must also be served by personal service, registered or certified mail:

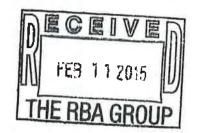
- Morris County Planning Board, PO Box 900, Morristown, NJ 07963-0900 X NJ Dept. of Transportation, 200 Stierli Ct, Mt Arlington, NJ 07856
- NJDEP, Division of Land Use Regulations, MC501-02A, PO Box 420, Trenton NJ 08625-0420 X
- NJ Transit, c/o Energy Solve-UBAR-14, PO Box 6077, Somerset, NJ 08873-6077 X
- Morris County DOT, PO Box 900, Morristown, NJ 07963-0900 X Chester Branch Co., PO Box 404, Succasunna, NJ 07876 Conrail, 6 Penn Center Plaza, Philadelphia, PA 19103 Adjacent Municipality of

I hereby certify that I have compiled the above list from current tax duplicates of the Township of Roxbury to the best of my ability and that said list complies with 13-2.507-7 (c) of the Land Use Ordinance.

eph J. McKeon, CTA

Tax Assessor

Township of Roxbury Received: Check#241



OWNER & ADDRESS REPORT

ROXBURY TOWNSHIP

02/06/15 Page 1 of 2

BLOCK	LOT	QUAL	CLA	PROPERTY OWNER		PROPERTY LOCATION	Add'1 Lots
10020	6		2	RIVERA, EMEDIN/MARY 111 KINGS HWY LANDING, NJ	07850	111 KINGS HWY	
10020	7.1010		4A	MARZANO ASSOCIATES 25 SHERWOOD DR MOUNTAIN LAKES, NJ	07046	101A KINGS HWY	
10020	7.2010		4A	LESSIG, DOUGLAS 25 SUNSET LN LANDING, NJ	07850	101B KINGS HWY	
10020	7.3010		4A	LESSIG, DOUGLAS 25 SUNSET IN LANDING, NJ	07850	201C KINGS HWY	
10020	8		4A	D & B CHICKEN SHACK 11B LANDING RD LANDING, NJ	07850	118 LEDGE-LANDING RD	
10020	5		4A	LANDING PROPERTIES LLC 120 LANDING RD LANDING, NJ	07850	120 LEDGE-LANDING RD	
10020	9		4A	TJ PROPERTIES LLC 124 LANDING RD LANDING, NJ	07850	124 LEDGE-LANDING RD	
10020	10		4A	MEGAMAC, LLC 130 LANDING RD LANDING, NJ	07850	130 LEDGE-LANDING RD	
10020	11		4A	GATES REAL ESTATE HOLDIN 132 LANDING RD LANDING, NJ	07850	132 LEDGE-LANDING RD	
10020	14		1	MEGAMAC, LLC 130 LANDING RD LANDING, NJ	07850	RIGGS AVE, LAND	
10021	5		4A	SCANTLEBURY ARTHUR B 112 KINGS HWY LANDING, NJ	07850	112 KINGS HWY	
10021	6		4A	SHIN, YOUNG S/KYOUNG I 102 LANDING RD LANDING, NJ	07850	100 KINGS HWY	
10101	28		4 A	133 LANDING ROXBURY TWP 6 DUMONT PL, 3RD FLOOR MORRISTOWN, NJ	LLC 07960	133 LEDGE-LANDING RD	
10101	29		4A	LANDING INVESTMENTS LLC 131 LANDING RD LANDING, NJ	07850	131 LEDGE-LANDING RD	
10101	29	B01	4A	LANDING INVESTMENTS LLC 131 LANDING RD LANDING, NJ	07850	131 LEDGE-LANDING RD	
10101	30		4A	LAKE HOPATCONG FOUNDATION 37 NOLAN'S POINT PARK RILLAKE HOPATCONG, NJ	ON D Q7849	125 LEDGE-LANDING RD	
10101	48		1	GEDICKE'S MARINE INC 91 MT ARLINGTON BLVD LANDING, NJ	07850	MT ARLINGTON BLVD	
10101	49		4A	101 LANDING COMPANY, LL 74 COMANCHE AVE ROCKAWAY, NJ	07866	101 LEDGE-LANDING RD	
10905	1		1	4C'S LAKEFRONT LODGING, 1D7 TULSA TAL HOPATCONG, NJ	LLC 07843	104 LEDGE-LANDING RD	

OWNER & ADDRESS REPORT

ROXBURY TOWNSHIP 02/06/15 Page 2 of 2

BLOCK	LOT QUAL	CLA	PROPERTY OWNER	PROPERTY LOCATION	Add'l Lots
10904	1	4A	N R & R REALTY LLC 334 MEADOWBROOK RD WYCKOFF, NJ 07481	109 LAKESIDE BLVD	
10904	2	2	ENGLISH, ELLEN P 111 DOVER ST LANDING, NJ 07850	111 DOVER ST	
10904	3	4A	4C'S LAKEFRONT LODGING, LLC 107 TULSA TAL HOPATCONG, NJ 07843	105 LAKESIDE BLVD	
10903	1	2	BROWN/MCCOY, ANTIONETTE N/RANDOLE 121 LAKESIDE BLVD LANDING, NJ 07850	PH 121 LAKESIDE BLVD	
10903	2	2	DIBLASI, THOMAS 174 MT ARLINGTON BLYD LANDING, NJ 07850	119 LAKESIDE BLVD	
10903	3	4A	LAKESIDE REALTY, LLC 117 LAKESIDE BLVD LANDING, NJ 07850	117 LAKESIDE BLVD	
10903	4	2	SNYDER, ABRAM LLOYD III 6 DOYER ST LANDING, NJ 07850	6 DOVER ST	
10902	5	4A	BAYVIEW LOAN SERVICING LLC 4425 PONCE DE LEON BLVD CORAL GABLE, FL 33146	131 LAKESIDE BLVD	
10902	6	2	MILLER/MEADE EDWARD J/ANDREIA S 125-127 LAKESIDE BLVD LANDING, NJ 07850	127 LAKESIDE BLVD	
10902	7	15F	BEARS, WAHNETA 333 BOONTON ST LANDING, NJ 07850	333 BOONTON ST	
11101	1	15F	NEW JERSEY, STATE OF LABOA/INDUSTRY BLDG,CN229 TRENTON, NJ 08625	LAKESIDE BLVD	
11101	2	4A	GEDICKE'S MARINE INC 91 MT ARLINGTON BLVD LANDING, NJ 07850	91 MT ARLINGTON BLVD	
11002	1	4A	FIRST STATES INVESTORS 3108, LLC PO BOX 167129 IRVINGTON, TX 75016	118 LAKESIDE BLVD	
11001	5	4A	RF-HOPATCONGPROPERTY MANAGEMENT, 136 LAKESIDE BLYD LANDING, NJ 07850	LLC 136 LAKESIDE BLVD	
11001	6	4A	MEADES/HELLER, ALAN R/JOHN PO BOX 700 MADISON, NJ 07940	130 LAKESIDE BLVD	
11001	7	4A	BELL ATLANTIC CORP/PROPERTYTAX#3 PO BOX 152206 IRVING, TX 75015	137 128 LAKESIDE BLVD	
10901	12	48	ABE SNYDER CO INC 6 DOVER ST LANDING, NJ 07850	LAKESIDE BLVD, REAR	
10101	27	2	KLINE HILDE W/ROBIN R 139 LANDING RD LANDING, NJ 07850	139 LEDGE-LANDING RD	
10020	7		Riggs Lake		
10101	31		Railroad		
9801	4		Railroad		
11501	1		Lake Hopatcong		

UTILITIES SERVING THE TOWNSHIP OF ROXBURY ALL UTILITIES MUST BE NOTIFIED

AT&T Legal Department Attn: Network Counsel 340 Mount Kemble Ave Morristown, NJ 07960 (Re: 10110944)

Cablevision of Morris Attn: Bruce Hickson or John Bartels 683 Route 10 E Randolph, NJ 07869 (973 659-2470)

Columbia Gas Transmission Corp. Attn: Robert Schini 1470 Poorhouse Rd Dowingtown, Pa 19335 (610 518-3547) (800 835-7191) Emergency only

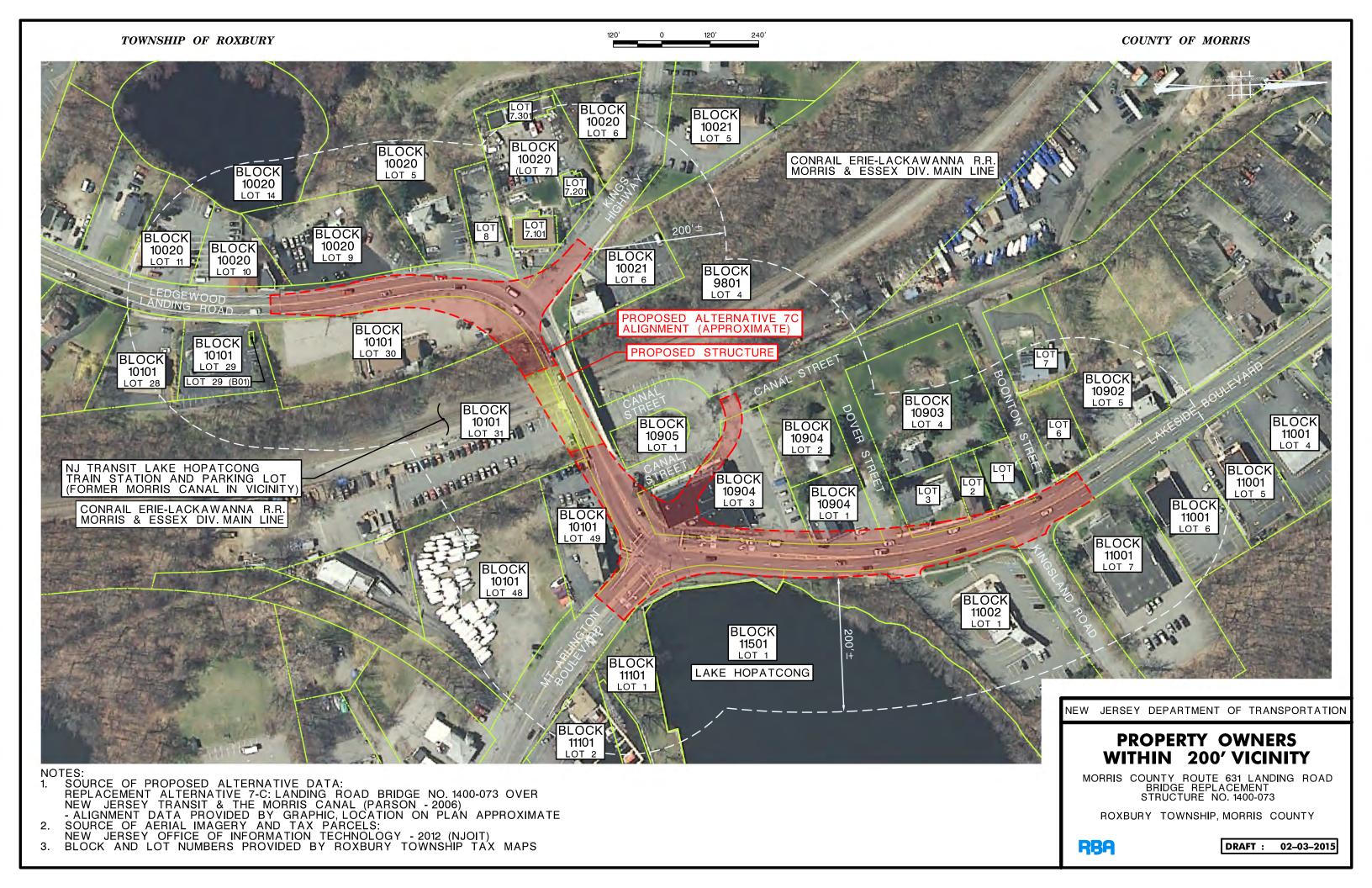
JCP&L 13 Richboynton Rd Dover, NJ 07801 (973 537-2600) Vicki Thiel

Musconetcong Sewerage Authority 110 Continental Dr Budd Lake, NJ 07828 (973 347-1525) James Schilling ischilling@msa-nj.org NJ Natural Gas Co Attn: Matthew Puzio 1415 Wyckoff Rd PO Box 1464 Wall, NJ 07719 (732 938-1030)

Roxbury Water Co Attn: John Hosking 79 Sunset Strip PO Box 560 Succasunna, NJ 07876 (973 584-4118)

Township of Roxbury 1715 Route 46 Water & Sewer Dept. c/o Engineering Dept Ledgewood, NJ 07852 (973 448-2018)

Verizon Communications Attn: Richard Flint 290 W Mt Pleasant Ave Bldg 4-Ground Floor Livingston, NJ 07039 (973 422-5132)



Classification No. 5 - BRIDGE REPAIR

Route	Landing Road Bridge	Section/Contract #	Rehabilitation Alternative
PM		UPC No.	

PAVEMENT

3.6 M WIDE LANE (from subgrade up)

Pav't. Type	Description of Pavement	Cost/Linear Meter
A	250 mm R.C. Pavement	510
	50 mm HMA Surf. Crs. & 200	
В	mm HMA Base	200
	75 mm HMA Surf. Crs. & 100	
С	mm HMA Base	150
	50 mm HMA Surf. Crs. & 50	
D	mm HMA Base	70
	Bridge Approach & Transition	
E	Slabs	510
	(Resurfacing Portion only F & 0	G)
F	50 mm HMA Surface Course	27
G	75 mm HMA Surface Course	40
Н	Milling 50 mm	10

Computation Table for Pavement. Cost

Туре	Cost from table above	x Length	x Pavement *W.F.	= Amount
				0
A	510	41	2	41820
E	510	35	2	35700
H	10	40	2	800
F	27	40	2	2160
				0
				0
				0
				0
PAVEMENT TOTAL			=	80480

^{*}Width Factors = Ratio of 3.6 meter wide lane to actual pavement width.

Example = actual pavement width = 7.5 meters = 7.5/3.6 = 2.05 W.F.

INCIDENTAL ITEMS

Item	Cost / L.M.	x Quantity	= Amount
Beam Guide Rail	55	80	4400
Fence 1.8 Meter High	60	80	4800
225 mm X 400 mm Conc. Vertical	45	160	7200
375mm X 1040 mm Conc. Barrier	165	0	0
600mm X 1040 mm Conc. Barrier	240	0	0
600mm X Variable Conc. Barrier 0	150	0	0
INCIDENTAL ITEMS TOTAL	=		16400

BRIDGES

Cost to be provided by the Bureau of Structural Engineering

TOTAL -	750000

SUMMARY

			Rehabilitation	
Route	Landing Road Bridge	Section/Proj. Id. #	Alternative	
PM		UPC No.	0	
1 141		01 0 110.		
			Totals from other	
Work Type			pages	
Pavement			80480	
Incidental Items			16400	
Bridges			750000	
			0.40000	
PROJECT SUBTOTAL			846880	
[a	T=	To: .	1.	
Other Items	Proj. Subtotal Range	Choice	Amount	
Lighting, Traffic Stripes, Signs		1% of Proj.		
and Delineators		Subtotal	8468.8	
		7% of Proj.		
Maintenance of Traffic		Subtotal	59282	
		1% of Proj.		
Training		Subtotal	8468.8	
			07750	
Mobilization	Project Cost (Mil.)	% of Proj. Subtotal	67750	
	Less than 1.0	8.00%	,	67750
	1.0 to 5.0	5.00%	,	C
	5.0 & above	5.00%		(
Clearing Site	Project Cost (Mil.)	\$	2000	
	Less than 1.0	2,000		2000
	1.0 & above	3,000		2000
Construction Layout	Project Cost(Mil.)	\$	4000	
Construction Layout	Less than 1.0	4,000		4000
	1.0 & above	6,000	-	
	1.0 & above	PROJECT TOTAL	996850	C
		TROOLOT TOTAL	990000	
CONTINGENCIES & ESCALAT	ION	Υ		
CONTINUENCIES & ESCALAT	<u>10</u> 14		1	3.00
V = Number of Vears until midpoint	of construction duration plus number	r		3.00
	nidpoint is less than 2 years from the			
	is required. Maximum value = 10%			
996849			1067825	
	al Contingencies (1+C)	1 + [0.01 (Y+1) (Y-		
1 Toject Tol	ai contingencies (110)	2)]	Estimate for Initial	
		2/]	Lotimate for initial	
		Average	1	
		Construction		
Project Cost(Mil.)	Contingonoios (C) Paracet	Duration in Years		
Project Cost(Mil.)	Contingencies (C) Percent		-	0.000
0-20 Over 20	3%		-	0.030
Over 20	2.0%	6 2	J	0.000
CONSTRUCTION ENGINEERIN	NG (CE)			
		% of Construction	1	
Project Cost (Mil.)		Cost		
Less than 1.0	1	14.90%	Í	(
1.0 to 5.0		12.20%		130275
5.0 to 10.0	1	10.80%		
				C
10.0 & above	IC AMOUNT	9.50%		0
CONSTRUCTION ENGINEERIN	NG AIVIOUN I	\$130,274.69		

CONSTRUCTION CHANGE ORDER CONTINGENCIES

Total Federal Participating Items

in Millions of \$	Construction Change Order Contingency Amount	
\$0 to 0.1	\$6,000	0
0.1 to 0.5	25,000	0
0.5 to 5.0	25,000 + 4% of amount in excess of \$500,000	47700
5.0 to 10.0	205,000 + 3% of amount in excess of \$5,000,000	0
10.0 to 15.0	355,000 + 2% of amount in excess of \$10,000,000	0
15.0 and above	455,000 + 1.5% of amount in excess of \$15,000,000 - max \$500,000	0
		0

For State Funded Projects, Contingencies for Change orders = 0

CHANGE ORDER CONTINGENC = 47700

UTILITIES RELOCATIONS BY COMPANIES/OWNERS

1067825	0.085	90765
Construction Cost for Initial Estimate		Utility Relocation Cost for Initial Estimate

If there are no utility relocations on the project indicate "No Utilities" in the box above.

RIGHT OF WAY COST

If there is no ROW cost on the project indicate "No ROW" the box

SUMMARY

Construction Estimate for Initial	1067825
Construction Engineering (CE)	130275
Contingencies	47700
Utilities Relocations	90765
Total Construction Cost	1336565
Right of Way Cost	0

APPENDIX – D RESOLUTION OF SUPPORT

Township of Roxbury

1715 Route 46 Ledgewood, New Jersey 07852 PLANNING/ZONING 448-2021
PUBLIO WORKS 448-2021
RECREATION 448-2021
TAX ASSESSOR 448-2021
TAX COLLECTOR 448-2021
TOWNSHIP CLERK 448-2001
WASTEWATER TREATMENT PLANT 594-5380

ZONING OFFICER

INFORMATION 448-2000
CONSTRUCTION/BLOG 448-2003
COURT COUNCIL 448-2013
ENGINEER 448-2012
HEALTH 448-2028
MAYOR & COUNCIL 448-2001
POLICE 448-2001

April 16, 2003

States

Surinder K. Thapar, P.E., P.P.
Assistant County Engineer
Morris County Department of Public Works
P.O. Box 900
Morristown, NJ 07963

Dear Mr. Thapar;

Enclosed please find a certified copy of Resolution 03-131 "A RESOLUTION SUPPORTING THE REPLACEMENT OF LANDING ROAD BRIDGE (ALTERNATIVE 7-C)", which was adopted by the Township Council on April 14, 2003.

If you have any questions, please feel free to contact my office.

Very truly yours,

TOWNSHIP OF ROXBURY

BettyLou DeCroce

Township Clerk

/jr

Enclosure

clerks/janot/restorr

COUNTY OF MORRIS

APR 2 1 2003

ENGINEERING DEPARTMENT

RESOLUTION NO. 03 - 131

A RESOLUTION SUPPORTING THE REPLACEMENT OF LANDING ROAD BRIDGE (ALTERNATIVE 7-C)

WHEREAS, the Morris County Department of Public Works studied the feasible options to improve the structural capacity and traffic safety of the Landing Road Bridge over New Jersey Transit and the Morris Canal in the Township of Roxbury; and

WHEREAS, the Township Council previously adopted Resolution No. 98-314 on October 6, 1998 which supported the proposed alignment #7, which specified the replacement of the existing bridge at its current alignment subject to conditions as specified in said resolution; and

WHEREAS, Morris County and their Engineering Consultant, Parsons Transportation Group, Inc. have developed alternatives to address these conditions; and

WHEREAS, the Engineering/Public Works Department has met with Morris County and Parsons to review the alternatives; and

WHEREAS, this department agrees with the preferred alternative 7-C as contained in their April 14, 2003 presentation package; and

WHEREAS, the Township Engineering/Public Works Department recommends adoption of a resolution supporting alternative 7-C as the preferred alternative for the Landing Road Bridge Replacement Project.

NOW, THEREFORE, BE IT RESOLVED, by the Township Council, of the Township of Roxbury, in the County of Morris and State of New Jersey that support is hereby established for the replacement of the Landing Road Bridge (Alignment 7-C).

BE IT FURTHER RESOLVED, that a signed copy of this resolution be provided to the Township Engineer/Public Works Director, Police Chief, Historical Advisory Committee, and Morris County Department of Public Works Administrative & Records Bldg., P.O. Box 900, Morristown, New Jersey 07963, Attention: Surinder K. Thapar, P.E., P.P., Assistant County Engineer.

ADOPTED:

April /4, 2003

BettyLou/DeCroce Township Clerk

MAK.bja.ResReplaceLandingRdBridgeAlter7-C.41003

RESOLUTION NO. 2015 - 310

A RESOLUTION SUPPORTING THE REPLACEMENT OF LANDING ROAD BRIDGE (ALTERNATIVE 7-C)

WHEREAS, the Morris County Department of Public Works studied the feasible options to improve the structural capacity and traffic safety of the Landing Road Bridge over the New Jersey Transit and the Morris Canal in the Township of Roxbury; and

WHEREAS, the Township Council previously adopted Resolution No. 03-131 on April 14, 2003 which supported the proposed alignment #7-3, which specified the replacement of the existing bridge at its current alignment subject to conditions as specified in said resolution; and

WHEREAS, the Engineering/Public Works Department has met with Morris County to review the alternatives; and

WHEREAS, this department agrees with the preferred alternative 7-C as contained in their June 9, 2015 presentation package; and

WHEREAS, the Township Engineering/Public Works Department recommends adoption of a resolution supporting alternative 7-C as the preferred alternative for the Landing Road Bridge Replacement Project; and

WHEREAS, the Alternative was presented to Mayor and Council on June 9, 2015; and

WHEREAS, the alternative was presented to the public by the Morris County Engineering Department on August 12, 2015.

NOW, THEREFORE, BE IT RESOLVED by the Township Council, of the Township of Roxbury, in the County of Morris and State of New Jersey that support is hereby established for the replacement of the Landing Road Bridge (Alignment 7-C).

BE IT FURTHER RESOLVED, that a certified copy of this resolution be provided to County of Morris Department of Planning & Public Works, Division of Engineering & Transportation, Attention: Roslyn Khurdan, PO Box 900, Morristown, NJ 07960-0900 and a copy be provided to Michael Kobylarz, Township Engineer/Director of Utilities; Richard Blood, Director of Public Works; Chief Simonetti, Police Department; Historical Advisory Committee.

ADOPTED: September 29, 2015

Attest:

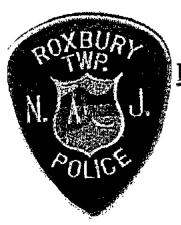
Amy E. Rhead

Township Clerk

APPENDIX – E

ACCIDENT DATA (2011)
CEEKF GP V'F CVC'*4236+"

9734489069



Roxbury Township Police Department

1715 State Highway Route 46 Ledgewood, New Jersey, 07852 (973) 448-2100 Main (973) 448-2081 Fax

FAX TRANSMITTAL

To:	HUGUSTO MOLINA
From:	SOT. DAN MURRAY RTPD
Date:	2/14/12
Ref:	LANDING RD MUC STATS 2005-2011
Pages:	(Including cover page)
Commer S	SEND 6-MATE RECEIPT CONFIRMATION.
	th nks.
	NURRAY @ ROXBURYPD, COM

Sgt. Daniel Murray Roxbury Twp. Police Dept. 1715 State Hwy. Rt. 46 Ledgewood, NJ 07852

01/16/2012

Augusto V. Molino 100 Broadway New York, NY 10005

Mr. Molino,

At your request, motor vehicle crash statistics have been compiled for Landing Road, specifically in the area of the Landing Road Bridge. Some of the data is unavailable due to non-specific recordation of those statistics for the location you requested. The statistics that are available for this location give a reasonable idea of the amount of motor vehicle crashes that occur in this area on a yearly basis.

Total MVC's on Landing Road (from Rt. 46 to Lakeside Blvd.)

2005	<u> 2006</u>	2007	2008	<u> 2009</u>	<u> 2010</u>	<u> 2011</u>
53	45	54	34	54	32	48

Number of those MVC's occurring on Landing Rd. between Hart St. and intersection with Lakeside Blvd. and Mt Arlington Blvd. (including the Landing Rd. Bridge)

(* - unable to locate specific data for this area of Landing Rd.)

Should I be of any further assistance with your project feel free to contact me via e-mail or phone at (973) 946-5016.

Regards,

Sgt. Daniel Murray Roxbury Twp. Police Dept.

Roxbury Township Police Department

Traffic Safety Bureau

Memo

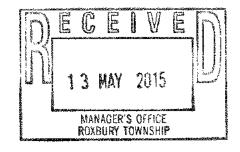
To: Sgt. D. Murray

From: Ptf. M Fournier

Date: May 13, 2015

Re: Breakdown of Landing Rd crashes

Sgt Murray,



As you a requested, here is a list of crashes that occurred in 2014 on Landing Rd in the particular areas you asked:

Landing Rd between Hart St and the Landing intersection: 16

Landing Rd at Hart St intersection: 2

Landing Rd at Kings Hwy intersection: 0

Landing Rd between Hart St and Kings Hwy: 12

Landing Intersection (Lakeside Blvd, Mt Arlington Blvd, and Landing Rd): 3

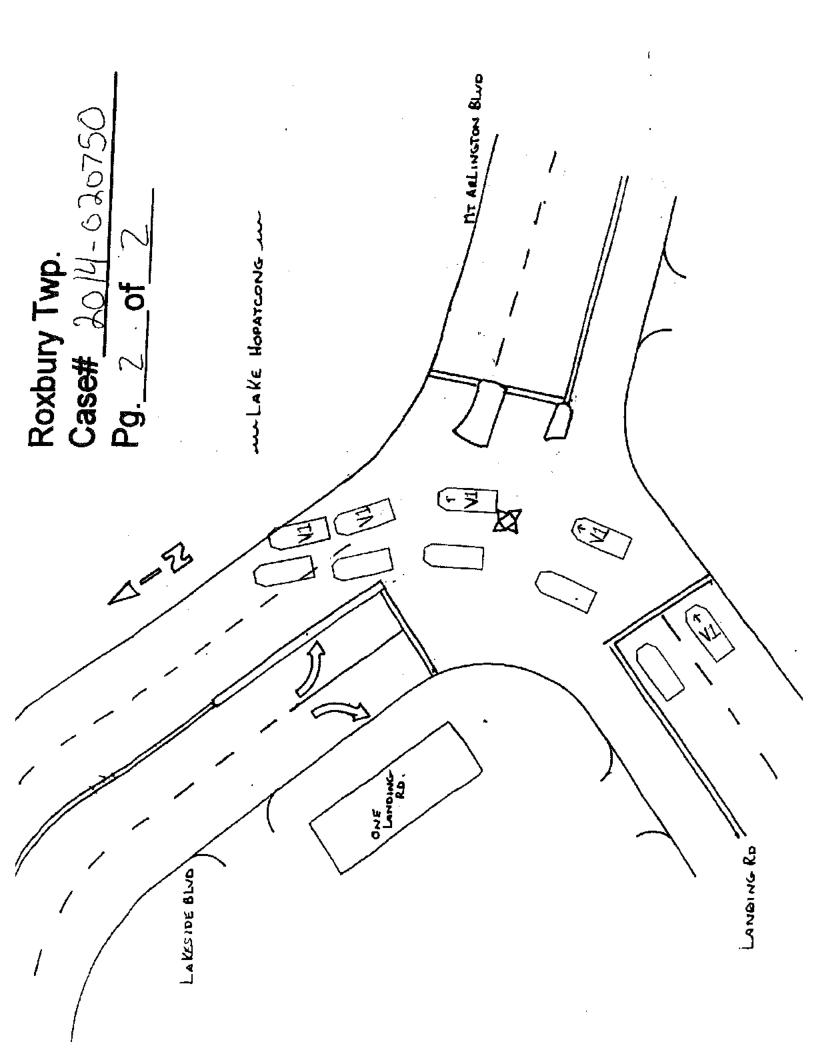
Landing Rd between Kings Hwy and Landing Intersection: 4

This information is being forwarded to the town manager along with copies of all crash reports from Landing Rd in 2014, including crashes that occurred south of Hart St.

