

ALTERNATIVES ANALYSIS

FOR

**LANDING ROAD BRIDGE OVER NJ TRANSIT
AND MORRIS CANAL**

MORRIS COUNTY, NEW JERSEY

Prepared for

**Morris County Department of Public Works
Morristown, New Jersey**

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EXECUTIVE SUMMARY

This Alternatives Analysis report documents the selection of the preferred alternative for the proposed Landing Road Bridge Project, providing information on the alternatives considered for the project and their effectiveness in addressing the issues generating the need for the project. The report includes a summary of the anticipated effects of the alternatives on socioeconomic resources, right-of-way, natural resources, cultural resources, and hazardous substances, as well as a summary of estimated project costs. This project, while utilizing both state and federal funding in its early stages, will now use only state funding as it moves forward into design and through construction.

The historic significance of the Landing Road Bridge (County Bridge No. 73) requires that preliminary cultural resources information be a focus of this report. The bridge is a contributing element within the Morris Canal Historic District, which was listed on the New Jersey Register of Historic Places on November 16th, 1973 and on the National Register of Historic Places on October 1st, 1974. The Landing Road Bridge was also determined individually eligible for listing on the New Jersey and National Registers of Historic Places in a SHPO Opinion on February 25, 1994. The bridge is also considered a key contributing resource to the Old Main, Delaware, Lackawanna & Western Railroad Historic District. The New Jersey Register of Historic Places Act requires that any undertaking that will “encroach upon, damage or destroy” a resource listed on the New Jersey Register is subject to review. This review commences with the submission of an Application for Project Authorization to the New Jersey Historic Preservation Office, with a determination of Encroachment or No Encroachment to result. In the event that the project is ruled to be an Encroachment, a further review by the New Jersey Historic Sites Council will be required. The process concludes with final action being taken by the Commissioner of the Department of Environmental Protection based on recommendations provided by the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council.

The bridge was constructed in 1907 and carries Landing Road over the NJ TRANSIT Railroad and the former Morris Canal in Roxbury Township, Morris County, New Jersey. The project is located near the town of Landing in the western portion of Morris County in north-central New Jersey. Landing lies at the southwestern tip of Lake Hopatcong, a portion of which is bisected by the Morris-Sussex county line. Landing Road extends from Interstate Route 80 to the northwest approximately 0.8 miles to the southern edge of Lake Hopatcong. Landing Road travels over the former Delaware, Lackawanna and Western Railroad (currently the NJ TRANSIT Montclair-Boonton Line) via the Landing Road Bridge (County Bridge No. 73) and intersects with Lakeside Boulevard (County Route 602) and Mount Arlington Boulevard. The Landing Road Bridge provides one of the few major access points for residents on both the east and west shores of Lake Hopatcong. Lakeside Boulevard provides access to the Landing Road Bridge from the north and to Mount Arlington Boulevard from the south.

A July 2003 engineering evaluation of the bridge concluded that the bridge is structurally deficient due to the “poor” condition of the superstructure and the “serious” condition of the substructure. In addition, the bridge was determined to be functionally obsolete due to inadequate deck geometry. Subsequent evaluations, including one conducted in July 2009 & October 2011, have resulted in the same conclusions. Although the latest inspection report dated September 2013 has upgraded the bridge to “fair” condition for both the superstructure and substructure due to recent concrete repairs that have been made, the inadequate deck geometry of the bridge continues to be classified as functionally obsolete (see Appendix B for the September 2013 evaluation). To respond to these overall needs, Morris County began evaluating options for replacing or rehabilitating the existing bridge.

The results of the 2003 evaluation were presented to the Roxbury Township Council at a Workshop Meeting on April 14, 2003, and the Council issued a resolution supporting the Replacement Alternative (Alternative No. 7-C) for the project. A copy of the resolution of support issued by the Roxbury Township Council is included in Appendix D of this report. The project was again presented on June 9, 2015 to the Roxbury Township Council updating them on the status of the project with the various alternates studied. On August 12, 2015 a Public Information was held at the Roxbury Township Municipal Building with Morris County and the RBA Group and alternate 7-C was again the recommended alternate presented and received support from the public and Township, Resolution 2015-310 was adopted on September 29, 2015 by the Township of Roxbury supporting this alternate and is included in Appendix D of this report.

The Township of Roxbury has requested that the sidewalk be extended on the south eastern side of the bridge to the end of the project limits and across the bridge on both sides. Additionally on Lakeside Boulevard the sidewalk will be widened and extended in front of the wall at Lake Hopatcong and the current fence replaced with new aesthetical pleasing fence. A schematic of Alternate 7-C with the sidewalk improvements is included in Appendix I.

The Replacement Alternative (Alignment No. 7-C) was selected as the Initially Preferred Alternative (IPA) because it would best address the project needs and would have a limited number of environmental impacts, at a comparatively low cost. The Replacement Alternative would improve safety and traffic operations in the project area, provide a new widened bridge meeting current design standards, and allow maintenance of traffic on the existing bridge while the new bridge is being constructed. The Replacement Alternative is recommended for the following reasons:

1. The alternative would satisfy the traffic operations needs of the project area.
2. The alternative is compatible with the proposed widening of Landing Road to I-80.
3. The alternative would improve the safety of the project area.
4. The new bridge would meet current structural design standards.
5. The proposed construction operations would not impact existing wetlands or regulated hazardous and solid waste sites in the project area.
6. The construction cost of the Replacement Alternative is significantly lower than that of the Bypass Alternative.
7. Traffic could be maintained during construction of the Replacement Alternative, although delays and increased travel time and distance are likely.

8. The alternative is compatible with the streetscape improvements project being undertaken by the Roxbury Township.

The IPA involves the demolition of the existing historic bridge. Consultation with the New Jersey Historic Preservation Office has preliminarily determined that the IPA will represent in an Encroachment under the New Jersey Register of Historic Places Act as the Landing Road Bridge is a contributing resource within the Morris Canal Historic District. In addition, archaeological features contributing to the eligibility of the Morris Canal Historic District may be impacted during construction, and this would also represent an Encroachment under the New Jersey Register of Historic Places Act.

The project team anticipates continuing consultation with the New Jersey Historic Preservation Office and with the New Jersey Historic Sites Council to ensure that the design of the new structure reflects the shape and appearance of the existing bridge to the greatest extent possible and that other roadway and landscape design elements are consistent with the character of the historic district. In addition, the project team will develop an archaeological monitoring plan as the initial phase of work necessary to address the above noted potential for archaeological resources associated with the district, again through continuing consultation with the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council.

Morris County will also consult with the Roxbury Township Historic Advisory Committee and the Landing Gateway Committee during design of the project to comply with their recommendations of a two span arch structure, the proposed railing design, concrete texture of the surfaces to mimic the historic look of the original bridge, lighting, plantings, pedestrian crossing, bollards, sidewalks, parking and the fence details at the lake.

This report was revised by The RBA Group in February of 2015 to reflect the project's planned use of only state funding for design and construction and to provide an updated list (with an associated graphic) of property owners located within 200 feet of the project area. Morris County further revised the report in October 2015 to reflect SHPO comments in their letter dated October 9, 2015.

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SECTION 1 - INTRODUCTION / SPONSOR

1.1 SPONSOR

The Landing Road Bridge Project considered various alternatives for the replacement or rehabilitation of existing County Bridge No. 73. This bridge was constructed in 1907 and carries Landing Road over the New Jersey TRANSIT (NJ TRANSIT) Montclair-Boonton Line railroad and the former Morris Canal in Roxbury Township, Morris County, New Jersey.

The sponsor of the Landing Road Bridge Project and this alternatives analysis is the Morris County Department of Planning & Public Works in the State of New Jersey. The work under this project also includes coordination with Morris County, the New Jersey Department of Transportation (NJDOT), the North Jersey Transportation Planning Authority (NJTPA), the Federal Highway Administration (FHWA), and other affected local groups, officials, and residents. Because the Landing Road Bridge continues to deteriorate, the County of Morris requested and obtained federal funding through NJTPA to complete this alternatives analysis, which examines alternatives considered throughout the course of the project and documents the selection of Alternative 7-C as the Initially Preferred Alternative (IPA).

1.2 PURPOSE OF THE ALTERNATIVES ANALYSIS

The purpose of this report is to document the evaluation of alternatives considered for the Landing Road Bridge Project. The evaluation focuses on those alternatives that were considered reasonable from a cost, engineering, and environmental perspective. For the evaluation, each alternative was assessed to determine how well it met the project purpose and need, its potential impacts on the natural and built environments, and its anticipated costs and affordability. Based on this information, input from the local community and other interested parties, and Resolutions of Support from the Roxbury Township Council, Alternative 7-C was selected as the IPA. Alternative 7-C involves the replacement of County Bridge No. 1400-073 along a similar alignment.

Federal funds for future phases of the project were formerly to be provided through NJTPA in partnership with NJDOT – it has now been determined that these future phases will be financed using only State funding. State regulations require that the lead agencies take into account the effects of the project on any recognized historic properties during the planning stages of the project.

The New Jersey Register of Historic Places Act of 1970 (Chapter 268 and Procedures for Project Authorization) outlines the procedures to follow if a proposed project will encroach on a property listed in the New Jersey Register of Historic Places. These procedures include consultation with the New Jersey Historic Preservation Office and other interested parties regarding the Area of Potential Effect (APE) (i.e., the area within which the project undertaking may have direct or indirect impacts on listed or eligible properties). If the project will encroach on historic properties, then the Act outlines the procedures to be followed, including:

- (1) Preparation of documentation to assess project impacts (encroachment) on the property;
- (2) Attendance at public meetings;
- (3) Consultation with local governments, property owners, historic preservation groups, and others; and
- (4) Preparation of an application for project authorization for review by the SHPO and the New Jersey Historic Sites Council. Based on the recommendations of the Historic Sites Council, the Commissioner of the Department of Environmental Protection may authorize the encroachment with or without conditions or deny the application with specific reasons.

While a number of historically significant cultural resources have been identified within the APE for the Landing Road Bridge Project, only one – the Morris Canal Historic District – is listed on the New Jersey Register of Historic Places. A *Phase 1a Archaeological Investigation* completed for the project in July of 2003 identified areas of archaeological potential associated with the Morris Canal Historic District and recommended that these areas be monitored during construction of the project. The three areas noted are area 1, around the eastern abutment, area 2, off the south-eastern corner of the bridge at the location of the former post office and area 3, the western abutment and to the south of the western abutment. The New Jersey Historic Preservation Office concurred with the findings of this report and the proposed strategy for addressing these potential archaeological issues in a letter dated February 7, 2006.

1.3 CONTENTS OF THE ALTERNATIVES ANALYSIS

This report documents the selection of the preferred alternative for the proposed project, providing information on the alternatives considered for the project and their effectiveness in addressing the issues generating the need for the project. The results of these evaluations and comparative analyses of the alternatives considered are compiled in the report's remaining sections:

- Section 2 - Project Location
- Section 3 - Detailed Description of Structure
- Section 4 - Purpose and Need
- Section 5 - Project Alternatives
- Section 6 - Environmental Impacts
- Section 7 - Evaluation of Alternatives
- Section 8 - Recommendation
- Section 9 - References

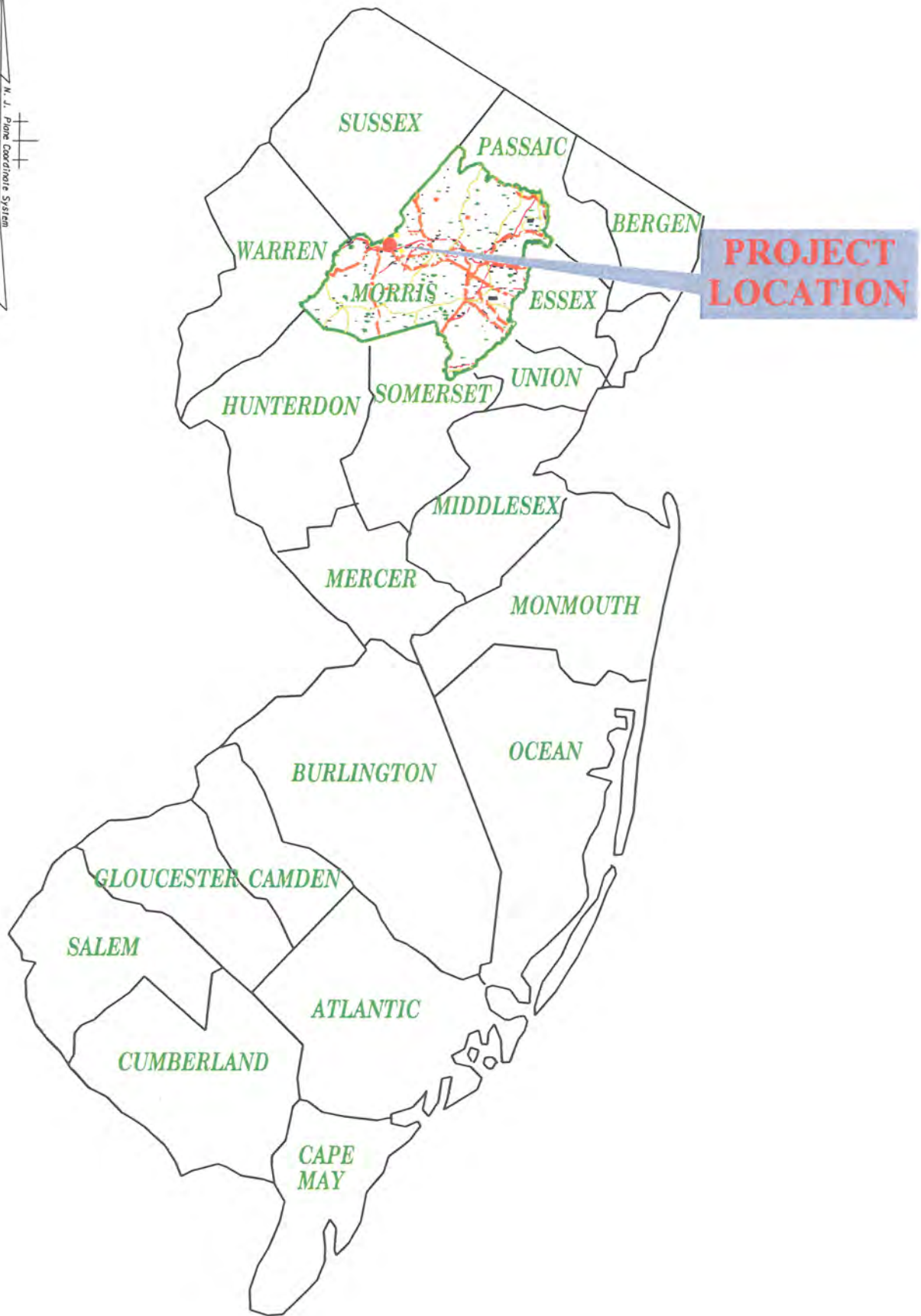
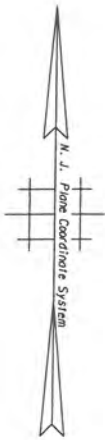
SECTION 2 - PROJECT LOCATION

The Landing Road Bridge (County Bridge No. 73) is located in Roxbury Township near the town of Landing, in the western portion of Morris County in north central New Jersey. Landing lies at the southwestern tip of Lake Hopatcong, a portion of which is bisected by the Morris-Sussex county line.

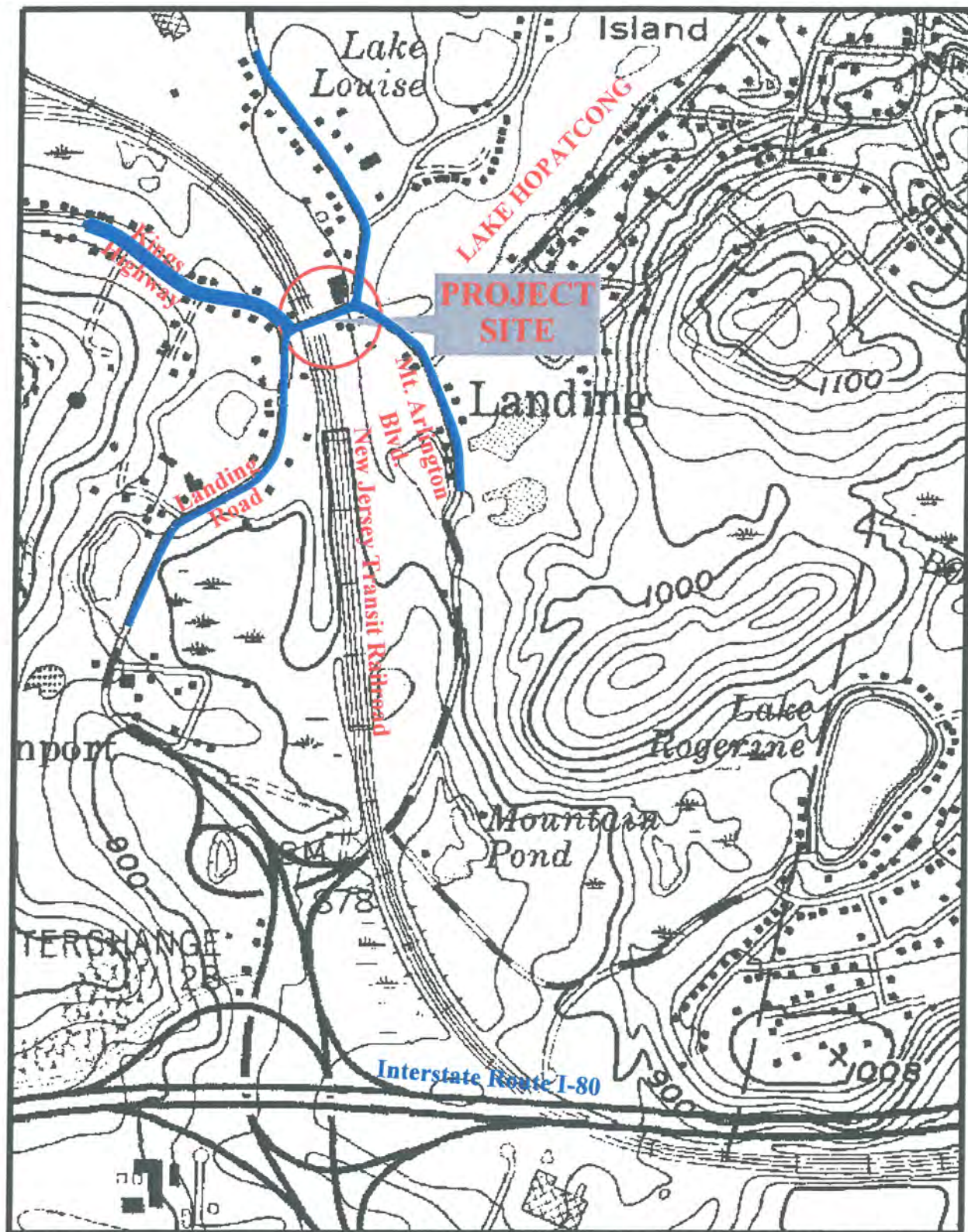
The Landing Road Bridge carries Landing Road, also known as Morris County Route 631, over the dual-track NJ TRANSIT Boonton Line and the former Morris Canal. Before crossing on the bridge, Landing Road extends approximately 0.8 miles in a northerly direction from its interchange with Route I-80 in the south to its intersection with Kings Highway at the southerly approach to the bridge. Landing Road then crosses the bridge in a general west-to-east direction before terminating at a signalized intersection with Lakeside Boulevard (County Route 602) and Mount Arlington Boulevard near the southern tip of Lake Hopatcong. Lakeside Boulevard runs to the north from that intersection while Mount Arlington Boulevard runs to the south.

Canal Street, which is accessed from its intersection with Landing Road immediately east of the bridge, runs southeast to northwest under the Landing Road Bridge on fill along the former alignment of the Morris Canal. Also located under the bridge along the former Morris Canal is a paved parking area serving the track-level Lake Hopatcong NJ TRANSIT station.

A general location map is shown in Figure 2-1 and a more detailed project location map is provided in Figure 2-2.



GENERAL LOCATION MAP



SCALE: 1"=2000'

LANDING LANE BRIDGE
 PROJECT AREA TOPOGRAPHIC MAP
 TOWNSHIP OF ROXBURY, MORRIS COUNTY



FIGURE 2-2

Source: Stanhope, NJ USGS 7.5 Minute Series Map, Quad #26

SECTION 3 - DETAILED DESCRIPTION OF STRUCTURE

The existing bridge, constructed in 1907, is approximately 136 feet long. The superstructure consists of twin reinforced concrete arches and a reinforced concrete deck with an asphalt overlay. The bridge has a curb-to-curb width of 29.6 feet and an overall width of 38.8 feet, including a 6-foot wide sidewalk on the north side and concrete parapets on both sides. The substructure is comprised of concrete abutments, which are at a 90-degree angle with the bridge deck and wingwalls. The Landing Road Bridge provides one of the few major access points for residents on both the east and west shores of Lake Hopatcong. Landing Road, Lakeside Boulevard and Mount Arlington Boulevard all meet at the eastern end of Landing Road Bridge; connections between the bridge and the west shore of Lake Hopatcong are provided via Lakeside Boulevard, while connections between the bridge and the east shore are provided via Mount Arlington Boulevard.

Photographs of the bridge are provided at the end of this section.

3.1 TECHNICAL BACKGROUND

Arch bridges are one of the oldest types of bridges and have great natural strength. They rely on the concept that the arch displaces half the weight of the structure diagonally to either side, rather than the entire weight pulling straight down on the supports. The most commonly used material for arch bridges is reinforced concrete, due to its ability to resist compression, tension, and torsion stresses.

The existing Landing Road Bridge is a twin reinforced concrete spandrel arch. The riding surface consists of a 5-inch base course with a bituminous concrete wearing surface over it. The base course and riding surface rest on earthen fill of various thicknesses. The spandrel area (i.e., the area between the arch and the roadway) is filled with earth fill material and retained by the vertical spandrel walls. The arch member is called a ring or a barrel and is continuous between the spandrel walls. The arch and its associated members supported by the arch are the primary superstructure elements. The arch itself is the primary load-carrying element of the superstructure. The primary reinforcing steel in the arch ring follows the shape of the arch from support to support and consists of a mat of reinforcing steel on both the top and bottom surfaces of the arch. The spandrel walls are designed to retain the back fill material. The primary tension steel for the walls is at the back, or unexposed, face of the wall. The front, or outside, face wall is reinforced in both directions with temperature and shrinkage steel.

The arch is supported at the ends by reinforced concrete abutments. The purpose of the abutments and pier is to receive the loads transmitted through the arch ring from the riding surface and to fill and redistribute these loads to the ground below. The Landing Road Bridge has two single abutments at each end of the bridge and a common center pier shared by both arches. The abutments and pier are at 90-degree angles with the bridge deck and wingwalls.

The arches carry Landing Road over the NJ TRANSIT Montclair-Boonton Line and a parking area (on the site of the former Morris Canal) and provide a 64-foot clear span from face of abutment to face of pier in both spans. The arches provide an 11-foot 5-inch rise from the spring line to the crown.

Figures 3-1 and 3-2 show a typical section and the general elevation of the existing bridge.

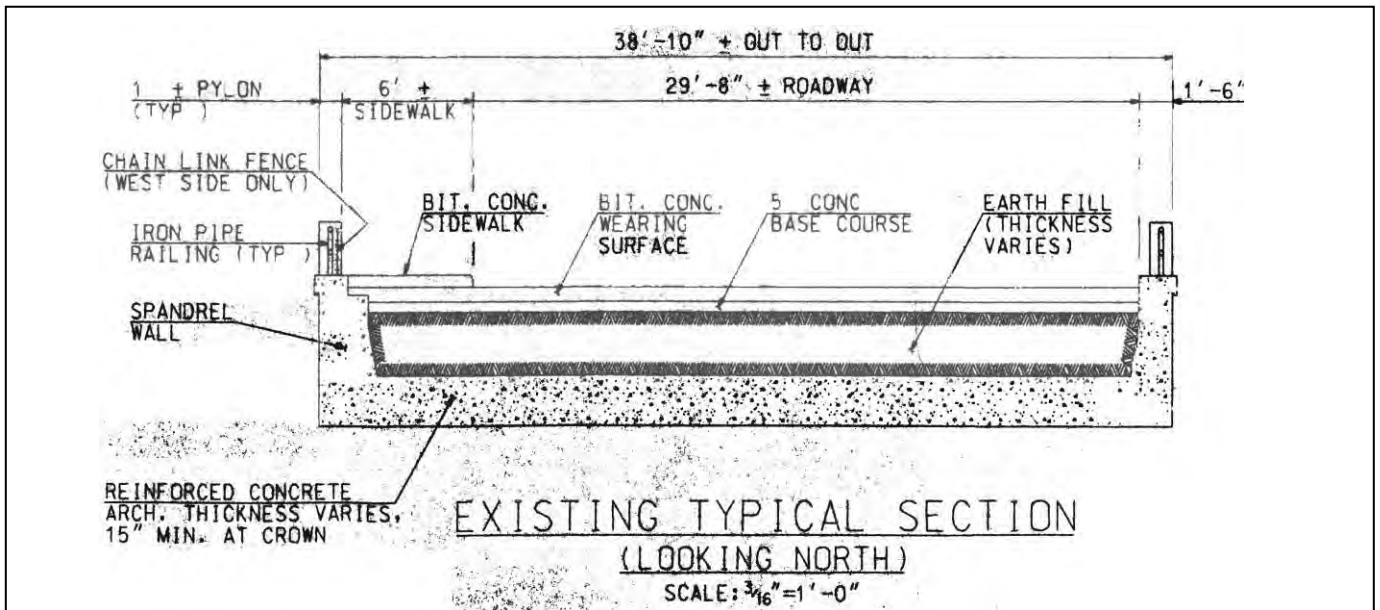


Figure 3-1: General Nomenclature of Existing Landing Road Bridge

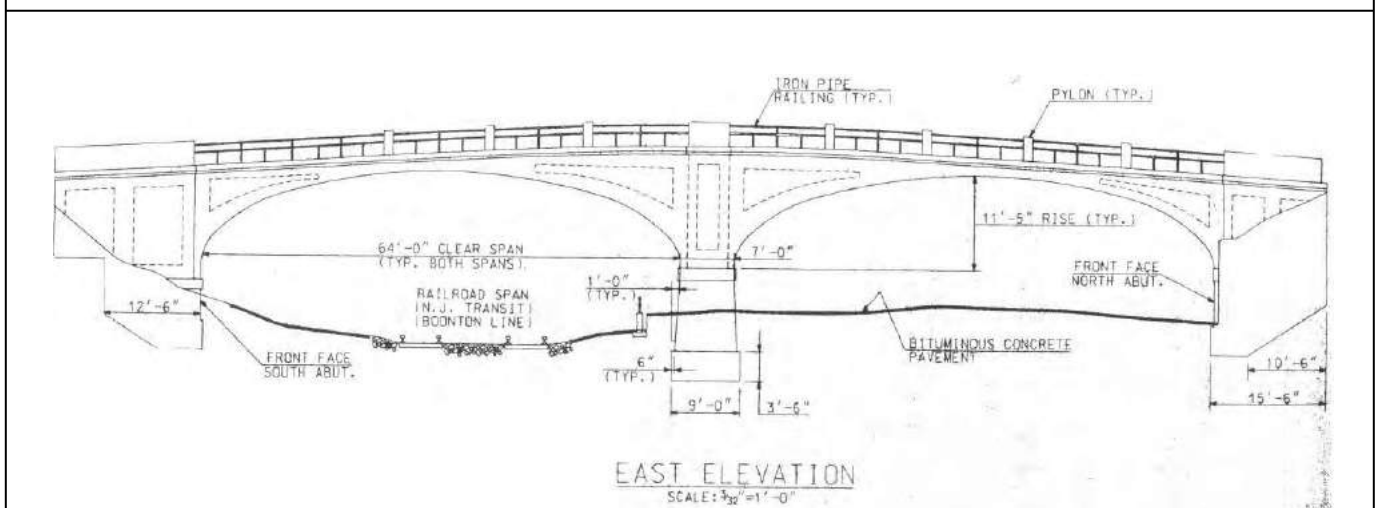


Figure 3-2: General Elevation of the Existing Landing Road Bridge

3.2 TECHNICAL INFORMATION

The existing Landing Road Bridge has remained largely unchanged since its construction in 1907. The undersides of the arch rings and fascias were treated with gunite in 1972 as a maintenance measure. In January 2000, the underside of the arch ring, spandrel wall fascias, pier, and abutments had the gunite treatment removed; the exposed concrete surface sounded using chipping hammers; areas of hollow sounding, delaminated, or spalled concrete removed to sound concrete; deteriorated reinforcing bars spliced with new bars; and areas patched with epoxy mortar to arrest falling concrete and gunite material from the underside of the arch.