Ptl. M. Fournier #193

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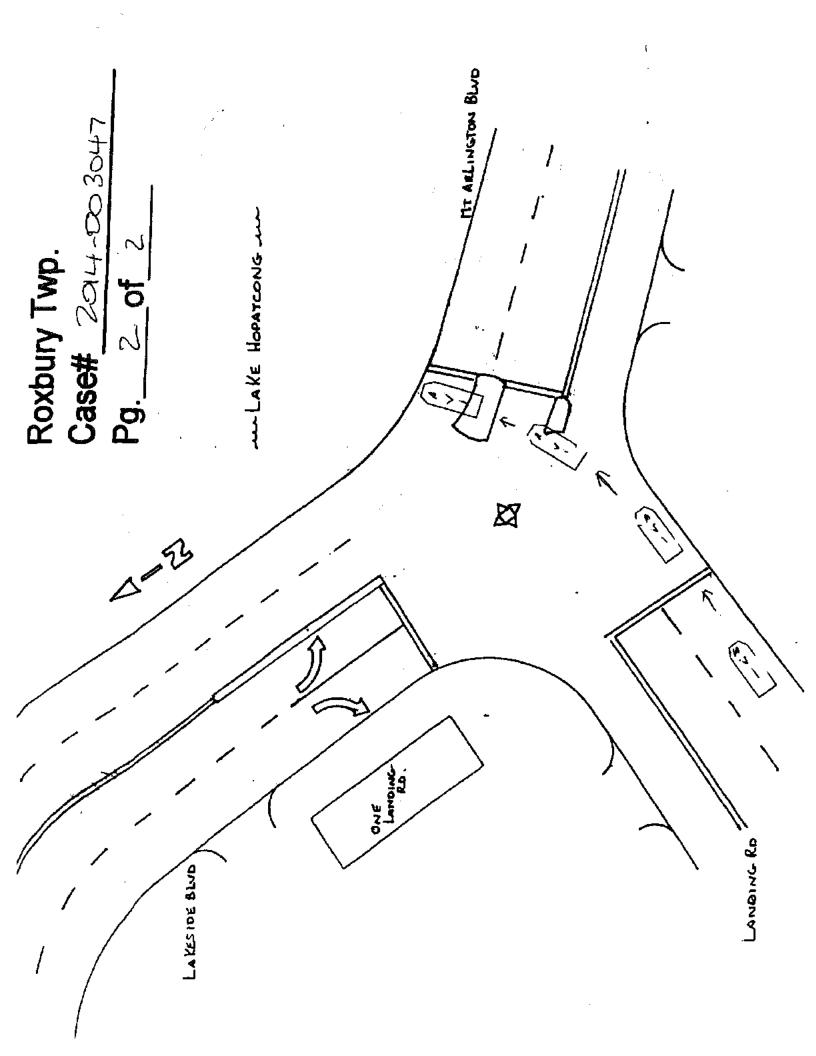


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N TR-1 (Re/10)

ROXBURY TOWNSHIP POLICE DEPARTMENT PTL J. SYLVESTER #205 CASE #2014-028801 **DECEMBER 9, 2014** MOUNT ARIMOTON BOLLEVARD LANDING ROAD 00 LAKESIDE BOULEVARD

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98	2 Police Dept of Code ROXBURY TWP 01						⊠ At I	Ros Intersactio	ed Name]. 	Dir			12	12 Route No. Suffix			13 Milepost				
01	1 3 Station/Precinct 14					⁴ □ Feet □ 5 🗍					of:			KESIDE BLY									
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100	mm.	of Cresh dd yy		Day of Wee MON		6 Tim 1989 2400		7 Munici Cod		8 T		9 To Injure	ei "	amp From	m:	20 Route/Na		□ SE	□ wa	1196			
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101 02	01	4243	-49-00-	PRENA	CVCUEY	/ DECA	ı	14R			33 Yell	- 1	•		DALCYCLIST CIR			ne Code	07				
102	26 Driver	a Firal Na	ame	Initial	LUI E DING	Lest)	T DRESP TO EMERGENCY DHIT & RUN 1 North						r'a Fira	ENCY DI	59 Sex	121							
01	27 Numb	W 6G	ROVE	STREET		SPT	CHAJ			F 30 Eye	4	57 Num				******			60 Eyes	122			
103 01	and Stree 26 City				 		State	<u></u>	Zip	4		and Stre 58 City	el		· · · · · · · · · · · · · · · · · · ·	•	State		ip	123			
104	STANHI 31 State		Xiver's ∐	cense No			NJ	33 DO8	078	34-210		O							<u> </u>	_			
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105	35 Owner			Įr	stial		Lest Na	me.		09/14		65 Owne	r's Firs	t Name	Initial		ast Name			126			
106	☐Same A		HARD				SPYCI	LAH			Į		Same As Driver										
_	36 Number and Street		ROVE S	TREET				****				66 Number and Street											
107	37 City STANHO)PE				State NJ		Z	p 7874-2	404		67 City	ur			State		127					
108	38 Make MER		Model	40 Co	lor	41 Year	г	42 Plate	No.	43 State		68 Make	Ţ	89 Model	70 Color	71 Year	72 Plate I	NO. 73	State	128a			
01	44 VIN			<u> mr</u>		2000	<u></u>	E15CU	45 8	NJ Expires		74 VIN				<u> </u>	<u> </u>	75 5	xpires	05			
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01	46 Alcohol	/Drug Te	হা					beldasi		☐ Po	lice					☐ C 78 Alcohol/Dr	beldezi		Police	58			
111	Given: Type:			Refus lood □ Ur												Given:	No □ Ye	s 🗆 R	efused	128d			
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112	49 Hazard On		riel		nce or acard											79 Hazardous			Name or	1296			
113	Board	Spill 			No.											On Boerd	Spill		Placard No.				
-		No. []	USDOT	□ Other												80 Carrier No	- Dueno	- 6		129c			
13	51 Comme	rcial Yeh	ide Weig	ht	[81 Commercia				129d			
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17	135 Cresh															<u>.J</u>				132			
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	Pro-												SI	DI		127	ľ						
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		1 0	1 0	1 -	54	F	=	_	01	09	04	-	<u> </u>	SPYCHAJ	DONNA L, 6 G	ROVE STREET	STANHOPE	∪ate & Th . NJ 0787	на от Death 4-2101	<u></u>			
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96 05	Page 1	of 1 Dec	stai	١	lew J	lerse	y Polic	e C	rash Ir	nvesti	gation	Report		Reportable	⊠ Non-Re	portable (Change Report	
97	1 Case Number 2014-010221			10 Cras		LAND	ING RD				NO	11 S PRT14	poed L 36		0631	Α	.90	118a 25
98	2 Police Dept of ROXBURY TWP		Code 01	100		⊒ At≴ni	Road tersection		_ _ _	E		Dir		12 (Route No. S	uffix 18 Speed Li		118b
	3 Station/Precinct			14		⊠ Feet ⊒ M#es			⊠s 🗆 1 16	W	of:			HELL ROAD		<u> </u>	25	1198
99 05	45-1-46-		-E18/	<u> </u>		5	7 Municipa	- dies.	8 Tol	tal [9 Total	19 □ ^{Ramp} □	To:		20 Route/Nam		□NB□E8 □SB□W8	119b
100	4 Date of Cras mm dd y 05/07/14	y Suary	of Week VED	(USG	2400 hr 1716		Code 1436	=ILY	Kille	d	Injured D	21 Latitud			22 Longitude		C3 45 C3 110	
04 101	23 Veh No	24 Policy No. 31383721-2			.,	L	72	5 Ins 34		5		54 Policy N			<u></u>		55 Ins Code 963	120 01
02	1	□PARKED □PEDE								N	6 Driver's	□PARKED (OPEDE:	TRIAN DEDA	LEST NO			121
102	26 Driver's First Nat JOSEPH		initial R		Last Nar FEOLA			·	29 Sex	_ N	MCHAEL			J	NICOT		M 60 Eyes	122
103	27 Number and Street	105 PAPAKA	TING RD						30 Eyes 2	а	7 Number nd Street	164 HUE	DSON	AVE			6	123
	28 City H OPATCON G					State NJ		Zip 078	843	H	e City I OPATC (State NJ	Zip 07843-1708	
104 02	31 State NJ	32 Driver's Licer F2605410790			i	(TO)	3 DOB 3d yy		34 Expire		1 State IJ			ense No 106726		63 DOB mm dd 9 06/10/72		124 08
105 01	35 Owner's First Na		Initia	<u> </u>		ast Nam		<u> </u>	03/16		5 Owner's			Initial		st Name	: Veri	125 07
108	☐Same As JOSI Driver		R		F	EOLA					Driver	MICHAEI		J	N	ICOTRA		128
02	36 Number and Street	105 PAPAKA	TING RD							a	6 Number nd Street	104 HUI	DSON	AVE				127
107 92	37 City HOPATCONG			N	itate Li			143-1		H	7 Cily I OPAT CI				State NJ		43-1708	04
10B	38 Make MYU	39 Model SOM	40 Calor RD		1 Year 011	- 1	42 Plate N YX B86Z		43 State NJ	F	B Make OR	69 Mode ESC	eł ;	70 Color GY	71 Year 2008	72 Plate No. RED91D	NJ	128a 26
	44 VIN 6NPEC4AC48H	305603						45 06 /	Expires 14		4 VIN FMCU93	Z98KB660	89				75 Expires 05/14	1286
109 01	46 Vehicle Remove	rd To 🔯 Driven	☐ Felt St 8	cere [npound	4? Authority		Owner Driver	7	6 Vehicle	Removed To	X D	riven 🗀 Left s	at Scene ☐ Tow In	ved 77 Author	M Owner fity ☐ Driver	128c
110						isabled		_	Police							sabled	☐ Police	- 128d
111		☐ Yes ☐ Re													Given: 🔀	No Yes		_
01	Type: 🔲 Breat Results: 0.	h □ Blood □ % □ Pendin													Type: Results: 0.	Breath Bto	od ∐ Urine Pending	129a 26
112	49 Hazardous Mate		Name Place												79 Hazardous On		Name or Placard	129b
113	Bo er d Spi⊪		No												Board []]	Spili 	No.	129c
	50 Carrier No.	USDOT 🗆 Oth	64	\neg											60 Carrier No	. Usbot	☐ Other	129d
114	51 Commercial Veh	icle Weight														1 Vehicle Weigh	nt	<u> </u>
115	□ < 10,000 lbs	00 lbs													< 10,00 10,001 •			130 06
118	> 26,001 lbs														32 Cerrier nan		···································	131
01	135 Crash Descript	ion													02 0011101 1121			132
117 01	Investigation		that ho	th ve	hicle	#1 s	and #2	we	ore trav	velino	ı Land	ino Roa	d no	rth houn	d in stop a	and on tra	offic Driver	133
	#2 was at fa																	12
	no damage.	No injurie:	were	repor	rted.													
	Patrol clear	ed.																
	136 Damage To Ot NONE STATED		VÉD															
	Oper.	137 Charg		ltiple Ch	arges	138 S	l enammu	Va. C	per.			139	8 Char	ge Multi	ple Charges	14	O Summons No.	
	141 Officer's Signal								42 Badge I	No.	i	Reviewed By	•		Badge N		144 Case Status CLOSED]
	BECK, WII							1	34			LANCH <i>O</i> w			138			
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	A 1 01		31	88	89	90	91	92	93	94	95	_			f Occupants - If KATING RD, HO	•	e & Time of Death J 07843	
	B 1 01	01 -	29	F				09	04	_		REIS, RAC	CHAL I	R. 105 PAPAK	ATING RD. HOP	ATCONG, NJ C	7843	
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	E	01 -		neri				-	_	-	<u> </u>	11001100	, micr	VILLO. 104 MC	-COCHAYE. IR.		, , , , , , , , , , , , , , , , , , , 	

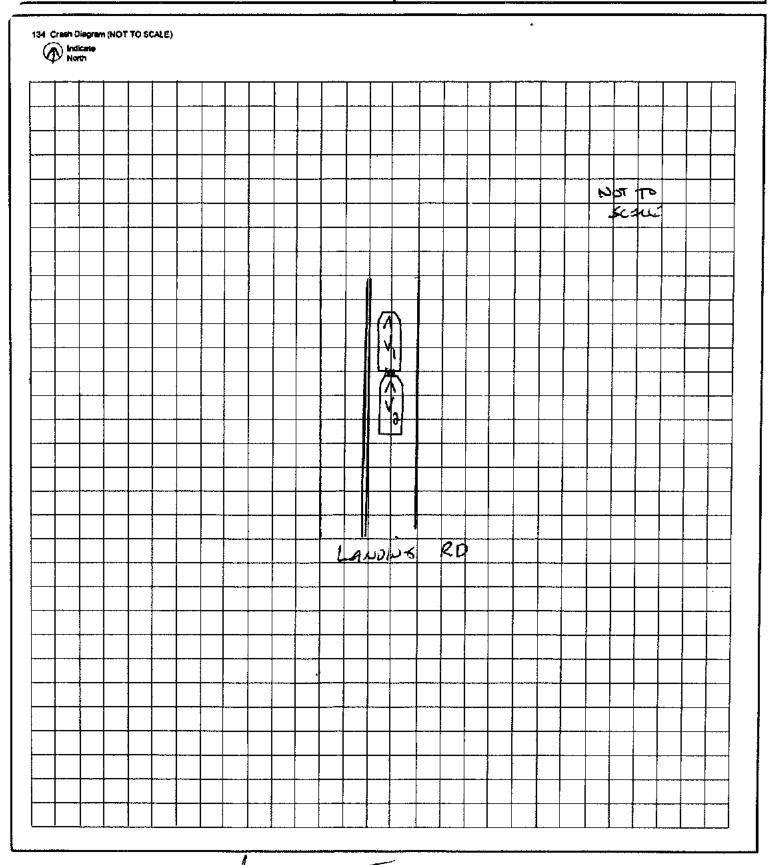
2 of 2

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

 Police Dept:
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 Code:
 143 %

 Station:
 Case No:
 14-0/022

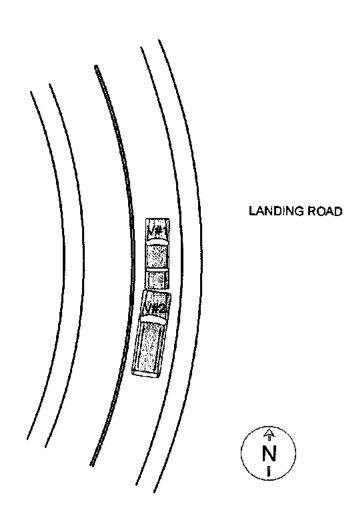


Officer's Signature

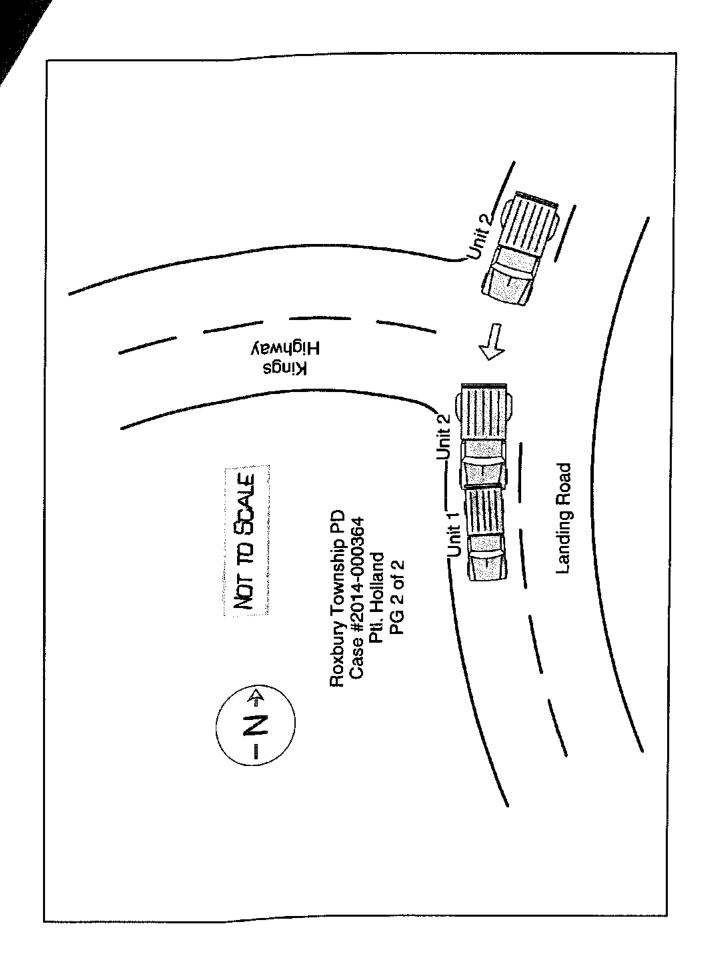
134 Badge Number

96 -	Page 1	of 1	☐ Fate	4	ı	New.	Jerse;	y Police	Cra	ash in	vestig	ation	Repo	rt [Reportab	te l	⊠ Non-Re	portable		Change Report	
97	1 Case Number 2014-011550	<u> </u>		<u></u>	10 Cra	ish red On:	LANDI	NG ROAL					N 1	1 Speed	Limit 6	0631)	A			118
01	2 Police Dept of			Code	1			Road Na	erive	a			Ďir			12 Route	No. S	uffix	1 eed Limit	3 Milapost	118
98 01	3 Station/Precinct	<u> </u>		01	2 00 14		_IAUm# ⊠TFeat	ersection wi]s □v		f:			HELL AVE					25	119
99	 -					_	□ Miles IS		16	6				□ To:	ross Road N	vama				NO 🗆 EB	0
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100 06	mm dd 05/23/14	·	FR	CF		1612		1436		_		Ven No	54 Polic				congrada	'	. 14	5 Ins Code	120
101	23 Veh No 81	24 Policy F021826	5					420			02		Y4643	39D183	OM				j	182	121
02	26 Driver's First Na			RIAN CIP Hisi		CLIST D		EMERGENO		iit a Run 29 Sex	56		First Nam		Inidal	PEDALCYC	Last Nar	ทอ	RBENCT	S9 Sex	- O
102 01	FRANCISCO 27 Number	4 TEM	PLE TRA	Ltl		ESTE	/EZ			M 30 Eyes		SAN Number	141 L	AKESI	DE BLVD		SHAW			F 60 Eyes	122
103	and Street 28 City						State		Zip	2		Street City						State	_,,,	Zip	123
104	HOPATCONG 31 State	22 000-	's License	. MA			NJ		0784	3-1738 4 Expired	HO	PATC State		Oriver's L	icense No			NJ 63	DC8	07843-1423 64 Expires	124
02	NJ		6700116				mm	dd yy 06/63	- 1	mm yy 03/17	NJ				0051614				dd yy 28/51	mm yy GB/16	0
105 01	35 Owner's First N			Înitia	si		ast Nam	ė					First Nam		inis W	al		ISI Name HAW		.1	125 0
106	Same As JON	ANKA		K		E	STEVE	:Z				Driver	WAY								126
02	36 Number and Street	4 TEM	LE TRA	HL							and	Number Street	141 (LAKE8	DE BLVD						127
107 02	37 City HOPATCONG					State N J			3-17			PATC				N	State (J		Zip 07843		0.
108	38 Make TOY	39 Mo		(D Color BK		1 Year 2007		2 Plate No. / JS60K	44 N	3 State IJ	68 HY	Make 'U	69 N		70 Color BL		Year 107	72 Pla NHF4	2X	73 State NJ	128a 20
04	44 VIN JTMBD33V676	129832					****		45 Ex 11/14			VIN SHDU4	6D47U1	37772						6 Expires	128£
109 01	45 Venide Remov		Driven 🗌	Left et S	Scene C			47	Σo	wner	76	Vehicle	Removed	To 🕮	Oriver 🗆 L	eft at Sca		red npound	77 Authority	Owner Driver	1280
110	1						mpound Disabled											sabled		Police	
01	48 Alcohol/Drug To		Refus												-		Alcohol/Co.		Yes [Refused	1280
111	Type: Brea	տ 🗆 Թև	od 🗆 Ur													1 "	ж: 🔲	Breath	☐ Blood	☐ Urine	129a
112	Results: 0.		Panding	Name	ө ог											79	Hazardous		6 □ P	Name or	1295
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114																L					7290
115	51 Commercial Ve		14														Commercia J < 10,00		VVeignt		130
-	10,001 - 26,0] 10,001 -] > 25,00		58		131
116 01	52 Carrier name		<u></u>														Carrier nen				_ O4
117	135 Crash Descri	otion					· ···· · ··	······				······································				L					132
O1	D#1 stated	he wa	s stop	ped ir	n trafi	fic w	nen V	/#2 stru	ıck l	his ve	hicle	from	behin	d. D#	2 state	d she	was sl	toppe	d in tr	affic	133
	behind V#1	when	her fo	ot slip	pped	off o	fthel	brake.	D#2	2 advi	sed h	er ve	hicle :	accel	erated f	orwar	d and	struck	(V#1	in the rear	ا
	D#1 advise medical att	d he h	ad bac	ck pai	n and	d was	s eva	luated h vehic	by i	Roxbu	ined r	ompa minor	ny #Z : dam:	First ane a	AIO. DR nd wer	FI SIGI e drive	ned an en from	KMA the s	retus Rene	ing turtner	
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	136 Damage To C	ther Prope	rty]
	Oper.	13	7 Charge	☐ Mc	Itiple Ci	herges	138 Si	ummons No	. Ор	er.				139 Ch	erge 🔲	Multiple C	harges		140	Summons No.]
	141 Officer's Sign					1			142	Badge N	40	- 1	Reviews				Badge N	lo .		4 Case Status OSED	1
	SYLVEST								20	15		PA			MARC		138		٦		
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	63 8	4 85	96	87	88	89	90	91	92	93	94	95		Name	s & Address	es of Occ	upants - if	Decease	d. Date 8	Time of Death	
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- - -NJTR-1 (R4/10) ROXBURY TWP POLICE DEPT CASE #2014-011550 PTL J.SYLVESTER #205 05/23/2014



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		·····		į	O Crash	On: L	ANDIN	G ROAD	,			SOUT	11 Speed H 3		0631	A	1.10
2 Police	Cept of			ode				Road Na	me		_	Dit		12 8	toute No.	Suffix 18 Speed Lit	13 Milepost
ROXBU 3 Station			01		14			section wif		N∐E S□W	of:		KIN	S HIGHWAY	,	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	36
ROXBU		t					Feet Miles		16				17 C	oss Road Name			
						15						19 Ra	☐ To: Promb		20 Route/Nan		🗋 NB 🗆 EB 🕒 SB 🗀 WE
4 Date	of Crass dd y		Day of We	eek.	6 T	'ime LOC hra)		funicipality Code	Y [B Total Killed	9 Tot Injure	* L	Lafitude		22 Longitud	<u>'</u>	
	/06/14	'	MUN			34	İ	1436		-							
23 Veh N	Vo 24 F								ris Cod	•	63 Veh 6		Policy No. 757504				55 ins Code 205
01	909	716462 0 RKED DIPE	DESTRIAN	N TIPEC	DALCYCLI	ST DRI	SP TO E	054 MERGENC	A DHI	T & RUN	"*		ARKED CIPED	STRIAN PEDA	LCYCLIST DRE	SP TO EMERGENC	CY OHIT & RU
26 Driver	r's First ?		fritiat		La	ist Nextk	•		29	9 Sex	56 Drive		Name	Initial	Last Na RIEGE		59 Se
HERMA 27 Numb			SIDE OF	100 A		AVIDO	VIC		N	C Eyes	RONAI 57 Numi		0 BROADY	ĀY	VIEAE	<u> </u>	60 E
and Street		14 LAKES	SHUE BL	.VU AI	P1 14				5		and Stre		* BI(VI)				2
26 City HOPAT	TCONG						iale IJ		ζφ 0 7843	-1461	58 City HOPAT	CONG	3			State NJ	Zip 07843- 1239
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21 SIMILE		9133270				1	mm d	kd yy	m	nn yy	NJ		R416966B			mm dd)	
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35 Owne	er's filst a As HE			(nıb al			t Name	AC.			⊠Same		i Italie	p.ab.		RIEGEL LA	INDSCAPE
Zusame Orive		n man						· · · ·			Orive		, ,	,	L	TC	
36 Numb	ber 13	4 LAKES	IDE BL	ND A	PT 12						66 Numb and Stre						
and Stree	 				\$ta	else		Zip			57 City	-1			State	Zφ	
HOPAT	TCONG				NJ	1		6784	3-146				69 Model	70 Color	71 Year	72 Plate No.	73 State
38 Make		9 Model COL	40 (WT	Color	41 \ 200	Year Na		Plate No. 67RH	43 NJ	State	68 Make FOR		69 Model F25	WT	2005	XD2121.	NJ
CHE 44 VIN			1 17 1		1 201	~~		T	45 Exp		74 VIN						75 Expires
1GCCS	314874	9132161						1	D9/14			1596	EC25866			wad 177	06/14 Dwns
46 Vehic	cle Rem	oved To 2	Oriven	Left	t et Scen			47 Auth	and the contract of	Owner		ie Rem	ю, трын трын трын трын трын трын трын трын	Dover 🔲 Left (mpound Author	idy Drive
1							□ Impo			□ Driver □ Police						Disabled	Police
48 Alcoh	hali (Carra 1	Test					Disal	Died [L. Police	<u>' </u>				78 AlcohoVD		2.2 (0
		Yes	□ Re	efused												No Yes	
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- On Boend	1 Sp	ł.		No.	٦										Board	Splii □	No.
	<u>`</u>															_ ∐ a. □ usdot	П о ч ь
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51 Comr	mercial V	ehide Wei	ighti													al Vehicle Welgi	ht
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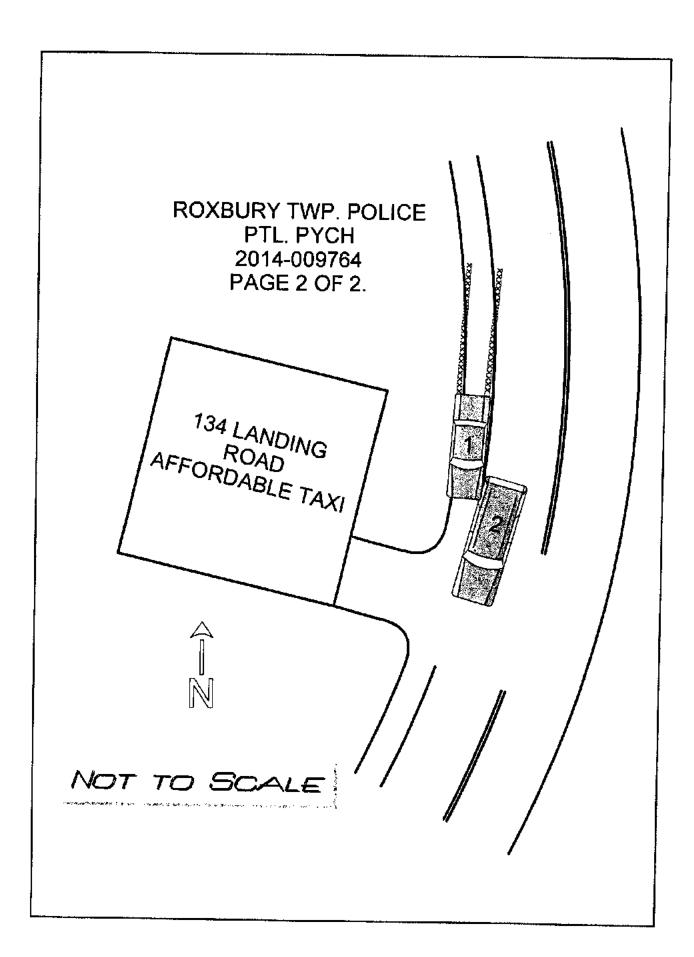
²⁰ 04	Page 1	of 1 [Falal		New .	Jerse	ey Poli	ce C	rash l	nves	tigation	Repo	rt	Reportable	□ Non-Re	eportable	□ cn	ange Report	
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-	36 Number and Street	114 WIND	SOR AVE								66 Number and Street	132	SHARP	AVE					
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NJTR-1 (R4/10)

Landing Road	Unit 1 Unit 2	Roxbury Township Police Department Pil. Pruden 2014-011260 Pg. 2 of 2 NOT TO SCALE

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NJTR-1 (R4/10)

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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept <u>forbury</u> [17] Code: 1731 Station: ______ Case No: 14-024868

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Officer's Signature

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104	28 City NEW YORK						State NY		Ζφ 1 0 4			58 City CEDAR N	(NOL	L.S.	"			State NJ		Zip 07927-1301	124
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107 —	and Street 37 City	<u>-</u>				State		Zip				and Street 67 City		······		Slate	Đ	ŽI.	5	······································	-\ ¹² '-
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117 03	135 Crash Descr	ption																			133
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103 02	28 City LONG VALLEY						Slate NJ		Zip			58 City HOPATCO	ONG.				State NJ		ip 7843-1569	123
104	31 State	32 Drive					3	3 DOB	T	34 Expin	85	61 State	6.7		Icanse No		63 DOI	8	64 Expires	124
105	NJ	G47861	U3/46				1.	dd yy 1/03/74	<u>.</u>	mm y 11/46	·	NJ	L_		8604615		mm dd 04/30/6		mm yy 12/17	125
°° 01	35 Owner's First No □Same As BRU			lniti H	i ał		Lasi Nai GJELS					65 Owner's □Same At		iwe	#nitial		ast Name CCURATE	PEST	CONTROL	08
106	Oriver 36 Number	5 CAR	RIAGE	RCAD								Driver 68 Number	143	LANDIN	VG RD	ti	NC .			126 04
107	and Street 37 City					Slate		Zij				and Street 67 City				State	Zip			127 04
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115	D 10,001 - 26,00															10,001	26,000 lbs			12
116	26,001 lbs 52 Carrier name															26,00 82 Carrier na:	ne			131
117	135 Crash Descript	tion									· · · · · · · · · · · · · · · · · · ·	·····	•	***************************************		ACCURATI	PEST COM	NIKOL	INC.	132 06
03	Driver two s	tated t	hat h	e was	trav	eling	sout	h on L	and	ling R	d. wi	hen he	activ	ated h	is left turn	signal o	turn into	Accu	rate	133
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ROXBURY TWP. POLICE 2014-02385 PTL EDMUNOS PAGE 2 OF 2. NOT TO SCALE N ENTRANCE TO 143 LANDING RD.

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103	and Street 26 City						State		7.	2	Ę	ind Street 8 City	TO TALLET	· · · · · · · · · · · · · · · · · · ·		04-4-	4	123
9 6	HOPATCONG				·· · ······		NJ		Zq 07	7843-112:) M	MENDHA				State NJ	Zip 07946 -	1701
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 -	36 Number and Street	30 WIL	LIAMS	TRAIL								6 Number	15 VALLEY	WAY				
107	37 City HOPATCONG				··········	State NJ		Zip		-1123	ė	7 City	<u> </u>		State NJ	Zip	45-1701	127
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01							Impoure		y E	Driver	'	,,,,			□ 4n	rpound Author	fity 🛭 Drive	1280
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	28,001 lbs														10,001 - > 26,00			131
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17	135 Crash Descripti	on												1 /				132 12
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NJTR-1 (84/10)

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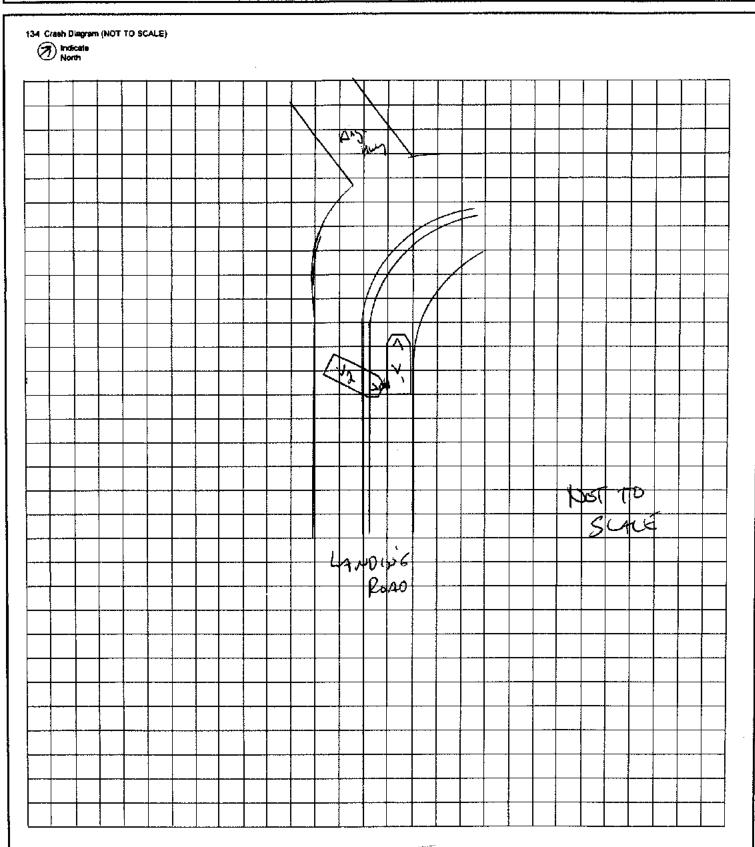
NJTR-18 (R3/06)

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

 Police Dept:
 Roy Burley
 Code:
 1436

 Station:
 Case No:
 14-14668



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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

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Station: _____ Case No: 년4-013 1기년

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New Jersey Police Crash Investigation Report Police Dept: ROXBURY TWP Code: 01

Motor Vehicle Crash Description Station: Case No: 2014-011772

Veh	Pos	Eject	Phys	A	T	Loc	Туре	Ref	Equip	Equip	Baq	Hosp	
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135 Crash Description

I spoke with Shawn Ryan O'Boyle of 15023 Ranson Road, Huntersville, NC 28078. I identified Shawn from his NC photo drivers license 23158267. O'Boyle stated he was lost and turned around in the parking lot of Landing Auto Body. He stated he started to pull out onto landing road when he heard then saw a motorcycle traveling at a high rate of speed. O'Boyle stated he and his family watched as the motorcycle lost control of his bike and dumped it in the oncoming lane of Landing Road.

He stated he then pulled out and parked on the side of the road and checked on the rider. He stated the rider became verbally abusive and then another 4 or 5 motorcycles arrived on scene and started getting hostile towards him and his wife so he left the scene after being there for five minutes. He knew he had nothing to do with the accident, and felt scared that these other motorcycle riders were blaming him when none of them had not witnessed the incident. O'Boyle advised the group he was leaving because he was not involved and he stated a short time later another motorcycle was following him on Route 80.

I inspected O'Boyle Enterprise rental (VA) VAU6903 black Chevy Tahoe which had no damage to the vehicle.

Due to the conflicting statements I am unable to determine fault for this incident.

No Diagram

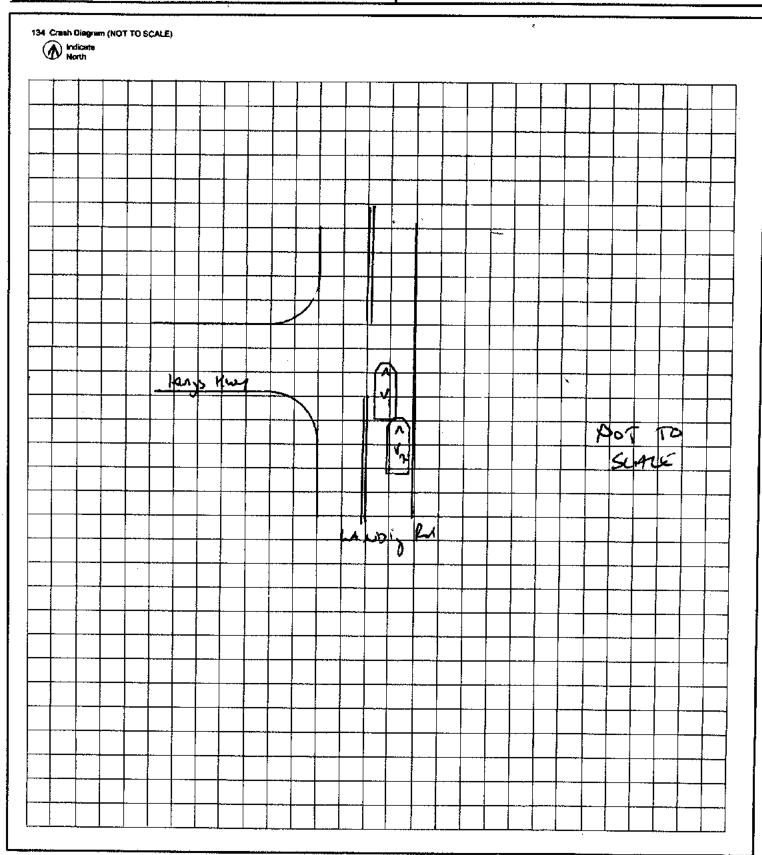
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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

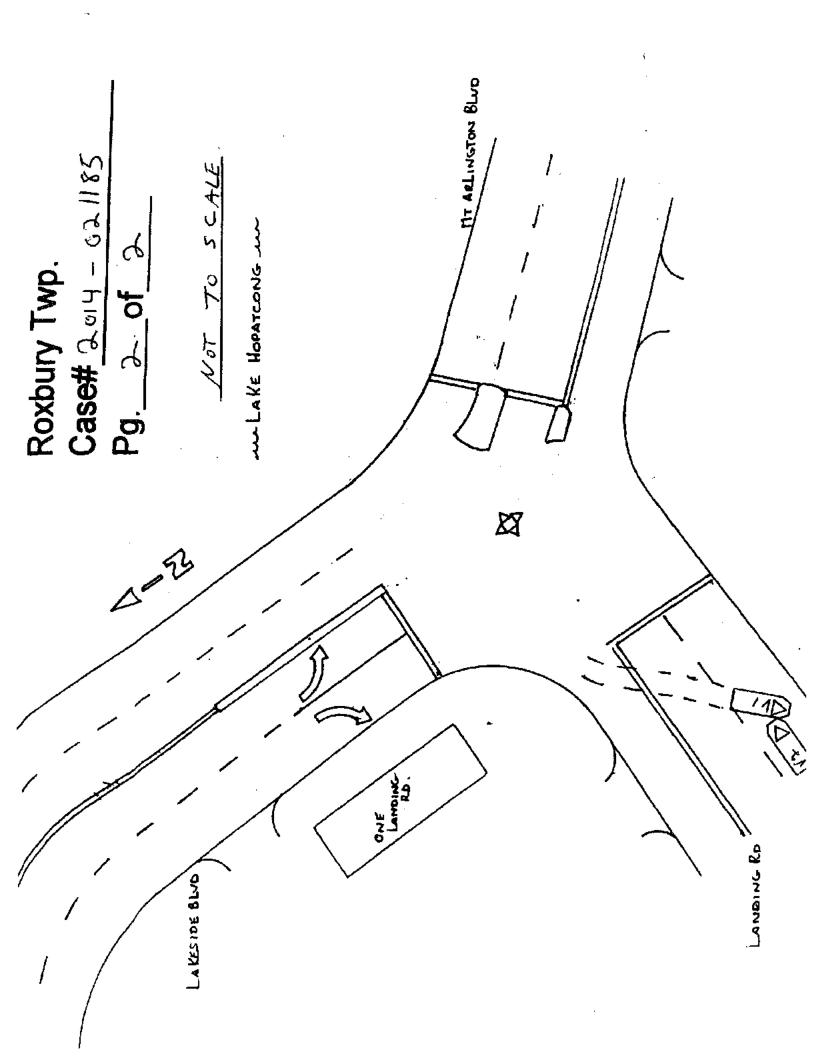
Police Dept: Residury TV | Code: 1435

Station: | Case No: 14-019521



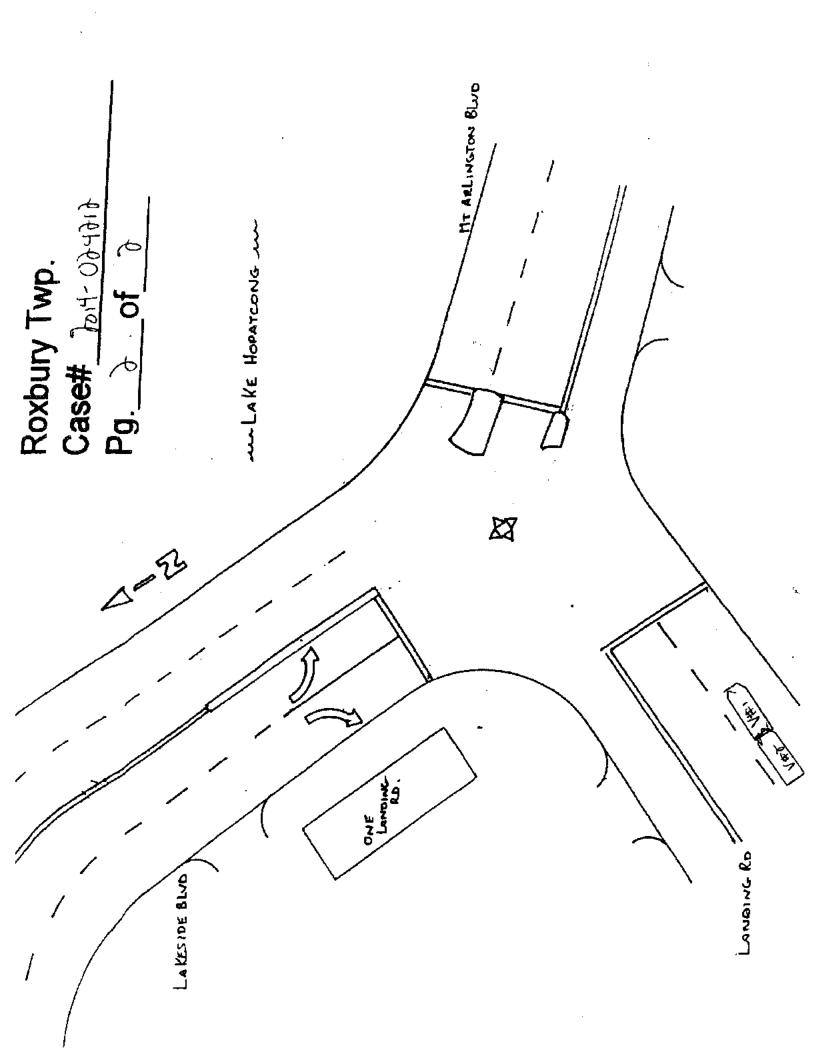
6 02	Page	. 1	of	1 🗆	Fatal		New J	lerse	y Polic	ce C	rash In	vesti	gation	Rep	ort 🗵 Rep	ortable	☐ Non-Re	portable [☐ Change Report	
7 01	1 Case	Number -021185			·	10 Cr Occu	ash rred On:	LANDI	NG RD					N	11 Speed Limit 35	06	31	Α	1.20	118a 02
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103		27 Number and Street	46-D	WEST F	RIVERS	TYX R	OAD				30 Ey	88	57 Number and Street	7 POW	ERVI	LLERD				60 Eyes	-
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104	02	HOPATCONG 31 State		er's Licer		······	· · · · · · · · · · · · · · · · · · ·		33 DOB	\top	34 Expi		MOUNTAL 61 State			icense No		NJ 63	DOB	07046-1001 64 Expires	124
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		37 City HOPATCONG					State NJ			ip 7843- 1	868		57 City BOONTON	4			State N.J		Zip 07005-	94.67	128a
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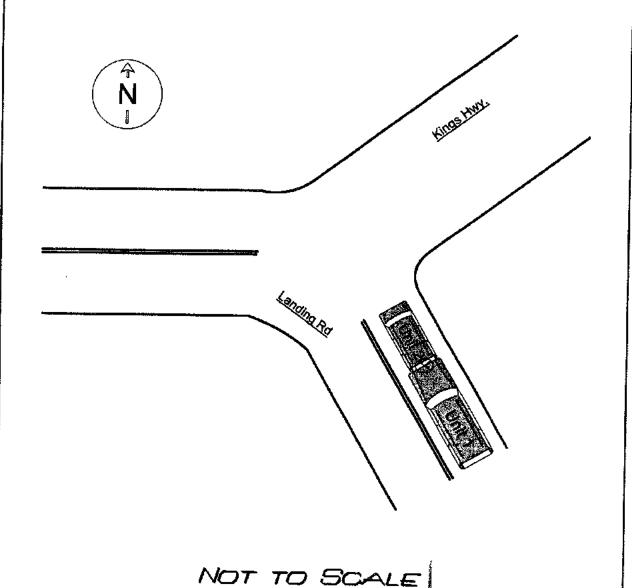
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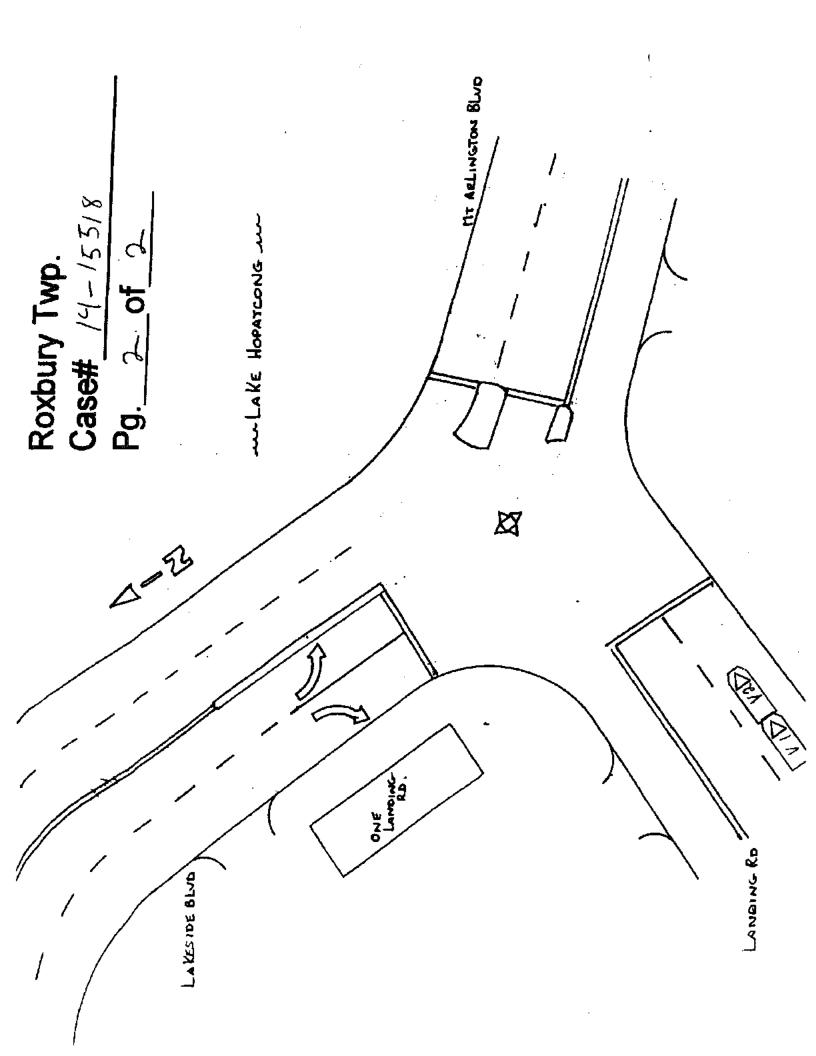
NJTR-1 (AL/10)

Case No.: 2014-004400 Agency: Roxbury Twp. Officer: Sgt. D. Murray #159



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	rea	ar of l	his ve	hicle	. Pas	senge	er two	o con	nplaii	ned of	ne	ck pa	ain an	id wa	as tra	insported	to St. Clar-	e's Hospi	tal Dove	r Car	nous for	
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NJTR-1 (R4/10)



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	2 Police	Dept of				Code	100	-		Road N	ame	<b>⊠</b> N□E			Ж	12 R	oute No. Su	iffix   18 Speed Lim	13 Milepost Il	1186
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03		rier nama		· · · · · · · · · · · · · · · · · · ·																132
117		rash Desc	•																	10
-03	Drive	er 1 st	tated	i he v	vas t	ravel	ing so	outh t	oung	d on L	.an	ding Ro	ad in	the l	eft lane ne	ar the entr	ance ram	p to Route	80 East.	133 10
	Drive	or 2 st	tated	d che	was	trave	elina s	couth	pour	nd on	Lai	ndina R	oad i	in the	right lane	near the e	ntrance ra	amp to Ko	ute so East	
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	dam	iage to	o the	righ	t pas	seng	er sid	e do	or pa	neis.	Q	n scen	e, Ma	ITISOL	Paviov, a p	oassenger	in ven. 1, void ootse	, stated in	e unver, . vehicles	
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	were	e able	to b	e dr	ven f	rom t	ne so	ene.			. hu	Driver	1							
	App	arent	caus	se or	crasi	n was	unsa	are la	ne cr	anye	: Uy	DIIAÈI	1.							
	-																			
		Priver 1 stated he was traveling south bound on Landing Ro Priver 2 stated she was traveling south bound on Landing Ro John 1 performed a right lane change and made contact with lamage to the right passenger side door panels. On scen- ladarn Pavlov, made a lane change near the intersection of the vere able to be driven from the scene.  Apparent cause of crash was unsafe lane change by Driver																		
	136 5	lamaas Ys	n Other	Dranad	<u>.</u>															1
	NON	Damage To Other Property  Damage To Other Property  ME													1			T 1A	Summons No.	-
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NJTR-1 (84/10)

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: ROX6vlY TwP. Code: 01

Station: ____

Case No: 14-019427

134 Cresh Diagram (NOT TO SCALE) 1 Indicate LANDING ROAD RT 80 € OV KAMP

96 05	5 Page 1 of 1 □ Fatal New Jersey Police Crash Investigation Report ⊠ Reportable □ Non-Reportable □ Change Re												nge Report						
97						10 Crash Occurred On: LANDING ROAD						11 Speed Limit S 45 0631				A 0.04			118a 02
99	2 Police Dapt of Code ROXBURY TWP 01			7	Road N									2 Route No. Suffix 1 18 Spee		13 Milepost		1186	
	3 Station/Precinct			V1	14	_	ol Atinio DFeet	etaection i		□s □ i		of:		UTE 80 RAMP		10 Spanu	65		
99	. 414					1	☐ Miles			16			17 C	Cross Road Name	1		□ NB		119a 25
05	4 Date of Cra			of Weak		i Time	7	Municipal	ilty	8 Tol		9 Total	Ramp   From	n:	20 Route/Nam				119b
01	mm da 09/11/14		Τ	HU		2400 hr <b>0640</b>	'S)	Code 1436		Killer O	†	Injured 1	21 Latitude		22 Longhude	,			تيا
101	23 Veh No 24 Policy No. PANJ-003576087				25 07				Ins Code		53 Veh No 02	54 Policy No. BAP644543624		<del>1</del>	55 Ins		s Code	120	
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03	27 Number and Street						30 Eyes 4		57 Number and Street	21 BROAD					60 Eyes	<b>-</b>			
	28 City			· · · ·		• • • • • • • • • • • • • • • • • • • •	State		Ζŀρ			58 City				State	<b>Z</b> i	p	123
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06	Same As Will.		OLIVIER					l'	☐Same As				SCHINDLER ELEVATOR CORP			126			
	36 Number 15 SPRUCE DRIVE and Street											65 Number 29 WHIPPANY RD and Street							05
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03	45 Vehicle Removed To ⊠ Driven ☐ Left at Scene PARKING LOT				Scene L	e I Towed 47 Authority			☐ Owner ☑ Driver			76 Vehicle F	temoved To	Oriven 🗀 Left at	at Scene Towed 77 Own				128c
10 01					· · · · · · · · · · · · · · · · · · ·		sabled			Police					D	beldss		Police	128d
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	B 2 01	01	<del>                                     </del>	39	M	<del></del> +			09	04		<del> </del>	CUBBERLY, JC	OHN A, 21 BROAD	PAVE, STANH	DPE, NJ 0787	74-3411	· · ·	$\dashv$
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NJTR-1 (84/10)

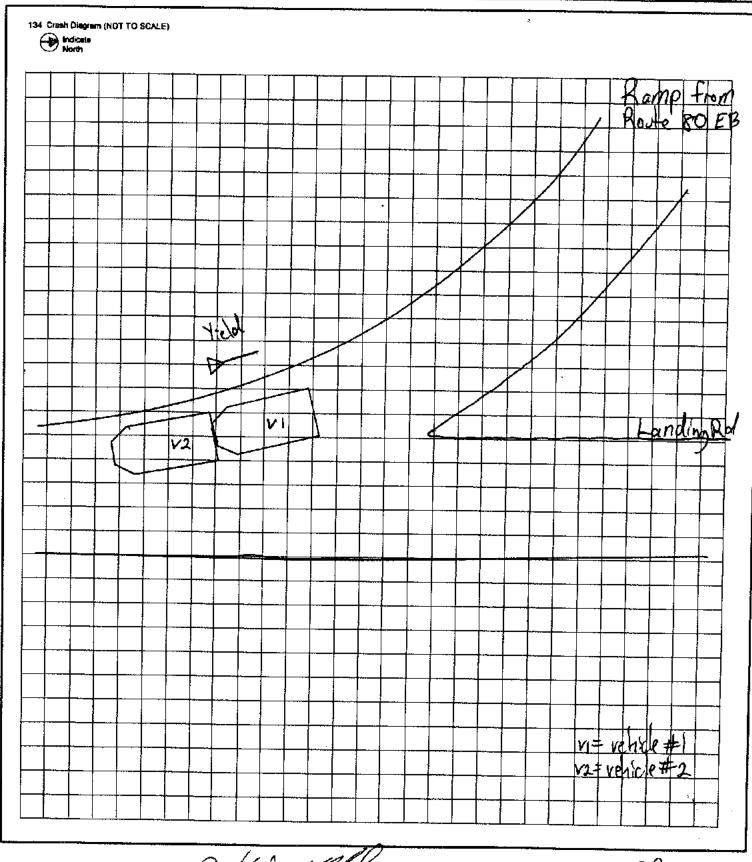
Of 2

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: Roxbury Tuy | Code: 01

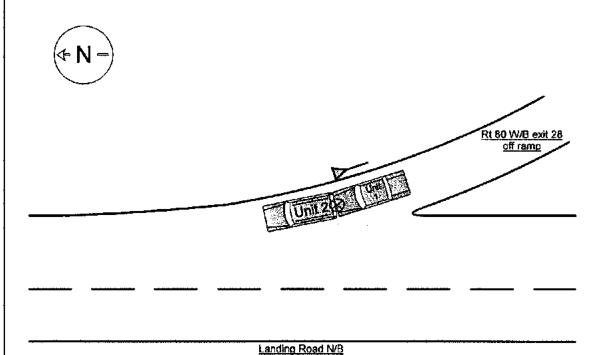
Station: Case No: 14 - 02|10|



NJTR-1B (R3/06)

96 Q2	Paga 1	of 1	☐ Fai		- 1	New J	erse	y Polic	e C	Crash Ir	ivest	igation	Repo	rt į	⊠ Reportable	Non	Reportable	□cı	hange Report	
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113																				129c
14			[] Othe	r												80 Carrier	No. Quedo	т Цо	ither	1290
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	the rear end stopped at t	he end	l of th	ne ram	p to	yleld i	to tra	affic. V	Vhil	le stop _l	ped l	her vet	icle v	as st	truck in th	e rear b	y the front	end	of veh.	
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Case No.: 2014-021660
Agency: Roxbury Twp.
Officer: Sgt. D. Murray #159



NOT TO SCALE

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NOT TO SCALE LANDING RD ROXBURY TOWNSHIP 2014-021333 PTL M. FOURNIER PAGE 2 OF 2 ROUTE 80 OFF RAMP

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New Jersey Police Crash Investigation Report

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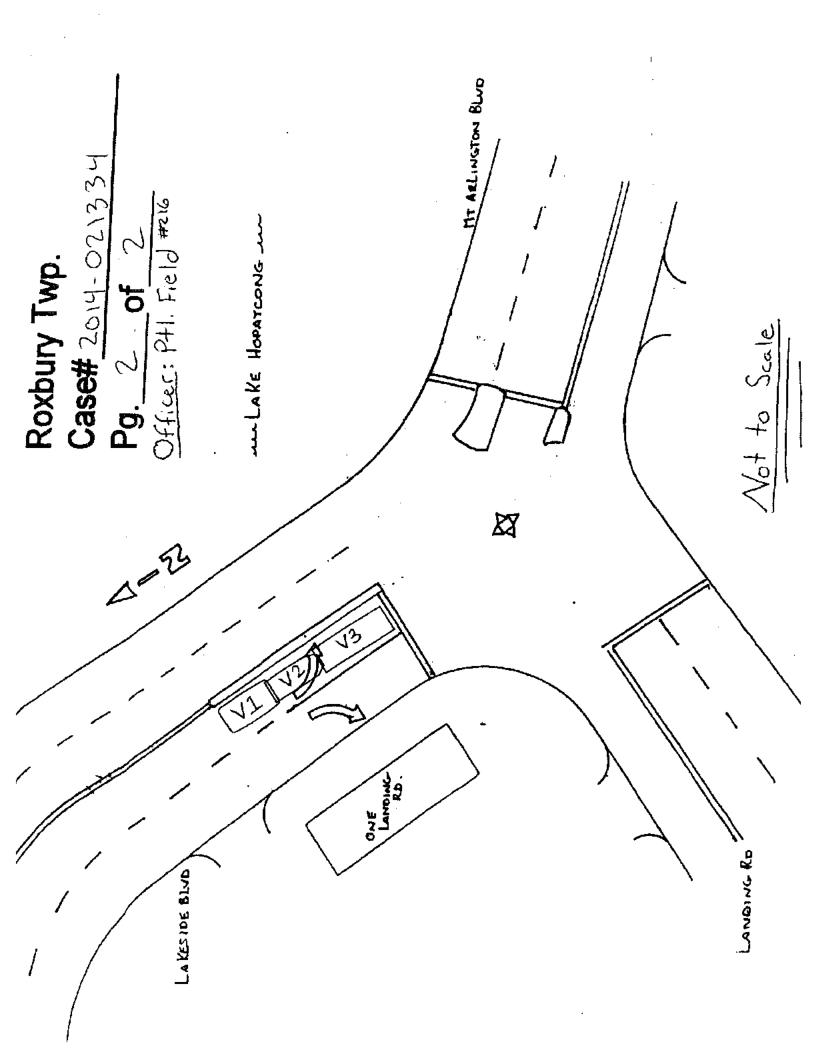
Officer's Signature