Utility lines are present both above and below the deck of the bridge. Electrical lines are present along the east side of the center pier, and overhead electrical, telephone and cable TV lines run along the north fascia of the bridge.

3.3 FIELD INSPECTION

3.3.1 Superstructure

As previously mentioned, the superstructure for both spans of the existing Landing Road Bridge is a reinforced concrete spandrel arch. The arch thickness varies, with a 15-inch deep minimum at the crown and increasing thickness towards the abutments and pier.

Based on the County’s maintenance records, the underside of the arch was treated with a gunite material (maintenance measures) in 1972, and major repairs (by contract) were made to the underside of the arch, abutment breastwalls, pier walls and spandrel walls in 2000. Since 2000, the following maintenance work has been performed by the County:

Work Description	Start Date	Stop Date	Equipment Cost	Labor Cost	Material Cost
Place 6 - 6' water filled barriers in front of the damaged section of the rail on top of sidewalk. The rail was damaged by a vehicle accident on 11-13-2004.	11/16/2004	11/16/2004	\$160.00	\$1,080.24	\$0.00
Cut plates, pipe, guide rail post in preparation for the rail repair. Prefabricate sections of pipe rail and posts in the yard. Replace the damaged sections of rail with prefabricated section. Repair the concrete balustrades. Install galvanized guide rail in front of the entire existing pipe rail.	11/17/2004	12/13/2004	\$5,160.00	\$11,882.64	\$4,317.90
Remove all unsound sections of concrete from the Arch over the railroad tracks. Remove the fallen material from the tracks.	6/5/2009	6/5/2009	\$180.00	\$665.72	\$0.00
Remove the asphalt and deteriorated concrete from a 3' x 3' area over the arch under direction of an engineering consultant. He wants to see the extent of the deteriorated concrete over the arch. Mix and pour concrete to fill the hole to the existing sidewalk height. Place a road plate over the concrete for the night. Remove the road plate in the morning.	12/11/2009	12/12/2009	\$840.00	\$1,860.58	\$61.35
Clean the debris from the sidewalks and curb line. Asphalt patch the sidewalk where needed.	4/9/2010	4/9/2010	\$180.00	\$270.06	\$0.00

In September 1990, a bridge evaluation study report prepared by Maitra Associates, Inc. gave the bridge an overall condition rating of fair, with severe spalling and cracking in the underside of the arches.

In June 1991, four core samples were extracted from the underside of each arch at mid-span. Five out of the eight cores crumbled during the coring drilling and extraction. The remaining three cores were tested for compression strength, and one of the cores failed before a load was registered on the compression machine. The other two cores obtained compression strengths of 4,000 and 6,000 psi. Chloride tests performed on the all samples revealed a very high level of chloride ion in the concrete, indicating electrochemical corrosion of the embedded reinforcing steel.

In December 1995, the seventh cycle inspection and evaluation report, prepared by Bettigole, found the superstructure in fair condition due to severe spalling, with exposed rusted reinforcement and medium to wide cracks with efflorescence and leaching. Rating calculations were performed under this cycle report using the Load Factor method. The mid-span crown was given an inventory rating of 65 tons for an HS truck, which exceeds the current design load of 36 tons for this configuration. The bridge also had a sufficiency rating of 20.0.

In May 1997, Storch Engineers performed a rehabilitation design study. This study found that replacement of the existing structure is necessary due to the deteriorated condition of the arches and the spandrel walls. The findings were based on core samples extracted from the arches, spandrel walls, abutments, pier and wingwalls, and once again several of the cores broke into several pieces after extraction. The remaining intact portions were tested for compressive strength with results from 2,700 to 5,150 psi. Two of the cores had chloride levels below the accepted threshold of two lb/cycle, and the four remaining cores exceeded the threshold with the highest level recorded at 7.56 lb/cycle. Freeze-thaw test showed that after 15 out of 25 freeze-thaw cycles, the cement paste in the tested core samples had completely separated from the course aggregate and broke into small pieces. This is an indication of very poor durability of the existing concrete.

The eleventh cycle report prepared in July 2003 by KS Engineers, P.C., rates the condition of the superstructure as poor due to cracking, scaling, and efflorescence throughout the arch intrados and spandrel walls. The bridge load rating of 65 tons for inventory and the sufficiency rating of 20.0 has not changed from the seventh cycle report.

The fourteenth cycle report prepared in July 2009 by S&R Engineers, P.C., rates the condition of the superstructure as poor due to cracking, scaling, leakage, and efflorescence throughout the arch intrados and spandrel walls. The intrados is reported to be contaminated up to 60% of the area. The bridge load rating of 65 tons for inventory has not changed from the eleventh cycle report. The sufficiency rating is now 19.0 compared to 20.0 in the eleventh cycle report.

The fifteenth cycle report prepared in October 2011 by Stantec Consulting Services, Inc., has upgraded the condition of the superstructure to fair due to the concrete repairs throughout both intrados and in the spandrel wall and coping at both spandrel walls. The bridge load rating of 65 tons for inventory has not changed from the fourteenth cycle report, but the sufficiency rating has improved to 58.5 compared to 19.0 in the fourteenth cycle report, presumably due to the number of interim repairs that have been made since that time.

The latest inspection, the sixteenth cycle report prepared in September 2013 by Stantec Consulting Services, Inc. rates the condition of the overall structure as fair due to the superstructure and substructure. The sufficiency rating still remains 58.3 and load rating of 65 tons.

3.3.2 Substructures

The existing center pier and both abutment and wingwalls are constructed of reinforced concrete.

Based on the County's maintenance records, the abutment breastwalls and pier stem walls were treated with a gunite material (maintenance measures) in 1972 and major concrete repairs (by contract) were made to the breastwall and pier walls in 2000 in conjunction with the superstructure repairs.

In December 1995, the seventh cycle inspection and evaluation report, prepared by Bettigole, gave the overall condition of the substructure as serious due to severe spalling and medium to wide cracks with large areas of efflorescence and leaching.

The eleventh cycle report prepared in July 2003 by KS Engineers, P.C., rates the condition of the substructure as serious due to medium to wide cracks and large areas of leaching, efflorescence, spalls, and hollow concrete.

The fourteenth cycle report prepared in July 2009 by S&R Engineers, P.C., rates the condition of the substructure as serious due to medium to wide cracks and large areas of leaching, efflorescence, spalls and hollow concrete on the pier and west abutment.

The fifteenth cycle report prepared in October 2011 by Stantec Consulting Services, Inc., has upgraded the substructure condition from serious to fair due to the concrete repairs in the coping and wall at the northwest edge and south face of the pier.

The latest inspection, the sixteenth cycle report prepared in September 2013 by Stantec Consulting Services, Inc., the substructure condition remains fair due to the large concrete patches and the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments.

3.3.3 Structure Geometry and Safety

Landing Road is classified as a minor urban arterial with an ADT of 37,079 vehicles per day. The existing Landing Road Bridge is considered functionally obsolete due to the substandard deck geometry. The curb-to-curb width of 29 feet 7 inches is less than the minimum NJDOT design width of 32 feet, which accommodates two 12-foot lanes and two 4-foot shoulders. The minimum width of traveled way and shoulders given by AASHTO for local bridges carrying more than 2,000 vehicles per day are 24 feet and 8 feet, respectively.

The Rehabilitation Design Study conducted by Storch Engineers in May 1997

recommended the structure be widened to accommodate an additional lane in the northbound direction. The study did not investigate the required number of lanes necessary to eliminate the existing traffic problem in the project area.

The bridge carrying capacity for inventory rating was calculated to be 65 tons for a HS20 truck at the crown of the arch in the seventh cycle report bridge inspection report. However, these ratings may be theoretical due to the condition of the concrete of the arch found during the past coring extraction operations, where core samples crumbled during the extraction process. There is currently no load limitation posted on the bridge.

3.4 GEOTECHNICAL (SUBSURFACE MATERIAL)

Geotechnical research information was collected from the NJDOT Bureau of Geotechnical Engineering, the New Jersey Soil Survey (Rutgers), and the Township of Roxbury. The bridge site is situated within the Appalachian Highlands subdivisions of the Piedmont Physiographic Province, which is one of six subdivisions covering southern New York and New Jersey. The bedrock in the Piedmont area consists of red shale and sandstones, interceded sandstones and siltstone, Lockatong argillites, and basalt (Wolf 1977).

Based on information obtained from the *Soil Survey of Morris County, New Jersey*, the soils at the project site consist of very stony sandy loams and Hibernia stony loam. The site is also covered with a thick mantle of glacial terminal moraine. The till can be reasonably classified as silty sand with gravel.

According to *Ground-Water Resources of Morris County, New Jersey* (Special Report No. 25, 1965), the ground water in Morris County occurs under unconfined or water table conditions. However, the lowland areas generally consist of consolidated rocks covered with unconsolidated deposits, which contain impermeable clay and silt beds. The capacity of wells in Morris County is classified as moderate to large. The water is generally acceptable for most uses with minimal to no treatment; however, there are hardness-forming constituents present.

3.5 TRAFFIC

Traffic volume data was collected at four locations in March 2011 (see Appendix H). This information consisted of both manual classification turning movement and Automatic Traffic Recorder (ATR) counts. Each count was collected in 15-minute intervals and summarized hourly. Morning and evening peak hour periods were identified from the combination of manual traffic counts and from the data collected by the ATRs installed on the bridge and on Mount Arlington Boulevard north of Shippenport Road. The morning peak was noted to occur between 7:00 AM and 8:00 AM, while the evening peak hour occurred between 5:00 PM and 6:00 PM. Listed below are the four locations that were counted:

Location # 1 - Landing Road and Kings Highway

Location # 2 – Landing Road and Lakeside Boulevard/Mt. Arlington Boulevard

Location # 3 - Mt. Arlington Boulevard and Shippenport Road

Location # 4 - Lakeside Boulevard and Kingsland Road

Utilizing the classification traffic volume data, capacity analyses were performed for existing conditions at the above locations for each of the peak periods. The method of analysis employed conforms to the Highway Capacity Manual 2000 Edition and was facilitated with computer software. Level of service (LOS) and delays were calculated to evaluate the operating characteristics for the four intersections under 2011 traffic demands. These results are presented in Table 3-1.

TABLE 3-1: 2011 Existing Condition Delay and LOS

Intersection Number	AM Peak			PM Peak		
	EB	NB LT		EB	NB LT	
INT. #1	Delay LOS 37.0" E	Delay LOS 13.1" B		Delay LOS 52.3" F	Delay LOS 9.1" A	
INT. #2	Delay LOS 23.8" C	Delay LOS 12.0" B	Delay LOS 30.4" C	Delay LOS 25.8" C	Delay LOS 19.9" B	Delay LOS 16.1" B
INT. #3	Delay LOS 8.0" A	Delay LOS 12.5" B		Delay LOS 7.9" A	Delay LOS 15.4" C	
INT. #4	Delay LOS 23.8" D	Delay LOS 13.7" B	Delay LOS 7.9" A	Delay LOS 39.2" E	Delay LOS 9.0" A	Delay LOS 12.6" B

As presented in the table, left turning movements onto Landing Road Bridge from Kings Highway (i.e., EB movement at Intersection #1) experience a Level of Service “E” during the morning peak travel period and Level of Service “F” during the evening peak. In addition, the westbound approach leg of Kingsland Road at its intersection with Lakeside Boulevard (i.e., WB movement at Intersection #4) experiences a Level of Service “E” during the evening peak travel period.

Each of the four intersections was also evaluated in the No-Build condition for the project horizon year of 2031 (see Table 3-2). Under this scenario, three out of the four intersections studied will experience extraordinarily long delays, at least on certain approaches. At the signalized intersection of Landing Road and Lakeside Boulevard/Mt Arlington Boulevard, southbound approaches along Lakeside Boulevard (i.e., SB movement at Intersection #2) during the morning peak hour would exceed the maximum saturated flow rate of 1,900 vehicles per hour (vph) and will require capacity increase in order to improve from a Level of Service “F”. In addition, the left turning movements onto Landing Road Bridge from Kings Highway (i.e., EB movement at Intersection #1) and onto Lakeside Boulevard from Kingsland Road (i.e., WB movement at Intersection #4) would be Level of Service “F” during both the morning and evening peak periods. These left turning vehicles would require substantial gaps in the through traffic in order to perform these movements safely during these periods.

TABLE 3-2: 2031 No-Build Condition Delay and LOS

Intersection Number	AM Peak			PM Peak		
	EB	NB LT		EB	NB LT	
INT. #1	Delay LOS 102.9" F	Delay LOS 18.0" C		Delay LOS 663.2" F	Delay LOS 10.0" A	
INT. #2	WB	NB	SB	WB	NB	SB

	Delay LOS 25.6" C	Delay LOS 12.3" B	Delay LOS 161.5" F	Delay LOS 34.9" C	Delay LOS 34.6" C	Delay LOS 24.0" B
INT. #3	<u>WB</u> Delay LOS 8.3" A	<u>NB</u> Delay LOS 16.5" C		<u>WB</u> Delay LOS 8.2" A	<u>NB</u> Delay LOS 31.7" E	
INT. #4	<u>WB</u> Delay LOS 65.5" F	<u>NB LT</u> Delay LOS 19.5" C	<u>SB LT</u> Delay LOS 8.3" A	<u>WB</u> Delay LOS 177.0" F	<u>NB LT</u> Delay LOS 10.0" A	<u>SB</u> Delay LOS 17.1" C

It should also be noted that the existing layout of the signalized intersection of Landing Road and Lakeside Boulevard / Mt. Arlington Boulevard does not have dedicated lanes for left turning vehicles. Increasing traffic volumes combined with the delay from left turning vehicles has made this intersection a major congestion point for the roadway network. It is also believed that many of the accidents experienced at this intersection occur because the signal layout does not accurately reflect the existing traffic needs.

Overall, the proposed project (assuming the Replacement Alternative) affords a design that is expected to provide acceptable levels of service at the study area intersections through the design year of 2031.

Accident data collected by the Roxbury Township Police Department for this segment of Landing Road is included in Appendix E. This data has been collected for Landing Intersection (which is the three-roadway intersection of Lakeside Blvd., Mt. Arlington Blvd. and Landing Rd.) and Landing Rd. (from the Landing Intersection to the Landing Rd. Bridge).

A review of the data indicates the following:

- At Landing Intersection most accidents occur during the months of June, July and August, while at Landing Rd. most accidents occur during the months of May and June.
- At Landing Intersection most accidents occur between the hours of 8 AM and 9 AM, while at Landing Rd. most accidents occur between the hours of 5 PM and 6 PM.
- At both locations most accidents involved vehicles struck from behind while stopped in traffic.

In April of 2015 Morris County took traffic counts at the three intersections and the following counts were documented:

Mount Arlington Blvd (616) - N of Shippenport Rd

Dates: 5/26 – 5/29/2015

AADT: 10,464

AM Peak (8-9am): 672

PM Peak (5-6pm): 900

Lakeside Blvd (631) - N of Landing Rd

Dates: 4/28 – 5/1/2015

AADT: 23,702

AM Peak (7-8am): 1,751

PM Peak (5-6pm): 1,915

Kings Hwy - W of Landing Rd

Dates: 4/28 – 5/1/2015

AADT: 577

AM Peak (6-7am): 47

PM Peak (5-6pm): 47

Landing Rd (631) - N of Shippenport Rd

Dates: 10/30 – 11/1/2013 (traffic counts for this intersection was done in 2013 not 2015)

AADT: 21,350

AM Peak (7-8am): 1,466

PM Peak (5-6pm): 1,553

3.6 HISTORY / SIGNIFICANCE

3.6.1 Bridge Significance

The Landing Road Bridge is a two-span reinforced concrete deck arch bridge with a simple metal railing. The bridge was designed by the Delaware, Lackawanna and Western (DL&W) Railroad Company and was constructed in 1907 by Salmon Brothers Construction Company of Netcong, New Jersey. The bridge was built to replace an earlier iron frame bridge with a wooden plank deck that had carried traffic over the DL&W tracks and the adjacent Morris Canal. The bridge was made necessary by the crossing of a trolley to the end of Lake Hopatcong and King's Store.

The Landing Road Bridge represents the earliest example of an earth-filled concrete deck arch bridge in Morris County and is the only two-span example of the type sited within the county. In 1994 the bridge received a State Historic Preservation Officer Opinion of Eligibility as an individual resource that met the requirements of both the New Jersey and National Registers of Historic Places under Criterion C. It was also recognized at this time as a contributing resource within the Morris Canal Historic District, which was listed on the New Jersey Register of Historic Places in 1973 and the National Register of Historic Places in 1974 (see Hall/Zerbe to Hajtovik, February 25th, 1994). It is the designation of the bridge as a contributing resource within the New Jersey Register-listed Morris Canal Historic District that necessitates project compliance with the requirements of the New Jersey Register of Historic Places Act.

The Landing Road Bridge is over 100 years old, and the superstructure and substructure have not been altered or modified from its conception in 1907. The longevity of the bridge can be contributed to the workmanship and materials of the era. The integrity of the bridge has survived years of environmental and weather exposure; however, with age comes wear and tear on the components of the bridge, as is the case with the existing Landing Road Bridge.

3.6.2. Project History

In the 1970s and 1980s, the County of Morris constructed major improvements to Lakeside Boulevard from the Sussex-Morris county line to its intersection with Landing Road. In addition, substantial improvements to Mount Arlington Boulevard and Howard Boulevard were made using federal funding, State Aid Road System grants, and County resources. Proposals to realign Landing Road to the west of its present location in the late 1970s and early 1980s failed due to lack of sufficient federal funding, environmental problems, and lack of

community support. In 1983, NJDOT retained Elam & Popoff, P.A. to investigate alternative alignments for the Landing Road Bridge. This investigation was concluded in May of 1985 with the submission of a report that studied six alternative alignments.

In 1992, the County retained Storch Engineers to complete the Elam & Popoff study and to investigate other alternatives as required. The Storch study, which was completed in March 1996, developed and studied eight new improvement alternatives, and also evaluated two of the six alignments studied in the 1985 Elam & Popoff report. The Storch study recommended the construction of a new bridge as an extension of Lakeside Boulevard joining Landing Road south of the existing bridge. The existing bridge was to remain for local access to Landing Road and King's Highway. However, as with previous studies, the Storch recommendations were never implemented.

Because the Landing Road Bridge continues to deteriorate, the County of Morris requested and obtained federal funding through NJTPA to complete a new alternatives analysis for the project, including the development of alternatives not considered in any of the previous studies. Steinman Boynton Gronquist & Birdsall (now Parsons) was selected in April 1998 to complete the study and select an alternative for implementation; this report is part of this ongoing effort. The work under this project also includes coordination with Morris County, NJDOT, NJTPA, FHWA and other affected local groups, officials, and residents to ensure support for the final alignment selected.

In the interim, the serious condition of the existing bridge necessitated immediate rehabilitation measures to prevent further deterioration and to extend the useful life of the bridge until a replacement structure is constructed. A *Rehabilitation Design Study* (May 1997) was performed by Storch Engineers under a separate agreement. The *Rehabilitation and Design Study* concluded that replacement of the existing structure is necessary due to the deteriorated conditions of the arches and spandrel walls. These findings were based on core samples taken from the arches and spandrel walls, which revealed high levels of chloride contamination causing corrosion of the embedded reinforcing steel. It is to be noted that five of the eight cores taken crumbled during the extraction operations, supporting the classification that the existing structure is severely deteriorated. The study also found that the structure should be wider to accommodate an additional lane in the northbound direction. The rehabilitation study did not investigate the required number of lanes that the replacement bridge would need to eliminate existing traffic problems in the project area.

Due to the different and distinct proposed work items between this contract and the rehabilitation work recommended by Storch Engineers, the County decided to proceed with each of the two contracts separately.

In coordination with the New Jersey Historic Preservation Office (NJHPO), two (2) interim rehabilitation contracts were completed on the Landing Road Bridge using County funds:

- From April 2000 to June 2000 unsound concrete on the underside of the arches was removed and repairs were performed at a cost of \$289,050.00.

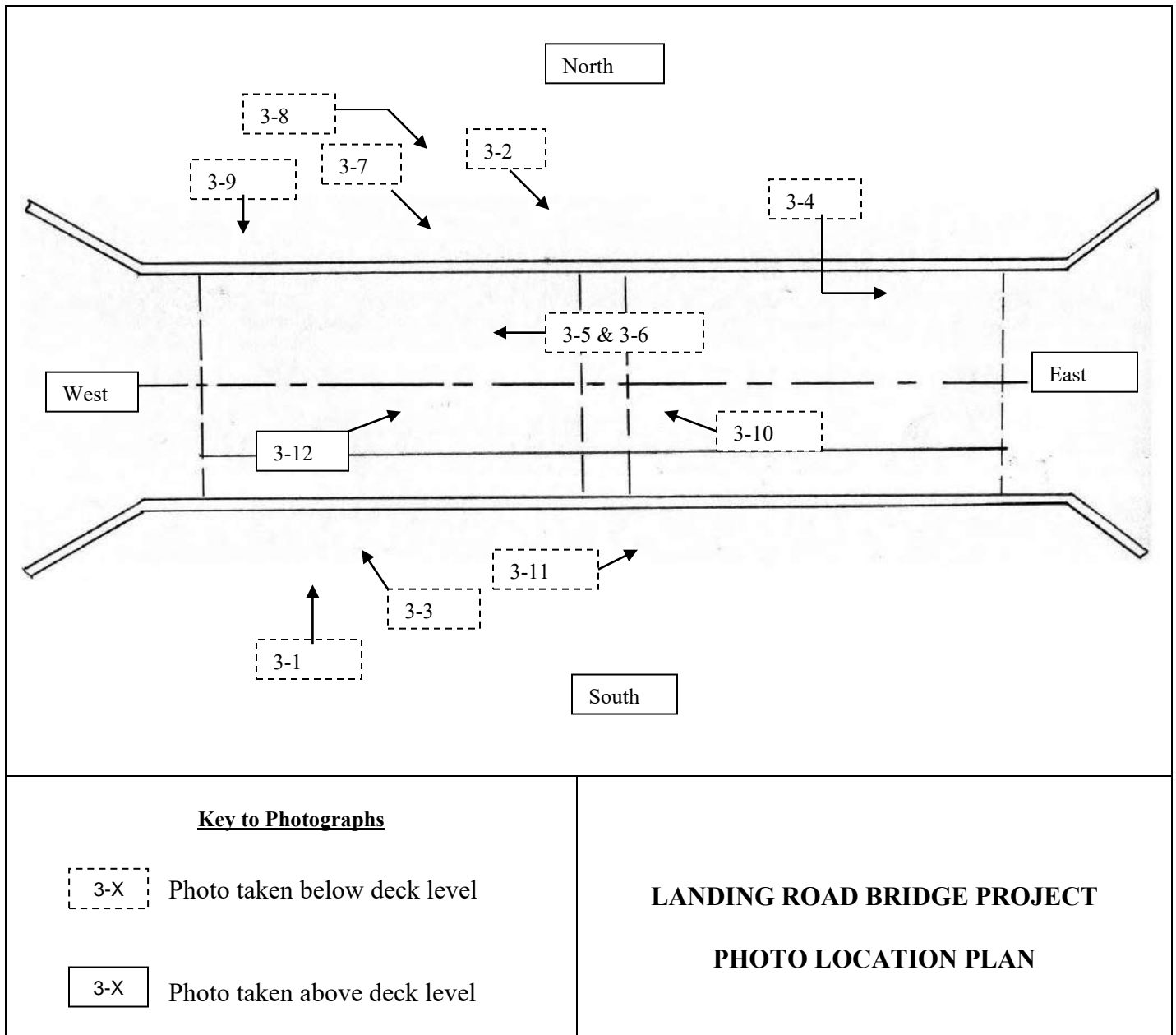
From September 2010 to November 2012 all unsound concrete was removed in the fascias and repairs were performed; the asphalt across the deck was milled to a depth of approximately 8” and a spray waterproof membrane was applied prior to resurfacing; cracks were routed and sealed in the roadway and concrete cracks were repaired by pressure injection; approach guide rails were replaced; and asphalt sidewalk was replaced with a tinted concrete sidewalk. All of these repairs were performed at a cost of \$661,707.24.

With the completion of the 2010 interim rehabilitation contract, it is anticipated that the Landing Road Bridge will require primarily routine maintenance repairs (performed by County personnel) until such time when the bridge is completely replaced.

The coordination with NJHPO associated with the Landing Road Bridge Replacement Project will be concerned with the new replacement bridge that would be constructed under the IPA, Alternative 7-C.

3.7 LANDING ROAD BRIDGE PHOTOS

Photos that depict Landing Road Bridge conditions prior to repairs undertaken in 2000 are provided on the next several pages, along with a plan view indicating the locations of each photo. More recent photos of Landing Road Bridge conditions are provided in the September 2013 *Bridge Re-Evaluation Survey Report* that is included in Appendix B.



Key to Photographs

3-X Photo taken below deck level

3-X Photo taken above deck level



Photo 3-1: Looking north at south fascia of the west arch over the NJ TRANSIT Montclair-Boonton Line (formerly the DL&W Railroad). Note: Large area of the spandrel wall and edge of arch ring heavily spall (Photo taken prior to repairs in 2000).



Photo 3-2: Looking south at north face of east and west arches at center pier. Note: Heavy medium cracking with efflorescence and leaching in arches, spandrel walls and piers and spalls in spandrel wall (arrows). Photo taken prior to repairs in 2000.



Photo 3-3: Looking northwest and up at the edge of the underside of the west span arch ring over NJ TRANSIT tracks. Note large spalls in underside and edge of arch ring. Photo taken prior to repairs in 2000.



Photo 3-4: Looking east at north edge of east span arch ring underside. Note: Spalled gunite treatments and large spall in underside of arch ring (arrow). Photo taken prior to repairs in 2000.



Photo 3-5: Looking west at large spall with exposed corroded reinforcing bars in north half of the west span arch ring underside (arrow). Photo taken prior to repairs in 2000.