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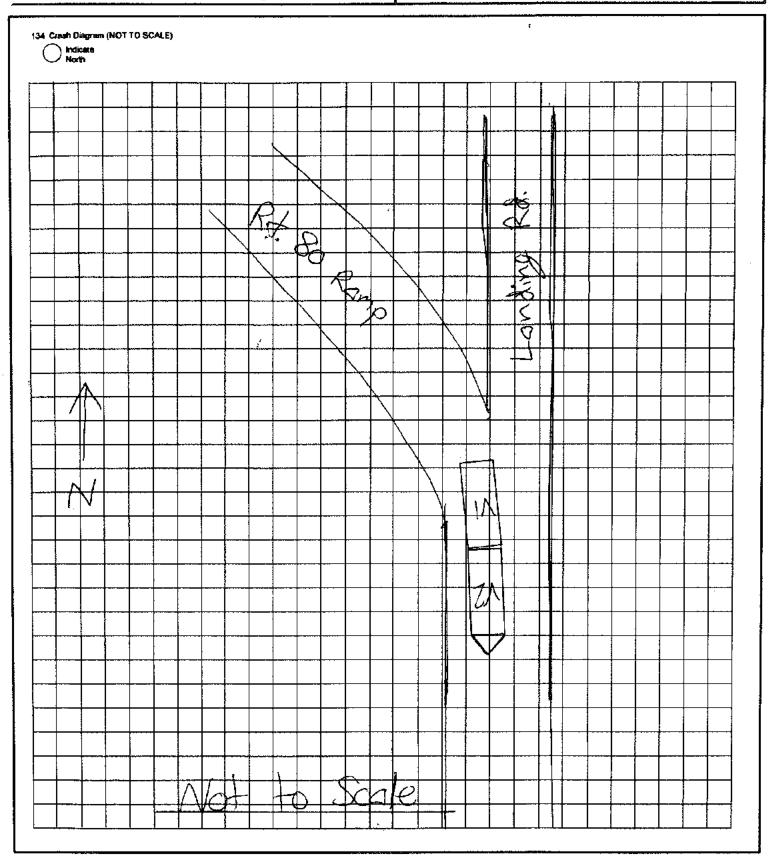
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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

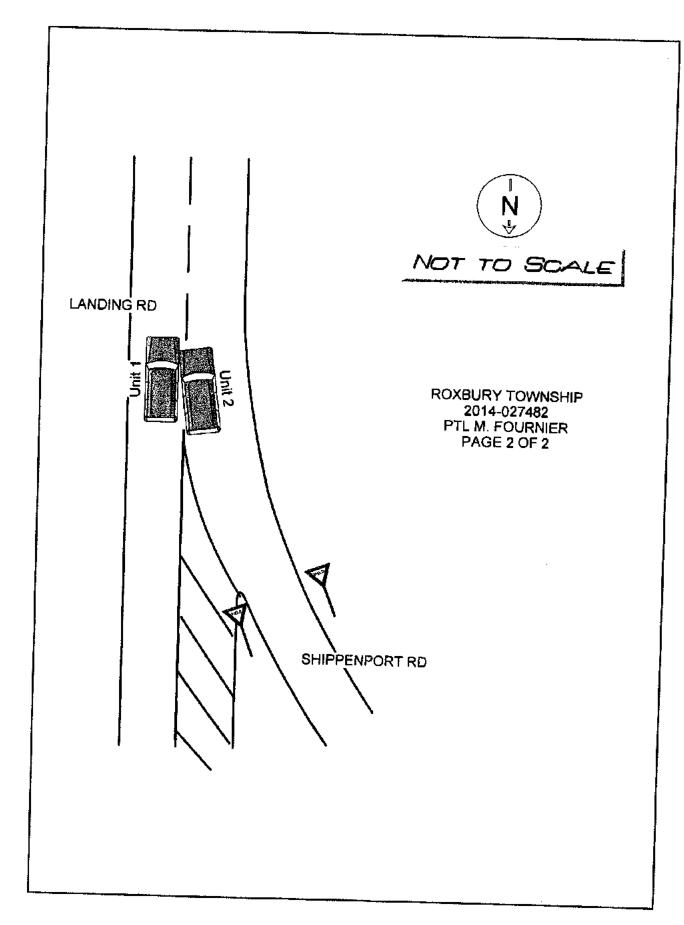
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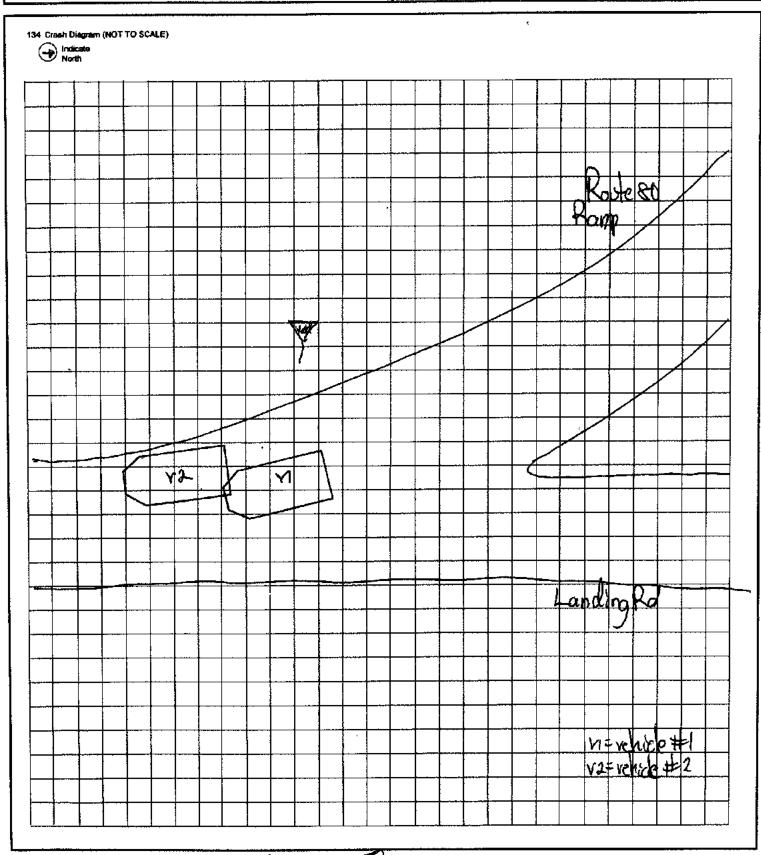
40 2 of 2

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: <u>Roxbury Tup</u> Code: <u>01</u>

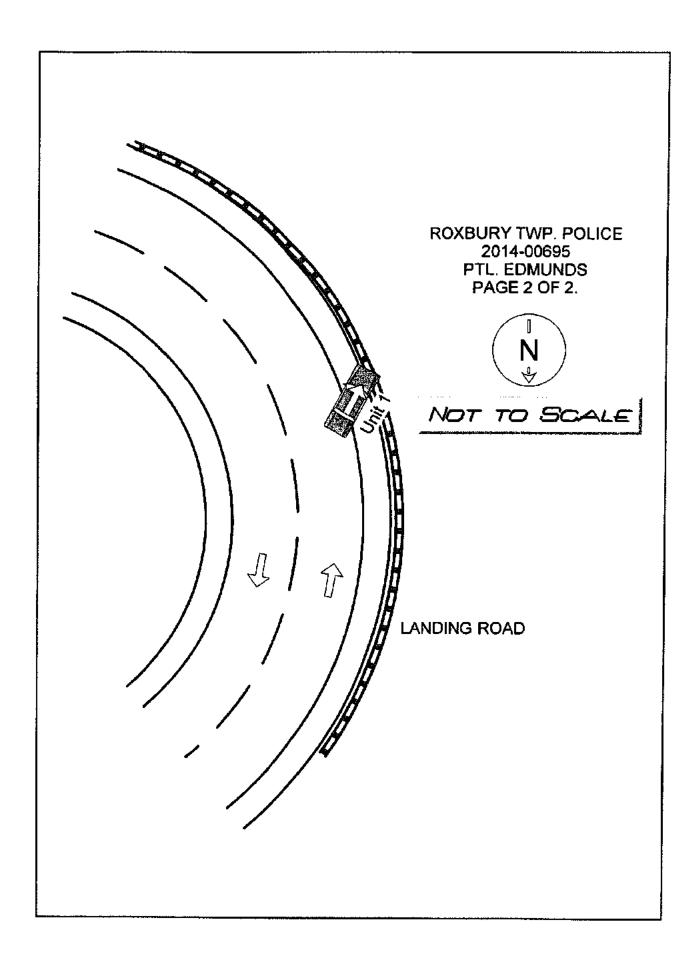
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		Moto	or Vehicle Crash Des	scription	Station:	Case No: 2014-023423

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135 Crash Description

OBSERVED TO V#1 WAS CONSISTENT WITH A TIRE MAKING CONTACT WITH A VEHICLE. V#1 ALSO COULD HAVE MADE CONTACT WITH MUD FLAP ON VEHICLE.

D#1 SAID FOLLOWING THE COLLISION, V#2 DID NOT STOP AND CONTINUED NORTHBOUND ON LANDING RD. V#2 PROCEEDED TO TRAVEL UNTIL IT PULLED INTO ENRITE GAS STATION IN HOPATCONG JURISDICTION (40 HOPATCHUNG RD). D#1 CONFRONTED D#2, AND SAID D#2 WAS DISMISSIVE OF HER PRESENCE. D#1 SAID SHE CALLED HOPATCONG POLICE, AND WAS ASSISTED BY SGT WOJTACH.

ACCORDING TO D#1, SGT WOJTACH INITIALLY DID NOT OBSERVE DAMAGE TO V#1 DUE TO SUN GLARE COUPLED WITH THE MINOR DAMAGE. SGT WOJTACH ADVISED SINCE HE BELIEVED AT THE TIME THERE WAS NO COLLISION HE DID NOT RETRIEVE D#2 INFORMATION, ONLY V#2 REGISTRATION. D#1 LEFT THE SCENE AND WENT HOME, WHERE SHE CONFIRMED THERE WAS DAMAGE TO HER VEHICLE. SHE RETURNED SHORTLY AFTER AND SHOWED THE DAMAGE SHE OBSERVED TO SGT WOJTACH, WHICH HE DOCUMENTED IN AN OPERATIONS REPORT (SEE HOPATCONG PD CASE 14-11606).

I ASCERTAINED V#2 INFORMATION FROM SGT WOJTACH'S REPORT, AND MADE CONTACT ON OCTOBER 6 WITH THE COMPANY V#2 WAS REGISTERED TO, JERSEY BULK CARRIERS CO INC. I SPOKE WITH A MALE ON THE PHONE WHO IDENTIFIED HIMSELF AS BORIS WHO ALSO SAID HE WAS THE OWNER OF THE BUSINESS. I ADVISED BORIS THE REASON I WAS CALLING. BORIS IDENTIFIED HIMSELF AS THE DRIVER OF V#2 BUT REFUSED TO PROVIDE ANY FURTHER INFORMATION. HE SAID HE HAD JUST SPOKEN TO THE POLICE ABOUT THE INCIDENT AND THEY LET HIM GO. I EXPLAINED TO BORIS THE REASON THE DAMAGE WAS NOT SEEN (SUN GLARE, MINOR DAMAGE) IN HOPATCONG. HE BECAME IRATE AND PUT ME ON HOLD FOR OVER TEN MINUTES UNTIL I DISCONNECTED. I ATTEMPTED TO CALL BORIS AGAIN ABOUT TWO HOURS LATER BUT WAS UNABLE TO MAKE CONTACT. I ALSO ATTEMPTED MAKE CONTACT ON OCTOBER 7 AND OCTOBER 11 WITH SIMILAR RESULTS.

I TRAVELED TO THE ADDRESS IN WHICH THE BUSINESS WAS LISTED - 132 ROUTE 46 IN BUDD LAKE NJ 07828. THAT ADDRESS WAS A DELTA GAS STATION, BUT THERE DID NOT APPEAR TO BE ANY AREA WHERE A TRACTOR TRAILER COULD BE PARKED. THE GAS STATION ATTENDANT STATED HE DID NOT KNOW A MAN NAMED BORIS NOR DID HE KNOW ANYTHING ABOUT JERSEY BULK CARRIERS. I THEN CONTACTED MOUNT OLIVE TAX ASSESSOR IN ATTEMPT TO LOCATE MORE INFORMATION ABOUT THE OWNER OF THE COMPANY. I WAS ADVISED BY THE ASSESSOR THE OWNER OF THE BUSINESS FROM THEIR RECORDS WAS LISTED AS PETRO STOPPING CENTER, ANOTHER BUSINESS NAME. THE MT OLIVE TAX ASSESSOR DID NOT HAVE ANY OTHER INFORMATION TO PROVIDE.

ON OCTOBER 15, I WAS ABLE TO MAKE CONTACT WITH BORIS AGAIN. I EXPLAINED TO BORIS HE MAY NOT HAVE NOTICED THERE WAS A COLLISION WITH V#1 BECAUSE OF THE SIZE OF HIS VEHICLE. I ADVISED BORIS IF HE LET ME SEE V#2 I COULD SHOW WHERE THE DAMAGE WOULD BE. BORIS SAID HE WOULD COME TO ROXBURY POLICE HEADQUARTERS ON OCTOBER 16. BORIS DID NOT COME TO POLICE HEADQUARTERS ON OCTOBER 16.

WITH ASSISTANCE FROM DET NIEMYNSKI OF THIS DEPARTMENT, I LOCATED ANOTHER OWNER OF JERSEY BULK CARRIERS THROUGH AN OFFLINE SEARCH (TLO). I SPOKE WITH GENE RABKIN WHO DID IDENTIFY HIMSELF AS PART OWNER OF JERSEY BULK CARRIERS. I EXPLAINED TO MR RABKIN I WAS ATTEMPTING TO MAKE CONTACT WITH BORIS ABOUT THE CRASH. MR RABKIN BECAME IRATE AND TOLD ME HE HAD ALREADY SPOKEN TO BORIS ABOUT THE CRASH AND SAID "IT WAS ONLY A FENDER" INDICATING THERE WAS CONTACT

## MADE BETWEEN V#1 AND V#2. MR RABKIN WENT ON TO SAY HE WOULD NOT HELP ME.

ON NOVEMBER 5, WITH THE ASSISTANCE OF MORRIS COUNTY DISPATCH, I REQUESTED V#2 REGISTRATION (XK263G) BE CHECKED IN ATTEMPT TO SEE IF ANYONE WITH THE NAME BORIS WAS LISTED AS A DRIVER IN THE PAST. DISPATCH ADVISED IN 2004 D#2 WAS LISTED AS THE DRIVER IN V#2 FOR A MOTOR VEHICLE VIOLATION. D#2 AT THE TIME POSSESSED A NEW YORK DRIVER LICENSE. ON NOVEMBER 8, DISPATCH SENT A MESSAGE TO NEW YORK MOTOR VEHICLE REQUESTING A DRIVER LICENSE PICTURE OF D#2 FOR POSITIVE IDENTIFICATION BY D#1 AND SGT WOJTACH. AS OF NOVEMBER 27 THERE HAS BEEN NO RESPONSE.

ON DECEMBER 15, 2014 I RECEIVED MENTIONED NEW YORK DMV PHOTOGRAPH OF BORIS PLOTKIN, SUSPECTED DRIVER #2. I SHOWED PHOTO OF PLOTKIN TO D#1 AND SGT WOJTACH OF HOPATCONG POLICE. BOTH STATED PLOTKIN WAS NOT THE DRIVER.

BOTH D#1 AND SGT WOJTACH DESCRIBED D#2 AS A HEAVY SET MALE, DARK COMPLEXION, IN HIS FORTIES. THEY ALSO STATED D#2 SPOKE WITH WHAT THEY DESCRIBED AS A RUSSIAN ACCENT.

AT THIS TIME SINCE D#2 COULD NOT BE IDENTIFIED, SUMMONSES WILL BE ISSUED TO GENE RABKIN, WHO IDENTIFIED HIMSELF AS PART OWNER OF JERSEY BULK CARRIERS. MR RABKIN STATED TO ME ON THE PHONE HE HAD SPECIFIC KNOWLEDGE THAT V#2 WAS INVOLVED IN THE CRASH. SUMMONSES SENT VIA MAIL TO 335 PRESTON AVE, STATEN ISLAND NY 10512 (ADDRESS LISTED ON MR RABKIN'S DRIVERS LICENSE) THE FOLLOWING SUMMONSES WERE ISSUED::39:4-129 AND 39:4-130.

I ADVISED D#1 TO SAVE PHOTOGRAPHS OF DAMAGE TO HER VEHICLE FOR COURT/INSURANCE PURPOSES.



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New Jersey Police Crash Investigation Report Police Dept: ROXBURY TWP Code: 01

Motor Vehicle Crash Description Station: Case No: 2014-001400

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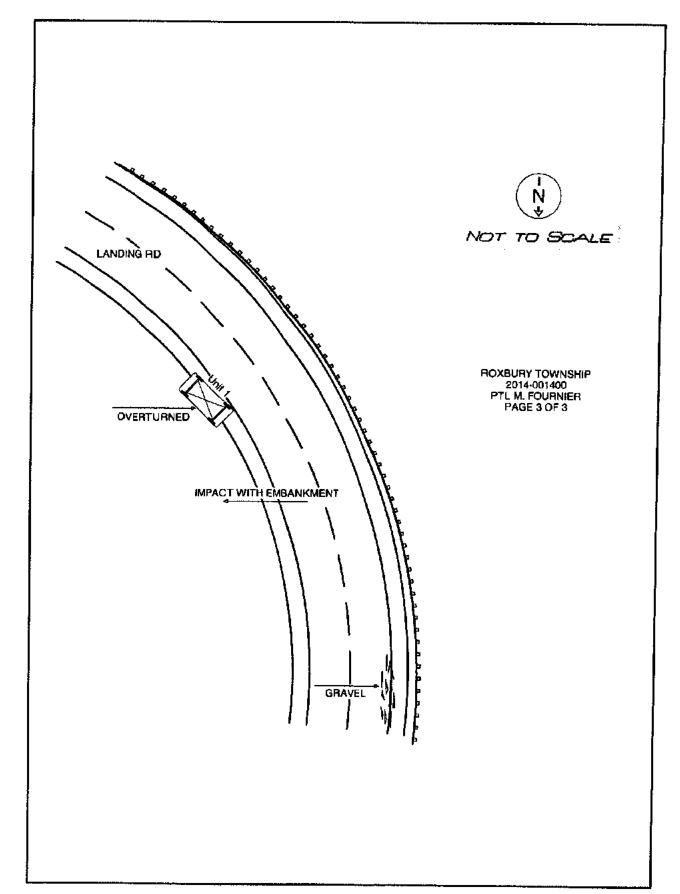
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135 Crash Description

AS THOUGH D#1 LOST CONTROL ON COMBINATION OF LOOSE GRAVEL/WET ROADWAY.

THERE WERE NO OTHER COLLISIONS IN THE AREA ON THIS DAY.

PASSENGER OF V#1 SUSTAINED INJURY TO HER NECK FROM SEATBELT HOWEVER SHE REFUSED MEDICAL ATTENTION AT THE SCENE.



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New Jersey Police Crash Investigation Report Police Dept: ROXBURY TWP Code: 01

Motor Vehicle Crash Description Station: Case No: 2014-001031

	1					-			r <u></u>				
Veh	Pos In/On	Eject	Phys Cond	Age	Sex	Loc	Type i	Ref Med		Efuip Used		Hosp Code	
63	84	85	66	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants + If Deceased, Date & Time of Death
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135 Cresh Description
BEFORE COMING TO REST. D#1 COMPLAINED OF HEAD PAIN (BUT REFUSED MEDICAL ATTENTION AT THE SCENE.

WI AT REST NOT TO SCALE GUARD RAIL DAMAGE ROXBURY TOWNSHIP 2014-001031 PTL M. FOURNIER PAGE 3 OF 3

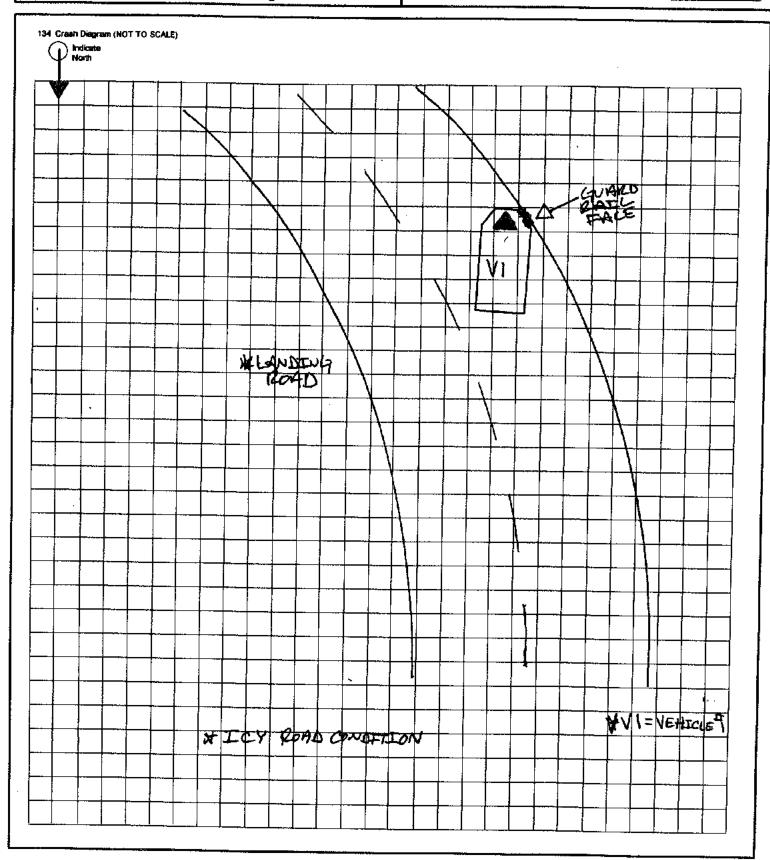
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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: Laxeur Ting. PD. Code: 01436 Station: Case No: 2014 - 60 1098



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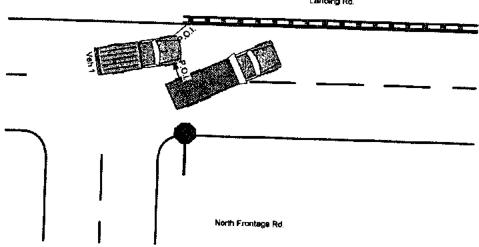
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NOT TO SCALE

Landing Rd.



New Jersey Police Crash Investigation Report 03 1 🗆 Fatal Page Reportable ⊠ Non-Reportable ☐ Change Report 1 Case Number 10 Crash 11 Speed Limit Occurred On: LANDING RD 2014-008874 61 SOUTH 36 0631 .02 2 Police Dept of Code Road Name Dir 12 Route No. 13 Milepoet ROXBURY TWP 01 500 186 At Intersection with DNDE 18 Speed Limit 01 3 Station/Precinct **⊠** Feet ⊠s⊡w of: N. FRONTAGE RD ☐ Miles 18 17 Cross Road Name 19a ☐ fo: 05 15 Ramp From: 4 Date of Crash 5 Day of Week 8 Time Municipality 20 Route/Name 8 Total 9 Total ☐ se ☐ we men det MON 100 (use 2400 hrs) Code Killed Injured 21 Latitude 22 Longitude 01 04/21/14 1148 1436 00 00 24 Policy No. PAA00002815271 20 53 Veh No 54 Policy No 55 Ins Code 101 01 963 02 SELF INSURED 02 OPARKED OPEDESTRIAN OPEDALCYCLIST GRESH TO EMERGENCY DHIT & RUN OPARKED □PEDESTRIAN □PEDALCYCLIST □RESP TO EMERGENCY □HIT & RUN Initial Last Name 29 Sex 56 Driver's initial Last Name 59 Sex ALBERT JOSEPHS JOSE 57 Numbe 01 MARRERO м 22 27 Number 280 LACKAWANNA DRIVE 30 Eyes 292 EAST 6TH ST 60 Eves and Street 103 and Street 01 28 City State 58 City State ANDOVER NJ 07821 CLIFTON NJ 07011-1740 32 Driver's License No 33 DOB 34 Ехрігая 61 State 63 DOB 62 Driver's License No 02 64 Expires NJ J66970176801382 mm dd yy mm yy NJ M06644107408582 dd yy mm 01/16/38 06/14 08/23/56 12/14 35 Owner's First Na Initial 85 Owner's First Name 99 □Same As PAULINE JOSEPHS 01 Same As TRANSPORTATION DEPT OF Oriver Driver 36 Numbe 288 LACKAWANNA DR PO BOX 600 BLDG1 04 and Street 37 City and Street 67 City 107 State State **BYRAM** NJ 07821-4113 TRENTON 04 08625 NJ 38 Make 40 Color 47 Year 42 Plate No. 60 Make 43 State 69 Model 70 Color 72 Plate No. 71 Year 282 TOY HKG NJ GN UDU34F 2001 INT TRUCK 1990 TD4306 NJ 12 04 44 VIN 45 Expires 74 VIN 5 Expire JTEGF21A910023323 128b 1H8GLGURELH281404 05/16 46 Vehicle Removed To 🖾 Oriven 🗌 Left at Scene 🗋 Towed 76 Vehicle Removed To 🔯 Driven 🗌 Left at Scene 🔲 Towed 23 Owner Authority [] Impound **⊠** Driver Authority ☑ Driver 128c 10 DeldariC 🗔 _ Police Police ☐ Disabled 01 48 Alcohol/Drug Test 284 78 Alcohol/Drug Test ⊠ No ☐ Yes ☐ Refused Given: ⊠ No ☐ Yes ☐ Refused -Туре: 02 ☐ Breath ☐ Blood ☐ Urine Type: □ Breeth □ Blood □ Urine 1294 % Pending Results: (). Results: 0. % 🔲 Pending 12 112 49 Hazardous Material Name or 79 Hazardous Material 29b Оп Placard Board No. 113 Board 29c 61 п Ò 50 Cerrier No. USDQT Cher 80 Carner No. USDOT Other 29a 51 Commercial Vehicle Weight 81 Commercial Vehicle Weight ☐ < 10.000 lb6 (10,000 lbs ☐ 10,001 - 26,000 bs 05 10,001 - 26,000 lbs 99 26,001 lbs. 26,001 lbs 116 131 52 Carrier name 03 82 Carrier name 99 135 Crash Description 32 117 03 D1 STATES HE WAS TRAVELING SOUTH ON LANDING RD WHEN A STONE KICKED UP FROM V2 CAUSING HIS 133 WINDSHIELD TO CRACK. D2 STATED HE DROPPED OFF EQUIPMENT AT THE LANDING RD MEDIAN AND _ PULLED ONTO LANDING RD TRAVELING SOUTH AND DIDN'T REALIZE A STONE KICKED UP. V2 IS A NJ STATE DOT TRUCK WHICH WAS TOWING A STATE DOT TRAILER (NJ REG#TD10766). THE STATE DOT TRUCK AND TRAILER ARE SELF INSURED THROUGH THE STATE OF NJ. 136 Camage To Other Property NONE Oper, 137 Charpe Multiple Charges Oper. 139 Charge Multiple Charges 40 Summors No. 01 141 Officer's Signature 142 Badge No. 143 Reviewed By

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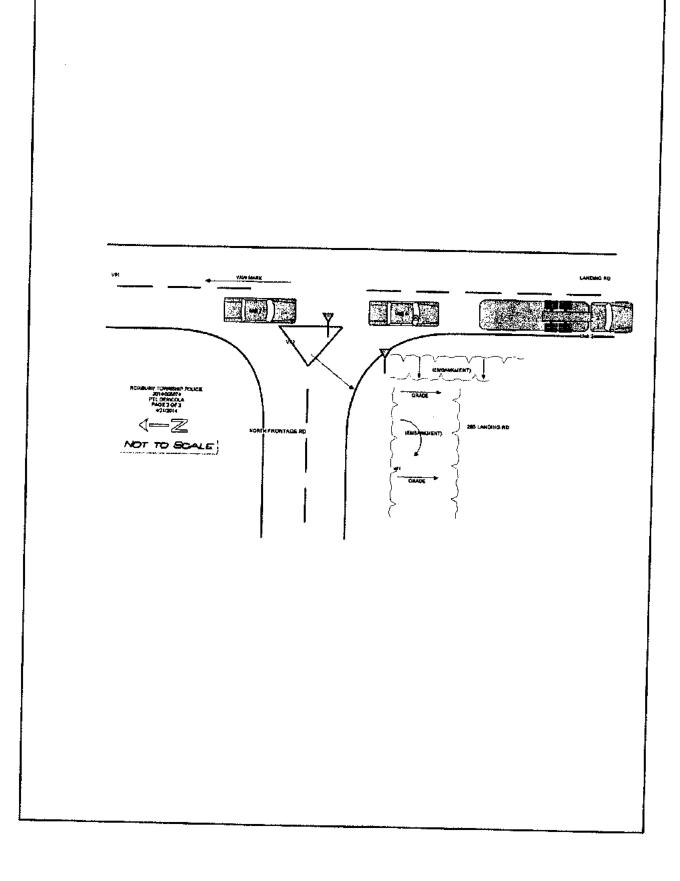
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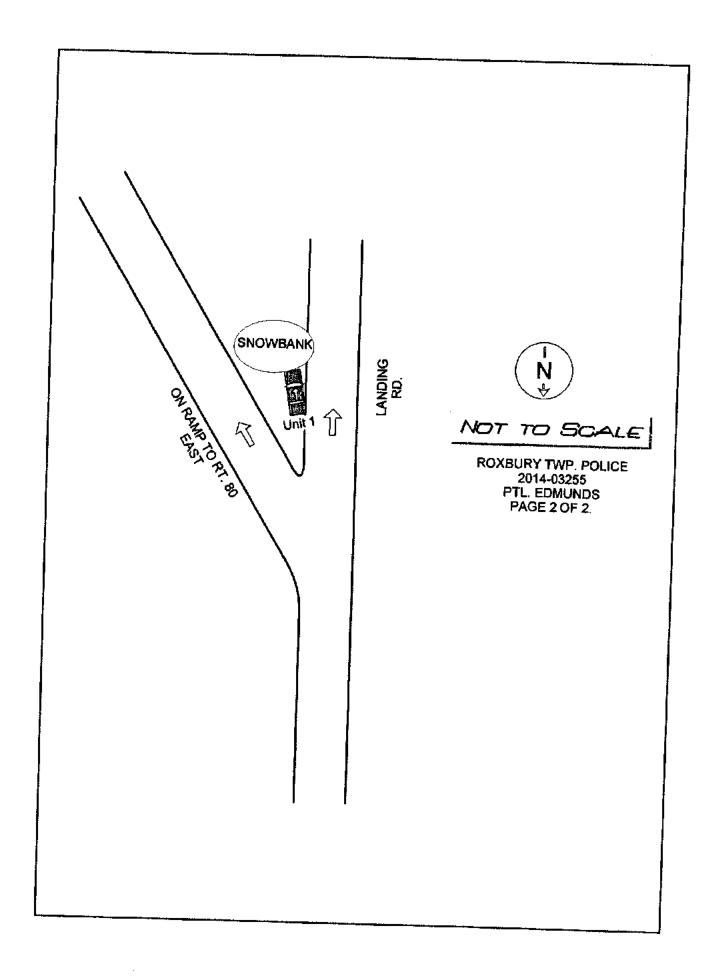
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ROXBURY TWP 2014-00 3047 PTL M. FOURNIER PAGE 2 OF 2

ROUTE 80 EB OFF RAMP

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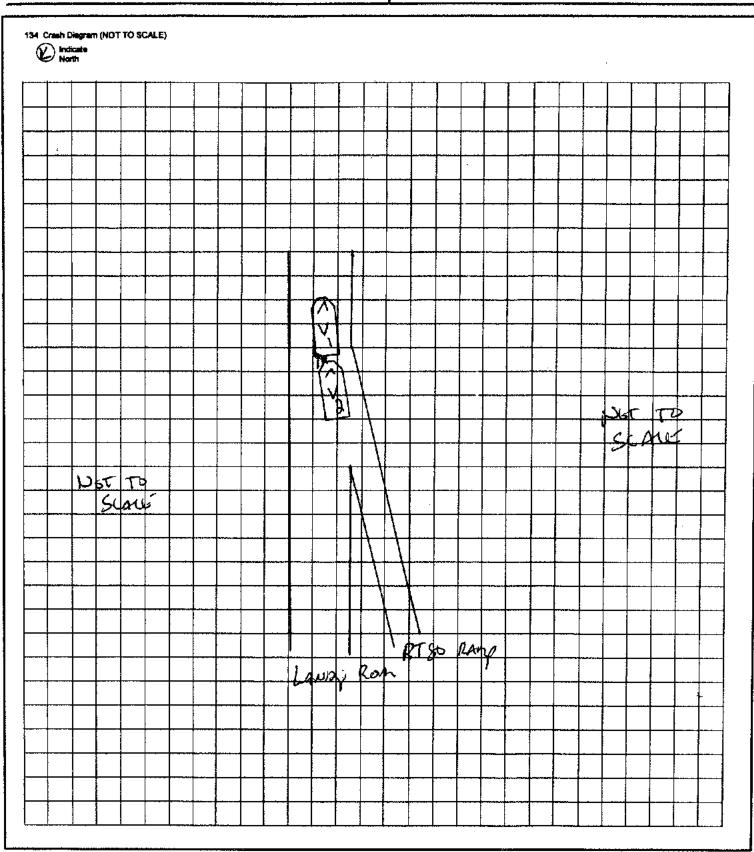
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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram



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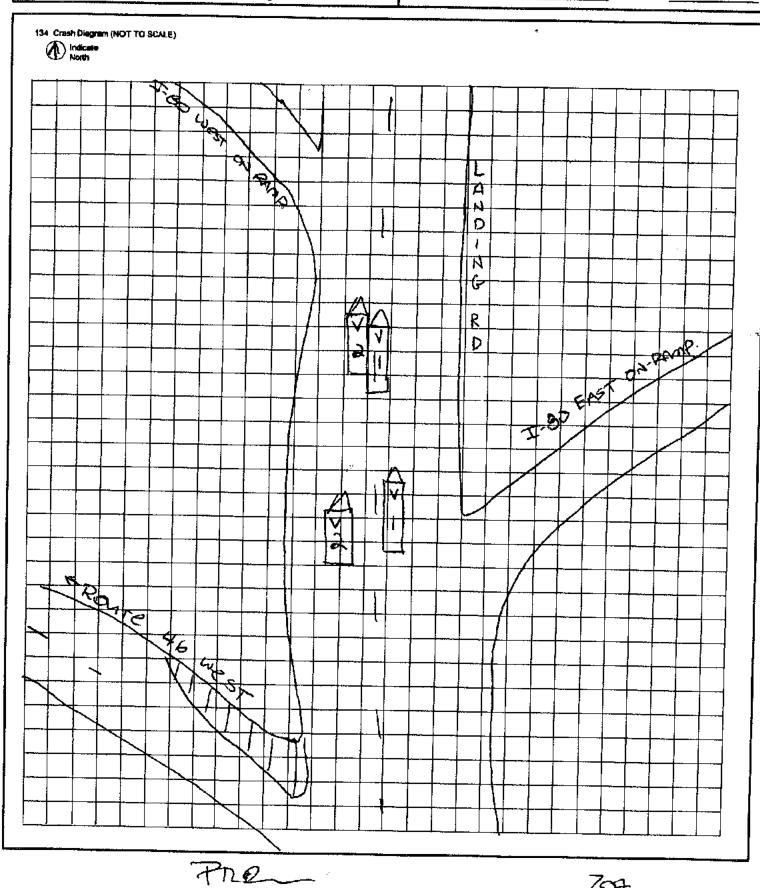
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New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: ROXBURY TWP Code: 01 Case No: 14-010462 Station:



NJTR-19 (R3/06)

Officer's Signature

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NJTR-1 (R4/18)

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New Jersey Police Crash Investigation Report	Police Dept: ROXBURY TWP	Code: 01	
Motor Vehicle Crash Description	Station:	Case No: 2014-008833	

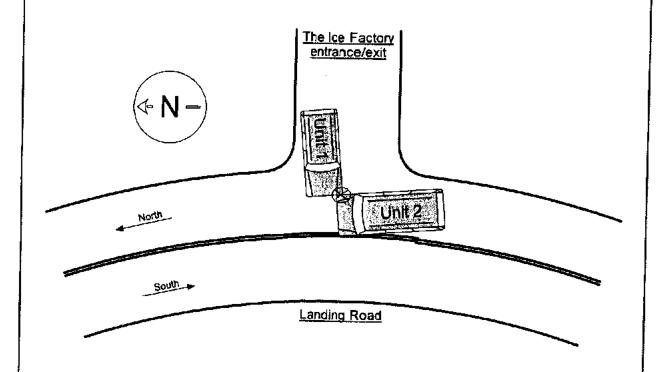
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135 Cresh Description

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Case No.: 2014-008833

Agency: Roxbury Twp.
Officer: Sgt. D. Murray #159



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ROXBURY TWP. POLICE 2014-008411 PTL. EDMUNDS PAGE 2 OF 2. LANDING RD

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NJTR-1 (R4/10)

New Jersey Police Crash Investigation Report Police Dept: ROXBURY TWP Code: 01 Motor Vehicle Crash Description Station: 1436 Case No: 2014-012589

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Upon speaking to the driver he advised he did not know he struck vehicle 1 while backing up. The witness to the accident was able to record the entire event due to traveling with a video recording system in his vehicle. (See flash

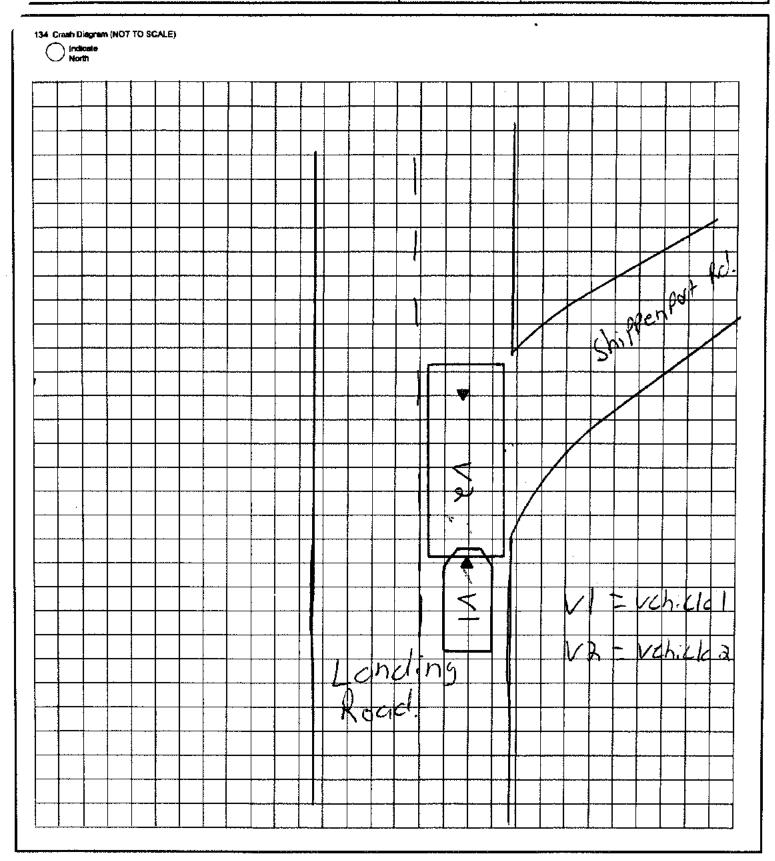
Driver 2 is a fault due to illegal backing and driver inattention.

3 of 3

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept <u>Roxbury PD</u> Code: 14/36
Station: <u>Roxbury</u> Case No: 30/4-013589



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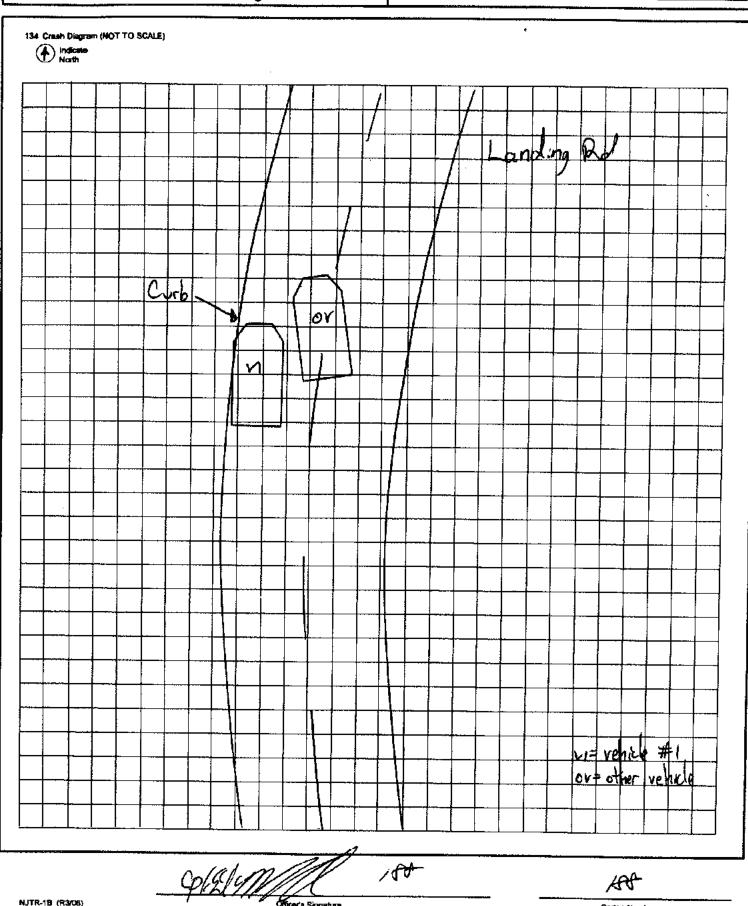
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NJTR-1 (84/10)

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: Roxburg Tup Code: 01 Station: ____ Case No: 20/4-010466



# APPENDIX – F CORRESPONDENCE WITH INTERESTED PARTIES

## Township of Roxbury

1715 Route 46
Ledgewood, New Jersey 07852
www.roxburynj.us
fax 973-448-2111
Historic Advisory Committee

PUBLIC WORKS RECREATION 973-448-2015 973-448-2021 TAX ASSESSOR TAX COLLECTOR 973-448-2022 TECHNOLOGY TOWNSHIP CLERK 973-448-2001 TREASURER 973-448-2005 WASTEWATER TREATMENT PLANT 973-448-5360 WATER PLANT 973-398-2818

June 26, 2006

973-448-2000

973-448-2009

973-448-2034

973-448-2018

973-448-2012

973-448-2002

973-448-2001

973-448-2100

INFORMATION

FIRE OFFICIAL

MAYOR & COUNCIL

PLANNING/ZONING

COURT

ENGINEER

HEALTH MANAGER

POLICE.

CONSTRUCTION/BLDG

S. Sajid Iqbal Project Manager PARSONS 100 Broadway, 20th floor New York, NY 10005

Dear Sajid:

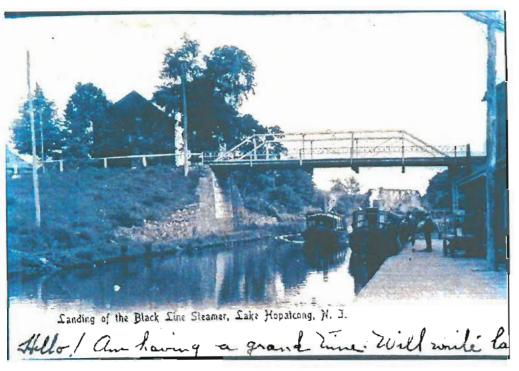
We are pleased to provide information on the historic preservation issues that replacement of this bridge (CR 631) 1400-073 over the Morris Canal and NJ Transit Morristown Line might impact. Enclosed you will find a sheet of photos of the old bridge and the current bridge as seen in different time frames. The photos are a visual reference to the historic nature of the location of the two bridges and what may be found during the replacement process, especially the new supports.

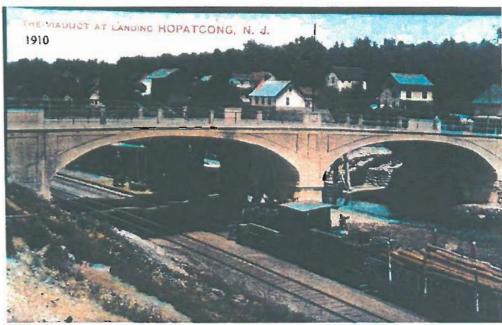
It is important to note that the arrangement of the Morris Canal adjacent to the railroad at the same level and separated by a platform for easy transfer is unique. The early 1900(upper left) view shows the steel truss section of the bridge over the Morris Canal and the lower left photo shows the railroad platform with the other section of the bridge. Also note that there was a tower from the platform to the bridge so that passengers could obtain livery or steamship service to their hotels on Lake Hopatcong. Passengers could also obtain steamship service to their hotel by crossing the platform and taking the boat up the feeder canal to Lake Hopatcong, as indicated in the upper left photo. The upper right photo shows the current bridge with the old bridge behind still in tact. The lower right photo shows the Morris Canal and the railroad side by side with the Landing Railroad Station to the left as well as the concrete towers that provided access to the east and westbound rails. Currently the land under the arch that the Morris Canal went though is filled in and paved over to provide Park and Ride parking for the railroad.

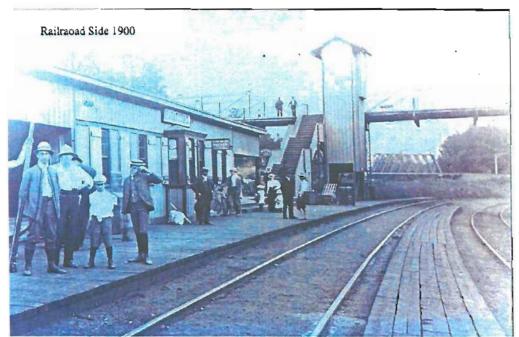
This review is to orient you and those who will be doing the 106 review where the Morris Canal was and what may be found relating to the historic resources of the canal and railroad when construction begins. If you have any questions please feel free to call me at 973-584-7903 so that I may be of service.

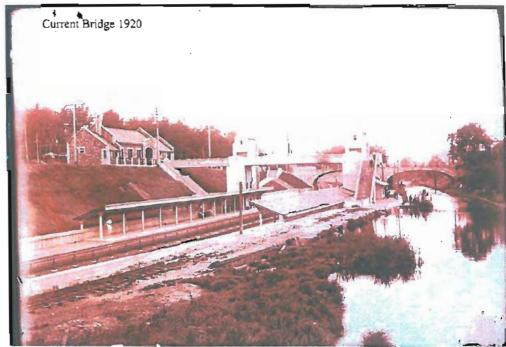
Sincerely.

Richard D. Cramond, Chairman









### CANAL SOCIETY OF NEW JERSEY

P.O. Box 737, Morristown, NJ 07963-0737 Answering Machine: (908) 722-9556

July 18, 2006

S. Sajid Iqbal Project Manager PARSONS 100 Broadway, 20th Floor New York, NY 10005

Dear Mr. Iqbal:

Thank you for the opportunity to provide the Canal Society of New Jersey's input regarding the proposed replacement of Landing Road (CR631) Bridge over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. We understand that this project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. We also understand from your letter dated June 14, 2006 that your firm has prepared a cultural resources survey for the project area. At this time we have not seen this survey and therefore are unable to comment on its completeness and accuracy. We would appreciate receiving a copy of the survey at your earliest convenience.

We are familiar with the project area. It is the Canal Society's opinion that the Landing Road Bridge is an important contributing structure to the Morris Canal Historic District, which is listed on both the National and New Jersey Registers of Historic Places. The loss of this historic bridge would be a serious negative impact to the Morris Canal Historic District. We therefore recommend that the bridge be repaired and reused on its original site.

The Landing Road Bridge was built in 1907, during the last decades of the Morris Canal's operation. It is one of the last extant road bridges in New Jersey built to span the historic Morris Canal. Of the hundreds of road bridges that once crossed the canal, nearly all have been removed, mostly by the State of New Jersey as part of the closing and dismantling of the Morris Canal in the mid and late 1920s. Therefore the rare surviving bridges are extremely important contributing resources to the Morris Canal Historic District. The Landing Road Bridge fits in this category.

In addition, the Landing Road Bridge is located at an especially important site within the overall Morris Canal Historic District. It was at this site that an extensive interchange of passengers occurred between the Delaware, Lackawanna & Western Railroad and the Morris Canal. This interchange of pedestrian traffic was part of the once prominent

tourist trade which flourished on Lake Hopatoong during the late nineteenth and early rwentieth centuries. Tourists traveled via the railroad from various parts of New Jersey. Upon reaching Landing they would disembark and then board steam launches that plied the Morris Canal and its Feeder. The Feeder led into Lake Hopatcong and enabled passengers to be carried to their lake resort hotels and camps. There is no other comparable site where such an extensive passenger interchange occurred between a railroad and the Morris Canal along its overall length. It is therefore a unique site within the Morris Canal Historic District. The Landing Road Bridge is an integral component of this unique site since its two arched spans define and separate the railroad corridor from the adjacent site of the Morris Canal's channel. In addition, the supporting footings of the bridge's center pier and end abutment toward Lakeside Boulevard would appear to be integrally connected with the historic fabric of the Morris Canal's prism, towpath, supporting embankments, laid-up stonework and potential remnants of the passenger interchange platform. It is anticipated that elements of all of this historic fabric from the canal would survive as archeological remains beneath the current blacktop-paved commuter parking lot covering the canal site. This parking lot serves New Jersey Transit passengers. Any construction to remove the existing bridge and to build a new bridge would have the strong potential to result in serious negative impacts to the Morris Canal's historic construction elements.

In addition, the Landing Road Bridge is a contemporary structure with the Delaware, Lackawanna & Western Railroad's nearby Cutoff, which was an engineering marvel of its time. The Lackawanna Cutoff and its associated structures would certainly be eligible for listing on the National Register of Historic Places. If in fact the Landing Road Bridge can be associated with the Lackawanna Cutoff's construction, the bridge would also appear to be eligible for listing on the National Register due to its railroad association.

The Landing Road Bridge is also of historical significance due its early twentieth century use by the Morris County Traction Company's trolley line. The bridge was used by the trolley line to access nearby Bertrand Island and its amusement park.

The Landing Road Bridge has added significance as a prominent landmark within the developing Morris Canal Greenway. The Canal Society of New Jersey has established the Morris Canal Greenway project to protect and preserve the historic remains of the Morris Canal as its site extends across the entire state.

The overall plan for the Morris Canal Greenway is to create a public greenway, linked with a trail where feasible, along the entire route of the Morris Canal. The Greenway's purpose is to preserve the surviving historic remains of the Morris Canal and its associated natural environment, to interpret canal sites to the public, and to offer recreational opportunities. The greenway concept combines historic preservation; environmental protection; open space preservation; recreational development; and economic development, relating to heritage tourism. The Greenway effort has many participating partners including the New Jersey Division of Parks & Forestry, the New Jersey Green Acres Program, Morris Land Conservancy and the Canal Society of New Jersey. At present, the Canal Society has received three matching grants from the New Jersey Green Acres Program to preserve portions of the Morris Canal and has mapped the Morris County portion of the canal site and the associated canal greenway. Interest continues to grow regarding the Morris Canal and the creation and preservation of a preserved corridor along its alignment. The Landing area and its associated Morris Canal sites, including the Landing Road Bridge, have been identified as extremely significant components of the Morris Canal Greenway.

Once again the Canal Society strongly recommends that the Landing Road Bridge and the surrounding Morris Canal site be preserved intact as part of the Morris Canal Historic District and the Morris Canal Greenway. Thank you for the opportunity to comment on the proposed project.

Sincerely,

Brian H. Morrell

President

cc. Senator Anthony Bucco

Assemblyman Richard Merkt

Assemblyman Michael Patrick Carroll

Dorothy Guzzo, NJ State Historic Preservation Office

NJ Highlands Coalition

Highlands Council

Marion Harris, Morris County Trust for Historic Preservation

Morris County Board of Chosen Freeholders

Margaret Schultz, Morris County Heritage Commission

Richard Cramond, Roxbury Township Historic Advisory Committee

Martin Kane, Lake Hopatcong Historical Museum

INFORMATION 449-2000
COURT 449-2000
COURT 449-2000
ENGINEER 449-2001
FIRE OFFICIAL 449-2001
HEALTH 449-2000
MANAGER 449-2000
MAYOR 3 COUNCIL 444-2000
POLICE 448-2000

Township of Roxbury
1715 Route 46
Ledgewood, New Jersey 07852

HISTORIC ADVISORY COMMITTEE

PLANNINGCONNIC 448-2008
PUBLIC WORKE 448-2008
RECREATION 448-2008
TAX ASSESSOR
TAX COLLECTION 448-2008
TOWNSHIP CLERK 448-2008
TREATMENT PLANT
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SEATMENT TREATMENT PLANT

Date:

09/23/98

To:

Hank Huelsebusch, Twsp. Engineer

From:

Richard D. Cramond, Chrm.

Subject:

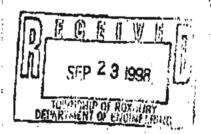
Landing Road Railroad Bridge

Replacement Study

Your memo to Gary Webb, Township Manager, dated August 24,1998 has been reviewed by the HAC. We concur with the findings of the referenced study and the recommendations that you have made. Since the core samples of the bridge indicate a condition that would necessitate it's replacement rather than reconstruction and widening, we agree that the best alignment is still Alignment #7.

As you recall in our conversation about the current status of the bridge the HAC agrees to the replacement of the bridge so long as the architectural character of the new bridge maintains the current double span look. We think it is important to maintain the historic importance of the Morris Canal and the railroad being parallel at this site. We therefore request that when the County and the NJDOT have plans available for review that the HAC be involved in the final acceptance process for the architectural design. Our initial request would be to reproduce the current architectural design but we will consider alternative proposals of compatible character.

Richard D. Cramond, Chrm.



Post-It* Fax Note 7671	Date 4-2-4 Pages
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Continue M.C. Engineering	C. Roxbury
Photo I	Phone 448-2018
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### **PARSONS**

100 Broadway - New York, New York 10005 - (212) 266-8300 - Fax: (212) 571-6825 - www.parsons.com

June 14, 2006

Richard Crammond, Chair Roxbury Township Historic Advisory Committee 1715 Route 46 Ledgewood, NJ 07852

Dear Mr. Crammond:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

Federal regulation requires public participation in the review process. We have received resolution of support from the Township of Roxbury for the replacement of this bridge. We are seeking any information you may have pertaining to historic preservation issues within the area.

Any comments you feel may be relevant should be communicated to the undersigned within 15 days of the receipt of this letter.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area.

Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,

S. Sajid Iqbal Project Manager

PARSONS

100 Broadway, 20th floor New York, NY 10005

Rojid Espal

#### PARSONS

100 Broadway - New York, New York 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

June 14, 2006

Ruth Ann Sealy, Local Historian 210 Emmans Road Flanders, NJ 07836

Dear Ms. Sealy:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

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Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,

S. Sajid (qbal Project Manager

PARSONS

100 Broadway, 20th floor New York, NY 10005

S. Sojid Egpal

100 Broadway • New York, New York | 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

June 14, 2006

Robert Barth Canal Society of New Jersey P.O. Box 737 Morristown, NJ 07963

Dear Mr. Barth:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

Federal regulation requires public participation in the review process. We have received resolution of support from the Township of Roxbury for the replacement of this bridge. We are seeking any information you may have pertaining to historic preservation issues within the area.

Any comments you feel may be relevant should be communicated to the undersigned within 15 days of the receipt of this letter.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area.

Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,

S. Sajid Iqbal Project Manager

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New York, NY 10005

100 Broadway • New York, New York 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

June 14, 2006

County College of Morris Local History Program C310 214 Center Grove Randolph, NJ 07860

#### Dear Sir/Madam:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

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100 Broadway - New York, New York 10005 - (212) 266-8300 - Fax. (212) 571-6825 - www.parsons.com

June 14, 2006

Brian Morrell Morris Canal Preservation Society P.O. Box 737 Morristown, NJ 07963-0737

Dear Mr. Morrell:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

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Sincerely,

S. Sajid Iqbal Project Manager

PARSONS

100 Broadway, 20th floor

S. Sojid Ispal

New York, NY 10005

Jeanne H. Watson, Director Morris County Historical Society 68 Morris Avenue Morristown, NJ 07960-0170

Dear Ms. Watson:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

Federal regulations require public participation in the review process. We are reaching out to you as someone knowledgeable about the Township of Roxbury. We are seeking any information you may have pertinent to historic preservation issues within the area.

Any comments you feel may be relevant should be communicated as soon as possible to Ms. Stacy E. Spies of ARCH², Inc. at 395 Main Street, Metuchen, New Jersey, 08840.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area. We thank you in advance for your assistance. Please call 732-906-8203 with any questions.

Thank You,

Nancy L. Zerbe

August 31, 2001

Mrs. Terry Schlatter, President Morris County Historical Society 18 Jeffrie Trail Whippany, NJ 07891

Dear Mrs. Schlatter:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Thank You,

Nancy L. Zerbe

August 31, 2001

Morris County Heritage Commission P.O. Box 900 Morristown, NJ 07963

To Whom It May Concern:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Thank You,

Nancy L. Zerbe

ARCH², Inc. 395 Main Street, Suite 8 Metuchen, NJ 08840-1806

Nancy L. Zerbe, President

August 31, 2001

Roxbury Township Historical Society PO Box 18 Succasunna, NJ 07876

To Whom It May Concern:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Thank You,

Nancy L. Zerbe

September 19, 2001

Marion Harris, Chair Morris County Trust for Historic Preservation 14 Oak Street Morristown, NJ 07960-5240

Dear Ms. Harris:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Thank You,

Nancy L. Zerbe

NLZ/ss

100 Broadway • New York, New York 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

July 21, 2006

643599

Mr. Surinder Thapar, P.E., P.P.
Assistant County Engineer
County of Morris
Department of Public Works
Administration and Record Building
Morristown, New Jersey 07963-0900

Attention:

Ms. Roslyn Khurdan

Subject:

Landing Road Bridge over NJ Transit and Morris Canal

Roxbury Township/Morris County

**Local Scoping Program** 

Historic Bridge Alternative Analysis Report

Dear Mr. Thapar:

Enclosed please find five (5) copies of the updated Historic Bridge Alternative Analysis Report for the subject project. This updated report is submitted as per request of NJTPA and for onward submission to the NJSHPO for their review and concurrence.