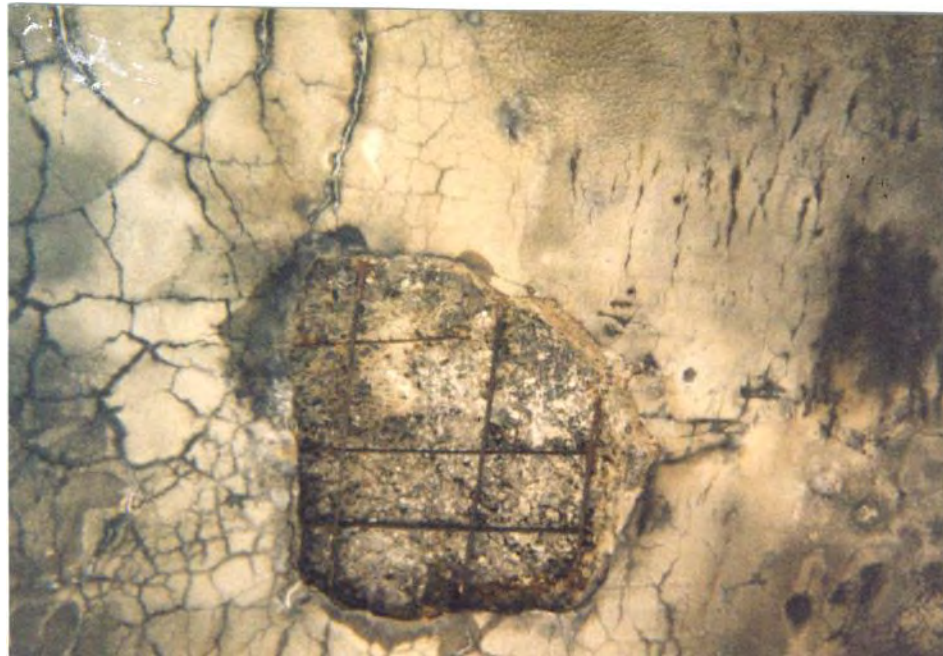


Photo 3-6: Looking up and west at underside of the west span arch ring. Note: Large spall with deteriorated reinforcing bars and medium to wide cracks in the surrounding concrete. Photo taken prior to repairs in 2000.



Photo 3-7: Looking southwest at spandrel at center pier for east and west spans. Note: Large concrete repair areas (light gray color in photo) done under repair contract in 2000.



Photo 3-8: Looking northwest at the south face spandrel wall, center pier and the edge of the west span arch ring. Note: Large areas of concrete repair done under repair contract in 2000.



Photo 3-9: Looking south at the north face of the east span arch ring and spandrel wall. Note: Large areas of concrete repair work to the spandrel wall (light color in photo) under repair contract in 2000.



Photo 3-10: Looking northwest at the east face of the center pier. Note: Concrete repair work done under repair contract in 2000.



Photo 3-11: Looking southwest at the north face of the west span arch and spandrel wall. Note: Large area of concrete repairs to spandrel wall and arch ring under repair contract in 2000.



Photo 3-12: Looking southeast at the west span roadway over NJ TRANSIT Montclair-Boonton Line. Note: Bridge deck geometry has an inadequate to curb-to-curb width of 29 feet 7 inches (present design standards require 34-foot minimum).

SECTION 4 – PURPOSE AND NEED

4.1 PURPOSE

The purpose of the Landing Road Bridge Project is to improve safety and traffic operations through the rehabilitation or replacement of the Landing Road Bridge and improvements to the intersecting roadways on both approaches. Improvements are needed to address structural and functional deficiencies in the bridge and the approach roadways. Prior to the latest engineer’s inspection report that was prepared in September 2013 (see Appendix B), the existing bridge was determined to be structurally deficient, due to the condition of the superstructure and substructure, and functionally obsolete, due to inadequate deck geometry. However, due to recent interim repairs to the superstructure and substructure that have been completed, the latest inspection report only cites that the bridge is functionally obsolete due to inadequate deck geometry, while the condition of the superstructure and substructure is considered to be fair. In addition, several intersection movements along the approach roadways experience Level of Service (LOS) “E” or “F”.

4.2 BRIDGE CONDITION

The Landing Road Bridge is over 100 years old and has far exceeded its structural design life. The design life is considered the safe duration a structure can remain in service without significant rehabilitation and is dependent upon the materials used in the bridge, the load history of the crossing, the maintenance record, and other factors. A 50-year design life is common for most of today’s modern bridge designs.

The most recent Bridge Re-Evaluation Survey report dated September 2013, which is included in Appendix B, shows that the bridge’s superstructure (comprised of the reinforced concrete arches) has been given a Condition Rating of 5 (fair condition) due to patching covering 60% of both arch intrados, as well as of the cracking, scaling, leakage and efflorescence throughout both intrados and spandrel walls. This condition was upgraded to fair due to the concrete repairs performed since the previous Bridge Re-Evaluation Survey report dated October 2011. The substructure units (the abutments and pier) were also given a Condition Rating of 5 (fair condition) due to the large concrete patches, the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments. This condition was upgraded to fair due to the concrete repairs in the coping and wall at the northwest edge and south face of the pier since the previous Bridge Re-Evaluation Survey report dated October 2011.

The previous Bridge Re-Evaluation Survey report dated October 2011, which is also included in Appendix B, shows that the bridge's superstructure had been given a Condition Rating of 5 (fair condition) due to cracking, scaling, leakage, and efflorescence throughout the arch intrados and spandrel walls. The intrados was reported to have moderate scaling and efflorescence up to 60% of the area. The substructure units were given a Condition Rating of 5 (fair condition) due to medium to wide cracks and large areas of leaching, efflorescence, spalls and hollow concrete on the pier and west abutment. Per the Federal Highway Administration (FHWA), bridge elements that have a Condition Rating of 4 (poor condition) are characterized by advanced section loss, deterioration, spalling or scour. Bridge elements that have a Condition Rating of 3 (serious condition) are characterized by loss of section, deterioration, spalling or scour. Fatigue cracks in steel or shear cracks in concrete may also be present. This level of deterioration can seriously affect primary structural components, and local failures are possible.

The top of the bridge deck has exhibited deterioration due to direct traffic loading and salt intrusions because of snow and ice removal operations. Based upon the results of the concrete coring and testing program, it has been determined that the overall quality of concrete is poor. Compressive strength tests indicated that the available concrete strength is approximately 20 percent less than the current standard values, and petrographic analysis performed on the cores indicated that the composition of the existing concrete lacks integrity. Extensive cracking and spalling of concrete has occurred, and chunks of loose concrete have fallen from the bridge onto the railroad right-of-way and the parking area beneath the bridge.

The concrete is not air entrained. This has contributed to the severe spalling, which could be attributed in part to the freeze/thaw cycles of the concrete over its life. The concrete exhibits high levels of chloride (several times higher than the threshold level established by NJDOT to indicate the need for replacement), and active alkali-silica reaction appears to be ongoing.

In 2000, Morris County undertook a project costing about \$300,000 to repair the bridge. This project consisted primarily of removing loose concrete, patching spalls, and sealing cracks. The primary purpose of the project was to repair the bridge in order to eliminate the safety hazard to the public and the railroad due to falling concrete and was not intended to significantly improve the structural integrity of the bridge. Furthermore, it did not eliminate the causes of the deterioration (i.e., lack of air entrainment, freeze/thaw cycles, and high chloride levels in the concrete). Miscellaneous repair and maintenance projects (including costs) related to the bridge that have occurred since that time are identified on the table on page 3-3.

The bridge load carrying capacity was rated using the Load Factor Method under the seventh cycle inspection dated December 1995. The ratings found the mid-span crown to an inventory rating of 65 tons for an HS truck configuration, which exceeds the current design of 36 tons for this truck. However, these ratings may be theoretical due to the condition of the concrete of the arch found during past coring extraction operations, where core samples crumbled during the extraction process. As such, it is likely that the bridge does not meet the current load rating requirements.

The bridge is also considered functionally obsolete due to the inadequate deck geometry. The curb-to-curb width is 29 feet 7 inches, which is less than the minimum NJDOT design width of 32 feet, as well as the AASHTO standard for design width of 34 feet (two 12-foot lanes and two 5-foot shoulders).

4.3 ROADWAY AND TRAFFIC

The Landing Road Bridge provides one of the few major access points for residents on both the east and west shores of Lake Hopatcong. The existing two-lane roadway on the bridge is not adequate to handle the design year traffic (as discussed in Section 3.5), and the approach roadways on both sides of the bridge are becoming increasingly congested.

As discussed in Section 3.5 and presented in Appendix H, traffic counts were taken in March 2011 at four intersections in proximity to the Landing Road Bridge. These counts indicate that left-turning movements onto Landing Road Bridge from Kings Highway experience a Level of Service “E” during the morning peak travel period and Level of Service “F” during the evening peak. In addition, the westbound approach leg of Kingsland Road at its intersection with Lakeside Boulevard experiences a Level of Service “E” during the evening peak travel period. No turning movements at either the Landing Road and Lakeside Boulevard/Mt. Arlington Boulevard or the Mt. Arlington Boulevard and Shippenport Road intersections experience levels of service worse than “C” during either the morning or evening peak periods.

Each of the four intersections was also evaluated in the No-Build condition for the project horizon year of 2031. Under this scenario, three out of the four intersections will experience extraordinarily long delays, at least on certain approaches. At the signalized intersection of Landing Road and Lakeside Boulevard/Mt Arlington Boulevard, southbound approaches along Lakeside Boulevard during the morning peak hour would exceed the maximum saturated flow rate of 1,900 vehicles per hour (vph) and will require capacity increase in order to improve from a Level of Service “F”. In addition, the left turning movements onto Landing Road Bridge from Kings Highway and onto Lakeside Boulevard from Kingsland Road would be Level of Service “F” during both the morning and evening peak periods. These left turning vehicles would require substantial gaps in the through traffic in order to perform these movements safely during these periods.

Finally, the existing layout of the signalized intersection of Landing Road, Lakeside Boulevard and Mt. Arlington Boulevard does not have provisions for left turning vehicles. Increasing traffic volumes combined with the delay from left turning vehicles has made this intersection a major congestion point for the roadway network. It is also believed that many of the accidents experienced at this intersection occur because the signal layout does not accurately reflect the existing traffic needs.

Additionally in Morris County took traffic counts in April of 2015 at the three intersections and the following counts were documented:

Mount Arlington Blvd (616) - N of Shippenport Rd

Dates: 5/26 – 5/29/2015

AADT: 10,464

AM Peak (8-9am): 672

PM Peak (5-6pm): 900

Lakeside Blvd (631) - N of Landing Rd

Dates: 4/28 – 5/1/2015

AADT: 23,702

AM Peak (7-8am): 1,751

PM Peak (5-6pm): 1,915

Kings Hwy - W of Landing Rd

Dates: 4/28 – 5/1/2015

AADT: 577

AM Peak (6-7am): 47

PM Peak (5-6pm): 47

Landing Rd (631) - N of Shippenport Rd

Dates: 10/30 – 11/1/2013 (Traffic counts were not taken at this intersection in 2015 only in 2013)

AADT: 21,350

AM Peak (7-8am): 1,466

PM Peak (5-6pm): 1,553

SECTION 5 - PROJECT ALTERNATIVES

This section provides a description of the alternatives considered for the Landing Road Bridge Project and an evaluation of how well each alternative satisfies the project needs of improving the structural capacity of the bridge and providing adequate traffic operations and safety. Each alternative was designed to incorporate good engineering practices and minimize the environmental and community impacts to the maximum extent possible. A comparative review of these alternatives is presented in Section 6 of this alternatives analysis.

During earlier stages of project development, eight preliminary conceptual alignment alternatives were developed and studied (see Figure 5-1). During the final scoping process, **Alternative 7** was selected for further development and study. Alternative 7 consists of replacing the existing bridge along roughly the same alignment and would provide for two lanes of traffic in each direction. Six different replacement alignments (Alternatives 7-A through 7-F) were developed and evaluated, with Alternative 7-C found to be the most feasible.

Subsequent to the conclusion of scoping, the SHPO requested that the **Bypass Alternative** also be advanced for comparison purposes. Alternative 1 consists of providing a new bridge on a bypass alignment as an extension of Lakeside Boulevard joining Landing Road south of the existing bridge.

Under both Alternative 1 and Alternative 7-C, the existing Landing Road Bridge would be demolished. The possibility of retaining the existing structure for alternate uses (such as a pedestrian overpass) was considered. However, if left standing, it is likely that the bridge would also be required to be available for emergency vehicle use. Regardless of its ultimate use, it would be imperative that the bridge structure be maintained for use. The cost of basic maintenance of the structure for some sort of alternate use over the next 20 years is estimated to be approximately \$1.0 million (in 2011 dollars), based on an assumed \$50,000 per year. Given the “fair” rating of the bridge condition in the most recent inspection report (September 2013), it is further assumed that the bridge would not require any major rehabilitation beyond basic maintenance during that 20-year period. However, if major rehabilitation should become necessary, that would result in a major additional cost. Due to the need for regular maintenance and the possible need for future rehabilitation, as well as the associated costs for each, alternatives that include maintaining the existing structure for alternate uses were not carried forward for further evaluation.

Thus, in accordance with the requirements of the Alternatives Analysis and as agreed upon with the SHPO, the following alternatives were considered and studied for this report:

- No-Build Alternative;
- Rehabilitation Alternative;
- Bypass Alternative (Alternative 1); and
- Replacement Alternative (Alternative 7-C).

5.1 NO-BUILD ALTERNATIVE

The No-Build Alternative would maintain the existing bridge in its current location without rehabilitation or replacement. It would include basic measures to stabilize the structure and maintain it until the end of its useful life, although without providing structural or associated roadway improvements, rehabilitation, or preventative measures. The alternative would not require any right-of-way taking of residential or business properties.

The existing concrete arch bridge is over 100 years old and has experienced extensive cracking, spalling of concrete, and rusting of steel reinforcement over the years. Chunks of loose concrete have fallen from the bridge onto the railroad right-of-way and the parking area beneath the bridge, endangering the public and presenting a safety hazard. Under the No-Build Alternative, the bridge would continue to deteriorate over time (even with recent interim repairs that have been made) and would eventually reach a point that would require limited weight usage and possibly complete restriction of vehicle use. There would also continue to be operational deficiencies associated with the bridge due to existing geometric and capacity limitations.

To continue use of the existing structure beyond its design life, a continuous program of maintenance would be required during the next 20 years, simply to extend its life to the end of that 20-year period. Although an initial outlay of capital costs to improve the bridge would not be required, the cost of continuous maintenance that would be required throughout the 20-year period could still be significant. As discussed above, the total maintenance cost during the 20-year period is estimated to be approximately \$1.0 million. This cost would not result in significant long-term benefit, but only the ability to extend the life of the bridge for an additional 20 years. There would also be no improvements to operational conditions on or near the bridge. Therefore, the return on the investment would be limited.

5.2 REHABILITATION ALTERNATIVE

The Rehabilitation Alternative would rehabilitate the bridge to the *Secretary of the Interior's Standards*, which would not alter the existing historic features or further modernize the bridge. The bridge would not be widened under this alternative, and some supporting structures may not be repaired or replaced. [Existing bridge maintenance requirements will continue to increase as rehabilitation within the parameters of historic integrity will result in repairs of failing and severely deteriorating structural elements with materials to match the old in design, color, and texture. Replacement of historic features will need to be substantiated with documentary and physical evidence.](#)

Under this alternative, rehabilitation of the existing bridge would be performed to increase its load carrying capacity. There are two options that fall under the general category of rehabilitation, both of which would have a similar footprint to the No-Build Alternative, although they would involve a more extensive level of repairs and possible widening of the bridge.

One rehabilitation option would be to replace the existing bridge's concrete arch in kind. The existing concrete arch, together with the asphalt pavement, would be demolished and replaced with a new concrete arch and pavement in sections. No widening of the existing bridge deck would occur. The construction duration for this rehabilitation alternative would be approximately 1 to 2 years.

The second rehabilitation option would be to replace the concrete arch in kind, replace the deck, and improve the integrity of the substructure along with possible widening of the bridge. This could result in the bridge being out of service for a longer duration; the approximate construction duration would be on the order of 3 to 4 years. This construction duration could possibly be shortened by using more modern construction methods such as pre-fabrication. The life expectancy under this rehabilitation alternative would be 75 years, which is in conformance with the latest AASHTO bridge design standards.

In either case, any rehabilitation effort would fall short in terms of increasing the strength of the bridge. Deterioration would continue for any sections that are not replaced. In addition, the same potential for long-term vehicle restrictions as the No-Build Alternative would result as the bridge nears the end of its useful life. Rehabilitation also would not increase capacity to better address the operational deficiencies that would exist on the bridge and its approaches.

Any rehabilitation alternative would conform to the Secretary of the Interior's Standards for the Treatment of Historic Properties in regards to retaining and preserving the historic character of the bridge. In this case, keeping the same structure type (i.e., a two-span reinforced concrete deck arch bridge) would comply with this requirement. In addition, where the severity of deterioration requires replacement of a distinctive feature, the new feature would match the old in design, color, texture and materials.

5.3 BYPASS ALTERNATIVE

The Bypass Alternative, shown in Figure 5-2, would involve the construction of a new bridge on a new alignment south of the existing bridge. The portion of Landing Road from its intersection with Lakeside Boulevard to the property at 151 Landing Road would be relocated to the south, crossing over the NJ TRANSIT Boonton Line on a new structure south of the existing bridge. The new curvilinear, two-span bridge would provide two lanes in each direction and would be approximately 340 feet long and 57 feet wide, with a 55 degree skew.

The intersection of Lakeside Boulevard, Mt. Arlington Boulevard, Landing Road, and the access ramp to the NJ TRANSIT station parking area would be reconstructed into a four-legged intersection providing channelization for the right turning vehicles. The intersection would be improved with a new signalized intersection and a new lane configuration. Westbound Mt. Arlington Boulevard and southbound Lakeside Boulevard would have shared through lanes with left turns; eastbound Landing Road over the new bridge and westbound Mt. Arlington Boulevard would have exclusive right turn lanes. At the southern end of the proposed bridge, access would be provided to Kings Highway via a new perpendicular intersection. The portion of Landing Road west of this intersection would become a dead-end terminating with a cul-de-sac.

Under this alternative, the existing bridge would be demolished. The possibility of retaining the existing structure for alternate uses (such as a pedestrian overpass) was considered. However, if left standing, it is likely that the bridge would also be required to be available for emergency vehicle use. Regardless of its ultimate use, it would be imperative that the bridge structure be maintained for use. The cost of basic maintenance of the structure for some sort of alternate use over the next 20 years is estimated to be approximately \$1.0 million (in 2011 dollars), based on an assumed \$50,000 per year. Given the “fair” rating of the bridge condition in the most recent inspection report (September 2013), it is further assumed that the bridge would not require any major rehabilitation beyond basic maintenance during that 20-year period. However, if major rehabilitation should become necessary, that would result in a major additional cost. Due to the need for regular maintenance and the possible need for future rehabilitation, as well as the associated costs for each, alternatives that include maintaining the existing structure for alternate uses were not carried forward for further evaluation.

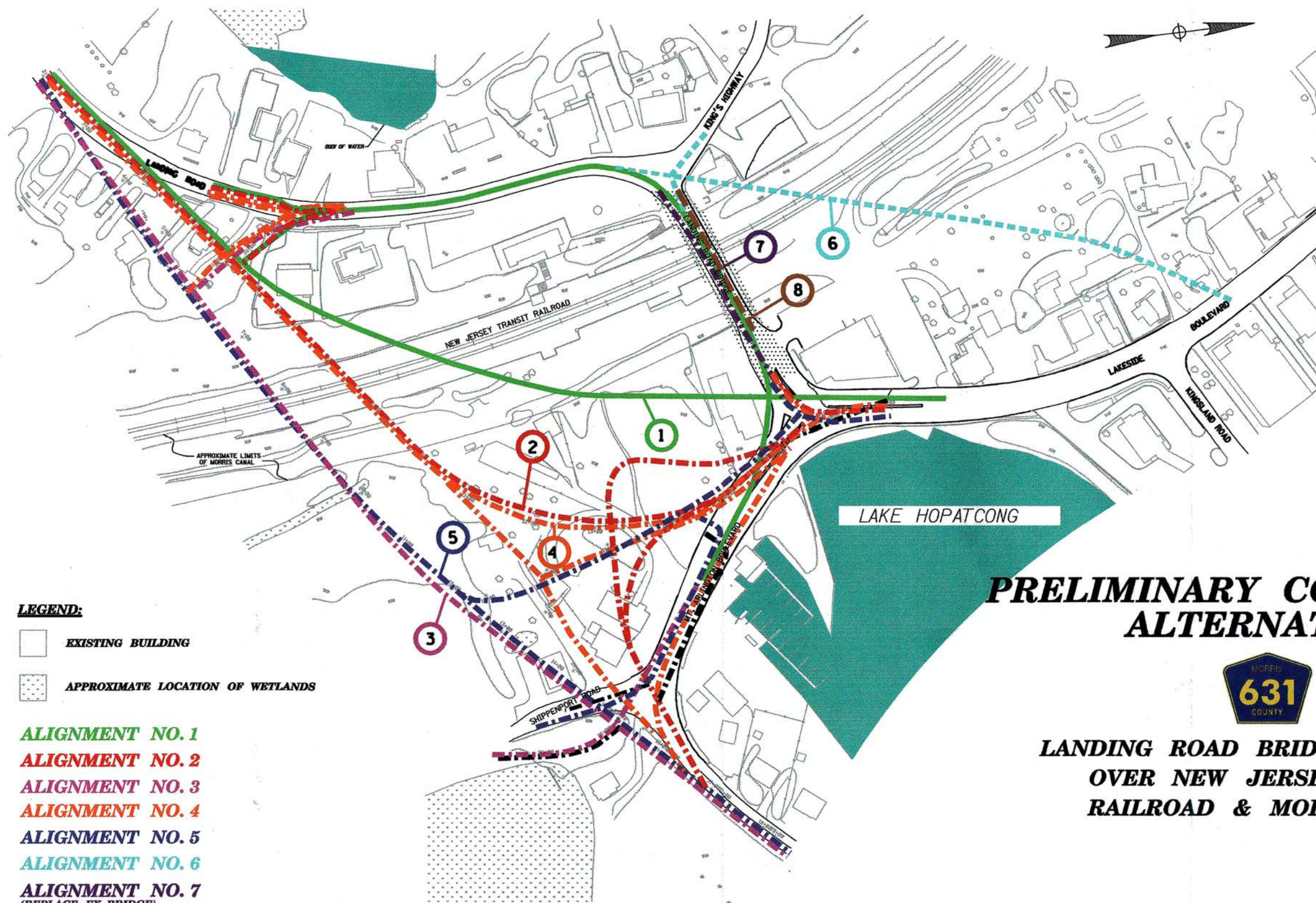
Additionally the Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter mile segment of Landing Road involving acquisitions of 29 properties with 6 complete takes.

As described under the No-Build Alternative, the costs associated with regular maintenance and periodic minor rehabilitation of the existing bridge would result in limited overall benefit and would not be financially feasible for the County. Therefore, the Bypass Alternative is only being considered in conjunction with the removal of the existing bridge.



5.4 REPLACEMENT ALTERNATIVE (ALTERNATIVE 7-C)

The Replacement Alternative (Alternative 7-C), shown in Figure 5-3, is the Preferred Alternative for this project. This alternative would provide a replacement bridge in the same general location as the existing bridge, with some adjustments of the bridge alignment and approaches for safety improvements and upgrading.

This alternative would replace the existing two-lane bridge with a new four-lane bridge and would improve the horizontal alignment of the bridge by rotating it slightly relative to the existing bridge. The new bridge would provide two 12-foot lanes in each direction and a 6-foot wide sidewalk on its west side. The south approach of Landing Road would be reconstructed to transition from the existing two lanes to four lanes on the bridge. The intersection of Landing Road/Mt. Arlington Boulevard and Lakeside Boulevard would be reconstructed to incorporate a fourth leg providing access to and from Canal Street and the NJ TRANSIT station parking area. The existing Canal Street access from Landing Road and Lakeside Boulevard to the parking area would be eliminated, improving traffic operations and safety on these roads. Southbound Lakeside Boulevard would be widened to provide an exclusive left turn lane, an exclusive through lane, and a shared through and right turn lane.