If you have any questions or need additional copies please contact me.

Thank you.

Very truly yours,

S. Sajid Iqbal, P.E.

S. Land Sepal

Project Manager

Enclosures

### ROXBURY TOWNSHIP HISTORICAL ADVISORY COMMITTEE

1715 Route 46, Ledgewood, NJ 07852

November 5, 2015

VIA REGULAR MAIL AND EMAIL

Ms. Roslyn Khurdan Morris County Engineering P. O. Box 900 Morristown, NJ 07930

RE: Morris County, Roxbury Township

Landing Road Bridge Improvements (Morris County Bridge 1400073)

Landing Road over NJ Transit Montclair-Boonton Line and the Morris Canal HPO Project No. 05-0435-5

Dear Ms. Khurdan:

The Historic Advisory Committee of Roxbury would naturally like to preserve the existing historic two-span concrete bridge dating to 1911, which is the only existing bridge that spanned the Morris Canal when the Canal was in operation. However, the Committee recognizes that due to the volume of traffic, and the age and condition of the existing bridge, a complete replacement of the bridge, with a slightly altered approach, is the only viable alternative. Therefore, we accept the Replacement Alternative (Alternative 7-C).

The Committee is, however, extremely concerned about the current configuration shown in the design we received with regard to pedestrian access safety and use. There are currently Township plans to provide a walkway from the recently constructed park and athletic fields on Landing Road to the bridge. The existing bridge, although beautiful in its original design, functioned well because of a walkway and step structure associated with the Landing Train Station that provided safe pedestrian access to the Lake, Morris Canal and Landing area. Since these features were later removed, and likely could not be replaced with the new handicap accessibility regulations, we are left with a bridge and intersection that is not currently safe for pedestrians, nor for its traffic volume and traffic flow.

The walkway across the bridge, on its northern side, does not provide safe access to the other side of the bridge for those pedestrians. Therefore the new bridge would have to include a pedestrian walkway on the southern side as well to accommodate those pedestrians. There is a historical precedent for this second walkway, as shortly after construction of the bridge, a second walkway was added on the southern side, spanning the Canal, and providing access from the lake via walkway and stairway down to the train/canal boat platform. (The footings of the eastern end of this pedestrian bridge remain visible next to the main bridge abutment. We recognize that the demolition of the existing concrete bridge will also result in the loss of these footings.)

We also feel that the pedestrian sidewalk along the Lake Hopatcong side retaining wall is too narrow and unsafe and does not provide any interaction with Lake Hopatcong. The historical pedestrian access from the train station and canal to Lake Hopatcong should be emphasized in any new design. Therefore, a minimum 10 foot wide pedestrian walkway should be provided. At a minimum, it should include safety bollards or features to separate the vehicular traffic and pedestrians and some features to view the lake should also be included. This could be in the form of openings in the wall as wide as 20 feet with an observation area for pedestrians to view the lake; possibly with stair access to the lakeside below.

In summary of this preliminary review, we recommend:

- 1) safe and signaled pedestrian access on the south side of the bridge from the train station to the lake,
- 2) a safe and wide pedestrian area between the realigned roadway and the lake,
- 3) provide interaction with the lake through open observation areas and access.

This area of Landing was once a beautiful gateway to Lake Hopatcong with trains and canal boats bringing tourists to various destination points on the Lake via boat, carriage and trolley. Now the once thriving area of Landing has deteriorated with many vacant business properties. We would like to see the replacement bridge designed with a street scape that includes attractive historic elements of the area during its heyday. Additionally, with the existing historical Landing Train Station being restored, the new bridge should retain its original style so as complement, rather than detract from, the Station's appearance.

In the future design of the bridge and the intersection improvements, we feel strongly that the County Engineering Department consider:

- A) a double arch bridge to match the historical significance and uniqueness of a bridge serving the Morris Canal and train service simultaneously,
- B) the historical surfaces of the bridge (historical photo attached),
- C) the railing design,
- D) the need of a fence and style of fence separating the roadway and pedestrian sidewalk from the lake.

We look forward to working together with the engineers and incorporating these historical elements in this important reconstruction.

Very truly yours,

Margaret Cushing, Vice Chairman of the Roxbury Historic Advisory Committee

CC: Daniel D. Saunders, Deputy State Historic Preservation Officer

Amy Rhead, Clerk, Roxbury Township

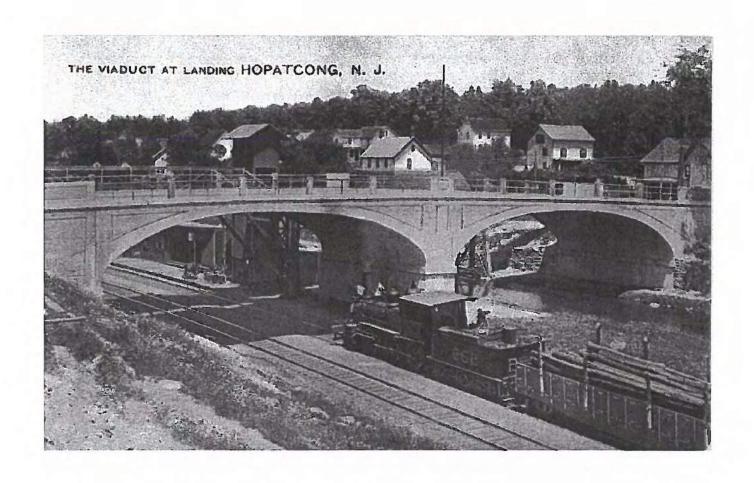
Joseph Macasek, Canal Society of New Jersey

Richard Porter, RBA Group

Peg Shultz, Morris County Heritage Commission

Marty Kane, Lake Hopatcong Foundation via email

Sean Ream, Bureau of Environmental Program Resources





# Township of Roxbury

1715 Route 46 Ledgewood, NJ 07852 www.roxburynj.us

Information 973-448-2000

Building & Construction 973-448-2009

Court 973-448-2034

Engineer 973-448-2018

Finance 973-448-2006

Fire Official 973-448-2012

Health 973-448-2028

Manager 973-448-2002

Mayor and Council 973-448-2001

Police 973-448-2100

Planning and Zoning 973-448-2008

Public Works 973-448-2069

Recreation 973-448-2015

Tax Assessor 973-448-2021

Tax Collector and Utilities 973-448-2022

Technology 973-448-2099

Township Clerk 973-448-2001

Sewer Plant 973-584-5360

Water Plant 973-398-2818 November 10, 2015

VIA EMAIL AND REGULAR MAIL

Roslyn Khurdan, Assistant Engineer Morris County Engineering P.O. Box 900 Morristown, NJ 07930

RE: Morris County, Roxbury Township, HPO Project No. 05-0435-5

Landing Road Bridge Improvements (Morris County Bridge 1400073)

Dear Ms. Khurdan:

The Landing Gateway Committee was established by Council Resolution to beautify the Landing area in the immediate area of the existing Landing Bridge. We received a copy of the State Historic Preservation Office's (SHPO) recommendation to Sean Ream of the Bureau of Environment Program Resources regarding the bridge replacement alternatives and would like to comment.

The 37,079 (as of 2011) cars that cross the bridge daily deserve to have a safe structure, intersection and an attractive entry to our community and our biggest amenity, Lake Hopatcong. Presently, the upper portion of the bridge is not historic, safe or attractive and is a hazard to pedestrians.

The correct design for a replacement bridge and intersection is key to the economic rejuvenation of Roxbury. The once thriving area of Landing has deteriorated with many vacant business properties. Recent projects hold potential for revitalizing the area (the new Landing Park and renovation of the existing historical train station). A design for the bridge and intersection that incorporates train/trail/lake/bike/pedestrian access would not only recreate the historic character of Landing but also create a tourist and business destination.

We support alternative 7C, the realignment and replacement of the bridge utilizing the double arch design. Our Committee would like input on the design of the bridge including sidewalks, pedestrian crossings, materials, lighting, bollards, street plantings, parking, fencing and Lake access.

We respect the SHPO's position of trying to preserve the old bridge but our community needs a safe, functional, attractive bridge and intersection as soon as possible.

Respectfully submitted, The Landing Gateway Committee Instrue Hand Chairman

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2011 "America's Promise Alliance 100 Best Communities for Young People"

CC: Daniel D. Saunders, Deputy State Historic Preservation Officer Sean Ream, Bureau of Environmental Program Resources Peg Shultz, Morris County Heritage Commission Richard Porter, RBA Group Roxbury Historic Advisory Committee via email Amy Rhead, Clerk, Roxbury Township Joseph Macasek, Canal Society via email Lake Hopatcong Foundation via email



# LANDING ROAD BRIDGE OVER NJ TRANSIT AND MORRIS CANAL

# Public Information Center August 12, 2015

6:00 – 8:00 PM, Roxbury Township Municipal Building 1715 Route 46, Ledgewood, NJ

# **MEETING NOTES**

**FROM:** Denice daCunha, Supervising Engineer

RBA Project Number J4804.01

### **ATTENDEES:**

Project Representatives:

Morris County Department of Planning & Public Works:

Christopher Vitz, P.E., County Engineer

Roslyn Khurdan, P.E., Assistant County Engineer

RBA Group: Richard L. Porter, Director of Cultural Resources

Denice daCunha P.E. Supervising Engineer

Roxbury Township / Members of the Public: see attached Sign-In Sheet

### **Purpose**

The purpose of this information center was to inform locals of the preferred alignment of the replacement of the Landing Road Bridge over NJ Transit and Morris Canal. The project includes the construction of a wider new bridge at its existing location.

Several presentation boards were utilized including: existing bridge condition photos, historic pictures/plans of bridge, previously considered alternatives, Alternative 7C with recent request for additional sidewalk and property owner aerial with Block and Lot (within 200 feet). The meeting format intent was informal where attendees could view boards at their convenience and ask questions. Most attendees arrived by 6:15 and remained after viewing boards and asking questions. Morris County gave a brief presentation of the project and offered a more formal question/answer opportunity.

Roslyn Khurdan introduced the project team representatives and gave a history of the project, noting numerous alternatives considered and were compared respective of environmental impacts, ROW, costs, etc. She discussed that both the No Build and Rehabilitation options were also considered and dismissed. She described the preferred alternative, state funding, anticipated schedule and estimated costs. Richard Porter explained the historic significance of the Landing Road Bridge as a contributing element within the Morris Canal Historic District, which is listed on the NJ Register of Historic Places and on the National Register of Historic Places. The NJ Register of Historic Places Act requires that any undertaking that will "encroach upon, damage

or destroy" a resource listed on the NJ register be subject to review. This review commences with the submission of an Application for Project Authorization to the NJ Historic Preservation Office, with a determination of Encroachment or No Encroachment. Since the structure is being replaced, it is anticipated that this project is an Encroachment thus further review by the NJ Historic Sites Council will be required. The process concludes with final action by the Commissioner of the DEP based on recommendations provided by the NJ Historic Preservation Office and the NJ Historic Sites Council.

The following are questions/comments observed by this memo author asked before, during and after the "formal" question and answer discussion:

1. What is the status of the Mount Arlington Avenue water main and is it part of this project?

Response: That project is a Roxbury Township project and not part of the bridge replacement improvements. The Roxbury Township Manger, Christopher Raths, discussed the status with concerned residents separately from the bridge replacement discussions.

2. I learned about this meeting from the local paper and some of my neighbors knew about it but what about people that live outside of the immediate area? How come they do not get notified?

Response: It is standard protocol on most projects to alert property owners with 200 feet and provide a public notice. It is not customary to reach out to other communities. Prior to construction, signing will alert the travelling public that construction will be starting.

3. Appreciate that this project is finally going through. The area is in need of attention. What is the projected schedule?

Response: Design 2016

Construction 2019, with a duration estimated at 18 months

- 4. Detours: the following questions were asked:
  - Concern for traveling public and local business that traffic remain open during construction. Will a detour be required and if so, will Shippenport Road be used and paved since most likely this roadway will be used as a detour route.

Response: The project is in the concept phase and the intent at this time is to stage construction so that one lane in each direction is maintained across the bridge. There may be the need for a temporary traffic signal during the staged construction for safety reasons but is the intent is to minimize traffic impacts. If a detour were planned, an evaluation of the detour route would be conducted. At this time a detour is not planned nor improvements to Shippenport Road included as part of the County project.

• Will an alternate route be suggested to motorists traveling through the area even if there is not a detour in an effort to lessen impacts? Concern is for local residents.

Reponses: A detailed maintenance of traffic evaluation will be conducted during design including if the need is warranted. It is noted that business owners voiced that they do not want traffic detoured for fear of business losses.



5. The Roxbury Planning Board Chairman requested that provisions be made on the new structure for a future sewer line.

Response: This can be evaluated/considered/coordinated during the next design phase, Preliminary Engineering.

6. Was the current alignment similar to the one previously approved by Township resolution?

Response: Yes, the alignment is similar to the 2003 however Roxbury has requested additional sidewalk. A new Resolution of Support is anticipated.

7. Will local police be used to manage traffic during construction?

Response: On Morris County projects, the County uses local police and neighboring towns or Sheriff's office if needed. These details get worked out much later in design.

8. Will there be future meetings with the public?

Response: Yes, another PIC would occur most likely in 2017 and a website will be setup for access about design, construction.

9. Will other improvements be made such as replacing the chain link fence on the Lake Hopatcong "sea wall" and other decorative improvements?

Response: The Roxbury Township Manager, Chris Raths, noted that a committee with the purpose for a streetscape vision has been created and the intent is to have a "gateway" in addition to decorative cross walks and lighting. The committee will coordinate with the County.

10. What will happen with train services access, parking during construction and will there be additional improvements to station that are much needed?

Response: NJ Transit is a key stakeholder in the project as bridge crosses tracks/property. They will be involved in design including clearance, parking, aces during construction etc. These are all deign issues that will be worked out. Any improvements to the station are not proposed as part of the project and would be a transit issue.

- 11. Resident and Lake Hopatcong Foundation (LHF) representative Donna Macalle-Holly wanted to thank the County for being great partners with the community. She was looking at this project as a positive opportunity for enhancements.
- 12. Will the "Lake Hopatcong Foundation building" (Lake Hopatcong Train Station) be impacted?

Response: Not the building, but there are impacts to the parking lot.

13. ROW Impacts. What will happen to existing businesses and properties shown as being completely impacted?



Response: As part of the ROW process, negotiations will take place. The ROW will be refined during design and impacted property owners will be contacted.

• A member of the audience spoke out that he was an attorney who handles these types of issues and noted that Morris County does an effective job at such negotiations. He added that several additional properties in addition to the 3 major impacts with partial takes will also have impacts.

Response: The alternative shown is conceptual and ROW will be refined during design.

14. A gentleman who introduced himself as a member of the Historic Society stated he likes the bridge the way it is.

Response: It was explain the rehabilitation of the bridge was investigated and the option eliminated since not practical. The goal of the replacement is to provide a new bridge with the appearance of the existing bridge.

15. Will the bridge deck be build off site in an effort to expedite construction delays at the site?

Response: During design options to expedite construction duration will be investigated including the use of precast bridge elements

16. Where does the four lane section (two lanes per direction) section end towards the park and will this just create another bottle neck? Will Landing Road be widened throughout? Response: The four lane section ends in the vicinity of the Foundation building. Two lanes in each direction across the bridge will help facilitate the operation of the traffic signal at Mount Arlington Boulevard. Providing four lanes on the bridge will also facilitate future traffic and since bridge is being replaced makes sense for the life of bridge. There are no other current plans to widen Landing Road.

17. Existing sidewalks are very narrow. Can other areas also be considered for new sidewalk such as by the Wave service station?

Response: Sidewalks will be improved to ADA compliant conditions. Additional sidewalk requests can be considered during design. In order to accommodate sidewalks in some areas, Right of Way may be required.

18. Will the Design and Construction be bid separately? Is funding allocated?

Response: Yes, Design and Construction will be bid separately. Project is now state funded and money has been allocated. Estimated construction cost is \$6.1M and \$1M for ROW. ROW cost will be evaluated and funding adjusted if needed.

19. Canal Street is in need of paving? Is this part of project?

Response: Canal Street will be altered by bridge demolition and/or construction so improvements would be part of this project.

20. Will the posted speed limit be the same as existing after construction is completed? Response: Yes, that is the intent.



21. Can the traffic islands have plantings? Do they need curbs? There are local volunteers that provide plantings now and additional areas would look nicer.

Response: These requested can be investigated during design and coordinated with Roxbury Township. Curbing is required around islands.

22. Can the size and shape of the island by Kings Highway be modified? It seems very large and would impact access to businesses.

Response: The graphics are conceptual and the size will be refined during design. The crossing distance for pedestrians is a safety improvement and it appears the island size can be reduced.

23. Are there meeting handouts of the presentation boards? How can we get updated plans? Responses: The County will provide pdfs of the preferred alternative to the Township. During design a website will be created and maintained.

It is noted that several attendees thanked the team representatives for the opportunity to learn more about the project and provide their input. The meeting ended at 8PM after all members of the public left.

These notes are the author's recollections of the meeting and represent a complete and accurate record of the decisions and agreements made. Amendments to this record shall be made in writing to the author.



### PUBLIC INFORMATION CENTER (WEDNESDAY AUGUST 12, 2015 AT 6:00 PM) FOR

REPLACEMENT OF MORRIS COUNTY BRIDGE NO. 1400-073 LANDING ROAD OVER NJ TRANSIT AND MORRIS CANAL IN THE TOWNSHIP OF ROXBURY, MORRIS COUNTY

# **SIGN-IN SHEET**

NAME & ADDRESS	(Phone Number)
Walneta Bears, 333 Boonton St,	Landing 398-9375
Barbara Bielia 169 Hen	elington Bod.
Denice da Cunha RBA Grup	973.946-5624
Maria & David Martorana	(473) 895-5584
1 1	anding
	169 Mg. ARlington Blud
Christine Houtz 576 Dell	Rd. Landing, 973-770-3723
John Yingling 209 Labe	ude Bld Hopaten 398-935
	11 6 ROAD LANDING N. J. 973-39847
JOE GATES 132 LANDING	G POIND, LINDING, No (473) 348 400
Chris RAFLS 1715 Rt 46	Leading Ledgewood AJ 07876
Maria Sudo 30 Corw	
ART SCANTLEBURY 112	Kincs Hawy
THON COATES 31 ONE	IDA AVE.

# PUBLIC INFORMATION CENTER (WEDNESDAY AUGUST 12, 2015 AT 6:00 PM) FOR

REPLACEMENT OF MORRIS COUNTY BRIDGE NO. 1400-073 LANDING ROAD OVER NJ TRANSIT AND MORRIS CANAL IN THE TOWNSHIP OF ROXBURY, MORRIS COUNTY

# **SIGN-IN SHEET**

NAME & ADDRESS 37 Robot PAPKAR	(Phone Number)
Donna Macolla Hely, LHF L.A.	973-663-2506
James Coates 1 IROR	1015 Ave Landing, NJ
Shown Polito 54 Laurie RD	201-230-6672
R GILFILLAN LATORI	973-398-6760
TOM & JUNE VALIANTE	973-418-6869
Jeff + Mark Toldman 101 Kings Highway	973-770-9710
PAUL CORRENTE Landing	973-229-5628
TERRY Melly 124 Paroing Ro	973-945-5633
ROSPHC. NAPPI 29 FORD RD. LANDING	(973)-398-5624
DAVID Klein Succesuma NJ	(973) 584-4971
Jan RWe7zel successione NZ	973-584-9597
Charle Bant	201-841-0552
8	

# APPENDIX – G REHABILITATION DESIGN STUDY

Ojile 6 43599

# STORCH

# **ENGINEERS**

220 RIDGEDALE AVENUE, P.O. BOX 267 FLORHAM PARK, NEW JERSEY 07932 201-822-2600 FAX 201-822-3904

pls. Poters

### LANDING ROAD BRIDGE REHABILITATION STUDY

Landing Road Bridge over New Jersey Transit Railroad Township of Roxbury, Morris County, New Jersey

Prepared for:

County of Morris
Department of Public Works
P.O. Box 900
Morristown, NJ 07963-0900

May 1997

BOSTON MASSACHUSETTS FLORHAM PARK NEW JERSEY MANCHESTER NEW HAMPSHIRE HICKSVILLE NEW YORK CROMWELL CONNECTICUT NEW YORK

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#### 1.0 EXECUTIVE SUMMARY

The County of Morris plans to replace or rehabilitate the existing County Bridge No. 73 on Landing Road over the New Jersey Transit Railroad and Morris Canal in Roxbury Township.

This study has been undertaken to examine the existing physical conditions of the structure and make recommendations with respect to replacement or rehabilitation. It will take into consideration the existing and projected traffic volumes carried by the structure and their impacts on the intersection (and its approaches) directly adjacent to the bridge.

This report summarizes the results of Storch Engineers' investigation on behalf of the County of Morris, Department of Public Works to recommend replacement or rehabilitation schemes for the Landing Road Bridge.

The results of this study have found that replacement of the existing structure is necessary due to the deteriorated condition of the arches and spandrel walls. It has also been determined that the structure should be widened to allow for an additional lane in the northbound direction. This will improve the level of service for the intersection and Landing Road in the northbound approach to the intersection from unacceptable levels of service to a level of service "B". Improvements to the intersection will provide additional capacity to the intersection and its approaches.

The cross section of the widened structure will consist of three traffic lanes (11 feet wide each), outside shoulders (2 feet wide each), a five foot wide sidewalk, and one foot wide parapets and/or bridge rails. The out-to-out of the new structure would be 44 feet.

The improvements to the intersection would include minor alignment changes and modifications to the traffic signal timing.

### 2.0 INTRODUCTION

The County of Morris plans to replace or rehabilitate the existing County Bridge No. 73 on Landing Road over the New Jersey Transit Railroad and Morris Canal in Roxbury Township.

Landing Road, also known as Morris County Route 631, is located in Roxbury Township. It extends from Interstate Route 80 in the northwest approximately 0.65 miles to the southern edge of Lake Hopatcong. Landing Road then travels over the Old Delaware, Lackawanna and Western Railway (currently known as New Jersey Transit) via Landing Road Bridge (County Bridge No. 73) and intersects with Lakeside Boulevard (County Route 602) and Shippenport Road. Please refer to the Site Location Map and U.S.G.S. Map contained in Appendix A.

The existing bridge, constructed in the beginning of the 20th century, is approximately 136 feet long. The superstructure consists of twin reinforced concrete arches and a reinforced concrete deck with an asphalt overlay having a curb-to-curb width of 29.6 feet and an overall width of 38.8 feet. The substructure includes concrete abutments, which are at a 90-degree angle with the bridge deck, and wingwalls.

In the 1970's and 1980's, the County constructed major improvements to Lakeside Boulevard from the County line to the intersection of Landing Road. In addition, substantial improvements to Mount Arlington Boulevard and to Howard Boulevard were made using Federal funding, State Aid Road System grants, and County resources. A study to realign Landing Road to the west of its present location in the late 1970's and early 1980's met its demise through lack of sufficient Federal funding, environmental problems and lack of community support.

A bridge evaluation study report prepared by Maitra Associates, Inc., in September 1990 gave the bridge an overall condition rating of fair. This report also stated that the bridge meets minimum tolerable limits to be left in place as is although eleven improvements/repairs to the bridge were recommended in order to preserve the bridge's structural integrity and extend its useful life.

The County of Morris determined the need to investigate alternatives for replacing or rehabilitating the existing County Bridge No. 73 or to construct a new roadway alignment and a new bridge within the local vicinity of the existing bridge.

Storch Engineers, on behalf of the County of Morris, Department of Public Works, was assigned this responsibility and in the process, developed eight (8) improvement alternatives. In addition, two (2) other alignments studied in 1985 by Elam and Popoff, P.A. were included in the evaluation.

The evaluation assessed the ten alternatives with respect to various criteria including, but not limited to, bridge impacts, intersection impacts, environmental impacts, utility impacts, right-of-way impacts, and economic impacts. Based on the results of that study, a recommendation was made to construct a new bridge. The new structure would be an extension of Lakeside Boulevard joining Landing Road south of the existing bridge. The existing bridge would be rehabilitated or have necessary repairs made and remain in service to provide local access to Landing Road and King's Highway. The cost of this alternative was estimated at \$7.6 million.

The amount of time to acquire the funding, right-of-way, and third party agency permits for this alternative was estimated to take ten to twelve years. This, along with the current condition of the existing bridge, has prompted the County of Morris to move forward with this study to determine replacement/rehabilitation/repair alternatives for the existing structure.

### 3.0 EXISTING CONDITIONS

### 3.1 HISTORY

The Landing Road Bridge provides one of the few major ingresses and egresses for residents on both the east and west shores of Lake Hopatcong. Lakeside Boulevard provides access to Landing Road Bridge from the north and to Mount Arlington Boulevard from the south. Howard Boulevard provides the only other major access to Interstate Route 80 from the southern end of Lake Hopatcong.

In the 1970's and 1980's, the County constructed major improvements to Lakeside Boulevard from the County line to the intersection of Landing Road. In addition, substantial improvements to Mount Arlington Boulevard and to Howard Boulevard were made using Federal funding, State Aid Road System grants, and County resources. A study to realign Landing Road to the west of its present location in the late 1970's and early 1980's met its demise through lack of sufficient Federal funding, environmental problems and lack of community support.

### 3.2 ROADWAY

The Landing Road Bridge is served by two forked intersections at each of its west and east approaches. The bridge connects Landing Road to the southwest and King's Highway to the northwest with Mount Arlington Boulevard to the southeast and Lakeside Boulevard to the northeast. The approaching roadways are relatively flat with the east approach serviced by a three-phase traffic signal.

As one approaches the bridge from Interstate Route 80 in Roxbury via Landing Road, a 620-foot radius curve to the left is encountered followed by a short tangent roadway alignment leading to a 173-foot radius curve to the right just before the juncture of the bridge and King's Highway. The average width of Landing Road is 25 feet, carrying one lane of traffic in each direction, and there is a posted speed limit of 30 m.p.h.

The King's Highway approach contains a 760-foot radius curve to the right just before the Landing Road intersection. The average width of King's Highway is 23 feet and there is a posted speed limit of 25 m.p.h.

Approaching the bridge from the east, a signalized forked intersection is encountered which includes Lakeside Boulevard to the north, Mount Arlington Boulevard to the southeast, and Landing Road to the southwest.

Approaching the bridge from Lake Hopatcong, a 740-foot radius curve to the right is encountered along Lakeside Boulevard. Beyond the curve, a short tangent alignment is followed by a sharp right turn onto Landing Road at the signalized intersection is at the east bridge approach.

Lakeside Boulevard in this region is a two lane approach to the bridge. The right lane serves as an exclusive right onto the bridge and the left lane serves as an exclusive left onto Mount Arlington Boulevard.

Turning left off of Lakeside Boulevard, via a 175-foot radius curve, the road becomes Mount Arlington Boulevard and heads toward Shippenport Road and Mount Arlington.

Approaching the bridge from Mount Arlington via Mount Arlington Boulevard, a 160-foot radius curve to the right is encountered prior to the left turn onto the east bridge approach at the signalized intersection.

Landing Road is a four lane roadway with a posted speed limit of 35 m.p.h. while Mount Arlington Boulevard has an average width of about 25 feet and a posted speed limit of 25 m.p.h.

### 3.3 BRIDGE

The existing bridge, constructed in the early 20th century, passes over the New Jersey Transit Railroad and a railroad station parking area over the filled in Morris Canal. An archaeological and historical survey report prepared by Herbert C. Kraft in August 1979, judged the bridge to be eligible for the National Register of Historic Places.

The structure is approximately 136 feet long. The superstructure consists of twin reinforced concrete arches and a reinforced concrete deck with an asphalt overlay having a curb-to-curb width of 29.6 feet and an overall width of 38.8 feet. It appears that the undersides of the arches have been treated with shoterete as a maintenance measure. The substructure includes concrete abutments, which are at a 90-degree angle with the bridge deck, and wingwalls. No weight limit is posted on the bridge.

A bridge evaluation study report prepared by Maitra Associates, Inc. in September 1990 gave the bridge an overall condition rating of fair with severe spalling and cracking on the undersides of the arches. This report also stated that the bridge meets minimum tolerable limits to be left in place as is, although eleven improvements involving safety upgrades and repairs to the bridge were recommended in order to preserve the bridge's structural integrity and extend its useful life.

Based on our observations during more recent site visits and concrete core samples, the existing bridge is in severely deteriorated condition. The gunite is cracked throughout the underside of the arches and is spalled in many areas. The spandrel walls and outside edges of the arches are severely cracked and spalled with exposed rusted reinforcing steel. It has been reported that pieces of gunite and arch concrete routinely fall from the underside of the arches when large trucks cross the bridge. Evidence of the fallen debris has been noted on the ground under both spans, especially under the southern spandrel wall. Please refer to various site photographs contained in Appendix B.

The Morris County Engineer's Office requested that Storch Engineers acquire cores of the arch and substructure concrete and have analyses performed to determine the feasibility and extent of repair and/or rehabilitation necessary to extend the bridge's serviceable life.

The test results and observations of the core samples and the following previous inspection material have been reviewed and incorporated into the evaluations and recommendations made in this report:

- * "Bridge Evaluation Survey Report" dated September 1990 by Maitra Associates, Inc.
- * "Cost Feasibility Study" dated July 1991 by A.G. Lichtenstein & Associates.
- * "Bridge Re-evaluation Survey Report" dated October 1994 by Bettigole, Andrews & Clark, Inc.

In June 1991, four core samples were taken at the mid-span of each arch for A.G. Lichtenstein & Associates. Five of the eight cores crumbled during the coring and extraction operations. The remaining three cores were tested for compression strength and one of the cores failed before a load was registered on the compression machine. The other two cores achieved compression strengths of 4,000 and 6,000 psi. Chloride tests were also performed on all of the cores and revealed very high levels of chloride ion in the concrete, indicating an adequate environment to support electrochemical corrosion of the embedded reinforcing steel.

It token from I char so sint,

These cores were taken at the tops of the arches where the depth of cover over the arches is minimal and it would be expected that the highest levels of chloride ion would be found. The additional cores proposed by Storch Engineers were located closer to the substructures to determine if similar conditions exist throughout the arches.

Due to the increasing thickness of the arches near the substructures and the increasing depth of cover over the arches, full depth cores could not be taken and partial depth cores from the top could not be extracted. Therefore, it was decided to take the cores from the underside of the arches. However, due to the railroad located under the west span, give partial depth cores from the underside of the arch were taken in the east span. It is assumed that the condition of the west span will be similar to the conditions found in the east span.

One core was taken in each of the abutments and the pier and cores were attempted in the spandrel walls and wingwall faces. The locations of these cores are shown on the Coring Location Plan in Appendix C.

The cores were visually examined and tested for compressive strength, chloride content, air content, and freeze-thaw durability as indicated in the coring test results in Appendix D.

Core No. 7 was eliminated when it was realized that the cores could not be taken from the west span. Core Numbers 8, 9, and 10 were attempted, but could not be taken because sound concrete for the core drill anchor bolts could not be found.

The general conditions of the arch cores were described by Materials Testing, Inc. as fair to poor. The aggregate to paste bond is fair to poor, the grading of the fine and coarse aggregate is fair and one sample had many vertical cracks throughout the sample.

*Several of the cores broke into several pieces after extraction. The remaining intact portions were tested for compressive strength with results ranging from 2,700 to 5,150 psi.*Two of the cores had chloride levels below the accepted threshold of two lb/cy and the levels of the remaining four cores exceeded the threshold. The highest level was indicated at 7.56 lb/cy.

WAfter 15 cycles of the 25 cycle freeze/thaw test, the cement paste in all of the tested cores had completely separated from the coarse aggregate and broke up into small pieces. This is indicative of very poor durability of the existing concrete.

Redeselt is unit

672-1187

### LANDING ROAD BRIDGE REHABILITATION STUDY

May 1997

### 3.4 TRAFFIC

Traffic data was collected for the intersections at each end of the Landing Road Bridge as well as the adjacent intersection of Shippenport Road and Mount Arlington Boulevard. Intersection counts were taken on Thursday, July 28, 1994, by Storch Engineers at the following three (3) locations:

- Site 1 The intersection of Landing Road and King's Highway at the western end of the Landing Road Bridge.
- Site 2 The intersection of Landing Road, Mount Arlington Boulevard and Lakeside Boulevard at the eastern end of the Landing Road Bridge.
- Site 3 The intersection of Shippenport Road and Mount Arlington Boulevard.

The intersection counts, which describe the movements of vehicles through the intersections, were taken during the a.m. (6:00 to 9:00) and p.m. (3:00 to 6:00) peak periods. These counts show the a.m. and p.m. hourly counts for each location at 15 minute intervals. The intersection peak hours and their corresponding volumes for the morning and afternoon counts were determined. A total of 3,499 vehicles used the bridge during the peak hours. The p.m. peak hour count of 1,906 vehicles was greater than the a.m. peak hour count of 1,593 vehicles by about 20%.

Analyses of the level of service (LOS) for the existing signalized and unsignalized intersections were performed using the traffic count data from the peak hour counts. The results of these analyses are included in Appendix E. The existing LOS for Sites 1 and 2, as described above, was found to be at an unacceptable level. Intersections or approaches are considered unacceptable when the LOS is E or below.

The approaches are described as follows:

Northbound Approach - Landing Road approaching the intersection with Lakeside Boulevard and Mount Arlington Boulevard. This approach consists of one lane until just before reaching the intersection where a second lane is developed.

Southbound Approach - Lakeside Boulevard approaching the intersection with Landing Road and Mount Arlington Boulevard. This approach consists of two lanes.

Westbound Approach - Mount Arlington Boulevard approaching the intersection with Landing Road and Lakeside Boulevard. This approach consists of a single lane.

With the current lane configuration and 1994 p.m. peak volumes, the northbound approach fails. This also causes the intersection to fail. By adding a second lane to the northbound approach, it improves to a LOS "B" using the 1994 volumes. For the purposes of this report, we will call this the "Amended Configuration". The amended configuration also improves the intersection to a LOS "B". Using the projected 2004 and 2014 traffic volumes, the widened northbound approach LOS would be "C" and "F", respectively. A summary of the analyses is contained below.

	LEVEL-OF-SERVICE (LOS)								
	Westbound Approach		Southbound Approach		Northbound Approach		Intersection		
	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak	
Existing Configuration w/1994 Volumes	N/A	C	N/A	В	N/A	*	N/A	*	
Amended Configuration w/1994 Volumes	С	c	C	В	A	В	В	В	
Amended Configuration w/2004 Volumes	С	C	*	С	D	D	*	С	
Amended Configuration w/2014 Volumes	С	D	*	*	Ď	*	*	*	

N/A No analysis was performed*
LOS 'F" or worse (failure)

As the above summary indicates, the southbound approach (and hence the intersection) exhibits failure for the 2004 and 2014 volumes. It should be noted that the failing LOS of this approach is not caused by the addition of the second lane to the northbound approach, but an increase in traffic volume.

### 4.0 CONCLUSIONS AND RECOMMENDATIONS

### 4.1 STRUCTURAL

Based on the previous evaluations, the observed condition of the concrete and the results of the core testing program, it is our opinion that the existing concrete arches are in poor condition and should be replaced.

Repair of the structure was considered and is determined to be impractical for the following reasons:

- 1. The limits of repair would be difficult to determine. Considering that the cores at the tops of the arches crumbled and that other cores broke apart after extraction, it cannot be guaranteed that concrete adjacent to areas being removed would remain intact during removal operations. The estimated removal quantities may not be accurate and could result in excessive costs to the County.
- 2. Since the middle portions of each arch, the spandrel walls, and the outside edges of the arches must be replaced, extensive temporary shoring of the portions of the arches to remain would be required.
- 3. It is possible that the remaining portions of the arches would continue to deteriorate and would not have the same life expectancy as the repaired areas.

The following replacement alternatives were considered:

- 1. Replace the existing structure with cast-in-place concrete arches. This option will maintain the existing appearance of the structure, but is expected to be more costly than the other alternatives due to the formwork and labor costs.
- Replace with a precast concrete arch. Several arch types were investigated and it was
  found that the existing arch profile cannot be matched by the pre-cast concrete arches
  available. Also, the available arch profiles would reduce vertical clearance over the
  railroad tracks to below acceptable levels.

- 3. Replace with adjacent prestressed concrete deck units with a concrete facade to match the original spandrels. The depth of the deck units required is greater than the depth of the arches at the midspan. This would require altering the arch shape.
- 4. Replace with continuous composite steel girders with a concrete facade to match the original spandrels. This option could maintain the appearance of the existing arches in elevation, but would not match the appearance from underneath. Preliminary discussions with the State Historic Preservation Office have indicated that this option would not be acceptable.

It is recommended that the existing Landing Road Bridge be completely replaced with cast in place concrete arches matching the original elevation and architectural treatment of the existing bridge.

### 4.2 TRAFFIC

In order to address the existing LOS failures of the northbound approach and the intersection of Landing Road with Lakeside Boulevard and Mount Arlington Boulevard we recommend the following:

- 1. Add an additional lane across the bridge in the northbound direction. Based on AASHTO standards, the minimum recommended cross section across the bridge would be three-11 foot wide lanes, 2 foot wide outside shoulders, a 5 foot wide sidewalk along the southbound side, and 1 foot wide parapets or railings. The minimum out-to-out of the bridge would be 44 feet. The existing structure is approximately 38.8 feet out-to-out.
- 2. Modify the existing signal timing to increase the Landing Road capacity and shorten the vehicle queues on Landing Road during the p.m. peak hours. This can be accomplished by setting the Mount Arlington Boulevard approach to a very low cycle length of between 8 and 10 seconds.
- 3. Make alignment improvements to the intersection of Landing Road with Lakeside Boulevard to improve traffic flow and hence the capacity through the intersection.

It should be noted that we are not recommending an additional southbound lane across the bridge because the additional width necessary would have a significant impact on existing buildings adjacent to the structure, the intersection, and along Lakeside Boulevard.

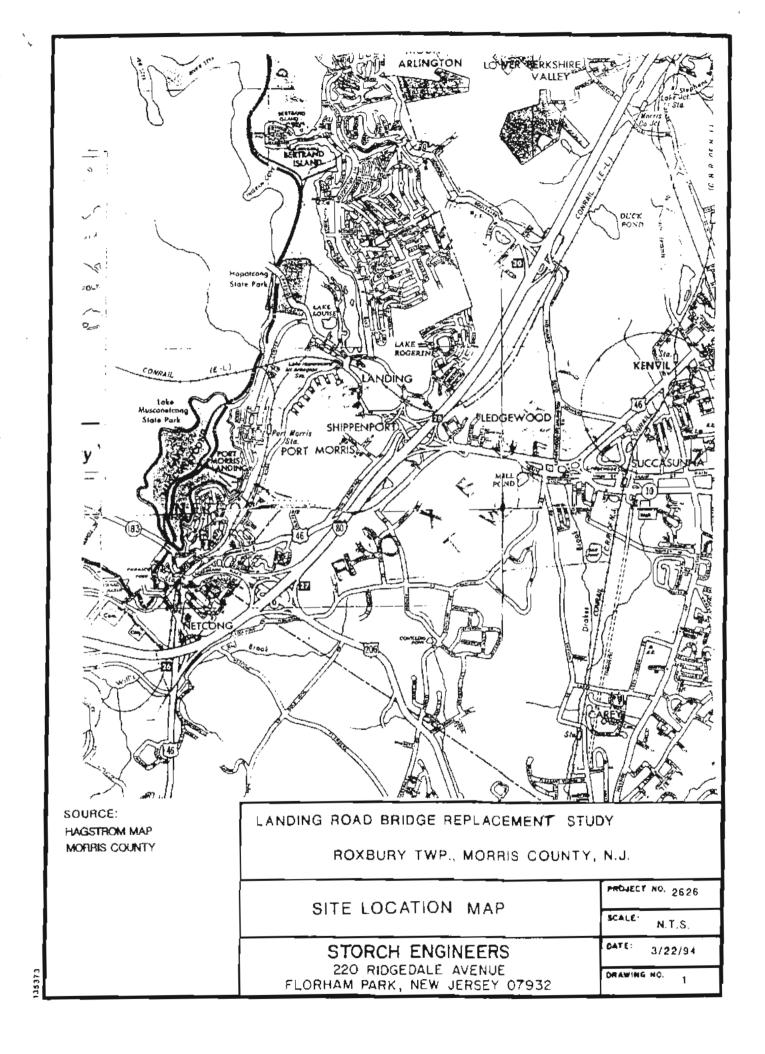
### 5.0 COST ESTIMATE

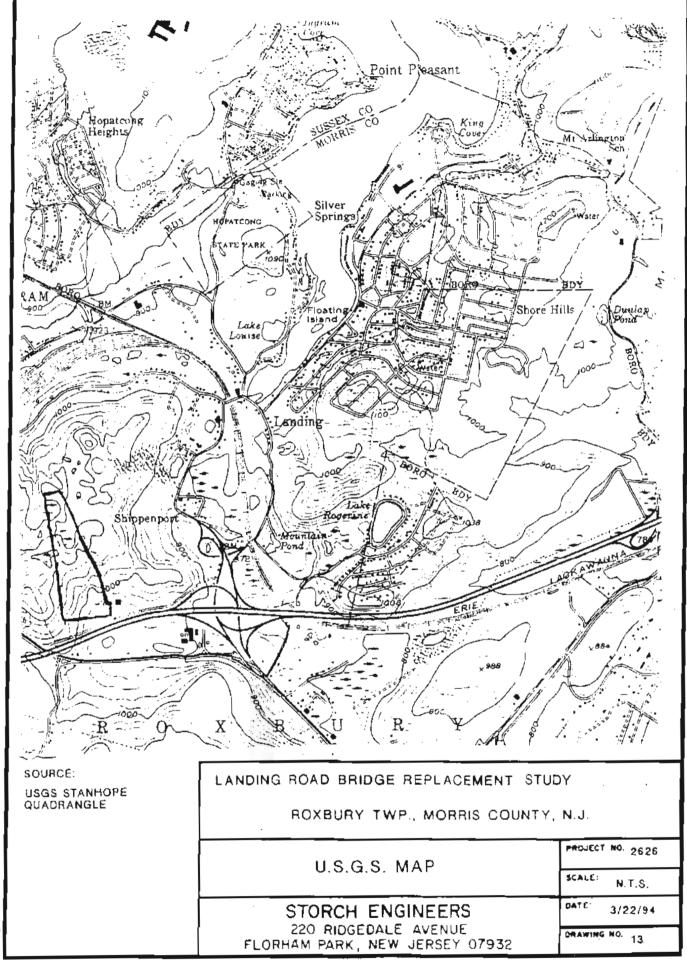
The recommended improvements noted in Section 4.0 above are similar to those specified for Alignment No. 7 of a previous report prepared by Storch Engineers entitled "Landing Road Bridge Replacement Study". This report was submitted to the County of Morris in March 1996. The cost for Alignment No. 7 was estimated at \$3.45 million.

It should be noted that Alignment No. 7 proposed widening the existing structure to four lanes and an out-to-out width of 63 feet. Our current recommendation is to widen the existing structure to only three lanes and an out-to-out width of 44 feet. Additionally, Alignment No. 7 did not take into consideration the cost of replacing the existing structure, but merely repairing it in conjunction with the widening. Therefore, we have revised the previous cost estimate.

The estimated cost to replace and widen the existing bridge and make improvements to the intersection is \$3.7 million. Please refer to Appendix G for a more detailed breakdown of the cost estimate.

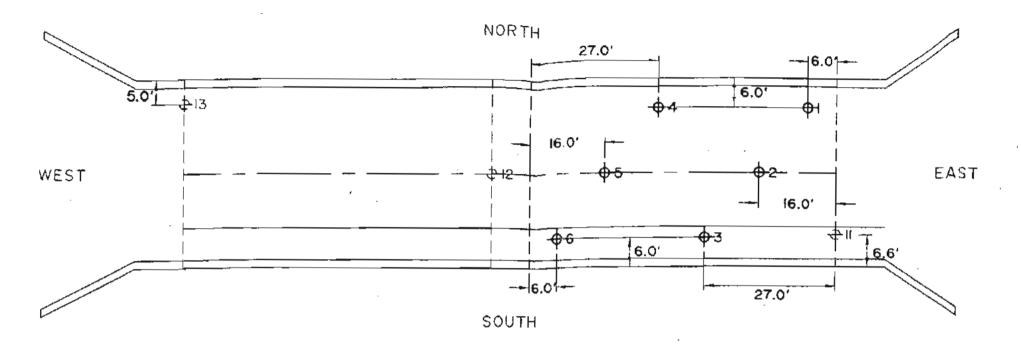
APPENDIX A



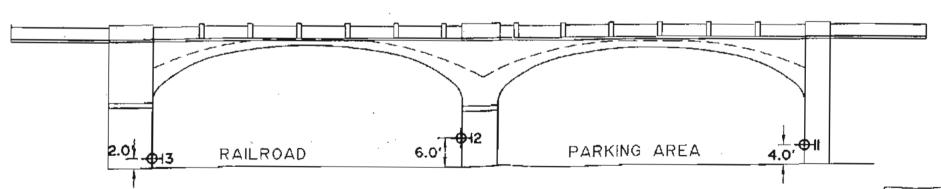


APPENDIX B





PLAN VIEW



SOUTH FASCIA ELEVATION

SCALE: I"=20"

LANDING ROAD BRIDG	E
ROXBURY TOWNSHIP MORRI	S COUNTY, N.J.
CORING LOCATION PLAN	PROJECT NO. 01-2626
CORNO ESCATION FEAN	SCALE: AS INDICATED
STORCH 220 REGISTER AVENUE FLORIGIM PARK NA 07932 1201 1222-2600	DRAWN BY: CPH
	CHECKED 9Y:
JAMES A. BAUMGARTNER	DATE 10/15/96
1/16/97	DWG, NO.

APPENDIX D



01-2626 con

NEW JERSEY DIVISION 180 MILL ROAD GIRSON, NEW JERSEY 08817 - (908) 248-3777 - FAX (908) 248-7979

DEC 10 1996

STORUM ENGINEERS

STORCH ENGINEERING

220 RIDGEDALE AVENUE

P.O. BOX 267

FLORHAM PARK, N.J. 07932

CLIENT:

PROJECT: LANDING ROAD BRIDGE

TEST:

Freeze Thaw

METHOD:

ASTM C666

#### CERTIFICATE OF ANALYSIS

On October 15, 1996 Materials Testing Inc. started a 25 freeze thaw in accordance with ASTM C666. On three cores numbered 3: 4 & 12. A visual examination was performed after five cycles. The cores appeared to have a loose aggregate paste bond. After 10 cycles the cores showed signs of deterioration. At 15 cycles the cores were in a state of total separation of cement paste from course aggregate. The cement paste broke up into small pieces.

Please see photo for visual.

Submitted

REPORT: MTSE-002

DATE: 11/01/96

/Materials Testing Inc.



NEW JERSEY DIVISION 180 MILL ROAD - EDISON, NEW JERSEY 08817 - (908) 248-3777 - FAX (908) 248-7979

> <u>REPORT</u>: MTSE-001 DATE: 10/15/96

CLIENT: STORCH ENGINEERING

220 RIDGEDALE AVENUE

P.O. BOX 267

FLORHAM PARK, N.J. 07932

PROJECT: LANDING ROAD BRIDGE Chloride Content

METHOD: ASTM C1218

CORE #1	CHLORIDE CONTENT LBS. PER CUBIC YARD
#01	6.35
#02	6.07
#03	<.81
#04	7.56
#05	1.80
#06A	2.69
#12	6.09

Submitted By:

Acterials Testing Inc.



NEW JERSEY DIVISION 180 MILL ROAD • EDISON, NEW JERSEY 08817 • (908) 248-3777 • FAX (908) 248-7979

Storch Engineering

#### Sample #1A & 1B

Core Length 22" (cores in 3 pieces)

Sample #1 showed stone fracture from the side of the core. The sample also showed a poor paste aggregate bond. The maximum coarse aggregate size was 1" trap-rock. Also included a soft aggregate. With high porosity. The grading on these materials were fair. The fine aggregate was crushed sand with some grains angular in shape over all conditions of sample fair.

#### Sample #2

Core Length 8" (core in 2 pieces)

Sample #2 coarse aggregate size was a maximum of 1" trap rock. The grading on the aggregate was fair. The fine aggregate was a natural sand with some grains angular in shape. The aggregate to paste bond was fair. The overall condition of the sample was found to be fair.

#### Sample #3

Core Length 12 1/2" (core in 2 pieces)

Sample #3 showed maximum aggregate size of 1 1/2:. The granite stone graded was not uniform. The fine sand was natural sand with some grains angular in shape. 1 piece of 3/4" square rebar was noted. Also several other smaller air void less than 1/4" in size. Aggregate to paste bond was fair. The overall condition of core was found to be fair.

#### Sample #5

Core Length 12 1/2"

Coarse aggregate size 1" maximum size trap rock with a fairly graded and uniform. Fine aggregate crushed natural sand that was fairly graded and uniform. Sample had many vertical cracks through the entire core. The bond between stone and paste was poor. No rebar was present in core the overall condition of sample was poor to fair island Edison, NJ NEWINGTON, CT DOVER, DE

"PUT US TO THE TEST"

NEW JERSEY DIVISION 180 MILL ROAD • EDISON, NEW JERSEY 08817 • (908) 248-3777 • FAX (908) 248-7979

## Sample #6A

Core Length 17 1/2" (core in 3 pieces)

Coarse aggregate size 1" maximum size trap rock with a fairly graded and uniform. Throughout core. The fine aggregate is crushed igneous sand that was uniformly graded. The sample appeared to be in good overall conditions.

### Sample #4

Core Length 13 1/2"

Coarse aggregate size of 1" maximum size crushed granite was fairly graded and uniform. Fine aggregate natural sand with some angular grains and uniformly graded paste to aggregate bond was poor. The overall condition of cores was fair.

#### Sample #6B

Core Length 17 1/2" (core in 3 pieces)

Coarse aggregate size 1" maximum size trap rock which was fair graded as well as the fine aggregate the core appeared to have a fair aggregate paste bond. The overall condition of core was fair.

#### Sample #11

Core in 2 pieces

Core #11 had 2 type of concrete one which appears dark gray in color with no coarse aggregate. The maximum aggregate size of 1/4"/ This section of core has noticeable air voids with its overall condition as being fair. The second type of concrete appeared to have 1" maximum size granite rock. The fine aggregate was natural sand with some angular grains. This section of core condition was fair.



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CLIENT: STORCH ENGINEERING

PROJECT: Landing Road Bridge

Date: 10/08/96 Lab No: ENC-001

TEST REQUIRED:

Identification		#01	#02	#03	#04	#05	#06a	#06b
Length of Core Submitted:		22"	8"	12 1/2"	13	12 1/2"	17 1/2"	17 1/2"
Length of Core Prepared:	<u>a</u> b		5.050	7.370	5.640	4.580	5.960	5.760
Diameter:	_	3.7"	3.7"	3.7"	3.7"	3.7"	3.7"	3.7"
Area		11.34	11.34	11.34	11.34	11.34	11.34	11.34
Ratio H to D:		1.84	1.36	1.99	1.52	1.24	1.61	1.56
Correction Factor:		.987 1.00	.942	. 998	.962	.929	. 929	.965
	<u>a</u> b	55,000 53,000	62,000	48,000	46,000	63,000	53,500	40,000
Corrected Load Lbs.:	a	4.850 4.720	5.470	4.230	4.060	2.910	4.720	3.530
Corrected P.S.I.	j	4780 4720	5150	4220	3910	2700	4570	3410
Cinder of Base Measurement:								

Location:



NEW JERSEY DIVISION 180 MILL, ROAD + EDISON, NEW JERSEY 08817 + (908) 248-3777 + FAX (908) 248-7979

Page #2 Report #ENC-001

Identification	#11	#12	#13				
Length of Core Submitted:	10 1/2"	12"	12"				
Length of Core Prepared:	6:720	7.450	7.420				<del> </del>
Diameter:	3.7"	3.7"	3.7"				
Area	11.34	11.34	11.34				
Ratio H to D:	1.82	2.0	2.0				
Correction Factor:	.986	1.00	1.00				
Crushing Load Lbs.:	46,500	44,000	35,000				
Corrected Load Lbs.:	4100	3880	3090		1		
Corrected P.S.I.	4040	3880	3090				
Cinder of Base Measurement:						···	

Location:

APPENDIX E

HCM: SIGNALIZED INTERSECTION SUMMARY Version 2.4a 05-28-1996

Center For Microcomputers In Transportation

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD File Name: LANDEPM.HC9

Area Type: Other 5-28-96 PM PEAK

Comment: EXISTING CONDITIONS (1994 DATA)

======================================	======	=====	====	======	======	:	======	====	=====	=====		
	Ea	stbou	nd	Wes	tbound		Nor	thbo	und	So	uthbou	ınd
	L	T	R	L	T	R	L	T	R 	L	T	R
No. Lanes Volumes PHF or PK15 Lane Width Grade % Heavy Veh Parking Bus Stops Con. Peds Ped Button Arr Type RTOR Vols Lost Time Prop. Share Prop. Prot.			0	1 0.90 12.0 2 (Y/N) (Y/N) 3	0 1 N Y 12.	1 304 .90 2.0 2 0 0 3 s 3 0	(Y/N)	ү 8 3	0.90 2 0.90 3.5 s	0.90	12.0 0 2 N Y 4 3	0 0
Phase Combin	ation	1	2	Signa:	l Oper	atio	ens	9	5	б	7	8
EB Left Thru						NB	Left Thru	*	r			
Right Peds							Righ Peds		ł.			
WB Left		*				SB	Left			*		
Thru						I	Thru	*	+	*		

Thru Thru Right Right Peds Peds EB Right NB Right WB Right SB Right 8.0A Green 48.0A 22.0A Green Yellow/AR 4.0 4.0 Yellow/AR 4.0 Cycle Length: 90 secs Phase combination order: #1 #5 #6

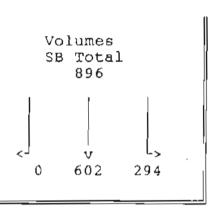
			<del></del>			<del></del>			
	Lane	Group:	Intersect Adj Sat	tion Perf v/c	ormance : g/C	Summary		Approa	ch:
	Mvmts	Cap	Flow	Ratio	Ratio	Delay	LOS	Delay	LOS
				~~				<del>-</del>	
WB	L	177	1770	0.356	0.100	25.0	С	16.2	С
	R	616	1583	0.549	0.389	14.6	В		
NВ	TR	996	1830	1.495	0.544	*	*	*	*
SB	L	452	1770	0.723	0.256	23.6	C	8.7	В
	${f T}$	1552	1863	0.431	0.833	1.4	Α		
		Inte	ersection	Delay =	* (sec/ve	eh) Int	ersect	tion LOS	<b>≒</b> ★
Laic	11 * / 17 / 61	is area	ter than	one Cal	culation	of DI is	infe	asable.	

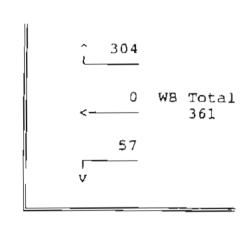
(g/C)*(V/c) is greater than one. Calculation of DI is infeasable.