LEGEND:

-  EXISTING BUILDING
-  APPROXIMATE LOCATION OF WETLANDS

- ALIGNMENT NO. 1**
- ALIGNMENT NO. 2**
- ALIGNMENT NO. 3**
- ALIGNMENT NO. 4**
- ALIGNMENT NO. 5**
- ALIGNMENT NO. 6**
- ALIGNMENT NO. 7**
(REPLACE EX. BRIDGE)
- ALIGNMENT NO. 8**
(EX. BRIDGE SAFETY IMPROVEMENTS/REPAIRS)

PRELIMINARY CONCEPTUAL ALTERNATIVES



**LANDING ROAD BRIDGE NO. 1400-073
OVER NEW JERSEY TRANSIT
RAILROAD & MORRIS CANAL**



PARSONS

FIGURE 5-1

BYPASS (ALTERNATIVE 1)

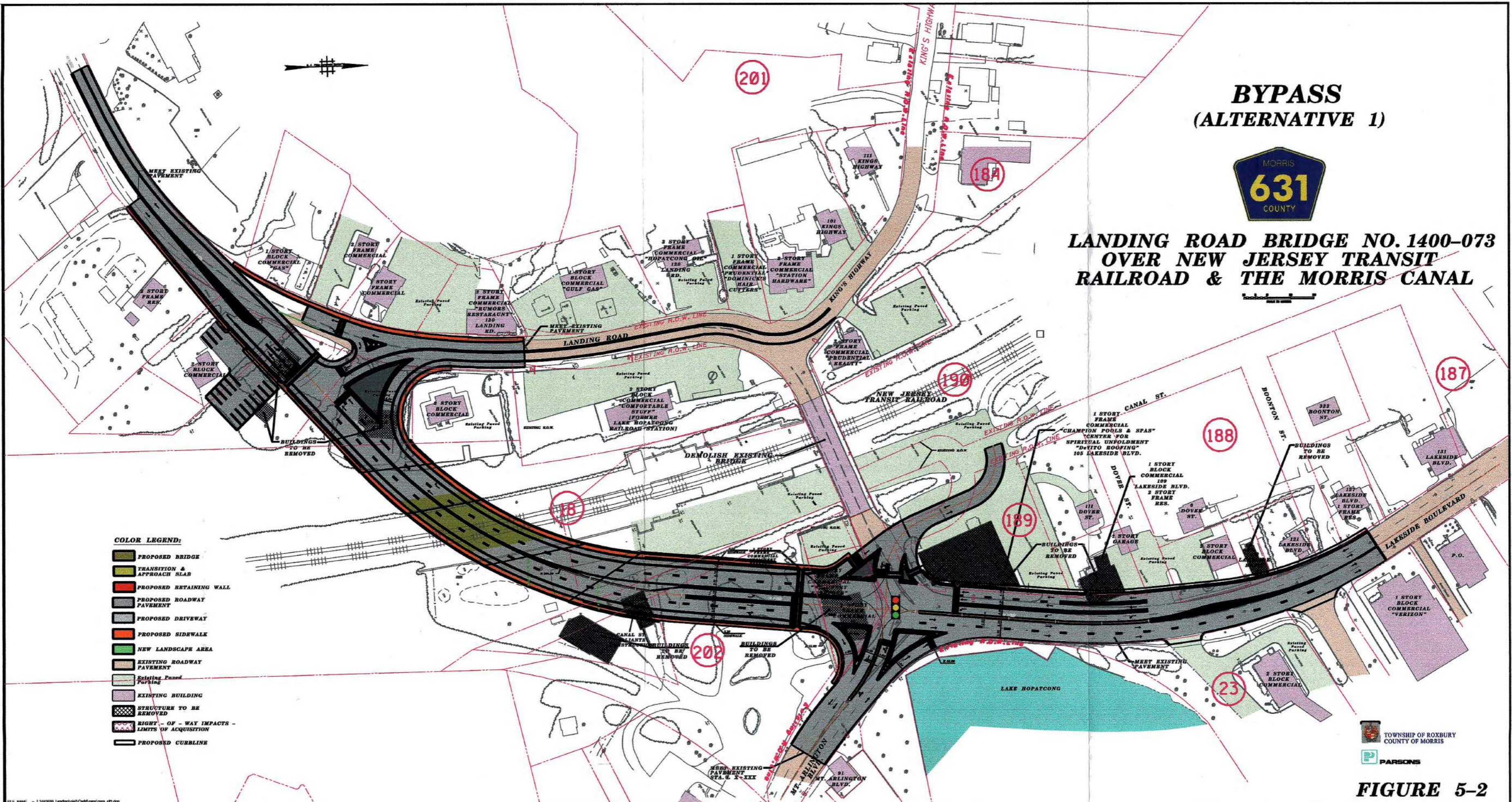


LANDING ROAD BRIDGE NO. 1400-073 OVER NEW JERSEY TRANSIT RAILROAD & THE MORRIS CANAL



COLOR LEGEND:

- PROPOSED BRIDGE
- TRANSITION & APPROACH SLAB
- PROPOSED RETAINING WALL
- PROPOSED ROADWAY PAVEMENT
- PROPOSED DRIVEWAY
- PROPOSED SIDEWALK
- NEW LANDSCAPE AREA
- EXISTING ROADWAY PAVEMENT
- Existing Paved Parking
- EXISTING BUILDING
- STRUCTURE TO BE REMOVED
- RIGHT-OF-WAY IMPACTS - LIMITS OF ACQUISITION
- PROPOSED CURBLINE



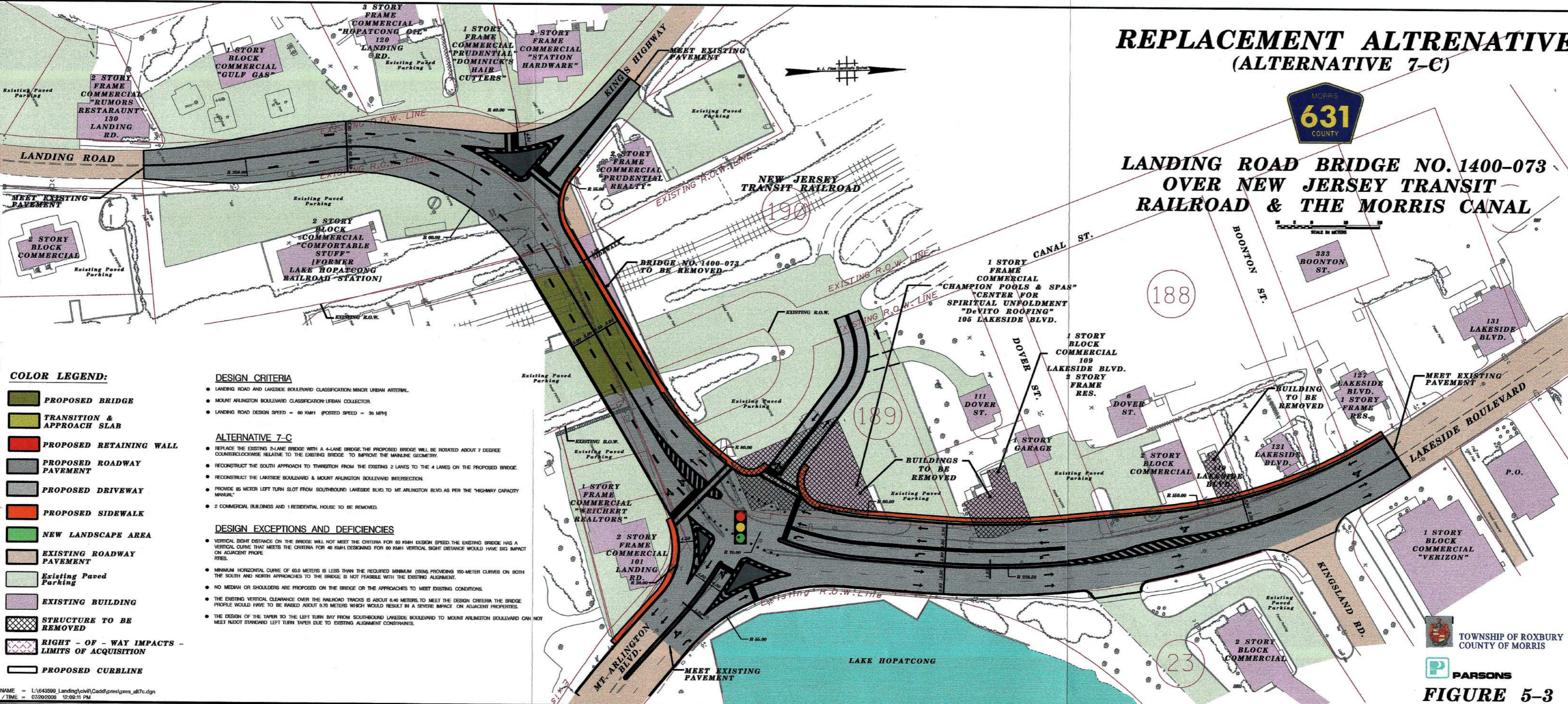
TOWNSHIP OF ROXBURY
COUNTY OF MORRIS
PARSONS

FIGURE 5-2

REPLACEMENT ALTERNATIVE (ALTERNATIVE 7-C)



LANDING ROAD BRIDGE NO. 1400-073 OVER NEW JERSEY TRANSIT RAILROAD & THE MORRIS CANAL



COLOR LEGEND:

- PROPOSED BRIDGE
- TRANSITION & APPROACH SLAB
- PROPOSED RETAINING WALL
- PROPOSED ROADWAY PAVEMENT
- PROPOSED DRIVEWAY
- PROPOSED SIDEWALK
- NEW LANDSCAPE AREA
- EXISTING ROADWAY PAVEMENT
- Existing Paved Parking
- EXISTING BUILDING
- STRUCTURE TO BE REMOVED
- RIGHT - OF - WAY IMPACTS - LIMITS OF ACQUISITION
- PROPOSED CURBLINE

DESIGN CRITERIA

- LANDING ROAD AND LAKESIDE BOULEVARD CLASSIFICATION: MINOR URBAN ARTERIAL
- MOUNT ARLINGTON BOULEVARD CLASSIFICATION: URBAN COLLECTOR
- LANDING ROAD DESIGN SPEED = 60 KM/H (POSTED SPEED = 35 MPH)

ALTERNATIVE 7-C

- REPLACE THE EXISTING 2-LANE BRIDGE WITH A 4-LANE BRIDGE. THE PROPOSED BRIDGE WILL BE ROTATED ABOUT 7 DEGREE COUNTERCLOCKWISE RELATIVE TO THE EXISTING BRIDGE TO IMPROVE THE MAINLINE GEOMETRY.
- RECONSTRUCT THE SOUTH APPROACH TO TRANSITION FROM THE EXISTING 2 LANES TO THE 4 LANES ON THE PROPOSED BRIDGE.
- RECONSTRUCT THE LAKESIDE BOULEVARD & MOUNT ARLINGTON BOULEVARD INTERSECTION.
- PROVIDE 65 METER LEFT TURN SLOT FROM SOUTHBOUND LAKESIDE BLVD. TO MT. ARLINGTON BLVD. AS PER THE "HIGHWAY CAPACITY MANUAL".
- 2 COMMERCIAL BUILDINGS AND 1 RESIDENTIAL HOUSE TO BE REMOVED.

DESIGN EXCEPTIONS AND DEFICIENCIES

- VERTICAL SIGHT DISTANCE ON THE BRIDGE WILL NOT MEET THE CRITERIA FOR 60 KM/H DESIGN SPEED. THE EXISTING BRIDGE HAS A VERTICAL CURVE THAT MEETS THE CRITERIA FOR 40 KM/H. DESIGNING FOR 60 KM/H VERTICAL SIGHT DISTANCE WOULD HAVE BIG IMPACT ON ADJACENT PROPE RTIES.
- MINIMUM HORIZONTAL CURVE OF 60.0 METERS IS LESS THAN THE REQUIRED MINIMUM (150M). PROVIDING 150-METER CURVES ON BOTH THE SOUTH AND NORTH APPROACHES TO THE BRIDGE IS NOT FEASIBLE WITH THE EXISTING ALIGNMENT.
- NO MEDIAN OR SHOULDERS ARE PROPOSED ON THE BRIDGE OR THE APPROACHES TO MEET EXISTING CONDITIONS.
- THE EXISTING VERTICAL CLEARANCE OVER THE RAILROAD TRACKS IS ABOUT 8.40 METERS. TO MEET THE DESIGN CRITERIA THE BRIDGE PROFILE WOULD HAVE TO BE RAISED ABOUT 0.70 METERS WHICH WOULD RESULT IN A SEVERE IMPACT ON ADJACENT PROPERTIES.
- THE DESIGN OF THE TAPER TO THE LEFT TURN BAY FROM SOUTHBOUND LAKESIDE BOULEVARD TO MOUNT ARLINGTON BOULEVARD CAN NOT MEET NJDOT STANDARD LEFT TURN TAPER DUE TO EXISTING ALIGNMENT CONSTRAINTS.

FILE NAME = L:\643599_Landing\civil\Cadd\pres\pres_alt7c.dgn
DATE / TIME = 07/20/2006 12:09:11 PM

...civil\Cadd\pres\pres_alt7c.dgn 07/20/2006 12:09:10 PM

TOWNSHIP OF ROXBURY
COUNTY OF MORRIS



FIGURE 5-3

SECTION 6 - ENVIRONMENTAL IMPACTS

This section provides an evaluation of the potential environmental and community impacts associated with each of the four alternatives introduced in Section 5: the No-Build Alternative, the Rehabilitation Alternative, the Bypass Alternative (Alternative 1), and the Replacement Alternative (Alternative 7-C). Alternatives considered earlier in the screening process that were determined to not be practicable or feasible were not carried through to this stage of the analysis.

Each of the four alternatives is evaluated in this section with reference to environmental, economic, and engineering attributes. The potential effects of each alternative are analyzed relative to other alternatives and relative to existing baseline environmental conditions. Impacts that preserve the integrity of an attribute are considered beneficial, whereas impacts that degrade the integrity of the attribute are considered adverse.

The impacts are identified as short-term or long-term and direct or indirect. Short-term impacts are usually associated with construction activities and are expected to diminish or end at the close of construction or shortly thereafter. Long-term impacts occur or continue after construction of the project is complete. Direct impacts occur as a primary result of the construction and/or operation of the project, whereas indirect, or secondary, impacts are reasonably foreseeable impacts that are caused by the action, but are later in time or farther removed in distance. Mitigation measures may be utilized to minimize non-significant impacts to an acceptable level of effect on the environment and/or community.

The attributes evaluated include the following:

- socioeconomic resources and right-of-way;
- physical resources;
- water resources;
- biological resources;
- cultural resources; and
- hazardous substances.

The narrative discussions are limited to those impacts that are shown to be adverse or beneficial. Discussions are not provided for areas where the attribute is not present in the project area and where there is no reasonably identifiable effect by the implementation of either the no-build or an action alternative. The following subsections provide discussions regarding the specific nature and extent of impacts associated with each of the four alternatives.

6.1 SOCIOECONOMIC RESOURCES AND RIGHT-OF-WAY

The project area is primarily within the town of Landing in Roxbury Township, which has a Limited Business Zoning District that is slated for service and retail businesses according to the Roxbury Land Development Ordinance. The properties located along the roadways within the project area include residences and small businesses, some of which have living quarters in the buildings, and some of which are currently vacant. The properties, land use, businesses, local economy, and people represent the socioeconomic resources of the project area. This section also includes a general assessment of each alternative's consistency with regional, county, and local planning goals.

The number and size of residential and business parcels affected by each alternative are included in the cost estimate tables found in Appendix C of this report. The property owner list for Alternative 7C was updated in February of 2015 and is also included (along with an accompanying graphic) in Appendix C.

6.1.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, but would require regular maintenance and minor rehabilitation over the next 20 years in order to extend the life of the bridge during that period. The No-Build footprint would be consistent with the existing bridge and roadways, and would not be perceived as any change to the current condition. The No-Build Alternative would not involve any taking of additional right-of-way or construction outside of the existing footprint of the bridge, and no residential or business properties or utilities would be impacted. Secondary indirect impacts that could potentially result from the deteriorating condition of the bridge include the loss of NJ TRANSIT station parking under the bridge due to hazards from spalling concrete, and reduced usage (load limits or single lane) or closing of the bridge due to long-term deterioration. The long-term lack of traffic improvements and the continued deterioration of the existing bridge could potentially result in a reduction in any planned business development, as well as an increase in local traffic congestion..

The No-Build Alternative is not in compliance with regional, county, and local planning goals. Because the alternative does not include improvements that would address the poor traffic conditions in the study area, conditions would be expected to deteriorate further as traffic levels increase in the future. Planned economic growth could be impaired as the congestion intensifies and motorists avoid the area. These would be viewed as long-term indirect adverse impacts.

6.1.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build (i.e., the same as the existing footprint) but it would require periodic construction activities in order to extend the useful life of the bridge for as long as possible. [Existing bridge maintenance requirements will continue to increase as rehabilitation within the parameters of historic integrity will result in repairs of failing and severely deteriorating structural elements with materials to match the old in design, color, and texture. Replacement of the historical features will be substantiated with documentary and physical evidence.](#)

Although this alternative may extend the life of the bridge beyond that expected under the No-Build Alternative, overall deterioration of the bridge would continue to exist, and the operational deficiencies of the bridge and its approaches would not be improved. This alternative would not involve any taking of additional right-of-way, and no direct impact to residential or business properties would occur; the potential for disruption to utilities would also be limited. However, this alternative would involve short-term adverse impacts during the construction

period due to increased congestion and traffic disruption. During the construction period, local businesses may have some loss of business due to users avoiding the area or being delayed by the construction. Because engineering studies have indicated the substandard condition of the bridge, the Rehabilitation Alternative would be envisioned to have the same long-term secondary impacts associated with bridge deterioration or restricted usage.

The Rehabilitation Alternative is not in compliance with regional, county, and local planning goals. Because the alternative does not include improvements that would address the poor traffic conditions in the study area, conditions would be expected to deteriorate further as traffic levels increase in the future. Planned economic growth could be impaired as the congestion intensifies and motorists avoid the area. These would be viewed as long-term indirect adverse impacts.

6.1.3 Bypass Alternative

The Bypass Alternative would involve extensive acquisition of right-of-way (29 partial or whole property takings) for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. It is anticipated that demolition of at least four residences and four businesses locations would be required. One of the buildings to be removed houses two businesses on the lower level and three residential units that provide lower-cost housing; this is consistent with a judicial mandate requirement for Roxbury Township. The removal of the needed properties from the tax base would have an impact on the community as a whole. Extensive utility impact and relocation would also be required.

This alternative would allow traffic to bypass the last quarter mile of Landing Road, which includes as many as 11 business locations with up to five business offices within them. This would eliminate the roadside exposure and would increase the effort required to access these business locations, resulting in a long-term adverse impact on the businesses similar to that of changing the business locations from a thoroughfare to a side street.

The short-term construction disruptions of the businesses would be more extensive than the Rehabilitation Alternative due to the length of construction and multiple locations.

The Bypass Alternative is also not in compliance with regional, county, and local planning goals. The regional and local land-use plans provide for business development in the Landing area. Planned economic growth could be impaired as the bypass reroutes motorists away from this portion of the Landing area. This would be viewed as a long-term, indirect, adverse impact.

6.1.4 Replacement Alternative

The Replacement Alternative incorporates safety improvements and advanced engineering factors that would require the acquisition of some additional right-of-way, although not to the same extent as with the Bypass Alternative (20 partial or whole property takings in comparison to 29). Specifically, two commercial buildings and at least one residence, all located along Lakeside Boulevard, would be acquired. Utility relocations would also be required. Replacement of the bridge would have similar short-term construction disruptions to those associated with the Rehabilitation Alternative, with a somewhat larger area and a longer duration.

The replacement of the bridge with upgraded engineering and a minor modification of the alignment would not have the long-term adverse impacts on the businesses from traffic diversion or potential restrictions or closure of bridge traffic. In fact, this alternative is generally

compatible with the planned business development, and would provide long-term roadway enhancements that would decrease congestion and improve access, although short-term traffic and business disruptions would occur during construction. The addition of turning lanes would reduce the congestion related to the signal light operation. The turning lanes and widening would allow for a greater ease of access and egress for the businesses.

The Replacement Alternative is in compliance with regional, county, and local planning goals by meeting the long-term transportation needs associated with business development in the Landing area. The Replacement Alternative would slightly modify the original alignment and retain the usage of Landing Road. Because the Replacement Alternative would retain existing traffic patterns and improve roadway operations in the Landing Road vicinity, it would have long-term benefits for the Landing Road businesses and community and would support planned economic growth in the Landing area.

6.2 PHYSICAL RESOURCES

The physical resources within the project area include geologic and topographic features, soils, air quality, and noise levels. The project area lies within the Highlands physiographic province that includes northeast trending ridges and valleys gradually dropping in elevation from west to east. The topography varies, as the landforms have been reworked as the area developed. The elevations range from 900 feet above mean sea level (msl) where the NJ TRANSIT tracks leave to the south, to the hillside east of Landing Road where the slopes pass 950 feet above msl. The Lake Hopatcong spillway holds the lake at approximately 924 feet. The reported elevation of the Landing Road Bridge is also 924 feet.

No unique geologic formations or outcrops have been identified within the project area. The soils on both sides of Landing Road were formed in young glacial till. In general, these soils belong to the Netcong-Rockaway associations. The soils throughout the project area are unsuited to cultivation. Many of the adjacent properties have had the soils modified through cut and fill operations. Much of the area adjacent to Landing Road and the bridge has been leveled and filled to support commercial structures or cutout to provide access. These soils are not associated with special agricultural designations, nor are they acid producing soils.

The project area is primarily within the town of Landing in Roxbury Township, which consists of service and retail businesses. The community roadways are two or four lanes with posted speed limits of 25 and 30 miles per hour. The properties located along the roadways within the project area include residences and a variety of small businesses, including offices, food service, and fuel service stations. Landing is adjacent to the recreational community associated with Lake Hopatcong. Other than the residences and Lake Hopatcong (a recreational lake), there are no other sensitive air or noise receptors such as motels, hotels, schools, churches, hospitals, nursing homes, or libraries within the project area. There are no major sources of air pollution or noise identified near the area.

The entire state of New Jersey, including Morris County, is classified as non-attainment for the 1997 (0.08 ppm) and 2008 (0.075 ppm) 8-hour ozone National Ambient Air Quality Standards (NAAQS). Morris County is also classified as non-attainment for the 1997 annual PM 2.5 standard (15 $\mu\text{g}/\text{m}^3$) and 2006 24-hour PM2.5 standard (35 $\mu\text{g}/\text{m}^3$).

6.2.1 No-Build Alternative

The No-Build footprint would be the same as the existing bridge and roadways. The No-Build Alternative would not involve any taking of additional right-of-way and therefore would

not alter the existing physical resources over the short term. The No-Build Alternative would not improve the peak-hour traffic congestion, which contributes slightly to the degraded air quality and noise levels in the project area.

6.2.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative, but would involve short-term adverse impacts during the construction period due to increased congestion and traffic disruption. Short-term air quality impacts could be expected during the construction associated with the bridge rehabilitation. During the construction period, increased amounts of dust or particulate matter would be generated during excavation, grinding, hauling, and other construction operations. The operation of heavy equipment during construction would increase dust emissions and vehicle exhaust emissions. Dust emissions can be mitigated through the implementation of dust control measures such as wetting exposed soils, covering material stockpiles, and cleaning trucks prior to their leaving the construction site.

During the construction period, short-term noise levels would also increase. Mitigation measures and controls could be incorporated into the project to reduce these to acceptable levels.

6.2.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and new bridge that would bypass a quarter-mile segment of Landing Road. The construction of the bypass would involve the reworking of the soil in the area to be developed. Short-term adverse impacts during the construction period would include increased congestion and traffic disruption. During the construction period, fugitive dust and noise levels would increase. Short-term air quality impacts would be expected during the construction of the bridge and associated highway improvements. During the construction period, increased amounts of dust or particulate matter would be generated during excavation, grading, hauling, and other construction operations. The operation of heavy equipment during construction would increase dust emissions and vehicle exhaust emissions. Dust emissions can be mitigated through the implementation of dust control measures such as wetting exposed soils, covering soil stockpiles, and cleaning trucks prior to their leaving the construction site.

The placement of the bypass on an elevated structure would create long-term impacts associated with the adverse increases in the noise levels around the bypass, some of which is currently undeveloped land. The removal of the trees in the bypass route would also accentuate the noise impacts by diminishing the noise-buffering effect of these resources. Further study may be required during design to determine if additional mitigation measures will be required to address the increases in noise levels.

6.2.4 Replacement Alternative

The Replacement Alternative consists of bridge replacement and widening with approach improvements. The safety improvements include lane widening, addition of turning lanes, and modification of existing intersections. The improvements would not involve a significant increase in traffic volumes or speed increase; therefore, detailed noise studies are not required for this project.

The Replacement Alternative would likely have more impacts on the physical resources than the No-Build and Rehabilitation alternatives, but notably less than the Bypass Alternative. Replacement of the bridge would have similar short-term construction disruptions to those

associated with the Rehabilitation Alternative, with a somewhat larger area and a longer duration. The incorporation of safety improvements and advanced engineering factors would require the acquisition of additional right-of-way involving the demolition of three structures.

Short-term air quality impacts would be expected during the demolition and reconstruction of the bridge and associated highway improvements. During the construction period, increased amounts of dust or particulate matter would be generated during excavation, grading, hauling, and other construction operations. The operation of heavy equipment during construction would increase dust emissions and vehicle exhaust emissions. Dust emissions can be mitigated through the implementation of dust control measures such as wetting exposed soils, covering soil stockpiles, and cleaning trucks prior to their leaving the construction site.

The addition of turning lanes would bring traffic noise closer to the adjacent buildings, although any noise level increase would likely not be significant. Unlike the Bypass Alternative, this alternative would not create a new noise corridor. The reduction in peak hour congestion would provide a secondary long-term beneficial reduction in the associated noise and air quality concerns. This alternative would not have an impact on regional emissions since it does not provide for additional capacity or new access. Further study may be required during design to determine if additional mitigation measures will be required to address the increases in noise levels.

6.3 WATER RESOURCES

Water resources include surface waters (ponds, lakes, and streams), associated flood plains, and ground water resources. The Landing area lies on an upland portion between the Delaware River and the Raritan River drainage basins. Lake Hopatcong drains to the upper reaches of the Musconetcong River basin that leads to the Delaware River. The spillway for the lake is approximately one mile northwest of Landing. A small pond named Riggs Lake (1.47 acres) is located behind the properties on the west-side of Landing Road just outside the project area. The project area does not include any running streams, but it does include the properties adjacent to the south end of Lake Hopatcong. A small wetland is located just outside of the project area at the south end of the Canal Street properties. Environmental mapping and the visual site inspection did not identify any ponds or wetlands within the immediate project area.

Because the Landing area is in the upper portions of the drainage basin, natural drainages have not developed to transport the runoff from the small drainage areas. No flood plains are identified within the project area other than the back portions of the properties adjacent to the southeast side of the lake and fronting on Mt. Arlington Boulevard (FEMA, 1982). These potential flood areas are outside of any construction areas.

The Landing area is located over glacial deposits with moderate permeability material and silty sand mixtures as identified in the county resource mapping (MCDPD, 2000). In the vicinity of the project area, ground water wells provide the water supply for many residences and businesses.

6.3.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. The No-Build Alternative would not impact water resources within the project area.

6.3.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build. This alternative may involve some minor short-term adverse impacts during the construction period due to runoff from the construction area.

6.3.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and new bridge that would bypass a quarter-mile segment of Landing Road. The Bypass Alternative involves construction over undeveloped areas and may generate short-term adverse effects from construction runoff and sedimentation. A long-term adverse impact would be the need for management of stormwater runoff from the additional paved areas associated with this alternative. The new pavement would cover approximately 2.5 acres that would include the new roadway and shoulders. This would require the management of approximately 68,000 gallons of runoff for each 1-inch of rainfall. Additional right-of-way acquisition would not be required for management of stormwater runoff. Construction and operational runoff would need to be properly managed to prevent impacts on the lake within the project area and the ponds and wetland that lie just outside the project area.

6.3.4 Replacement Alternative

The Replacement Alternative would result in more surface water resource impacts than the No-Build and Rehabilitation alternatives, but much less than the Bypass Alternative. Replacement of the bridge would have similar short-term construction disruptions to those associated with the Rehabilitation Alternative, with a somewhat larger area and a longer duration. The incorporation of safety improvements and advanced engineering factors would require the acquisition of additional right-of-way involving previously developed areas, most of which are paved road shoulders, paved parking or roadside lawns. The expanded construction area would create short-term adverse impacts through the sedimentation associated with runoff. Over the long-term, this alternative would not create a substantial amount of additional paved area and is not expected to create adverse impact on surface water resources. The new pavement would cover less than 0.1 acres. This would require the management of approximately 2,700 gallons of runoff for each 1-inch of rainfall.

6.4 BIOLOGICAL RESOURCES

Biological resources include fish, wildlife, vegetation, wetlands, and potential for threatened and endangered species. The land surface of the entire project area has been reworked, in some cases several times, as historical development occurred. The only portion of the project area that is considered vacant or undeveloped is a 1.27-acre lot between Canal Street and Mt. Arlington Boulevard next to the NJ TRANSIT parking area. This area is often used for overflow parking from the NJ TRANSIT lots. It is covered with grasses with some herbaceous vegetation and young deciduous hardwood species such as maple, ash, oak, and hickory.

The wildlife in the area is typical of those occupying developed areas, including small mammals (mice, shrews, bats, rabbits, skunk, opossum, raccoon, woodchuck, and squirrel), and occasional large mammals such as fox and deer. A variety of reptiles and amphibians also inhabit the area. Most of the project area consists of developed yards or paved areas that have minimal value as wildlife habitat. Lake Hopatcong is classified as Trout Maintenance, indicating that it is capable of supporting trout throughout the year. The developed nature of the project area precludes the existence of habitat for threatened and endangered species, and none were noted during the site inspection or in the literature reviewed.

The project area does not include any running streams providing aquatic wildlife habitat, but it does include the properties adjacent to the south end of Lake Hopatcong. A small wetland is located just outside of the project area at the south end of the Canal Street properties. Environmental mapping and the visual site inspection did not identify any ponds or wetlands within the immediate project area.

6.4.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. The No-Build Alternative would not involve any taking of additional property and therefore would not be expected to have any impacts on the biological resources in the project area.

6.4.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative. The Rehabilitation Alternative would not involve any taking of additional property and therefore would not be expected to have any impacts on the biological resources in the project area.

6.4.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. The new bypass roadway and bridge would be approximately one-quarter mile in length. The noise and construction activities would cause short-term impacts on the animals as they try to avoid the construction zone. However, approximately half of the length would be constructed in vacant field or semi-wooded areas that would have some wildlife value. This alternative would add another break in the limited undeveloped tree covered areas that remain in this area. This terrestrial habitat and the associated wildlife would have long-term adverse impacts associated with the loss of habitat through the clearing for construction and maintenance of the new bypass roadway and bridge. The location of the bypass also threatens to have indirect short-term and long-term impacts on the wetland just outside of the project area. Land-clearing activities, construction, and operational runoff may encroach on the associated transition area or disrupt the ecology of this small wetland. These indirect impacts would be considered short- and long-term adverse impacts on the wetland.

6.4.4 Replacement Alternative

The Replacement Alternative would have more biological impacts than the No-Build and Rehabilitation alternatives, but much less than the Bypass Alternative. Replacement of the bridge would have similar short-term construction disruptions to those associated with the rehabilitation alternative with a somewhat larger area and a longer duration. Short-term construction adverse impacts include the potential for wildlife inhabiting nearby properties to leave the area in order to avoid the noise and disruption. Some field areas may be used as temporary areas for stockpiling materials and equipment.

The incorporation of safety improvements and advanced engineering factors will require the acquisition of additional right-of-way involving previously developed and paved areas. The acquired right-of-way for this alternative has negligible wildlife habitat value and should not result in long-term impacts on the biological resources in the area. Some incidental tree loss may

occur adjacent to the reworked roadway and bridge; however, none of the trees appear to be of specimen quality.

6.5 CULTURAL RESOURCES

Several cultural resource investigations of the Area of Potential Effect (APE) were conducted as part of this study, including a *Historic Architectural Sites Survey* completed in September 2001 and revised in October 2004 and a *Phase IA Archaeological Investigation* completed in July 2003. The results of these studies, as well as the prehistoric and historic context of the Landing area, are described below.

6.5.1 Identification of Resources

During most of the prehistoric period (ca. 10,000 B.C. to A.D. 1650), northern New Jersey was occupied by various groups of nomadic hunters and gatherers. However, during the last 1,000 to 1,500 years, prehistoric populations became more numerous and more sedentary. The diffusion and subsequent adoption of agricultural practices, supplemented with hunting, fishing, and intensive plant collecting, permitted the production of food surpluses as well as the development of sedentary populations. Prehistoric campsites (and later hamlets or villages) were generally located in proximity to potable water and in locations that provided access to diverse resources. The APE is located in a shallow valley south of Lake Hopatcong, a natural glacial lake. Prehistoric sites in New Jersey are frequently associated with level to moderately level terrain, well drained or moderately well drained soils such as those associated with stream terraces, and proximity to potable water. Although much of the APE is relatively level, the soils are steep to very steep, stony or gravely, and moderately well drained to poorly drained.

Based on the diverse land use history within the town of Landing and in the vicinity of the bridge, railroad station, and canal, it is possible that a variety of historical archaeological sites may be located beneath fill deposits within the project area. In addition to the Morris Canal, other possible sites in the project area include artifacts or features associated with the canal, the railroad, the railroad station (both the original station and the current station), commercial businesses, and historical residences.

The Village of Landing developed in the third quarter of the nineteenth century as a result of the Morris Canal and the Delaware, Lackawanna & Western (DL&W) Railroad. The Morris Canal (described in detail below), opened in 1831 and in operation until the 1920s, was designed to transfer coal, iron ore, and other goods from eastern Pennsylvania and northwestern New Jersey to market centers like Jersey City, Paterson, Newark, and New York. Within decades, however, the concern for faster and cheaper year-round transportation of coal and iron ore, and the desire to increase tonnage, resulted in the formation and construction of a railroad. The DL&W Railroad was constructed in the 1850s and followed a circuitous route between Phillipsburg on the Delaware River to Jersey City on the east, the same terminus points as used by the Morris Canal. The Morris Canal was abandoned in 1924 and subsequently filled.

No archaeological sites that are eligible for listing in the NRHP were identified within the APE for archaeology, which corresponds to the footprint of anticipated ground disturbance directly associated with the project actions. The July 2003 *Phase IA Archaeological Investigation* identified three areas of moderate historic archaeological potential within the APE for archaeology. One of these locations, designated as Area 1, is located at the eastern abutment of the Landing Road Bridge and may possess archaeological deposits associated with the eastern bank and towpath of the Morris Canal. It was recommended that no further investigation of this area be conducted until site preparation work for the bridge rehabilitation/replacement begins. In

the event that potentially significant canal-related archaeological resources are identified, consultation between the New Jersey Department of Transportation and the New Jersey Historic

Preservation Office will define the procedures necessary to proceed with the handling of said resources.

With state funding to be utilized for all forthcoming project actions, only a single cultural resource – **the Morris Canal Historic District** – that requires consideration as a consequence of its listing on the New Jersey Register of Historic Places on November 26th, 1973. The New Jersey Register of Historic Places Act requires that any undertaking that will “encroach upon, damage or destroy” a resource listed on the New Jersey Register is subject to review. This review commences with the submission of an Application for Project Authorization to the New Jersey Historic Preservation Office, with a determination of Encroachment or No Encroachment to result. In the event that the project is ruled to be an Encroachment, a further review by the New Jersey Historic Sites Council will be required. The process concludes with final action being taken by the Commissioner of the Department of Environmental Protection based on recommendations provided by the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council.

The Morris Canal was instrumental in the development of the nineteenth-century village of Landing, now part of the Township of Roxbury. The waterway, which operated from 1831 to 1924, was designed to transfer coal, iron ore, and other goods from eastern Pennsylvania and northwestern New Jersey to markets such as Jersey City, Paterson, Newark, and New York City. The summit of the canal was Lake Hopatcong, and a 0.65-mile navigable feeder connected Lake Hopatcong with the main canal at Landing. A series of locks and dams as well as a system of inclined planes was constructed to negotiate the mountainous terrain of northwestern New Jersey. With the opening of the Morris and Essex Railroad in 1853, which included stations at Landing and Port Morris, passengers and freight were transferred from boat to railroad car at the Landing (Lake Hopatcong) Station. A detailed history of the Morris Canal can be found in the October 2004 *Historic Architectural Sites Survey*. Within the project’s APE, the site of the Morris Canal has been filled, with the sections adjacent to the Lake Hopatcong Station now serving as an asphalt-paved parking lot and a gravel service road associated with the station.

6.5.2 Evaluation of Effects

The potential effects on of the four alternatives on the Morris Canal Historic District can be summarized as follows:

- No-Build Alternative – no effect
- Rehabilitation Alternative – no adverse effect
- Bypass Alternative – **Adverse Effect** resulting from the demolition of the Landing Road Bridge, a contributing resource within the district and **Potential Adverse Effect** resulting from impacts to potential canal-related archeological resources that may be disturbed during construction
- Replacement Alternative – **Adverse effect** resulting from the demolition of the Landing Road Bridge, a contributing resource within the district and **Potential Adverse Effect** resulting from impacts to potential canal-related archeological resources that may be disturbed during construction

No-Build Alternative

The No-Build Alternative would not involve any taking of additional right-of-way and therefore not involve excavation or modification of existing structures. This alternative would have no effect on the Morris Canal Historic District.

Rehabilitation Alternative

The Rehabilitation Alternative would not involve any taking of additional right-of-way and would have a similar footprint to the No-Build Alternative, but it would involve short-term adverse impacts during the construction period due to increased congestion and traffic disruption. During the construction period, the bridge and the Morris Canal Historic District would be disturbed by the rehabilitation activities. Assuming that the rehabilitation of the bridge was completed in a historically appropriate manner, this alternative would have no adverse effect on the Morris Canal Historic District.

Bypass Alternative

The Bypass Alternative would have an adverse effect on the Morris Canal Historic District as the bridge would be demolished to the expense of continued maintenance during the next 20 years and beyond. The New Jersey Historic Preservation Office has determined that the construction of the project as proposed in this alternative will have an adverse effect on the Morris Canal Historic District due to removal of the Landing Road Bridge, a contributing resource to this historic district. It is also possible that archaeological features contributing to the eligibility of the Morris Canal Historic District could be discovered during construction, which may also result in an adverse effect. An archaeological monitoring plan will be developed through consultation between the New Jersey Department of Transportation and the New Jersey Historic Preservation Office as an initial step in dealing with these potential archaeological issues.

Replacement Alternative

The Replacement Alternative would also have adverse effect on Morris Canal Historic District as it will involve the demolition of the existing bridge. The New Jersey Historic Preservation Office has determined that the construction of the project as proposed in this alternative will have an adverse effect on the Morris Canal Historic District due to removal of the Landing Road Bridge, a contributing resource to this historic district. It is also possible that archaeological features contributing to the eligibility of the Morris Canal Historic District could be discovered during construction, which may also result in an adverse effect. An archaeological monitoring plan will be developed through consultation between the New Jersey Department of Transportation and the New Jersey Historic Preservation Office as an initial step in dealing with these potential archaeological issues.

6.5.3 Mitigation

To mitigate the adverse effect of the project on the Morris Canal Historic District the project team will coordinate with the New Jersey Department of Transportation and the New Jersey Historic Preservation Office to ensure that the design of the new structure reflects the shape and appearance of the existing bridge to the greatest extent possible, and that other

roadway and landscape design elements are consistent with the character of the said historic district. In addition, to minimize potential effects on unknown archeological resources within the district an archaeological monitoring plan will be developed by the project team for the approval of the New Jersey Department of Transportation and the New Jersey Historic Preservation Office.

6.6 HAZARDOUS SUBSTANCES

Two of the project alternatives would involve the taking of properties that include buildings that were constructed prior to 1970. These buildings may have asbestos containing materials (ACM) in the insulation, fireproofing, flooring, or other construction materials. This concern needs to be addressed on a building-by-building basis and is beyond the scope of this screening study. Site-specific surveys need to be conducted for lead-based paint and other household hazardous substances that also need to be properly managed. Older heating oil USTs also represent a potential for contamination that need to be checked on a specific property basis when the selected alternative is chosen and the preliminary design is complete. Water lines and other utilities may also need verification for the absence of ACM and PCB containing transformers and capacitors. These items need to be identified during the final design and provisions developed to provide for their proper management.

Properties considered to have potential concerns during the hazardous substance contamination screening are identified in the *Hazardous Substance Screening Report* prepared by Parsons in July 2002. Further investigations such as reviewing site-specific studies, remediation, and monitoring records will be required if the selected routing involves the taking of a specific property or subsurface excavation near a site of potential contamination. Soil and groundwater sampling may also be required depending on the involvement with each potentially contaminated property. Resurfacing of an existing roadway within an existing right-of-way represents limited involvement with the property and would most likely not cause exposure or release of contaminated areas. Acquisition of and/or excavation within properties that have been identified as having a potential for contamination would require special precautions and proper classification and management of contaminated media (soil, ground water, and rock).

Specific properties of concern identified during the hazardous substance contamination screening include the following:

Kingsland Road

- **20 Kingsland Road**, Residence - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the leaking heating oil UST has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination.

Lakeside Boulevard

- **105 Lakeside Boulevard**, Commercial, Multiple Businesses - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of reports and remediation records may provide sufficient

information that the threat of contamination from the septic system has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The building would also need to be checked for ACM, lead-based paint, PCB-containing transformers and capacitors, and any potential heating oil tanks. The property has a long record of commercial use (since the late 1800s) and excavators should be aware of potential concerns.

- **130 Lakeside Boulevard**, U.S. Postal Service, Landing Post Office - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of the reports, remediation records, and monitoring records may provide sufficient information that the threat of contamination from the leaking heating oil UST has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination.

Landing Road

- **101 Landing Road**, 101 Landing Company LLC/Weichert Realty - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, leaded gasoline, and waste oil has been adequately addressed (the site was a former gas station). Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The building would also need to be checked for ACM, lead-based paint, PCB-containing transformers and capacitors, and any potential heating oil tanks.
- **124 Landing Road**, Gulf Service Station - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of the reports, remediation records, and monitoring records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, leaded gasoline, and waste oil has been adequately addressed. The site is reported to have free product recovery continuing. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system and floor drain fields may also be of concern if in the path of the improvements.
- **130 Landing Road**, Rumors Restaurant - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the leaking heating oil UST has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination.

- **146 Landing Road, Speedway (formerly Sunoco) Service Station** - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of the reports, remediation records, and monitoring records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, leaded gasoline, and waste oil has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system and floor drain fields may also be of concern if in the path of the improvements. The property has a record of other oil and fuel spills and excavators should be aware of potential concerns.

Mt. Arlington Boulevard

- **91 Mt. Arlington Boulevard, Lakes End Marina** - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property. A specific review of the reports and remediation records may provide sufficient information that the threat of contamination from the former USTs holding heating oil and gasoline has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system may also be of concern if in the path of the improvements. The property has a record of other oil and fuel spills and excavators should be aware of potential concerns.
- **99 Mt. Arlington Boulevard, Amoco Service Station (formerly Landing Mobil)** - The potential for contamination at this property needs to be reviewed if subsurface excavation is involved within 250 feet of the property or if a portion of the property is to be acquired. A specific review of reports and remediation records may provide sufficient information that the threat of contamination from the former USTs holding heating oil, gasoline, and waste oil has been adequately addressed. Otherwise soil sampling at the appropriate levels of excavation may be needed to verify the absence of potential contamination. The septic system and floor drain fields may also be of concern if in the path of the improvements.

6.6.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. The No-Build Alternative would not involve any taking of additional right-of-way and therefore would not involve properties that have potential hazardous substance concerns.

6.6.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative, but would involve short-term adverse impacts during the construction period due to construction work associated with bridge rehabilitation. The properties at 105 Lakeside Boulevard, 101 Landing Road and 124 Landing Road are in close proximity to the bridge and any excavation plans should be reviewed for the potential to involve hazardous substances.

Generation of contaminated media would have short-term adverse impacts involved with its management or remediation.

6.6.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. This may avoid potential involvement with several of the properties of concern along Landing Road except for those between 130 and 146 Landing Road, where the bypass roadway would split off. On the north end of the bypass, properties on Canal Street, 101 Landing Road, and 91 Mt. Arlington Boulevard represent potential short- and long-term impacts associated with the potential management and remediation of any remaining residues.

Any excavation plans should be reviewed for the potential to involve hazardous substances. Generation of contaminated media would have short-term adverse impacts involved with its management or remediation. Long-term impacts would result if the hazardous substance findings warrant long-term remediation activities.

6.6.4 Replacement Alternative

The Replacement Alternative would have more potential interference impacts than the No-Build and Rehabilitation alternatives. Replacement of the bridge would have similar short-term construction potential impacts to those associated with the rehabilitation alternative with a somewhat larger area and a longer duration. The incorporation of safety improvements and advanced engineering factors would require the acquisition of additional right-of-way involving additional properties.

The properties at 105 Lakeside Boulevard, 101 Landing Road, and 124 Landing Road are in close proximity to the bridge and any excavation plans should be reviewed for the potential to involve hazardous substances. The additional work upgrading Landing Road and Lakeside Boulevard could also have associated impacts with properties between 101 and 130 Landing Road and between 105 and 130 Lakeside Boulevard. Generation of contaminated media would have short-term adverse impacts involved with its management or remediation. Long-term impacts would result if the hazardous substance findings warrant long-term remediation activities.

SECTION 7 – EVALUATION OF ALTERNATIVES

This section provides a comparative evaluation of the alternatives under consideration for the Landing Road Bridge Project. The evaluation is intended to demonstrate the relative effectiveness of each alternative in addressing the project’s stated needs and to highlight the relative costs, benefits, and impacts of each alternative.

The evaluation framework addresses the following key questions for each alternative:

- How well does the alternative address the project purpose and need?
- What are the alternative’s effects on the natural and built environments?
- What is the alternative cost and would it be financially feasible for the County?
- What degree of community support does the alternative have?

7.1 CONFORMANCE WITH PROJECT PURPOSE AND NEED

The purpose and need for this project is articulated in Section 4 of this Alternatives Analysis. The purpose of the Landing Road Bridge Project is to improve safety and traffic operations through the rehabilitation or replacement of the Landing Road Bridge and improvements to the intersecting roadways on both approaches. Improvements are needed to address structural and functional deficiencies in the bridge and the approach roadways. As shown in previous Bridge Re-Evaluation Survey Reports, the existing bridge has been considered to be structurally deficient due to the condition of the superstructure and substructure, although the most recent report (September 2013) does not specifically mention “structurally deficient” due to recent interim repairs that have been made (see Appendix B). These reports, including the most recent report, also identify the bridge as being functionally obsolete, due to inadequate deck geometry. In addition, several intersections along the approach roadways experience a Level of Service (LOS) “F” and have substandard geometry.

7.1.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. Normal maintenance would stabilize the structure, preserving the structural integrity of the bridge and extending its useful life. However, these repairs would not address the structural deficiencies of the bridge. Moreover, the bridge would continue to remain functionally obsolete for both the existing and projected future level of daily traffic. The failing levels of service at intersections approaching the bridge would be expected to continue, and as traffic increases, the number of accidents on Landing Road would be expected to increase. Traffic operations would also be negatively affected by the increased need for maintenance and stabilization repairs as the existing bridge continues to age and deteriorate. In the future, without comprehensive rehabilitation, restrictions will likely need to be placed on traffic loads on the bridge, ultimately culminating in its closure to traffic.

Overall, the No-Build Alternative would not meet the purpose and need for the project.

7.1.2 Rehabilitation Alternative

The Rehabilitation Alternative would partially address the structural deficiencies of the bridge. In the near term, the proposed rehabilitation would increase the bridge's load carrying capacity; however, the repairs would not increase the overall strength of the bridge for the long term. Because the bridge has far exceeded its design life and its structural elements have experienced substantial deterioration, such repairs would only be possible by completely replacing the bridge.

The Rehabilitation Alternative would not include widening of the bridge; therefore, under this alternative, the bridge would continue to remain functionally obsolete for both the existing and projected future level of daily traffic. The failing levels of service at intersections approaching the bridge would be expected to continue, and as traffic increases, the number of accidents on Landing Road would be expected to increase.

Similar to the No-Build Alternative, traffic operations under the Rehabilitation Alternative would be negatively affected in the long term by the increased need for maintenance and stabilization repairs as the existing bridge continues to age and deteriorate. In the future, restrictions would likely need to be placed on traffic loads on the bridge, ultimately culminating in its closure to traffic. Due to the structural improvements associated with the rehabilitation work, such a scenario would likely occur at a later time than expected under the No-Build Alternative.

Overall, the Rehabilitation Alternative would partially address the project purpose and need, but most needs would remain unmet.

7.1.3 Bypass Alternative

Under the Bypass Alternative a new bridge would be constructed on a new alignment south of the existing Landing Road Bridge. The new bridge would be designed to meet NJDOT and AASHTO design standards for the current and projected levels of daily traffic in the Landing area. As such, the alternative would address both the structural and functional needs for the project. Moreover, the alternative would include roadway modifications that would improve traffic operations in the Landing area. The associated reduction in traffic congestion would be expected to reduce accident rates in the study area.

Overall, the Bypass Alternative meets the project purpose and need.

7.1.4 Replacement Alternative

Under the Replacement Alternative a new bridge would be constructed to replace the existing Landing Road Bridge in the same general location. The new bridge would be designed to meet NJDOT and AASHTO design standards for the current and projected levels of daily traffic in the Landing area. As such, the alternative would address both the structural and functional needs for the project. Moreover, the alternative would include roadway modifications that would improve traffic operations. The associated reduction in traffic congestion would be expected to reduce accident rates in the study area.

Overall, the Replacement Alternative meets the project purpose and need.

7.2 PROJECT COSTS

Project costs include the cost of relocation of utilities, cost of right-of-way acquisition, and the initial construction cost. Please see Appendix C for detailed cost estimates for the various alternatives. Right-of-way acquisition costs do not include relocation costs.

7.2.1 No-Build Alternative

The No-Build Alternative would involve neither utility relocation nor right-of-way takings. Because no initial construction is required under this alternative, there would be no initial construction cost. However, the need for regular maintenance of the bridge would be required during the next 20 years, simply to extend its life to the end of that 20-year period. This cost would not result in significant long-term benefit, but only the ability to extend the life of the bridge for an additional 20 years.

7.2.2 Rehabilitation Alternative

The cost of utility relocations under the Rehabilitation Alternative would be \$0.12 million (in 2011 dollars), with no right-of-way cost. The initial construction cost of this alternative would be \$1.8 million (in 2011 dollars). These costs total \$1.92 million (in 2011 dollars).

7.2.3 Bypass Alternative

The Bypass Alternative would involve the acquisition of new right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. This new right-of-way would involve acquisition from 29 properties, with six complete takes. The cost of right-of-way acquisition and utility relocations for this alternative would be \$3.8 million and \$1.9 million, respectively, with an initial construction cost of \$9.7 million (in 2011 dollars). These costs total \$15.4 million (in 2011 dollars).

7.2.4 Replacement Alternative

Total capital costs associated with the Replacement Alternative (approximately \$8.7 million in 2011 dollars) would be higher than those of the No-Build and Rehabilitation alternatives but substantially lower than the capital costs associated with the Bypass Alternative (\$15.4 million in 2011 dollars). The Replacement Alternative would also provide for the incorporation of safety improvements and advanced engineering factors, which require the acquisition of additional right-of-way involving 20 properties, with three complete takes. The cost of right-of-way acquisition and utility relocations for this alternative would be \$1.6 million and \$1.0 million, respectively, with an initial construction cost of \$6.1 million (in 2011 dollars).

7.3 COMMUNITY SUPPORT

After considering the results of this alternatives analysis, the Roxbury Township Council issued a resolution supporting the Replacement Alternative (Alternative No. 7-C) on April 14, 2003 and also on September 29, 2015 (see Appendix D). The Roxbury Township Historic Advisory Committee had previously confirmed its support for the Replacement Alternative in a letter dated September 23, 1998 (see Appendix F).

The Canal Society of New Jersey submitted a letter in support of repairing and reusing the bridge on its original site, stating that the loss of the historic bridge would be a serious

negative impact to the Morris Canal Historic District and the planned Morris Canal Greenway (see Appendix F). Efforts will be made to meet with the Canal Society to alleviate their concerns regarding the Replacement Alternative.

7.4 SUMMARY EVALUATION

The evaluation of alternatives is summarized in Table 7-1 included on the next two pages. This table indicates the results / impacts of each alternative studied in terms of a variety of factors including Traffic Operations and Safety Improvements, Engineering / Construction Impacts, Costs, Socioeconomic Resources, Physical Resources, Water Resources, Biological Resources, Cultural Resources, Hazardous Substances and Ability to Achieve Project Needs. A further description of the evaluation of each alternative follows.

7.4.1 No-Build Alternative

The No-Build Alternative does not provide for any additional improvements, only normal maintenance. The No-Build footprint would be consistent with the existing bridge and roadways. With the No-Build Alternative, the long-term lack of traffic improvements and the continued deterioration of the existing bridge would result in no benefit to any planned business development or benefit to traffic operations and safety conditions.

The No-Build Alternative would not involve any taking of additional right-of-way or construction outside of the existing footprint of the bridge. Water, biological and cultural resources and potential hazardous substance sites would not be directly impacted.

The following are the advantages of this alternative:

1. No right-of-way impact.
2. No utility impact.
3. No initial outlay of cost, although overall increase of maintenance cost would be spread out during the next 20 years.
4. The No-Build Alternative would not directly affect any environmental resources.
5. No short-term traffic impact or associated air quality / noise impact during construction.

The following are the disadvantages of this alternative:

1. It would not address the documented deterioration of the bridge.
2. It would not improve traffic operations and safety.
3. Minimal, if any, construction employment benefit.
4. Potential long-term adverse impacts to the Landing Road Bridge and the Morris Canal Historic District (of which the bridge is a contributing feature) due to continued deterioration of the bridge.
5. Does not meet the overall needs of the community.

TABLE 7-1: ALTERNATIVES EVALUATION MATRIX

DESCRIPTION	No-Build Alternative	Rehabilitation Alternative (Maintain existing bridge geometry)	Bypass Alternative (Alt. No. 1)(New bridge on a new alignment)	Replacement Alternative (Alt. No. 7C) (New bridge on existing alignment)
Traffic Operations And Safety Improvements				
Improve Access to RR Station Parking Area	NO	NO	YES	YES
Increased Width of Landing Rd. and Bridge	NO	NO	YES	YES
Improve Vertical Sight Dist. Over Bridge	NO	NO	YES	YES
Reconst. Int. of Landing Rd & Lakeside Blvd	NO	NO	YES	YES
Other Adverse Impacts				
Utility Relocation	NO	MINOR	YES	YES
Right of Way	NO	NO	YES	YES
Traffic During Const.	NO	YES	YES	YES
Costs				
Utility Cost (Millions in 2011 Dollars)	\$0.00	\$0.12	\$1.90	\$1.00
ROW cost (Millions in 2011 Dollars)	\$0.00	\$0.00	\$3.80	\$1.60
Initial Const. Cost (Millions in 2011 Dollars)	\$0.00	\$1.80	\$9.70	\$6.10
Total Cost (Millions in 2011 Dollars)	\$0.00	\$1.92	\$15.40	\$8.70
Socioeconomic Resources				
Residential Properties	NONE	NONE	HIGH IMPACT	MODERATE IMPACT
Businesses	NONE	SLIGHT IMPACT	HIGH IMPACT	MODERATE IMPACT
Local Economy	NONE	MODERATE IMPACT	MODERATE IMPACT	MODERATE IMPACT
Compliance with Regional, County & Local Plans	NO	NO	NO	YES
Construction Employment	NO BENEFIT	SLIGHT BENEFIT	HIGH BENEFIT	MODERATE BENEFIT
Physical Resources				
Geologic / Topographic	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Soils	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Air Quality	NONE	SLIGHT IMPACT	HIGH IMPACT	MODERATE IMPACT
Noise Levels	NONE	SLIGHT IMPACT	MODERATE IMPACT	SLIGHT IMPACT

TABLE 7-1: ALTERNATIVES EVALUATION MATRIX (CONT'D)

DESCRIPTION	No-Build Alternative	Rehabilitation Alternative (Maintain existing bridge geometry)	Bypass Alternative (Alt. No. 1)(New bridge on a new alignment)	Replacement Alternative (Alt. No. 7C) (New bridge on existing alignment)
Water Resources				
Surface Waters	NONE	SLIGHT IMPACT	MODERATE IMPACT	SLIGHT IMPACT
Ground Water	NONE	NONE	MODERATE IMPACT	NONE
Biological Resources				
Fish / Wildlife	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Vegetation	NONE	NONE	MODERATE IMPACT	SLIGHT IMPACT
Wetlands	NONE	NONE	MODERATE IMPACT	NONE
Cultural Resources				
Morris Canal Historic District	POTENTIAL FUTURE IMPACT	POTENTIAL FUTURE IMPACT	POTENTIAL ARCHAEOLOGICAL IMPACT	POTENTIAL ARCHEOLOGICAL IMPACT
Hazardous Substances				
Kingsland Road	NONE	NONE	NONE	NONE
Lakeside Boulevard	NONE	SLIGHT IMPACT (POTENTIAL)	NONE	MODERATE IMPACT (POTENTIAL)
Landing Road	NONE	SLIGHT IMPACT (POTENTIAL)	MODERATE IMPACT (POTENTIAL)	MODERATE IMPACT (POTENTIAL)
Mt. Arlington Boulevard	NONE	NONE	MODERATE IMPACT (POTENTIAL)	NONE
Conformance with Project Needs	DOES NOT MEET PURPOSE AND NEED	DOES NOT MEET PROJECT PURPOSE AND NEED	PARTIALLY MEETS PROJECT PURPOSE AND NEED	MEETS PROJECT PURPOSE AND NEED

The No-Build Alternative overall does not meet the needs of the community or the purpose and need for the project. Even with stabilization measures, the bridge would continue to deteriorate, resulting in a continued decline in transportation services and of the integrity of the bridge as a resource. Although capital costs are the lowest, maintenance costs would continue to increase and would result in less improvement for the investments made.

7.4.2 Rehabilitation Alternative

The Rehabilitation Alternative would have a similar footprint to the No-Build Alternative but would require some construction activities to rehabilitate the bridge to the *Secretary of the Interior's Standards*, which would not alter the existing historic features or further modernize the bridge. Existing bridge maintenance requirements will continue to increase as rehabilitation within the parameters of historic integrity will result in repairs of failing and severely deteriorating structural elements with materials to match the old in design, color, and texture. Replacement of missing features will be substantiated with documentary and physical evidence.

Similar to the No-Build, the Rehabilitation Alternative footprint would be consistent with the existing bridge and roadways. Also, the long-term lack of traffic improvements and the continued long-term deterioration of the existing bridge would result in no benefit to any planned Short-term impacts associated with traffic disruptions, noise, and dust during construction would be likely, while some minor adverse impacts related to socioeconomic, physical, water, and cultural resources may also occur. If the rehabilitation involves any excavation, some properties with potential hazardous substance concerns represent potential impacts.

The following are the advantages of this alternative:

1. The historic bridge would be retained and rehabilitated, although long-term deterioration would continue.
2. No right-of-way impact.
3. Minimal utility impact.
4. Relatively low initial outlay of cost, although need for maintenance would continue in the future.
5. Relatively limited direct impact to environmental resources.
6. Minor construction employment benefit.

The following are the disadvantages of this alternative:

1. The long-term historic nature of the bridge could be compromised due to continued long-term deterioration.
2. It would not improve traffic operations and safety.
3. Impact on traffic during construction.
4. Impact on noise and air quality during construction.
5. Potential impact to businesses during construction.

6. Potential long-term adverse impact to the Landing Road Bridge and Morris Canal Historic District (of which the bridge is a contributing feature) due to continued deterioration of the bridge in the long term.
7. Does not meet the overall needs of the community.

Similar to the No-Build Alternative, the Rehabilitation Alternative overall does not meet the needs of the community or the purpose and need for the project. Due to the deterioration and substandard condition of the bridge, the Rehabilitation Alternative would have the same long-term adverse impacts on not meeting the transportation and infrastructure needs of the area.. The capital costs of the Rehabilitation Alternative are more than the No-Build Alternative, but substantially less than the Bypass or Replacement alternatives. Maintenance costs would also continue to increase, resulting in less improvement for the investments made in the long term.

7.4.3 Bypass Alternative

The Bypass Alternative would involve extensive acquisition of right-of-way for construction of a new roadway and bridge that would bypass a quarter-mile segment of Landing Road. The removal of the needed property from the tax base and the bypassing of the portion of Landing would have adverse impacts on the businesses along that portion of Landing Road. This would be adverse to the long-term planned business development in Landing. These adverse socioeconomic impacts, as well as other environmental impacts associated with the alternative, are substantial.

In addition, the long-term commitment of additional lands to this alternative was viewed as an adverse impact to the physical, water, and biological resources of the area. Construction activities would also cause short-term adverse impacts to these resources. A wetland on an adjacent property could also be impacted by construction runoff. This alternative cuts through some of the last remaining undeveloped portions of the Landing area. Also, this alternative, which includes the demolition of the subject bridge, represents an adverse impact to the Morris Canal Historic District. The various socioeconomic and environmental impacts associated with this alternative are considered to be the worst in comparison to all of the other alternatives.

The following are the advantages of this alternative:

1. This alternative would improve traffic operations and safety.
2. It would provide improved access to and from the NJ TRANSIT station parking area.
3. No design exceptions are anticipated.
4. Major construction employment benefit.

The following are the disadvantages of this alternative:

1. Extensive utility impact and relocation.
2. Requires extensive right-of-way acquisition.
3. Requires demolition of at least four residential buildings and four commercial buildings.

4. Adverse impact to the Morris Canal Historic District as the Landing Road Bridge (a contributing resource within the said district) would be demolished as part of this alternative.
5. Potential adverse impact on archaeological resources contained within the Morris Canal Historic District.
6. It would have greater overall long-term and short-term environmental impacts than the Replacement Alternative or either of the other alternatives.
7. Highest construction cost of all alternatives.
8. Bypassing of existing businesses could have an adverse economic effect.
9. Does not meet the overall needs of the community.

Although traffic and safety conditions would be improved, the Bypass Alternative does not meet the overall needs of the community given the extent of the impacts associated with it. The Bypass Alternative also has the highest construction cost of any of the alternatives, including the costs associated with right-of-way acquisition and utility relocations.

7.4.4 Replacement Alternative

The Replacement Alternative incorporates safety improvements and advanced engineering factors that would require the acquisition of some additional right-of-way. This alternative is compatible with the planned business development with roadway enhancements that would decrease congestion and improve access. Under the Replacement Alternative, the socioeconomic resources would have some loss of right-of-way and several buildings, but the Landing business district would be maintained.

Long-term and short-term impacts on physical, water, and biological resources would occur, but at a lesser level than with the Bypass Alternative. Similar to the Bypass Alternative, the Replacement Alternative requires the demolition of the Landing Road Bridge, although a new bridge would be constructed at the same location as the existing bridge instead of on new alignment. As a result, the adverse impact of this alternative on the Morris Canal Historic District is somewhat less than that of the Bypass Alternative as said impact can be reduced through the recordation of the existing bridge and the incorporation of historic design elements in the new structure.

The following are the advantages of this alternative:

1. This alternative would improve traffic operations and safety.
2. It would provide improved access to and from the NJ TRANSIT station parking area under the bridge.
3. Both the load capacity and life expectancy of the bridge would be increased.
4. Improved vertical sight distance over the bridge.
5. No detour would be required during construction.
6. Moderate construction employment benefit.
7. Meets the overall needs of the community.

The following are the disadvantages of this alternative:

1. Adverse impact to the Morris Canal Historic District as the Landing Road Bridge (a contributing resource within the said district) would be demolished as part of this alternative.
2. Potential adverse impact on archaeological resources contained within the Morris Canal Historic District.
3. It would require demolition of two commercial buildings and one residential building.
4. It would require utility relocations, but fewer than the Bypass Alternative.
5. It would require right-of-way acquisition, but fewer properties than the Bypass Alternative.
6. It would have greater overall environmental impacts than the Rehabilitation Alternative, but less than the Bypass Alternative.
7. It would have a higher construction cost than the Rehabilitation Alternative, but substantially less than the Bypass Alternative.

7.4.5 Summary

Based on the above comparative analysis and the summary in Table 7-1, it has been determined that the Replacement Alternative best meets the needs of the community and provides the best combination of overall benefits and impacts. It adequately addresses the long-term transportation needs without encroaching on the community's plans and provides a long-term solution with the least disruption. The capital costs of the Replacement Alternative would also be much lower than those for the Bypass Alternative, which involves greater right-of-way acquisition and roadway construction, and the maintenance costs of the Replacement Alternative would be less than for either the No-Build or the Rehabilitation Alternatives. ***For these reasons, the Replacement Alternative (Alternative 7-C) was selected as the Initially Preferred Alternative.*** Further discussion of this recommendation is provided in the next section.

SECTION 8 - RECOMMENDATION

The preceding section presents the comparative analysis used by the County of Morris in selecting an Initially Preferred Alternative (IPA) for the Landing Road Bridge Project. The reader is referred to the various sections of this report for detailed discussions of each category and overall impact of each alternative.

The results of this alternatives analysis were presented to the Roxbury Township Council at a Workshop Meeting on April 14, 2003, and the Council issued a resolution supporting the Replacement Alternative (Alternative No. 7-C) for the project. A copy of the resolution of support issued by the Roxbury Township Council is included in Appendix D of this report.

The project was again presented on June 9, 2015 to the Roxbury Township Council updating them on the status of the project with the various alternates studied. On August 12, 2015 a Public Information was held at the Roxbury Township Municipal Building with Morris County and the RBA Group and alternate 7-C was again the recommended alternate presented and received support from the public and Township, Resolution 2015-310 was adopted on September 29, 2015 by the Township of Roxbury supporting this alternate and is also included in Appendix D of this report.

The Replacement Alternative (Alignment No. 7-C) was selected as the IPA because it best addresses the project needs. The Replacement Alternative would improve safety and traffic operations in the project area, provide a new widened bridge meeting current design standards, and allow maintenance of traffic on the existing bridge while the new bridge is being constructed. The Replacement Alternative is recommended for the following reasons:

1. The alternative would satisfy the traffic operations needs of the project area.
2. The alternative is compatible with the proposed widening of Landing Road to I-80.
3. The alternative would improve the safety of the project area.
4. The new bridge would meet current structural design standards.
5. The proposed construction operations would not impact existing wetlands or regulated hazardous and solid waste sites in the project area.
6. The construction cost of the Replacement Alternative is significantly lower than that of the Bypass Alternative.
7. Traffic could be maintained during construction of the Replacement Alternative, although delays and increased travel time and distance are likely.
8. The alternative is compatible with the streetscape improvements project being undertaken by the Township of Roxbury.

The Replacement Alternative requires the replacement of the existing Landing Road Bridge, which is a contributing resource within the Morris Canal Historic District. The project team will coordinate with the New Jersey Historic Preservation Office to mitigate the adverse impact created by the demolition of the bridge by ensuring that the design of the new structure reflects the appearance of the existing bridge and that other roadway and landscape design elements are consistent with the character of bridge and the district. In addition, the project team will develop an archaeological monitoring plan as the initial phase of work necessary to address the above noted potential for archaeological resources associated with the district, again through continuing consultation with the New Jersey Historic Preservation Office and the New Jersey Historic Sites Council

Morris County will also consult with the Roxbury Township Historic Advisory Committee and the Landing Gateway Committee during design of the project to comply with their recommendations of a two span arch structure, the proposed railing design, concrete texture of the surfaces to mimic the historic look of the original bridge, lighting, plantings, pedestrian crossing, bollards, sidewalks, parking and the fence details at the lake.

SECTION 9 - REFERENCES

ARCH², 2001/2004

Historic Architectural Sites Survey for Landing Road Bridge (Str. # 1400073) Improvements, Roxbury Township, Morris County, New Jersey. Prepared by ARCH², Inc., Metuchen, NJ, September 2001 and revised October 2004.

EDR, 2002a

The EDR Radius Map with GeoCheck for Landing Road Bridge, Landing Road, Landing, NJ 07850, Inquiry Number 783040.2s. Prepared by Environmental Data Resources, Inc., Southport, CT, May 17, 2002.

EDR, 2002b

The EDR NEPA Check for Landing Road Bridge, Landing Road, Landing, NJ 07850, Inquiry Number 783040.3s. Prepared by Environmental Data Resources, Inc., Southport, CT, May 17, 2002.

E&PPA, 1985

Landing Road & Bridge Alternative Alignment Study, Roxbury Township, Morris County, New Jersey. Submitted by Elam & Popoff Professional Association, Glen Rock, NJ, May 1985

FEMA, 1982

Flood Insurance Rate Map, Community-Panel Number 340362 0003 B, Township of Roxbury, Morris County, New Jersey. Federal Emergency Management Agency, December 15, 1982.

GEOD, 1999

Aerial Photography Contact Prints, 9x9, black and white, taken December 8, 1999. 12-08-99 1008 153.001 1:3000 GEOD 1641, Photos 1-1 through 1-6 and 2-7 through 2-12. GEOD Corporation, Newfoundland NJ, December 1999.

Kraft, 1979

Archaeological, historical, and Architectural Cultural Resources Survey of the Landing Road and Bridge Project, Roxbury Township, Morris County NJ. Prepared by Herbert C. Kraft, Archaeological Research Center, South Orange, NJ, August 1979.

MCDPD, 2000

A Natural Resource Management Guide for the County Of Morris, 2000. County of Morris, Department of Planning and Development, Morristown NJ, 2000.

MCDPW, 1999

Local Scoping Program Landing Road Bridge (No. 1400-073) over NJ Transit Railroad and the Morris Canal, RFP. County of Morris, Department of Public Works, Morristown, NJ, June 28, 1999.

NJHPO, 1973

National Register of Historic Places Inventory-Nomination Form for the Morris Canal, filed by New Jersey Historic Preservation Office, Trenton NJ, November 26, 1973.

Parsons, 2002

Hazardous Substance Contamination Screening Report for the Landing Road Bridge Improvements Morris County, New Jersey. Prepared by Parsons, Newark NJ, July 2002.

Parsons, 1998

Preliminary Cultural Resources Investigations for Alternative Analysis for Landing Road Bridge No. 73, Roxbury Township, Morris County, New Jersey. The Parsons Transportation Group, Steinman, Boynton, Gronquist & Birdsall, Newark NJ, July 1998. This report was resubmitted in April 2002.

Steinman, 1998

Improvements to Landing Road Bridge over the Morris Canal & New Jersey Transit, Morris County Department Of Public Works, Township Council Meeting, Tuesday August 11, 1998. Steinman, Boynton, Gronquist & Birdsall, Newark NJ, August 1998.

Steinman, 1999

Bridge Replacement Study, Landing Road Bridge No. 1400-073 over New Jersey Transit Railroad & the Morris Canal, Township of Roxbury, Morris County. The Parsons Transportation Group, Steinman, Boynton, Gronquist & Birdsall, Newark NJ, June 1999.

Steinman, 2000

Drawings of Alternatives 1-6, Reconstruction, Landing Road Bridge No. 1400-073 over New Jersey Transit Railroad & the Morris Canal, Township of Roxbury, Morris County. The Parsons Transportation Group, Steinman, Boynton, Gronquist & Birdsall, Newark NJ, June 2000.

APPENDIX – A

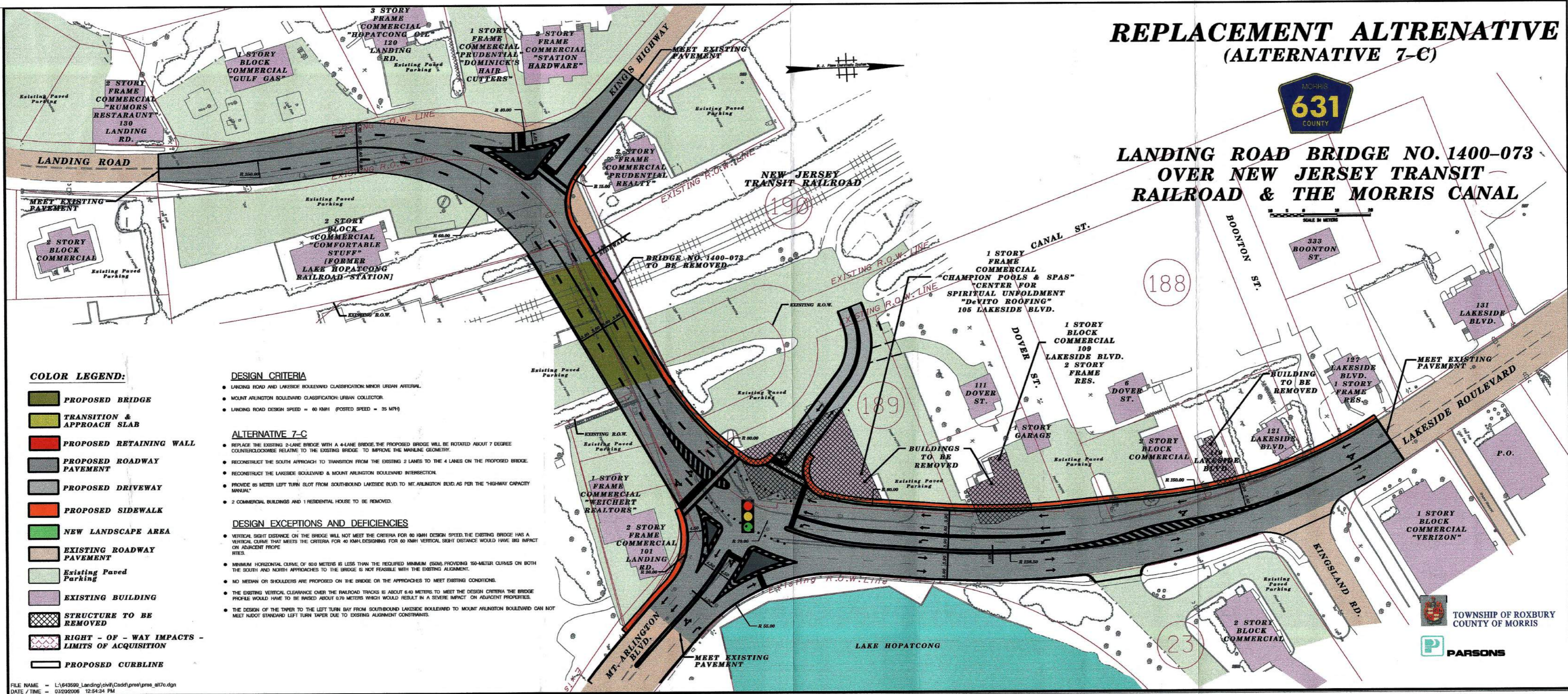
REPLACEMENT ALTERNATIVE FIGURE

ALTERNATIVE 7-C

REPLACEMENT ALTERNATIVE (ALTERNATIVE 7-C)



LANDING ROAD BRIDGE NO. 1400-073 OVER NEW JERSEY TRANSIT RAILROAD & THE MORRIS CANAL



COLOR LEGEND:

- PROPOSED BRIDGE
- TRANSITION & APPROACH SLAB
- PROPOSED RETAINING WALL
- PROPOSED ROADWAY PAVEMENT
- PROPOSED DRIVEWAY
- PROPOSED SIDEWALK
- NEW LANDSCAPE AREA
- EXISTING ROADWAY PAVEMENT
- Existing Paved Parking
- EXISTING BUILDING
- STRUCTURE TO BE REMOVED
- RIGHT - OF - WAY IMPACTS - LIMITS OF ACQUISITION
- PROPOSED CURBLINE

DESIGN CRITERIA

- LANDING ROAD AND LAKESIDE BOULEVARD CLASSIFICATION: MINOR URBAN ARTERIAL.
- MOUNT ARLINGTON BOULEVARD CLASSIFICATION: URBAN COLLECTOR.
- LANDING ROAD DESIGN SPEED = 60 KMH (POSTED SPEED = 35 MPH)

ALTERNATIVE 7-C

- REPLACE THE EXISTING 2-LANE BRIDGE WITH A 4-LANE BRIDGE. THE PROPOSED BRIDGE WILL BE ROTATED ABOUT 7 DEGREE COUNTERCLOCKWISE RELATIVE TO THE EXISTING BRIDGE TO IMPROVE THE MAINLINE GEOMETRY.
- RECONSTRUCT THE SOUTH APPROACH TO TRANSITION FROM THE EXISTING 2 LANES TO THE 4 LANES ON THE PROPOSED BRIDGE.
- RECONSTRUCT THE LAKESIDE BOULEVARD & MOUNT ARLINGTON BOULEVARD INTERSECTION.
- PROVIDE 65 METER LEFT TURN SLOT FROM SOUTHBOUND LAKESIDE BLVD. TO MT. ARLINGTON BLVD. AS PER THE "HIGHWAY CAPACITY MANUAL".
- 2 COMMERCIAL BUILDINGS AND 1 RESIDENTIAL HOUSE TO BE REMOVED.

DESIGN EXCEPTIONS AND DEFICIENCIES

- VERTICAL SIGHT DISTANCE ON THE BRIDGE WILL NOT MEET THE CRITERIA FOR 60 KMH DESIGN SPEED. THE EXISTING BRIDGE HAS A VERTICAL CURVE THAT MEETS THE CRITERIA FOR 40 KMH. DESIGNING FOR 60 KMH VERTICAL SIGHT DISTANCE WOULD HAVE BIG IMPACT ON ADJACENT PROPE RTIES.
- MINIMUM HORIZONTAL CURVE OF 80.0 METERS IS LESS THAN THE REQUIRED MINIMUM (150M). PROVIDING 150-METER CURVES ON BOTH THE SOUTH AND NORTH APPROACHES TO THE BRIDGE IS NOT FEASIBLE WITH THE EXISTING ALIGNMENT.
- NO MEDIAN OR SHOULDERS ARE PROPOSED ON THE BRIDGE OR THE APPROACHES TO MEET EXISTING CONDITIONS.
- THE EXISTING VERTICAL CLEARANCE OVER THE RAILROAD TRACKS IS ABOUT 6.40 METERS. TO MEET THE DESIGN CRITERIA THE BRIDGE PROFILE WOULD HAVE TO BE RAISED ABOUT 0.70 METERS WHICH WOULD RESULT IN A SEVERE IMPACT ON ADJACENT PROPERTIES.
- THE DESIGN OF THE TAPER TO THE LEFT TURN BAY FROM SOUTHBOUND LAKESIDE BOULEVARD TO MOUNT ARLINGTON BOULEVARD CAN NOT MEET NDOT STANDARD LEFT TURN TAPER DUE TO EXISTING ALIGNMENT CONSTRAINTS.



APPENDIX – B

BRIDGE RE-EVALUATION REPORTS

CYCLE NO. 15 (OCTOBER 2011)

&

CYCLE NO. 18 (UGRVGO DGT'4235)



Morris County
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
COURT STREET
PO Box 900
MORRISTOWN, NEW JERSEY 07963-0900

BRIDGE RE-EVALUATION SURVEY REPORT

STRUCTURE NO. 1400-073
COUNTY ROUTE 631 (LANDING ROAD)
OVER NJ TRANSIT-MORRISTOWN LINE
TOWNSHIP OF ROXBURY
MORRIS COUNTY

15TH CYCLE

OCTOBER 17, 2011

**NOTE: This Bridge Re-evaluation Report
shall be filed immediately after the
14TH Cycle Inspection Report.**

Prepared By



Stantec

Stantec Consulting Services Inc.
365 West Passaic Street
Rochelle Park NJ 07662

TABLE OF CONTENTS

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2 Conclusions and Recommendations	15-2
3 Structural Inventory & Appraisal and Pontis Sheets	15-3
4 Load Rating Summary Sheet (LRSS)	15-6
5 Drawings, and Photographs	15-8
6 Field Notes	15-20
7 Memorandum	15-34

**N.J.D.O.T. - STRUCTURAL EVALUATION
RE-EVALUATION BRIDGE SURVEY REPORT**

CYCLE NO. 15

STRUCTURAL DATA:

Bridge No.:	1400-073	Year Built:	1907	Widened/Rehab:	1972
Route No.:	9014	Length:	136.0'	Width:	38.8'
Mile Point:	1.31	Date of this Evaluation:	10/17/2011 & 01/04/2012		
Name:	County Route 631 (Landing Road) Over NJ Transit-Morristown Line	By:	Stantec Consulting Services, Inc.		
		Date of Previous Evaluation:	07/21/2009		
		By:	S & R Engineers, PC		
		Special Equipment Used:	None		
Structure Type:	Two Span, Closed Spandrel, Reinforced Concrete Arch	Underwater Inspection:	Not Required		
		Scour Critical:	N/A		

WORK DONE: New deck waterproofing (see attached email and photos from Morris County) with new asphalt overlay on top that extends over both approaches (Photo 15-15). New concrete sidewalk along north side (Photo 15-16). New concrete curbs and guide rails along both sides (Photo 15-15). Concrete repairs throughout both arch intrados (Photos 15-05 and 15-06). Concrete repairs in the north parapet/headwall along the west span (Photo 15-17). Concrete repairs in the spandrel wall and coping at both spandrel walls of both spans (Photos 15-17 and 15-18). Concrete repairs at the south wingwall (Photo 15-19) and in the coping and vertical portion below the arch at north end of the west skewback (Photo 15-20). Concrete repairs in coping and wall at the northwest edge (Photo 15-22) and the south face of the pier stem (Photo 15-21).

OVERALL PHYSICAL CONDITION: Fair due to the superstructure and substructure.

OVERALL CONDITION (ITEM 67): Fair due to the superstructure and substructure.

Inspection Team Leader: Thane Syverson, P.E.

Certifying Engineer: Abdel A. Denho, P.E.

N.J. P.E. Number: GE 39572

Initials: TJS

I certify that this report is an accurate description of the subject structure, to the extent determinable by visual inspection and testing performed.

Signature: _____
Date: _____

Original Signed and
Sealed

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- 10/17/2011 &
Morristown Line Insp. Date: 01/04/2012

CONCLUSIONS AND RECOMMENDATIONS:

The overall condition of the structure is fair due to the superstructure and substructure.

The superstructure is in fair condition due to the numerous patching covering 60% of both arch intrados and the cracking, scaling, leakage and efflorescence throughout both arch intrados and spandrel walls.

The substructure is in fair condition due to the large concrete patches and the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments.

Since the previous inspection, the superstructure has been upgraded from poor to fair condition due to the concrete repairs throughout both intrados and in the spandrel wall and coping at both spandrel walls. The substructure has been upgraded from serious to fair condition due to the concrete repairs in the coping and wall at the northwest edge and south face of the pier. The approach roadway has been upgraded from satisfactory to good condition due to the new bituminous concrete overlay in the top of roadway at both approaches.

The structure is classified as functionally obsolete due to the inadequate deck geometry (Item 68 = 2). Therefore, we recommend the following remedial action:

1. Increase the roadway width from 28.8' to 32.0' by widening the bridge 3.2'. <ul style="list-style-type: none"> a. Demolition of existing south bridge railing: (Lump Sum) \$25,000 b. Bridge Widening: \$130,560 3.2' x 136' = 435.2 SF @ \$300/SF c. Associated approach work: (Lump Sum) <u>\$50,000</u>
Subtotal \$205,560
Preliminary Engineering (15%) \$30,834
Traffic Maintenance (10%) <u>\$20,556</u>
Total \$256,950
SAY \$260,000

In the interim until the bridge is widened, we recommend that the following Emergency/Priority repairs be made to retard further deterioration, preserve the structural integrity of the bridge, improve safety and extend its useful life:

None.

StructNum: 1400073

NJDOT SI and A Sheet

Name: CO RT631(LANDING RD) / NJT-MORRISTOWN LINE S.R.: 58.5 SD/FO- 2 -Functionally Obsolete

IDENTIFICATION

1 State: 34 New Jersey 8 Struc Num: 1400073
 7 Facility Carried: CR631 (Landing Rd) 9 Location: At Kings Highway Junction
 5A Rte.(On/Under): Route On Structure 5B Rte. Signing Prefix: 4 -County Hwy
 5C Level of Service: 1 -Mainline 5D Rte. Number: 00631
 5E Directional Suffix: 0- Not Applicable % Responsibility : NA
 2 SHD District: 01- North 3 County Code: Morris
 4 Place Code: Roxbury (Township of), M 11 Mile Post: 1.310 mi
 6 Feature Intersected : NJT-Morristown Line
 16 Latitude: 40d 54' 17.00" 17 Longitude: 074d 39' 57.42"
 98 Border Bridge Code: -2 Not Applicable (P)
 99 Border Bridge Number: Unknown

INSPECTION

91 Frequency: 24 months 90 Inspection Date: 10/17/2011 Next Inspection: 10/17/2013
 92A FC Frequency: NA 93A FC Inspection Date: NA Next FC Inspection: NA
 92B UW Frequency: NA 93B UW Inspection Date: NA Next UW Inspection: NA
 92C SI Frequency: NA 93C SI Date: NA Next SI: NA
 Element Frequency: 24 months Element Inspection Date: 10/17/2011 Next Elem. Insp. Due: 10/17/2013

CLASSIFICATION

100 STRAHNET Highway: 0 -Not a STRAHNET hwy 101 Parallel Structure: N -No || bridge exists
 102 Direction of Traffic: 2 -2-way traffic 103 Temporary Structure: -1
 104 Highway System: 0 -Not on NHS 112 NBIS Length: Y - Long Enough
 20 Toll Facility: 3 -On free road 26 Functional Class: 16- Urban Minor Arterial
 37 Historical Significance: 2 -Eligible for NRHP
 22 Owner: 02 County Hwy Agency
 21 Custodian: 02 County Hwy Agency

STRUCTURE TYPE AND MATERIALS

46 Number of Approach Spans : 0 45 Number of Spans Main Unit: 2
 43A/B Main Span Material/Design:
 1 -Concrete 11 -Arch-Deck
 107 Deck Type: 1- Conc.-Cast-in-Place
 108A Wearing Surface: 6 -Bituminous
 108B Membrane: 3- Epoxy
 108C Deck Protection: 8- Unknown

CONDITION

58 Deck: 7 -Good 59 Super: 5 -Fair 60 Sub: 5 -Fair
 62 Culvert: N -Not applicable 61 Channel/Channel Protection: N -Not applicable

LOAD RATING AND POSTING

65 Inventory Rating Method: 1- LF Load Factor 63 Operating Rating Method: 1 -LF Load Factor
 66 Inventory Rating: HS65.0 64 Operating Rating: HS99.9
 31 Design Load: 0 -Other or Unknown 70 Posting: 5 At/Above Legal Loads
 41 Posting status: A -Open, no restriction

AGE AND SERVICE

27 Year Built: 1907 106 Year Reconstructed: 1972
 42A Type of Service On: 5 -Highway-pedestrian
 42B Type of Service Under: 2 -Railroad
 28A Lanes on: 2 28B Lanes Under: 0 19 Detour Length: 2.0 mi
 29 ADT: 22,606 109 Truck ADT: 4 % 30 Year of ADT: 2011

APPRAISAL

36A Bridge Rail: 0 -Substandard 36C Approach Rail: 0 -Substandard
 36B Transition: 0 -Substandard 36D Approach Rail Ends: 0 -Substandard
 67 Str. Evaluation: 5 68 Deck Geometry: 2 Intolerable - Replace
 69 Underclearance, Vertical and Horizontal: 4 -Tolerable
 71 Waterway Adequacy: N -Not applicable 72 Approach Alignment: 5 -Above Tolerable
 113 Scour Critical: N -Not Over Waterway

GEOMETRIC DATA

48 Length Max Span: 64.0 ft 49 Structure Length: 136.0 ft
 50A Curb/Sdwik Width L: 0.0 ft 50B Curb/Sidewalk Width R: 5.7 ft
 Width Curb to Curb 51: 28.8 ft 52 Width Out to Out: 38.8 ft
 32 Approach Roadway Width: 28 ft 33 Median: 0 No median (w/ shoulders)
 Deck Area: 5,277. sq. ft
 34 Skew: 0.00 ° 35 Structure Flared: 0 -No flare
 53 Minimum Vertical Clearance Over Bridge: 9999
 54A Minimum Vertical Underclearance Reference: R -Railroad beneath str.
 54B Minimum Vertical Underclearance: 20.91 ft
 55A Minimum Lateral Underclearance Reference R: R -Railroad beneath str.
 55B Minimum Lateral Underclearance R: 20.50 ft
 56 Minimum Lateral Underclearance L: 0.00 ft

PROPOSED IMPROVEMENTS

94 Bridge Cost: \$130,560 75 Type of Work: 33
 95 Roadway Cost: \$50,000 76 Length of Improvement: 136 ft
 96 Total Cost: \$260,000 114 Future ADT: 30,450
 97 Year of Cost Estimate: 2011 115 Year of Future ADT: 2031

NAVIGATION DATA

38 Navigation Control: N N -N/A-No waterway
 39 Vertical Clearance: 0.0 ft 40 Horizontal Clearance: 0.0 ft
 111 Pier Protection: -1 116 Lift Bridge Vertical Clearance:

ELEMENT CONDITION STATE DATA

Str Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
0	14/3	P Conc Deck/AC Ovly	(SF)	5,277	100 %	5,277	0 %	0	0 %	0	0 %	0	0 %	0
0	144/3	R/Conc Arch	(LF)	136	0 %	0	0 %	0	100 %	136	0 %	0	0 %	0
0	210/3	R/Conc Pier Wall	(LF)	39	0 %	0	100 %	39	0 %	0	0 %	0	0 %	0
0	215/3	R/Conc Abutment	(LF)	78	0 %	0	100 %	78	0 %	0	0 %	0	0 %	0
0	334/3	Metal Rail Coated	(LF)	272	100 %	272	0 %	0	0 %	0	0 %	0	0 %	0
0	503/3	Curbs/Walks - Conc	(LF)	136	100 %	136	0 %	0	0 %	0	0 %	0	0 %	0

StructNum: 1400073

NJDOT SI and A Sheet

Str Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
0	506/3	Wingwalls - Abut	(LF)	32	50 %	16	50 %	16	0 %	0	0 %	0	0 %	0
0	507/3	Headwalls - Other	(LF)	272	50 %	136	50 %	136	0 %	0	0 %	0	0 %	0

NJDOT BRIDGE FIELDS

(V1.1)

Brkey: 1400073

Inspkey

GENERAL

(A) Town: Roxbury Twp.
 (AA) Route: 9014
 (AD) Admin: -1
 (AE) Alt Agency: -

COUNTER MEASURES

(FI) Recommended:
 (FJ) Cost: 0
 (FK) Installed: 1 _ 2 _ 3 _
 (FL) Monitoring Req: -

TEMPORARY STRUCTURES

(GV) Bridge: N
 (GW) Shoring: N
 (GX) Reports: N
 (GY) Measures: N
 (GZ) Cond Desc:

STRUCTURE AND APPURTENANCES

(AC) Non-Inv Feature: RR Rdwy and/or RR ovr RR
 (AF) Alt Struct #:
 (AG) Rail Type: 30
 (AH) Rail Height: 2.25
 (AJ) Slope Protection: -1
 (AK) Abutment: 03 Full Height
 (AL) Pier: 02 Solid Wall Pier
 (AM) Depth of Fill: 0.00
 (AP) Fender System: -1
 (AT) Special Material 1: -
 (AT) Special Material 2: -
 (AU) Add Struct: 1 _
 2 -
 (AV) Widen Stru: Material Design
 1st Widen _ -1
 2nd Widen _ -1

PROJECT PROGRAMMING

(HB) Bridge List ID: 14C1_
 (HC) Consultant: A47
 (HD) 2-Cy Insp Contr: Y
 (HE) Proceed Date: I 5/8/2003
 II 6/6/2005
 (HF) State Proj Mgr: MAK

LOAD RATINGS

Type	Inv	Opr
H (BQ)	46	(CA) 77
HS (BR)	65	(CB) 99
3 (BS)	58	(CC) 98
3S2 (BT)	37	(CD) 63
3-3 (BU)	59	(CE) 99
M (BV)	--	(CF) --
Misc (CH1)	L	
(CH2)	0	

BRIDGE NOISE BARRIER (HA)

Material 1: -
 2: -
 Height 1: -
 2: -

MISCELLANEOUS

(CQ) Bridge List: -
 (BK) Overstress %: 0
 (FV) Route Milepost: .
 (FM) Incident Reprtd: -
 (AO) Utilities: 1: -
 2: -
 3: -
 4: -
 (CR) Off-Route Bridge: N
 (BB) Orphan Bridge: N
 (AN) Plan Available: N
 (FX) Fed Error Uncorr N
 (FW) Estimated ADT N

FUNDING AND COST

(BL) Discretionary: -
 (BM) Fed Job #: BR NBIS755
 (BN) State Job #: 2205215
 (BO) St Maint Cost: 0
 (BP) Brdg Demo: N

PAINTING

(GB) Environment: -1
 (GR) Last Painting:
 (GA) Painting Req: N

FENCING

(AQ) Chain Link:
 (FN) Warranted: Y

SCOUR EVALUATION

(FA) FHWA Category: -1
 (FB) Stage 1 Date:
 (FC) Stage 1 Consultant: -1
 (FD) Prioritization: -
 (FE) Sufficiency: 0
 (FF) Date Stage II
 (FG) Stage II Consultant: -1
 (FH) Crit. Element:

SIGN STRUCTURES

(GS) Overhead: 0
 (GT) Cantilever: 0
 (GU) Fascia Mounted: 0

POSTINGS

(CG) Load Type: -
 (CG) Tons: 0
 (AI) Speed: 20

RAILROAD

(BC) USRA Code: 6101
 (BD 1) Rail On/Under: 0
 (BD2) JRRs intersected: 2
 (BE) Rail Milepost: 45.53

StructNum: 1400073

NJDOT SI and A Sheet

NJDOT INSPECTION FIELDS

(V1.2)

<p>GENERAL</p> <p>(CI) Cycle Number: 15 (CJ) Insp Type: S (CK) Inspection Crew: R (CM) Consultant: V08 (CO) Prev Consultant: A47 (CP) Federal Report: -</p>	<p>MISCELLANEOUS</p> <p>(B) Deletion Code: N (BA) Apr Rdwy Cond: 7</p>	<p>REMARKS</p> <p>(BF) Deck: 1: _ 2: _ 3: _ 4: _ 5: _ (BG) Super: 1: 9 2: Z 3: _ (BH) Sub: 1: A 2: C 3: B (BI) Channel: - (BJ) Culvert: -</p>	<p>PAINTING</p> <p>(GD) Fascia Beam: -1 (GE) Fascia B. Flange: -1 (GF) Interior Beam: -1 (GH) Interior B. Flange: -1 (GI) Beam Ends: -1 (GJ) Connections: -1 (GK) Bracings: -1 (GL) Bearings: -1 (GM) Substructure: -1 (GN) Above Deck Super: -1 (GO) Railings/Fence: -1 (GP) Remarks 1: (GQ) Remarks 2:</p>
<p>INSPECTION DATES</p> <p>(AW) Mech/Electrical: (AX) Deck: (AY) Special Testing: (GC) Paint: (AR) Equipment: 1: _ 2: _ 3: _ (AS) Testing: 1: _ 2: _ 3: _</p>	<p>FATIGUE DETAIL (AZ)</p> <p>Location 1: -1 Location 2: -1 Location 3: -1</p>	<p>IN-DEPTH PIN-HANGER</p> <p>(FR) Consultant: -1 (FS) (FT) Combo: N (FQ) FCM/Pin-Hngr Insp Date:</p>	

ROADWAY DATA

Bridge Id: 1400073
SRI 14000631__

(V1.1)

<p>ROADWAY IDENTIFICATION</p> <p>NBI Roadway?: 1 Roadway Name: CR 631 (LANDING ROAD) Item 5A: 1 Item 5B: 4 Item 5C: 1 Item 5D: 00631 Item 5E: 0</p>	<p>TRAFFIC AND ACCIDENTS</p> <p>28 Lanes : 2 Num Median: 0 Road Speed: 35 mph ADTclass: 04 29 ADTtotal: 22606 30 Year of ADT: 2011 114 Future ADT: 30450 115 Year of Future ADT: 2031 109 Truck ADTT % : 4 19 Bypass Length: 2.00 mi Detour Speed: 30 mph</p>
<p>HWY NETWORKS AND SERVICE CLASSIFICATION</p> <p>11 Milepost: 1.310 mi 12 Base Hwy Network: 0 20 Toll Facility: 3 13A LRS Inventory Route Subroute No.: 26 Functional Class: 16 102 TraffDirection: 2</p>	<p>CLEARANCES</p> <p>10 Vertical Clearance: 99.99 ft 47 Invent Route Horiz Clear: 28.80 ft (DJ) Min Vert Undrcir: 00.00 ft</p>
<p>ALTERNATE CLASSIFICATION</p> <p>100 Strahnet Hwy: 0 Transit Rt: 0 104 NHS System: 0 Emergency 0 105 Fed. Lands Hwy: 0 110 Truck Hwy Net: 0 School Bus: 0</p>	<p>WIDTHS</p> <p>32 Appr Rdwy Width: 28 ft 51 Brdg Rdwy With Curb-Curb: 28.8 ft</p>

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

LOAD RATING SUMMARY SHEET (LRSS)

(Form NJ-BI-101 Created 1/25/2011)

Project Information:

Group: 14C1 Agreement No.: 2011BI840C Contract ID: 12-50808 Agree/Mod No.: 00

Rating Information:

Method: LRFR: No LFR: Yes ASR: No Other (Specify): N/A
Rating Date: 05/05/95 Computer Software Used: STAAD-III Version: Rev. 20.0W
Load Testing: No Cycle Rating Performed: 6th Design Load: Unknown

Structure Information:

Plans Available? No Contract Designation: Unknown
Overlay? Yes Considered in Rating? Yes Type/Thickness: Bit. Conc./Unknown
Section Losses? No Considered in Rating? N/A Item 59 Cond.: Poor

For LRFR Use Only:

Surface Roughness Factor: -- Condition Factor: -- System Factor: --
ADTT (one direction): -- Resistance Factor: -- FCM: Yes/No

Load Rating Engineer (LRE):

Name: Unknown Firm: Unknown Initial: K.P.L.

Load Rating Reviewer (LRR) certification as per the NBIS Title 23 CFR Section 650.309(c):

Name: T.J. N.J. P.E. No.: Unknown
Firm: Unknown

I certify that this rating is an accurate representation of the subject structure, considering all deterioration and/or changes to loading conditions, to the extent determinable by research and visual inspection and testing performed. I am charged with the overall responsibility for bridge capacity evaluation for the above mentioned structure.

--
Sign Date

Sign and Seal if
Rating Performed
in this Cycle

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

LOAD RATING SUMMARY SHEET (LRSS) (cont.)

Rating Comments:

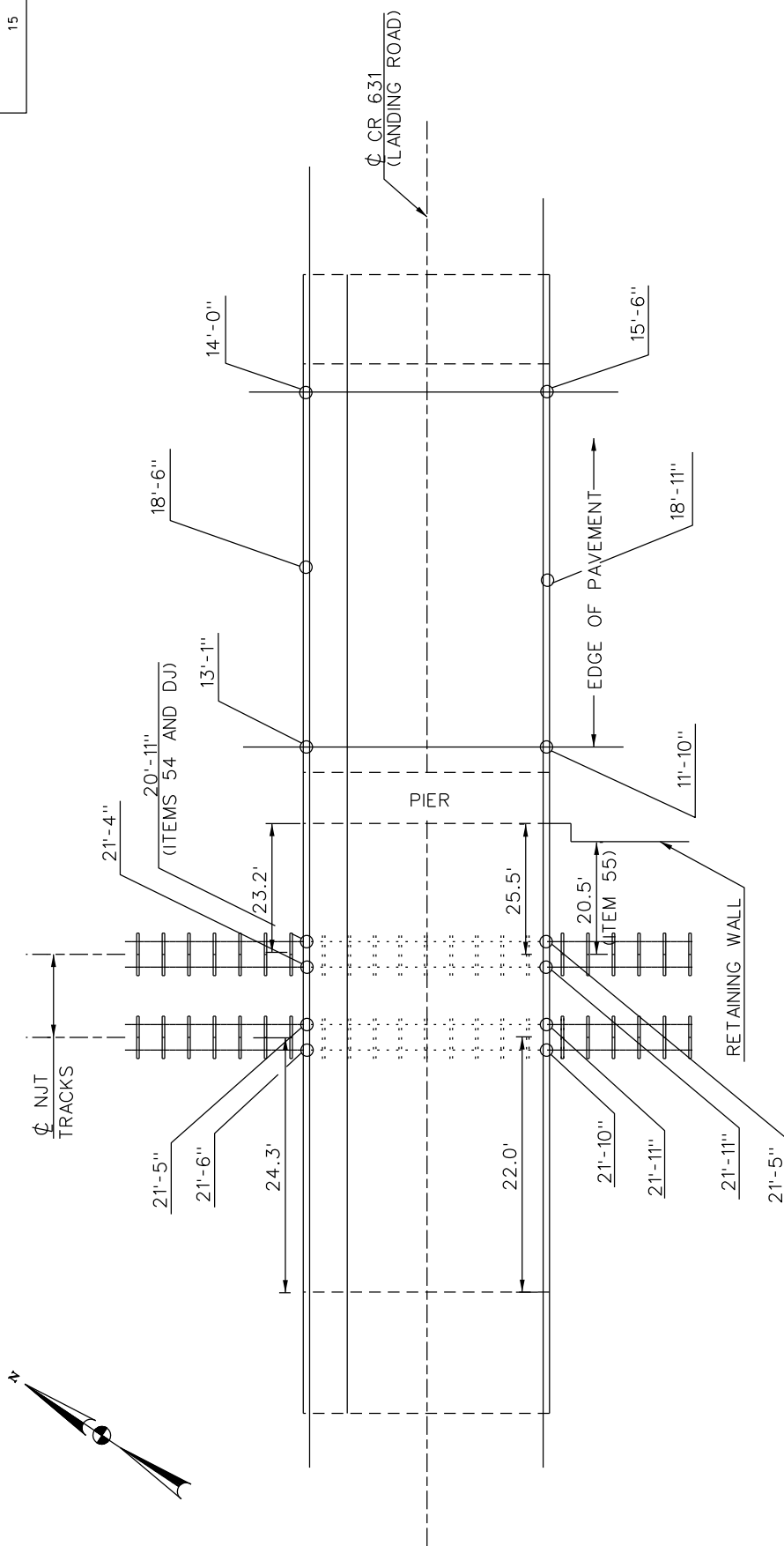
None.

The Load Factor ratings, computed in the 6th Cycle report in accordance with the FHWA directive dated November 1993 and AASHTO Manual for Condition Evaluation of Bridges, 1994, as modified by Division 4 of the New Jersey Department of Transportation Design Manual, Bridges and Structures, are as follows:

<u>Material</u>	<u>Compressive Strength f'c</u>	<u>Allowable Stresses (Psi)</u>		
		<u>Yield</u>	<u>Inventory</u>	<u>Operating</u>
Concrete	2,500	N/A	800	1,200
Reinforcing Steel	N/A	33,000	18,000	25,000

<u>Member</u>	<u>Truck Type (Tons)</u>	<u>Rating (Tons)</u>	
		<u>Inventory</u>	<u>Operating</u>
Arch at Crown Cond. Rating = 5	H15 (15T)	46	77
	HS20 (36T)	65	109
	3 (25T)	58	98
	3S2(40T)	37	63
	3-3 (40T)	59	99
	Type HL-93	---	---

ROUTE 9014	STRUCTURE NO. 1400-073	DATE OF INSP. 10/17/11
		CYCLE NO. 15



HORIZONTAL & VERTICAL
UNDERCLEARANCE PLAN

N.T.S.

NEW JERSEY DEPARTMENT OF TRANSPORTATION
CR 631 (LANDING ROAD) OVER NJ TRANSIT-MORRISTOWN LINE MUNICIPALITY: TOWNSHIP OF ROXBURY COUNTY: MORRIS
CLEARANCE DIAGRAM
STANTEC CONSULTING SERVICES INC. 365 WEST PASSAIC STREET ROCHELLE PARK, NJ 07662

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-01

Location:	North elevation, looking south
Description:	General view. Note the leaning utility pole on the spandrel wall between the two spans.



Photo No: 15-02

Location:	South elevation, looking north
Description:	General view.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-03

Location: Roadway, looking east

Description: General view.



Photo No: 15-04

Location: Roadway, looking west

Description: General view.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-05

Location: Underside of intrados, west span, looking west

Description: General view. Work Done: Concrete repairs throughout intrados. Note the light to moderate scaling with fine to medium cracks, efflorescence and numerous large concrete patches throughout.



Photo No: 15-06

Location: Underside of intrados, east span, looking west

Description: General view. Work Done: Concrete repairs throughout intrados. Note the moderate scaling, efflorescence and numerous large concrete patches throughout.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-07

Location: South pipe railing, 3rd section from west, west span, looking southwest

Description: Moderate rusting with holes in upper and lower rails.



Photo No: 15-08

Location: South pipe railing, 5th section from west, looking northwest

Description: Severe impact damage with fractured middle rail and middle post. Note the spall along the cold joint in the southwest approach parapet outside face near the bottom west end.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-
 Morristown Line Insp. Date: 10/17/2011 &
 01/04/2012



Photo No: 15-09

Location: West skewback, looking northwest
Description: Fine vertical and map cracks with efflorescence and spalls/incipient spalls throughout.



Photo No: 15-10

Location: Northeast wingwall, east end, looking southeast
Description: Full height wide vertical crack with spall in the gunite coating.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-
 Morristown Line Insp. Date: 10/17/2011 &
 01/04/2012



Photo No: 15-11

Location:	Southwest retaining wall, looking
Description:	Spalls with active leakage at bottom.



Photo No: 15-12

Location:	Southeast retaining wall, looking east
Description:	Voids and missing pointing throughout.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-
 Morristown Line Insp. Date: 10/17/2011 &
 01/04/2012



Photo No: 15-13

Location: West face of pier, looking southeast

Description: Efflorescence and seepage stain at middle below coping.



Photo No: 15-14

Location: Northeast approach roadway, looking west

Description: 20 MPH speed advisory posting.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

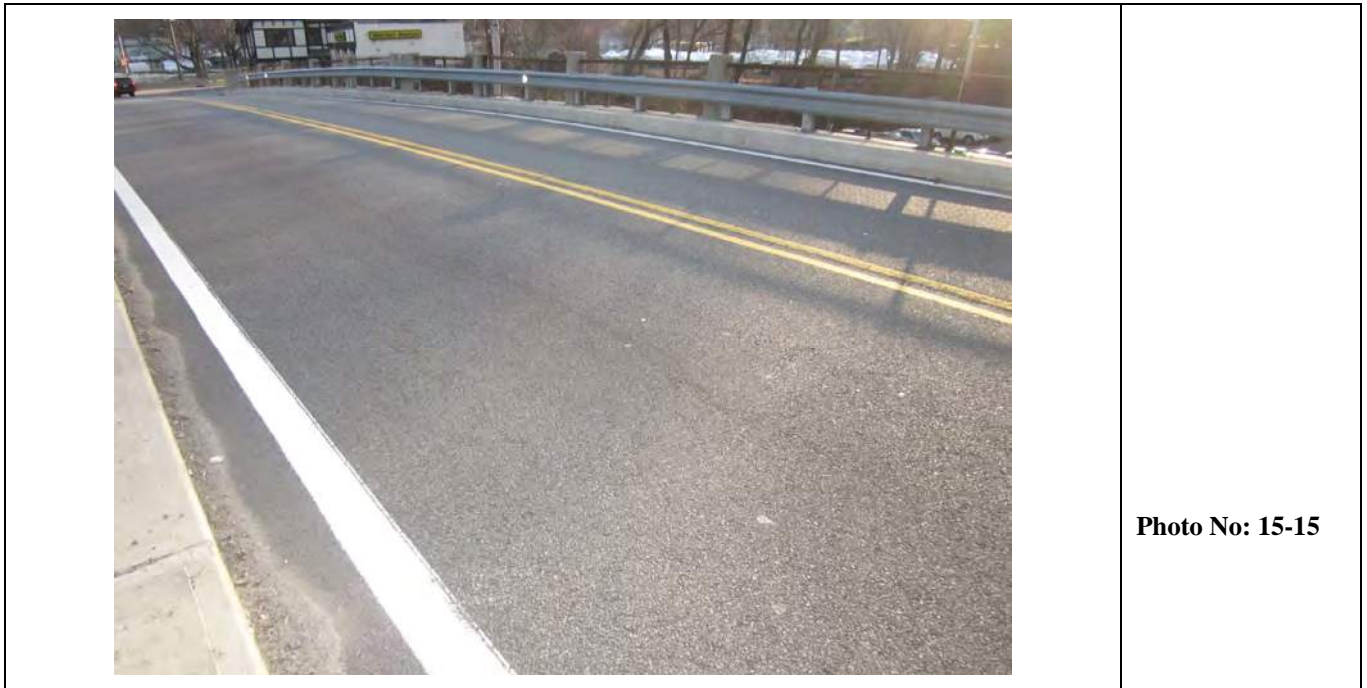


Photo No: 15-15

Location:	Top of roadway, looking southeast
Description:	Work Done: New asphalt overlay and new concrete curb and guide rail along south side.



Photo No: 15-16

Location:	Roadway, looking northeast
Description:	Work Done: New concrete sidewalk and curb and new guide rail installed at north side

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-17

Location:	North spandrel wall, west span, looking southwest
Description:	Work Done: Concrete repairs to spandrel wall and coping. Note the fine map cracking with efflorescence and moisture staining.



Photo No: 15-18

Location:	South spandrel wall, east span, looking north
Description:	Work Done: Concrete repairs to spandrel wall and coping. Note the fine map cracks with efflorescence at east end and the broken section of conduit along the east face of the pier.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-19

Location:	Southwest wingwall and south end of west skewback
Description:	Work Done: Patched with concrete.



Photo No: 15-20

Location:	North end of west skewback and northwest corner of arch intrados
Description:	Work Done: Patched with concrete.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012



Photo No: 15-21

Location:	South face of pier, looking north
Description:	Work Done: Concrete repairs in coping and wall. Note the medium horizontal and vertical cracks with efflorescence.



Photo No: 15-22

Location:	North face of pier, looking southeast
Description:	Work Done: Concrete repairs in coping and wall at northwest edge. Note the rust/water stains on delaminated gunite.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

**NEW JERSEY DEPARTMENT OF TRANSPORTATION
 STRUCTURAL EVALUATION
 FIELD NOTES
 MORRIS COUNTY**

Inspectors: M. Ramirez, P.E. Name: County Route 631 (Landing Road) over NJ Transit-Morristown
A. La Bianca Line
 Crew Chief: Thane Syverson, P.E. Weather: Cloudy / Clear
 Temperature: 55°F / 10°F Special Equipment Used: None

RATINGS:

- N Not applicable.
- 9 Excellent Condition.
- 8 Very Good Condition – no problems noted.
- 7 Good Condition – some minor problems.
- 6 Satisfactory Condition – some minor deterioration of structural elements.
- 5 Fair Condition – minor section loss to primary structural elements.**
- 4 Poor Condition – advanced section loss to primary structural elements.
- 3 Serious Condition – seriously deteriorated primary structural elements.
- 2 Critical Condition – facility should be closed until repairs are made.
- 1 Imminent Failure Condition – facility closed. Study of repairs is feasible.
- 0 Failed Condition – facility is closed and beyond repair.

GPS COORDINATES			
@ Southwest corner			
N	40°	54' 17.00"	Lat.
W	74°	39' 57.42"	Long.

GENERAL

Type of Bridge: Two Span, Closed Spandrel, Reinforced Concrete Arch.

Year Built: 1907 Year of Widening / Major Repairs: 1972

No. of Lanes: On 2 Under None

Vertical Clearances: Over Deck: Unlimited

Minimum Under: 20.91' from the top of east rail of the east track under north fascia.

Maximum Under (Item 10): N/A

Horizontal Underclearance: Total Horizontal Clearance: N/A

Right 20.50' from the centerline of the east track to the pier retaining wall at the south fascia.

Left N/A

Overall Physical Condition of Structure: Fair due to the superstructure and substructure.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

DECK

SI&A Item 58 Condition Rating: 7

SPAN # West

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	No significant defects. Work Done: New deck waterproofing (see attached email and photos from Morris County) with new asphalt overlay on top (Photo 15-15).
N	Underside of Deck	Not visible.
N	Median	N/A
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
8	Sidewalks/ Safetywalk <i>Concrete @ North only</i>	No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
7	Parapets/ Balustrades	No significant defects. Work Done: Concrete repairs in north parapet/headwall (Photo 15-17).
7	Railings <i>W-beam guide rail and pipe railing with concrete pylons</i>	The original deteriorated pipe rails on both sides are now protected by newly installed bridge mounted w-beam guide rail (Photos 15-07 , 15-15 and 15-16). Work Done: New w-beam guide rail installed along both sides (Photos 15-15 and 15-16).
N	Deck Joints / Filler Material	N/A
N	Drains and Scuppers	N/A
N	Light Stands	N/A
N	Utilities	None. Overhead wires along north side.
N	Others	N/A

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

DECK

SI&A Item 58 Condition Rating: 7

SPAN # East

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	No significant defects. Work Done: New deck waterproofing (see attached email and photos from Morris County) with new asphalt overlay on top (Photo 15-15).
N	Underside of Deck	Not visible.
N	Median	N/A
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
8	Sidewalks/ Safetywalk <i>Concrete @ North only</i>	No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
7	Parapets/ Balustrades	No significant defects.
7	Railings <i>W-beam guide rail and pipe railing with concrete pylons</i>	The original deteriorated pipe rails on both sides are now protected by newly installed bridge mounted w-beam guide rail (Photos 15-08 , 15-15 and 15-16). Work Done: New w-beam guide rail installed along both sides (Photos 15-15 and 15-16).
N	Deck Joints / Filler Material	N/A
N	Drains and Scuppers	N/A
N	Light Stands	N/A
N	Utilities	None. Overhead wires along north side.
N	Others	N/A

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over NJ Transit-
 Name: Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

APPROACHES

SI&A Item BA Rating: 7

SI&A Item 72 Rating: 5

APPROACH West

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement <i>Bituminous Concrete</i>	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
7	Approach Shoulder	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from the bridge. Horizontal: Sharp (90°) turn south.
N	Guide Rail Condition	N/A
8	Sidewalks <i>Concrete @ North only</i>	Continuous. No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. Continuous. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
7	Utilities	Overhead wires crossing over roadway.
7	Approach Roadway Embankment	No significant defects.
7	Others/ Parapet	No significant defects.

Additional Remarks:

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

APPROACHES

SI&A Item BA Rating: 7

SI&A Item 72 Rating: 5

APPROACH East

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement <i>Bituminous Concrete</i>	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
7	Approach Shoulder	No significant defects. Work Done: New asphalt overlay (Photo 15-15).
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from bridge. Horizontal: T-intersection 100' to east. Speed reduction to 20 MPH.
7	Guide Rail Condition <i>W-beam @ NE only</i>	No significant defects.
8	Sidewalks <i>Concrete @ North only</i>	Continuous. No apparent defects. Work Done: New concrete sidewalk (Photo 15-16).
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. Continuous. No apparent defects. Work Done: New concrete curbs on both sides (Photos 15-15 and 15-16).
7	Utilities	Overhead wires along north side.
7	Approach Roadway Embankment	No significant defects.
6	Others/ Parapet	Small spall along cold joint in southwest approach parapet outside face near bottom west end (Photo 15-08).

Additional Remarks:

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

SUPERSTRUCTURE
(ARCH)

SI&A Item 59 Condition Rating: 5

SPAN # West

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Light to moderate scaling with fine to medium random cracks with efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 15-05). Work Done: Concrete repairs throughout intrados (Photo 15-05).
N	Spandrel Columns/ Extradados	N/A
5	Spandrel Walls	North: Fine map cracks with efflorescence and moisture staining (Photo 15-17). South: Scattered fine map cracks and random cracks. Work Done: Spandrel wall and coping has been patched with concrete at both openings (Photo 15-17).
N	Others	N/A

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

SUPERSTRUCTURE
(ARCH)

SI&A Item 59 Condition Rating: 5

SPAN # East

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Moderate scaling and efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 15-06). Work Done: Concrete repairs throughout intrados (Photo 15-06).
N	Spandrel Columns/ Extrados	N/A
5	Spandrel Walls	Fine map cracks with efflorescence throughout north wall and east end of south wall (Photo 15-18). North: Wide vertical crack at the east end from the top of the pylon, 15 LF. Work Done: Spandrel wall and coping has been patched with concrete at both openings (Photo 15-18).
5	Others	Utility pole is leaning onto north spandrel wall between the two spans (Photo 15-01).

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

SUBSTRUCTURE
(Arch/Frame)

SI&A Item 60 Condition Rating: 5

ABUTMENT West

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Bottom Portion-Crashwall: Fine vertical and map cracks with efflorescence and spalls/incipient spalls along the full length of the horizontal face (shotcrete) (Photo 15-09). Work Done: Concrete repair at coping and vertical portion below the arch at the north end (Photo 15-20).
5	Wingwalls/ Retaining Walls	South: Fine map cracks and large concrete patch at east end. North: 50 LF of medium cracks with efflorescence. Work Done: Concrete repair at south wingwall, 120 SF (Photo 15-19).
4	Others <i>Retaining Walls</i>	Spalls, 40 SF, with active leakage along bottom at southwest wall (Photo 15-11).

**Additional
Remarks:**

ABUTMENT East

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Area of thick and heavy efflorescence and a few fine vertical cracks at few locations. Hollow (5%) areas of gunite throughout.
5	Wingwalls/ Retaining Walls	North: Full height wide vertical crack with a spall (2 SF) in the gunite coating (Photo 15-10). Medium horizontal cracks (30 LF) with efflorescence. South: Scattered fine map cracks with efflorescence.
5	Others <i>Retaining Walls for Parking Area</i>	North: Several loose stones protruding up to 6"; missing and loose mortar (80 LF). South: Half covered with gunite; scattered areas of discoloration; remaining half has missing pointing, 150 SF (Photo 15-12).

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- Insp. Date: 10/17/2011 &
Morristown Line 01/04/2012

SUBSTRUCTURE

SI&A Item 60 Condition Rating: 5

PIER Center

RATING	COMPONENT	REMARKS
5	Columns/ Stem Crashwall <i>Gunite cover</i>	West Face: Spalled/delaminated bottom along exposed footing (20 SF) full width. Heavy efflorescence and water seepage stain at middle below coping (Photo 15-13). Hollow (30%) areas throughout gunite cover. East Face: Heavy efflorescence and stains on delaminated gunite cover at north end (2' x 8' high). Incipient spalled coping top at several locations (5 SF). Hollow (30%) areas throughout gunite cover. North Face: Heavy rust/water stains on delaminated gunite, 20 SF (Photo 15-22). South Face: Medium horizontal and vertical cracks with efflorescence (Photo 15-21). Work Done: Concrete repairs in coping and wall at northwest edge (Photo 15-22) and at south face (Photo 15-21).
N	Pier Cap	N/A
N	Bridge Seat	N/A
5	Others/Fender	Broken section of conduit hanging from cable at south end of pier along east face (Photo 15-18).

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

HIGHWAY SAFETY

Coding of SI&A Item 36: 0000
 1: Good
 0: Not Good
 N: Not Applicable

RATING		COMPONENT	REMARKS
0		Bridge Railing	W-beam guide rail mounted on top of short concrete parapet/headwall (2" to 8" high from grade). There is a pipe railing behind guide rail (3'-4" high). Chain link fence between guide rail and steel railing.
0	0	Transition to Bridge Railing	Northeast corner: Leading end within clear zone. Substandard single rail w-beam, posting 6'-3" spacing, no spacer blocks. No guide rails on southeast, southwest and northwest due to urban built-up areas.
	1	Curb / Sidewalk Terminations	Continuous sidewalks and curbs. No sidewalk at southeast and southwest.
0		Approach Guide Rails	Northeast only: Very short and no spacer blocks. 6'-3" post spacing. No guide rail at other three corners.
0		Approach Guide Rail End Terminals	Northeast only: Buffer end treatment only.

DECK GEOMETRY

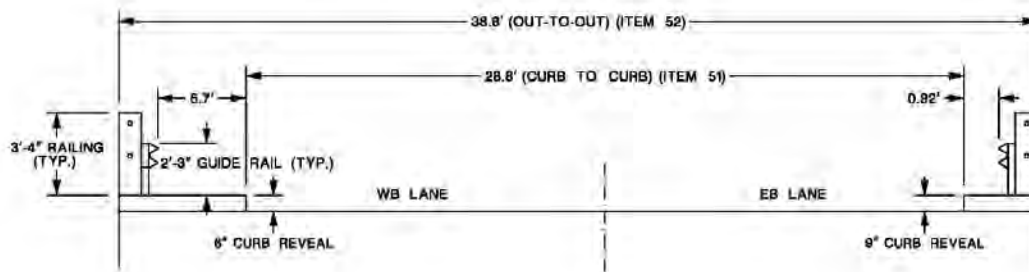
SI&A Item 68 Rating: 2

COMPONENT	REMARKS
Bridge Cross Section	Approaches are wider than bridge cross section. See sketch on following page.
Adequacy of Lane / Shoulder Widths	Two-way traffic, 2 lanes, 1' wide shoulders (typical). ADT = 22,606 (2011 projection from Traffic Count Summary by Morris County). Curb to curb width = 28.8', Table 2A.
Vertical Clearance over Deck	Unlimited.

*Posting for Load / Speed / Clearance Restrictions	20 MPH speed posting (Photo 15-14).
--	--

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
County Route 631 (Landing Road) over NJ Transit-
Name: Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

DECK CROSS SECTION



LOOKING EAST
N.T.S.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit- 10/17/2011 &
Morristown Line Insp. Date: 01/04/2012

CLEARANCES

FEATURE ON STRUCTURE: County Route 631 (Landing Road) SI&A SHEET 1

Minimum Vertical Clearance (SI&A Item 10)	Unlimited.
Total Horizontal Clearances (SI&A Item 47)	28.80' curb to curb.

CONTROLLING UNDERCLEARANCE DATA:	
Minimum Vertical Underclearance (SI&A Item 54)	20.91' from the top of east rail of the east track at the north fascia.
Minimum Vertical Underclearance (incl. shoulders) (SI&A Item DJ)	N/A
Lateral Right (SI&A Item 55)	20.50' from the centerline of the east track to the pier retaining wall at the south fascia.
Lateral Left (SI&A Item 56)	N/A

FEATURE UNDER STRUCTURE: N/A SI&A SHEET 2 or A

*Minimum Vertical Clearance (SI&A Item 10)	N/A
Total Horizontal Clearance (SI&A Item 47)	N/A
Minimum Vertical Underclearance (incl. shoulders) (SI&A Item DJ)	N/A

FEATURE UNDER STRUCTURE: N/A SI&A SHEET B

*Minimum Vertical Clearance (SI&A Item 10)	N/A
Total Horizontal Clearance (SI&A Item 47)	N/A
Minimum Vertical Underclearance (incl. shoulders) (SI&A Item DJ)	N/A

*** Minimum clearance for a 10 foot width of the pavement or traveled part of the roadway where the clearance is greatest shall be coded in feet and inches.**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

FENCING

Coding of SI&A Item FN: Y
 Coding of SI&A Item FO: 0
 Coding of SI&A Item FP (in thousands): 20

Warranted (Per Design Manual Section 23):	Yes	
If Yes: (#1) Description: Highway carrying, grade separation or high level bridges with facility for pedestrian traffic.		
<u>Current Status of Fence & Sidewalk:</u>	<u>Left Side</u>	<u>Right Side</u>
a. Fence:	No	No
b. Sidewalk Width:	5.67 FT	0.00 FT
c. Total Height of fence above curb/sidewalk:	N/A	N/A
d. Type of Fence (per Design Manual Section 23):	N/A	N/A
Action Recommended: Install a curved top, chain link fence at the north fascia.		
Estimated Cost: \$19,176		

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over NJ Transit-
 Name: Morristown Line Insp. Date: 10/17/2011 & 01/04/2012

WORK DONE HISTORICAL DATA

CYCLE NO.	YEAR	WORK DONE SUMMARY
15	2011	New deck waterproofing with new asphalt overlay on top that extends over both approaches. New concrete sidewalk along north side. New concrete curbs and guide rails along both sides. Concrete repairs throughout both arch intrados. Concrete repairs in the north parapet/headwall along the west span. Concrete repairs in the spandrel wall and coping at both spandrel walls of both spans. Concrete repairs at the south wingwall and in the coping and vertical portion below the arch at north end of the west skewback. Concrete repairs in coping and wall at the northwest edge and the south face of the pier stem.
14	2009	Concrete repair to the southwest abutment wingwall. Concrete repair patches to both intrados.
13	2007	None.

La Bianca, Anthony

From: Denho, Abdel
Sent: Tuesday, January 24, 2012 2:13 PM
To: La Bianca, Anthony; Ramirez, Mark
Cc: Lambrinos, Laura
Subject: FW: MC Bridge N0. 1400-073 Repair Photos
Attachments: Deck Waterproofing 011.jpg; Deck Waterproofing 008.jpg; Deck Waterproofing 009.jpg; Deck Waterproofing 010.jpg

From: Caruso, Brian [<mailto:BCaruso@co.morris.nj.us>]
Sent: Tuesday, January 24, 2012 1:51 PM
To: Denho, Abdel
Subject: MC Bridge N0. 1400-073 Repair Photos

Attached are some photos of the condition of the top of the arch and the sprayed-on / painted membrane sealant applied before it was overlaid with asphalt. I am mailing you the plans and supplemental specs. I spoke to Roslyn and we agreed that the County does not have a problem with the rating going up to a 5. The Feds are still looking to fund the replacement project; so, it shouldn't be an issue.

*Brian M. Caruso, P.E.
Senior Engineer
Morris County Engineering
bcaruso@co.morris.nj.us
Phone# 973-285-6986
Fax# 973-539-3141*

Photos received from Morris County



Photo No. 1



Photo No. 2



Photo No. 3



Photo No. 4



Morris County
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
COURT STREET
PO Box 900
MORRISTOWN, NEW JERSEY 07963-0900

BRIDGE RE-EVALUATION SURVEY REPORT

**STRUCTURE NO. 1400-073
COUNTY ROUTE 631 (LANDING ROAD)
OVER NJ TRANSIT-MORRISTOWN LINE
TOWNSHIP OF ROXBURY
MORRIS COUNTY**

16TH CYCLE

SEPTEMBER 13, 2013

**NOTE: This Bridge Re-evaluation Report
shall be filed immediately after the
15TH Cycle Inspection Report.**

Prepared By



Stantec Consulting Services Inc.
365 West Passaic Street, Suite 175
Rochelle Park NJ 07662

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**N.J.D.O.T. - STRUCTURAL EVALUATION AND BRIDGE MANAGEMENT
RE-EVALUATION BRIDGE SURVEY REPORT**

CYCLE NO. 16

STRUCTURAL DATA:

Bridge No.:	1400-073	Year Built:	1907	Widened/Rehab:	1972
Route No.:	9014	Length:	136.0'	Width:	38.8'
Mile Point:	1.31	Date of this Evaluation:	09/13/2013 & 11/08/2013		
Name:	County Route 631 (Landing Road) over NJ Transit-Morristown Line	By:	Stantec Consulting Services Inc.		
		Date of Previous Evaluation:	10/17/2011 & 01/04/2012		
		By:	Stantec Consulting Services Inc.		
		Special Equipment Used:	None		

Structure Type: Two Span, Closed Spandrel,
Reinforced Concrete Arch

WORK DONE: None.

OVERALL PHYSICAL CONDITION: Fair due to the superstructure and substructure.

OVERALL CONDITION (ITEM 67): Fair due to the superstructure and substructure.

Inspection Team Leader: Mark V. Ramirez, P.E.

Certifying Engineer: Abdel A. Denho, P.E.

N.J. P.E. Number: GE 39572

Initials: MVR

I certify that this report is an accurate description of the subject structure, to the extent determinable by visual inspection and testing performed.

Signature: _____
Date: _____

Original Signed
and Sealed

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over 09/13/2013 &
NJ Transit-Morristown Line 11/08/2013

CONCLUSIONS AND RECOMMENDATIONS:

The overall condition of the structure is fair due to the superstructure and substructure.

The superstructure is in fair condition due to the numerous patching covering 60% of both arch intrados and the cracking, scaling, leakage and efflorescence throughout both arch intrados and spandrel walls.

The substructure is in fair condition due to the large concrete patches and the fine to medium cracks with efflorescence and the incipient spalls and hollow sounding concrete areas in the pier and both abutments.

Since the previous inspection, there has been no change in the overall condition of the structure.

The structure is classified as functionally obsolete due to the inadequate deck geometry (Item 68 = 2). Therefore, we recommend the following remedial action:

1. Increase the roadway width from 28.8' to 32.0' by widening the bridge 3.2'.		
a. Demolition of existing south bridge railing: (Lump Sum)		\$25,000
b. Bridge Widening: 3.2' x 136' = 435.2 SF @ \$300/SF		\$130,560
c. Associated approach work: (Lump Sum)		<u>\$50,000</u>
	Subtotal	\$205,560
	Preliminary Engineering (15%)	\$30,834
	Traffic Maintenance (10%)	<u>\$20,556</u>
	Total	\$256,950
	SAY	\$260,000

In the interim until the bridge is widened, we recommend that the following Emergency/Priority repairs be made to retard further deterioration, preserve the structural integrity of the bridge, improve safety and extend its useful life:

None.

StructNum: 1400073

NJDOT SI and A Sheet

Name: CO RT631(LANDING RD) / NJT-MORRISTOWN LINE S.R.: 58.3 SD/FO- 2 -Functionally Obsolete

IDENTIFICATION			
1 State:	34 New Jersey	8 Struc Num:	1400073
7 Facility Carried:	CR631 (Landing Rd)	9 Location:	At Kings Highway Junction
5A Rte.(On/Under):	Route On Structure	5B Rte. Signing Prefix:	4 -County Hwy
5C Level of Service:	1 -Mainline	5D Rte. Number:	00631
5E Directional Suffix:	0- Not Applicable	% Responsibility :	NA
2 SHD District:	01- North	3 County Code:	Morris
4 Place Code:	Roxbury (Township of), M	11 Mile Post:	1.310 mi
6 Feature Intersected :	NJT-Morristown Line		
16 Latitude:	40d 54' 17.00"	17 Longitude:	074d 39' 57.42"
98 Border Bridge Code:	-2 Not Applicable (P)		
99 Border Bridge Number:	NA		

INSPECTION			
91 Frequency:	24 months	90 Inspection Date:	9/13/2013 Next Inspection: 09/13/2015
92A FC Frequency:	NA	93A FC Inspection Date:	NA Next FC Inspection: NA
92B UW Frequency:	NA	93B UW Inspection Date:	NA Next UW Inspection: NA
92C SI Frequency:	NA	93C SI Date:	NA Next SI: NA
Element Frequency:	24 months	Element Inspection Date:	09/13/2013 Next Elem. Insp. Due: 09/13/2015

CLASSIFICATION			
100 STRAHNET Highway:	0 -Not a STRAHNET hwy	101 Parallel Structure:	N -No bridge exists
102 Direction of Traffic:	2 -2-way traffic	103 Temporary Structure:	-1
104 Highway System:	0 -Not on NHS	112 NBIS Length:	Y - Long Enough
20 Toll Facility:	3 -On free road	26 Functional Class:	16- Urban Minor Arterial
37 Historical Significance:	2 -Eligible for NRHP		
22 Owner:	02 County Hwy Agency		
21 Custodian:	02 County Hwy Agency		

STRUCTURE TYPE AND MATERIALS			
46 Number of Approach Spans :	0	45 Number of Spans Main Unit:	2
43A/B Main Span Material/Design:			
1 -Concrete	11 -Arch-Deck		
107 Deck Type: 1- Conc.-Cast-in-Place			
108A Wearing Surface:	6 -Bituminous		
108B Membrane:	3- Epoxy		
108C Deck Protection:	8- Unknown		

CONDITION			
58 Deck:	7 -Good	59 Super:	5 -Fair 60 Sub: 5 -Fair
62 Culvert:	N -Not applicable 61 Channel/Channel Protection: N -Not applicable		

LOAD RATING AND POSTING			
65 Inventory Rating Method:	1- LF Load Factor	63 Operating Rating Method:	1 -LF Load Factor
66 Inventory Rating:	HS65.0	64 Operating Rating:	HS99.9
31 Design Load:	0 -Unknown	70 Posting:	5 At/Above Legal Loads
41 Posting status:	A -Open, no restriction		

AGE AND SERVICE			
27 Year Built:	1907	106 Year Reconstructed:	1972
42A Type of Service On:	5 -Highway-pedestrian		
42B Type of Service Under:	2 -Railroad		
28A Lanes on:	2	28B Lanes Under:	0
		19 Detour Length:	2.0 mi
29 ADT:	23,061	109 Truck ADT:	4 %
		30 Year of ADT:	2013

APPRAISAL			
36A Bridge Rail:	0 -Substandard	36C Approach Rail:	0 -Substandard
36B Transition:	0 -Substandard	36D Approach Rail Ends:	0 -Substandard
67 Str. Evaluation:	5	68 Deck Geometry:	2 Intolerable - Replace
69 Underclearance, Vertical and Horizontal:	4 -Tolerable		
71 Waterway Adequacy:	N -Not applicable 72 Approach Alignment: 5 -Above Tolerable		
113 Scour Critical:	N -Not Over Waterway		

GEOMETRIC DATA			
48 Length Max Span:	64.0 ft	49 Structure Length:	136.0 ft
50A Curb/Sdwik Width L:	0.0 ft	50B Curb/Sidewalk Width R:	5.7 ft
Width Curb to Curb 51:	28.8 ft	52 Width Out to Out:	38.8 ft
32 Approach Roadway Width: (w/ shoulders)	28 ft	33 Median:	0 No median
Deck Area:	5,277. sq. ft		
34 Skew:	0.00 °	35 Structure Flared:	0 -No flare
53 Minimum Vertical Clearance Over Bridge:	9999		
54A Minimum Vertical Underclearance Reference:	R -Railroad beneath str.		
54B Minimum Vertical Underclearance:	20.92 ft		
55A Minimum Lateral Underclearance Reference R:	R -Railroad beneath str.		
55B Minimum Lateral Underclearance R:	20.50 ft		
56 Minimum Lateral Underclearance L:	0.00 ft		

PROPOSED IMPROVEMENTS			
94 Bridge Cost:	\$130,560	75 Type of Work:	33
95 Roadway Cost:	\$50,000	76 Length of Improvement:	136 ft
96 Total Cost:	\$260,000	114 Future ADT:	31,060
97 Year of Cost Estimate:	2013	115 Year of Future ADT:	2033

NAVIGATION DATA			
38 Navigation Control:	N N -N/A-No waterway		
39 Vertical Clearance:	0.0 ft	40 Horizontal Clearance:	0.0 ft
111 Pier Protection:	-1 116 Lift Bridge Vertical Clearance:		

ELEMENT CONDITION STATE DATA

Str Unit	Elm/Env	Description	Units	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4	% in 5	Qty. St. 5
0	14/3	P Conc Deck/AC Ovly	(SF)	5,277	100 %	5,277	0 %	0	0 %	0	0 %	0	0 %	0
0	144/3	R/Conc Arch	(LF)	136	0 %	0	0 %	0	100 %	136	0 %	0	0 %	0
0	210/3	R/Conc Pier Wall	(LF)	39	0 %	0	100 %	39	0 %	0	0 %	0	0 %	0
0	215/3	R/Conc Abutment	(LF)	78	0 %	0	100 %	78	0 %	0	0 %	0	0 %	0
0	334/3	Metal Rail Coated	(LF)	272	100 %	272	0 %	0	0 %	0	0 %	0	0 %	0
0	503/3	Curbs/Walks - Conc	(LF)	136	100 %	136	0 %	0	0 %	0	0 %	0	0 %	0

StructNum: 1400073

NJDOT SI and A Sheet

NJDOT INSPECTION FIELDS

(V1.2)

<p>GENERAL</p> <p>(CI) Cycle Number: 16 (CJ) Insp Type: S (CK) Inspection Crew: M (CM) Consultant: V08 (CO) Prev Consultant: V08 (CP) Federal Report: -</p>	<p>MISCELLANEOUS</p> <p>(B) Deletion Code: N (BA) Apr Rdwy Cond: 7</p>	<p>REMARKS</p> <p>(BF) Deck: 1: _ 2: _ 3: _ 4: _ 5: _</p> <p>(BG) Super: 1: 9 2: Z 3: -</p> <p>(BH) Sub: 1: A 2: C 3: B</p> <p>(BI) Channel: - (BJ) Culvert: -</p>	<p>PAINTING</p> <p>(GD) Fascia Beam: -1 (GE) Fascia B. Flange: -1 (GF) Interior Beam: -1 (GH) Interior B. Flange: -1 (GI) Beam Ends: -1 (GJ) Connections: -1 (GK) Bracings: -1 (GL) Bearings: -1 (GM) Substructure: -1 (GN) Above Deck Super: -1 (GO) Railings/Fence: -1 (GP) Remarks 1: -1 (GQ) Remarks 2: -1</p>
<p>INSPECTION DATES</p> <p>(AW) Mech/Electrical: 1/1/1901 (AX) Deck: 1/1/1901 (AY) Special Testing: 1/1/1901 (GC) Paint: 1/1/1901</p> <p>(AR) Equipment: 1: _ 2: _ 3: -</p> <p>(AS) Testing: 1: _ 2: _ 3: -</p>	<p>FATIGUE DETAIL (AZ)</p> <p>Location 1: -1 -1 Location 2: -1 -1 Location 3: -1 -1</p>	<p>FENCING</p> <p>(FO) Pedestrian Traffic: 0 (FP) Improvement Cost: \$ 20 Thousands</p>	
	<p>IN-DEPTH PIN-HANGER</p> <p>(FR) Consultant: -1 (FS) -1 (FT) Combo: N (FQ) FCM/Pin-Hngr Insp Date: 1/1/1901</p>		

ROADWAY DATA

Bridge Id: 1400073
 SRI 14000631__

(V1.2)

<p>ROADWAY IDENTIFICATION</p> <p>NBI Roadway?: 1 Roadway Name: CR 631 (LANDING ROAD) Item 5A: 1 Item 5B: 4 Item 5C: 1 Item 5D: 00631 Item 5E: 0</p>	<p>TRAFFIC AND ACCIDENTS</p> <p>28 Lanes : 2 Num Median: 0 Road Speed: 35 mph ADTclass: 04 29 ADTtotal: 23061 30 Year of ADT: 2013 114 Future ADT: 31060 115 Year of Future ADT: 2033 109 