#### INTERSECTION DIAGRAM

Intersection: LANDING RD/MT ARLI'G and LAKESIDE BOULVARD

Time period: PM PEAK





Legend

[X] = Level of Service

# - I = No. of Lanes

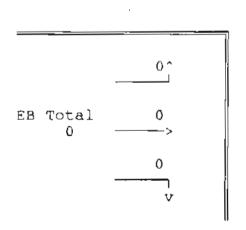
D-J ≈ De facto Turn

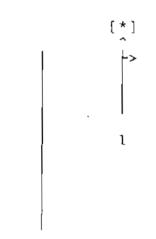
_____

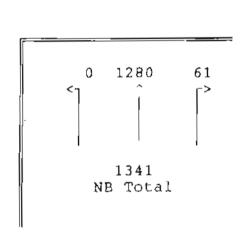


Intersection
[*]

LANDING RD/MT ARLI'G







HCM: SIGNALIZED INTERSECTION SUMMARY Version 2.4a 05-28-1996

Center For Microcomputers In Transportation

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD Analyst: ST File Name: LANDEAM.HC9

Area Type: Other 5-28-96 AM PEAK

Comment: EXISTING CONDITIONS (1994 DATA)

Eastbound   Westbound   Northbound   Southbound													
	L	Т	Ř	L	T	Ŕ	L	T	R	L	T	R	
							4						
No. Lanes Volumes PHF or PK15 Lane Width Grade % Heavy Veh Parking Bus Stops Con. Peds Ped Button Arr Type RTOR Vols Lost Time Prop. Share Prop. Prot.			0	1 100 0.90 12.0 2 (Y/N) (Y/N) 3	0 N Y 14.	1 137 .90 2.0 2 0 0 5 s 3 0	(Y/N) (Y/N)	211 0.90 12.0 0 2 N	2 0 0 8.5 s	0.90 12.0 2 (Y/N)	12.0 0 2 N Y 4	0 0	
	· <b>-</b>			Signa	l Oper	atio	 ons				·		
Phase Combin	ation	1	2	3	4			. 5	5	6	7	8	
EB Left					_	NB	Left					•	
Thru							Thru		-				
Right							Righ		•				
Peds							Peds						
WB Left		*				SB	Left			*			
mb						1	m b			+			

Pila	se committant	OH I	∠.	2	4			J	D	,	Ö
EB	Left				1	NB	Left				
	Thru						Thru	*			
	Right						Right	*			
	Peds						Peds				
WB.	Left	*			] (	SB	Left		*		
	Thru						Thru	*	*		
	Right .	*					Right				
	Peds						Peds				
NB	Right				1 5	ΞB	Right				
SB	Right				V	√B	Right		*		
Gree	en	12.0A				Gree	en 54	.OA :	12.0A		
Yell	low/AR	4.0			7	Yel]	Low/AR 4	1.0	4.0		
Cycl	le Length:	90 secs	Phase	combi	natio	on c	order: #	1 #5	#6		

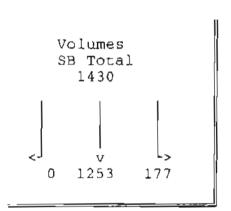
	Lane	Group:	Intersect Adj Sat	v/c	g/C	_		Approa	
	Mvmts	Cap	Flow	Ratio	Ratio	Delay	LOS	Delay	LOS
			1222					10.6	
WB	L	256	1770	0.434	0.144	23.5	C	18.5	С
	R	510	1583	0.298	0.322	14.9	В		
NB	TR	2142	3505	0.189	0.611	5.0	A	5.0	Α
SB	L	256	1770	0.771	0.144	33.0	Ď	17.0	С
	${f T}$	1470	1863	0.947	0.789	14.7	В		
			ersection 1						= B
7	mi = -10	Trees T			-:1/	- / 44 \ -	- 0 0 5 6	3	

Lost Time/Cycle, L = 6.0 sec Critical v/c(x) = 0.868

### INTERSECTION DIAGRAM

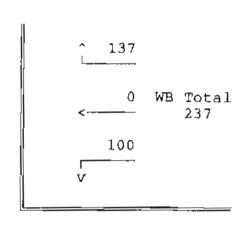
Intersection: LANDING RD/MT ARLI'G and LAKESIDE BOULVARD

Time period: AM PEAK



LAKESIDE BOULVARD

[B][D]



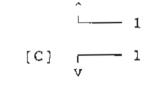
Legend

[X] = Level of Service

#- No. of Lanes

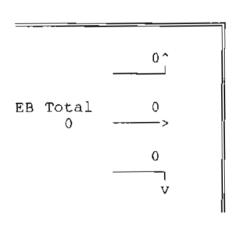
D- = De facto Turn

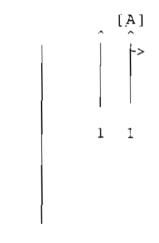
_____

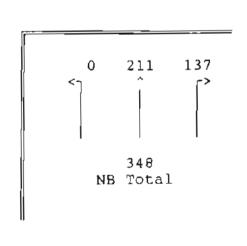


Intersection [B]

LANDING RD/MT ARLI'G







BLIDGE

HCM: SIGNALIZED INTERSECTION SUMMARY Version 2.4a 05-28-1996

Center For Microcomputers In Transportation

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD Analyst: ST File Name: LANDEPM.HC9

Area Type: Other 5-28-96 PM PEAK

Comment: EXISTING CONDITIONS (1994 DATA)

==========	=====	====	-====	=====	===	=======		====	=====	=====	======	
	Ea	astbo	und	Wes	tbo	ound	No	rthbo	ound	Sou	ithbou	ınd
	L	${f T}$	R	Ŀ	${\mathbb T}$	R	L	$\mathbf{T}$	R	L	${f T}$	R
			<del>-</del>						<del>-</del>		~	
No. Lanes				1		1		2	<	1	1	
Volumes	]			57		304		1280	61	294	602	
PHF or PK15				0.90		0.90		0.90	0.90	0.90	0.90	
Lane Width				12.0		12.0		12.0		12.0	12.0	
Grade						0		0	1		0	
% Heavy Veh				2		2		2	2	2	2	
Parking				(Y/N)	N		(Y/N)	N		(Y/N)	N	
Bus Stops	J					0			0			0
Con. Peds			0			0			0			0
Ped Button				(Y/N)	Y	14.5 ៩	(Y/N)	Y	8.5 8	(Y/N)	Y 4	.0
Arr Type				3		3		3		3	3	
RTOR Vols				ĺ		0			0			0
Lost Time				3.00		3.00		3.00	3.00	3.00	3.00	
Prop. Share												
Prop. Prot.												

			\$.	ignal	Opera	atio	ns				
Pha	se Combinati	on 1	2	3	4			5	6	7	8
EB	Left					ИB	Left				
	Thru						Thru	*			
	Right					ĺ	Right	*			
	Peds						Peds				
WB	Left	*				SB	Left		*		
	Thru						Thru	*	*		
	Right	*					Right				
	Peds						Peds				
ИВ	Right					EB	Right				
SB	Right					WB	Right		*		
Gre	**	8.0A				Gre		.0A 2	22.0A		
Yel	low/AR	4.0				Yel	low/AR 4	.0	4.0		
		90 secs	Phase	e comb	oinat:	ion	order: #	1 #5	#6		

Intersection Performance Summary

Lane Group: Adj Sat v/c g/C Approach:

Mvmts Cap Flow Ratio Ratio Delay LOS Delay LOS

L 177 1770 0.356 0.100 25.0 C 16.2 C

R 616 1583 0.549 0.389 14.6 B

TR 2014 3700 0.776 0.544 11.8 B 11.8 B

L 452 1770 0.723 0.256 23.6 C 8.7 B

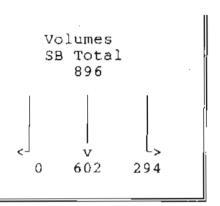
T 1552 1863 0.431 0.833 1.4 A WB NB SB

Intersection Delay = 11.4 sec/veh Intersection LOS = B Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.714

#### INTERSECTION DIAGRAM

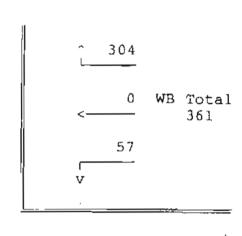
Intersection: LANDING RD/MT ARLI'G and LAKESIDE BOULVARD

Time period: PM PEAK



LAKESIDE BOULVARD 1 1

[A][C]



Legend

{X) = Level of Service

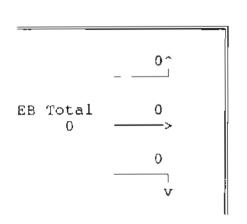
#— = No. of Lanes

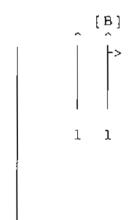
D- = De facto Turn

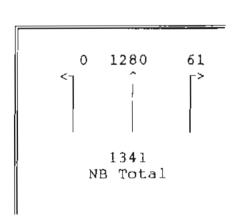
- 1

Intersection [B]

LANDING RD/MT ARLI'G







Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD
Analyst: ST File Name: LANDAM04.HC9

Area Type: Other 5-28-96 AM PEAK

Comment: 2004 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	Eastbound		Westbound			No	rthbou	ind	Southbound			
	L	T	R	Ľ	T	R	Ļ	T	R	L	${f T}$	R
No. Lanes Volumes PHF or PK15 Lane Width Grade % Heavy Veh Parking Bus Stops Con. Peds Ped Button Arr Type RTOR Vols Lost Time Prop. Share Prop. Prot.			0	1 128 0.90 12.0 2 (Y/N) 3.00		1 175 0.90 12.0 2 0 4.5 s 3 0 3.00	(Y/N)		175 0.90 2 0 0 1.5 s		) Y 4	0 0 0 0 0
				Signa	1 Ope	eratio	<b>-</b>					
Phase Combir	nation	1 1	2	3		4		5		6	7	8
EB Left						ИВ	Left					
<b>Thru</b> Right							Thru Righ					
Peds							Peda					
WB Left		*				SB	Left			*		
Thru							Thru			*		
Right		*					Righ Pede					
Peds NB Right	•					EB	Righ					
SB Right						WB	Righ			*		
Green	10	.OA				Gre	_		A 60.	OΑ		
Yellow/AR	4	.0				Yel	llow/A	R 4.0	4.	0		

	Lane	Group:	Intersect: Adj Sat	ion Perfo V/c	ormance q/C	Summary		Approac	ch:
	Mvmts	Cap	flow	Ratio	Ratio	Delay	LOS	Delay	LOS
							<b>-</b>		
WB	L	195	1770	0.729	0.110	36.5	D	16.8	С
	R	1187	1583	0.163	0.750	2.3	A		
NB	TR	666	3506	0.779	0.190	29.0	D	29.0	D
SB	L	1080	1770	0.233	0.610	5.7	В	*	*
	T	1546	1863	1.152	0.830	*	*		

Cycle Length: 100 secs Phase combination order: #1 #5 #6

Intersection Delay = * (sec/veh) Intersection LOS = * (g/C)*(V/c) is greater than one. Calculation of D1 is infeasable.

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD Analyst: ST File Name: LANDPM04.HC9

Area Type: Other 5-28-96 PM PEAK

Comment: 2004 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	E	stbou	nd	Wes	tboun	 d	Noi	rthbo	und	Soi	ıthboı	und
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes Volumes PHF or PK15 Lane Width Grade % Heavy Veh Parking Bus Stops Con. Peds Ped Button Arr Type RTOR Vols Lost Time Prop. Share Prop. Prot.			0	1 73 0.90 12.0 2 (Y/N) 3 3.00	0 N Y 14	1 389 0.90 12.0 2 0 .5 6 3 0	(Y/N)	1638 0.90 12.0 0 2 N	78 0.90 2 0 8.5 s	0.90 12.0 2 (Y/N) (Y/N)	12.0 0 2 N Y 4	0 0
				Signa	1 Ope	ratio	ons					
Phase Combin	ation	1	2	3	4				5	6	7	8
EB Left Thru						NB	Left Thru		k			
Right							Righ		` <b>k</b>			
Peds							Peds					
WB Left		*				SB	Left			*		
Thru							Thru		k	*		
Right Peds		•					Righ Peds					
NB Right						EB	Righ					
SB Right						WB	Rìgh			*		
Green		.0A				Gre			A 24.			
Yellow/AR	4		<b>53</b> b		_b :		llow/A					
Cycle Length	: 100		Pn8 	se com	mpinat		order 	: #1	#5 #b	) 		

	Lane Mvmts	Group:	Intersect: Adj Sat Flow	ion Perf v/c Ratio	ormance : g/C Ratio	Summary Delay	LOS	Approa	ch: LOS
	MAIIICB	Cap	T TOW	Kacio	Vacto	Deray	1103	Deray	700
	~-								
WB	L	195	1770	0.416	0.110	27.7	D	20.3	С
	R	617	1583	0.700	0.390	19.0	C		
NB	TR	2034	3698	0.984	0.550	26.6	D	26.6	D
SB	L	442	1770	0.945	0.250	45.0	E	16.2	C
	T	1546	1863	0.554	0.830	2.1	A		
			ersection D	-					= C
roct	Time //	Cracle I	- 0.0 04	ar Cri	tical v/c	¬/vr\ =	- 0 904	3	

Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.905

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD

Analyst: ST File Name: LANDAM14.HC9
Area Type: Other 5-28-96 AM PEAK

Comment: 2014 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	Eastbo	ind R	   Wes	tbound	i R	Nor	thbou T	ınd R	So L	uthbor T	ind R
No. Lanes Volumes PHF or PK15 Lane Width Grade % Heavy Veh Parking Bus Stops Con. Peds Ped Button Arr Type RTOR Vols Lost Time Prop. Share Prop. Prot.		0	1 164 0.90 12.0 2 (Y/N) (Y/N) 3	0 N Y 14.	1 225 1.90 2.0 2 0 0 5 6 3 0	(Y/N) (Y/N)	2 < 346 0.90 12.0 0 2	225 0.90 2 0 0 .5 s	0.90	12.0 0 2 N Y 4	0 0 0
Phase Combine EB Left Thru Right Peds WB Left Thru Right Peds NB Right SB Right SB Right Green Yellow/AR Cycle Length	* * 16.0A 4.0	2 Pha	Signa 3	l Oper 4 mbinat	NB S8 EB WB Gre	Left Thru Right Peds Left Thru Right Peds Right Right	t * t t 27.0	A 65.	0	7	8

			Intersect	tion Perf	ormance	Summary			
	Lane	Group:	Adj Sat	V/C	g/C	-		Approa	⊊h:
	Mvmts	Cap	Flow	Ratio	Ratio	Delay	LOS	Delay	LOS
				<b></b>		~~			
WB	L	251	1770	0.726	0.142	38.6	D	18.4	C
	R	1134	1583	0.220	0.717	3.7	A		
NB	TR	819	3508	0.814	0.233	32.5	D	32.5	D
SB	L	974	1770	0.331	0.550	9.7	В	*	*
	T	1506	1863	1.515	0.808	*	*		
		Int	ercection	Delay = :	<pre>* /sec/v</pre>	eh) Int	ersect	tion LOS	= *

Intersection Delay = * (sec/veh) Intersection LOS = * (g/C)*(V/c) is greater than one. Calculation of D1 is infeasable.

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULVARD File Name: LANDPM14.HC9

Area Type: Other

5-28-96 PM PEAK

Comment: 2014 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate) 

1	Εε	astbou	nd	Wes	tbo	und		Nox	thbo	ınd	l So	uthbou	ind
	L	${f T}$	R	L	T	R	1	L	$\mathbf{T}$	R	L	T	R
No. Lanes Volumes PHF or PK15 Lane Width Grade % Heavy Veh Parking Bus Stops Con. Peds Ped Button Arr Type RTOR Vols Lost Time Prop. Share Prop. Prot.			0	1 93 0.90 12.0 2 (Y/N) (Y/N) 3	N	1 49 0.9 12.0	.0 2 0 0 s 3	(Y/N)	2097 0.90 12.0 0 2 N	100 0.90 2 0 3.5 6	0.90 12.0 2 (Y/N) (Y/N) 3	12.0 0 2 ) N	0 0 0
				Signa	1 0	perat	io	ns					
Phase Combin	ation	1	2	3		4			9	5	6	7	8
EB Left						l N	18	Left					
Thru								Thru	*				
Right						ľ		Righ		,			
Peďs								Peds					
WB Left		*				S	SB	Left			*		
Thru								Thru	*		*		
Right		*						Righ	t				
Peds								Peds					
NB Right						E	EВ	Righ	t				
SB Right							√B	Righ			*		
Green	14	.0A					Sre			A 28.	0A		
Yellow/AR		.0						low/A					
Cuala Langth	1 720	0000	Dha		<u>- h i -</u>								

	Lane	Group:	Intersect Adj Sat		formance q/C	Summary		Approa	ch:
	Mvmts	Cap	Flow	Ratio	Ratio	Delay	LOS	Delay	LOS
									<b>-</b> -
WB	L	221	1770	0.466	0.125	32.7	D	32.8	D
	R	620	1583	0.892	0.392	32.8	D		
NB	TR	2065	3698	1.241	0.558	*	*	*	*
SB	L	428	1770	1.253	0.242	*	*	*	*
	${f T}$	1537	1863	0.714	0.825	4.0	Α		
		Inte	ersection	Delay =	* (sec/v	eh) Int	ersect	tion LOS	= *
1 - 11	71 + / YT / m			" (" - )	~l ~ + +	- £ D1 4.	. :		

Cycle Length: 120 secs Phase combination order: #1 #5 #6

(q/C)*(V/c) is greater than one. Calculation of D1 is infeasable.

APPENDIX F

APPENDIX G

1/00,000 10,000 30,000 0 2,300,000 10,000 2,500 0 20,000
10,000 30,000 0 2,300,000 10,000 70,000 2,500 0 20,000
0 2,300,000 10,000 70,000 2,500 0 20,000
2,300,000 10,000 10,000 2,500 0 20,000
10,000 10,000 2,500 0 20,000
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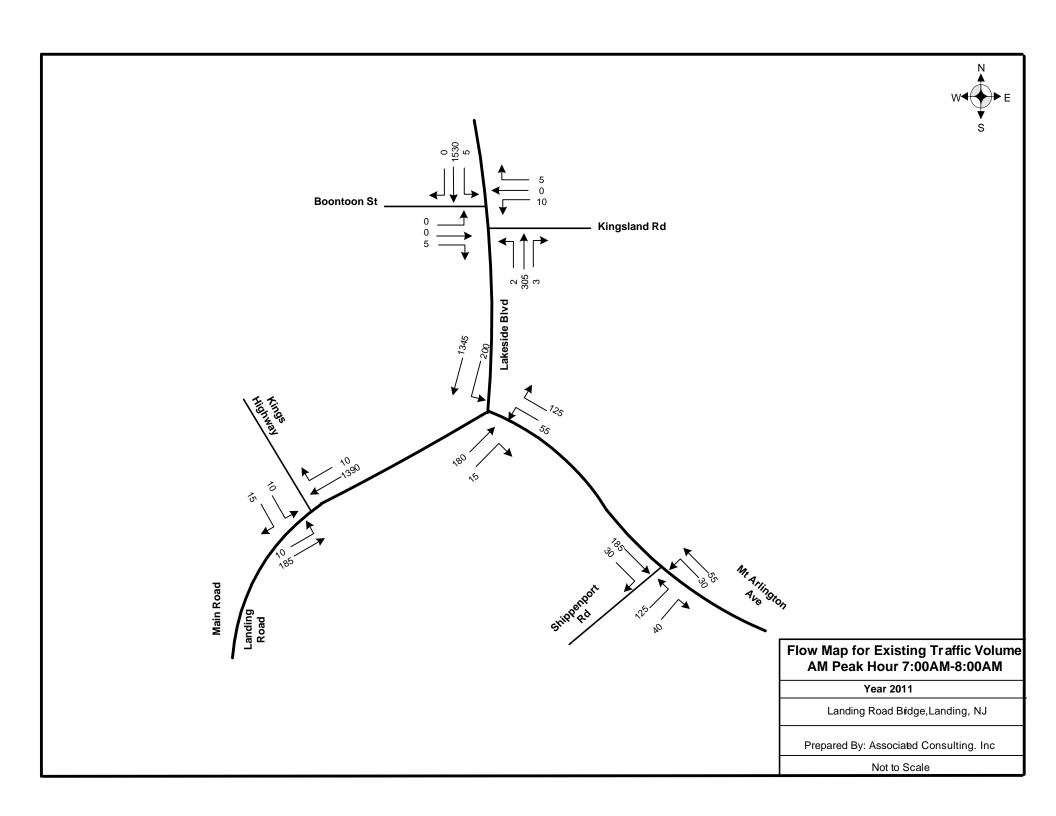
Classification No. 2 - RECONSTRUCTION, WIDENING & DUALIZATION

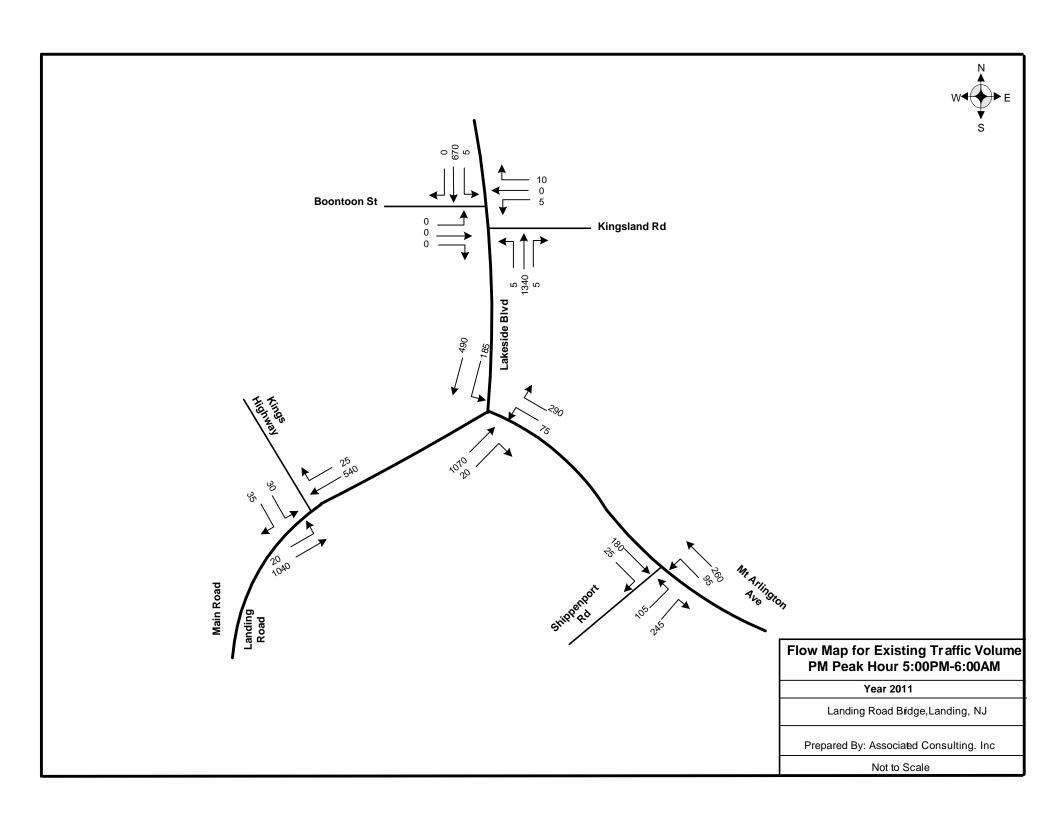
### **ESCALATION**

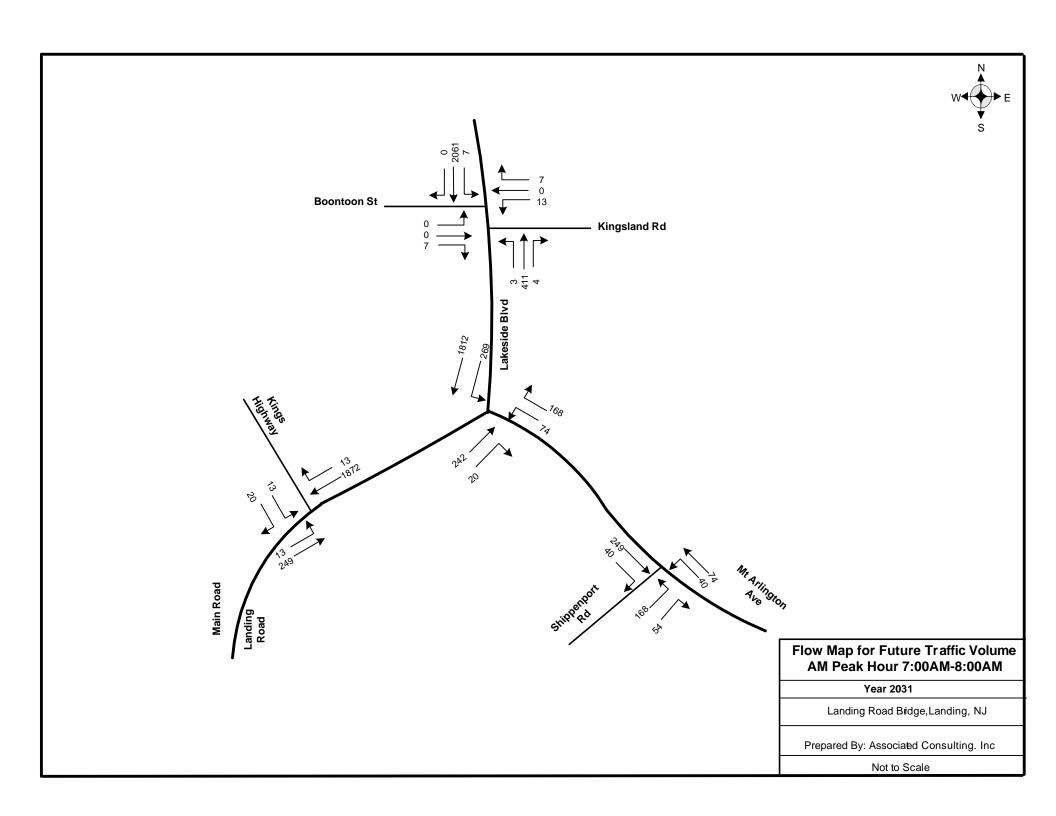
P.T.W.C. Y = Number of Years until midpoint of construction. If midpoint is less than 2 years no escalation is required. F- y=3

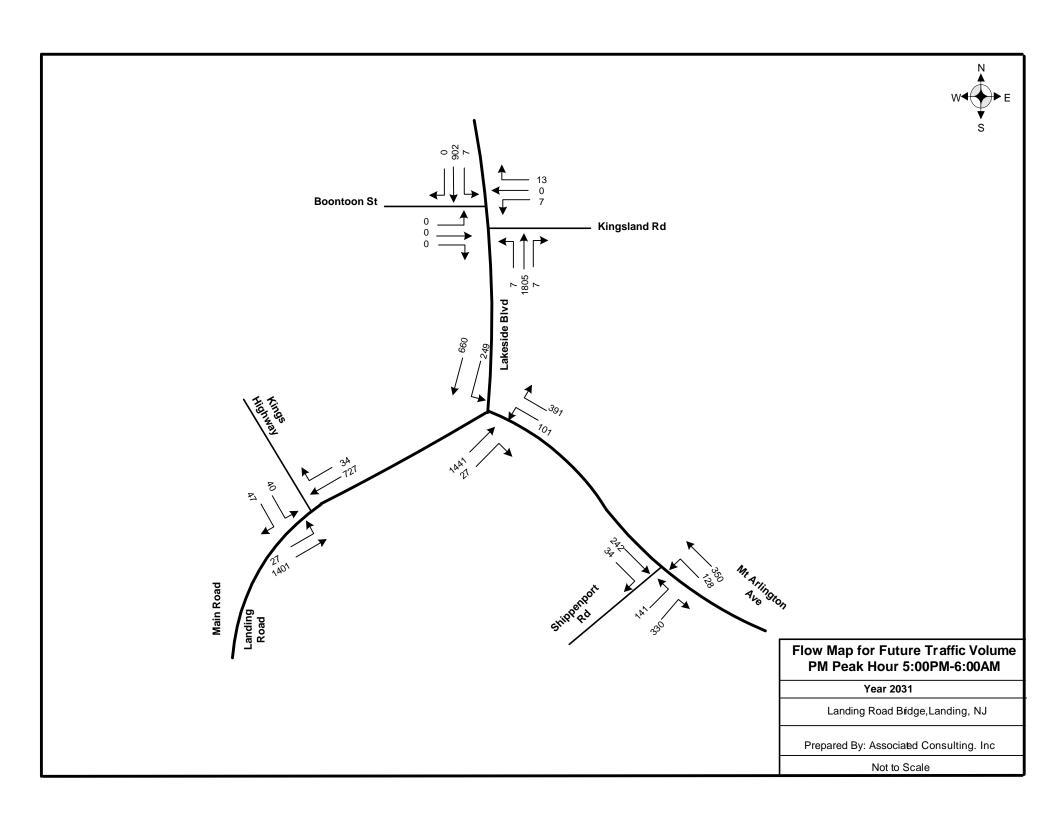
UTILITY RELOCATION COST

# APPENDIX – H TRAFFIC STUDY 2011









# AM Peak Hour Volume Summary Existing: Year 2011 Reconstruction of Landing Road Bridge, New Jersey

			V	olume									
S. No	Intersection		NB			SB			EB			WB	
5. 110	mersection	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	10	185			1390	10	10		15			
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.		180	15	200	1345					55		125
3	Mt. Arlington Blvd and Shippenport Rd.	125		40					185	30	30	55	
4	Lakeside Blvd. and Kingsland Rd/Canal St.	2	305	3	5	1530	0	0	0	5	10	0	5

# PM Peak Hour Volume Summary Existing: Year 2011 Reconstruction of Landing Road Bridge, New Jersey

			V	olume									
S. No	Intersection		NB			SB			EB			WB	
5. 110	intersection	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	20	1040			540	25	30		35			
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.		1070	20	185	490					75		290
3	Mt. Arlington Blvd and Shippenport Rd.	105		245					180	25	95	260	
4	Lakeside Blvd. and Kingsland Rd/Canal St.	5	1340	5	5	670	0	0	0	0	5	0	10

# AM Peak Hour Volume Summary (No Build: Year 2031) Reconstruction of Landing Road Bridge, New Jersey

			Vo	olume									
S. No	Intersection		NB			SB			EB			WB	
5. 110	mersection	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	13	249	0	0	1872	13	13	0	20	0	0	0
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.	0	242	20	269	1812	0	0	0	0	74	0	168
3	Mt. Arlington Blvd and Shippenport Rd.	168	0	54	0	0	0	0	249	40	40	74	0
4	Lakeside Blvd. and Kingsland Rd/Canal St.	3	411	4	7	2061	0	0	0	7	13	0	7

# PM Peak Hour Volume Summary ( No Build : Year 2031) Reconstruction of Landing Road Bridge, New Jersey

			V	olume									
S. No	Intersection		NB			SB			EB			WB	
S. NO	intersection	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	27	1401	0	0	727	34	40	0	47	0	0	0
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.	0	1441	27	249	660	0	0	0	0	101	0	391
3	Mt. Arlington Blvd and Shippenport Rd.	141	0	330	0	0	0	0	242	34	128	350	0
4	Lakeside Blvd. and Kingsland Rd/Canal St.	7	1805	7	7	902	0	0	0	0	7	0	13

# **APPENDIX I**

SCHEMATIC OF ALTERNATE 7C WITH SIDEWALKS ON BOTH SIDES OF BRIDGE

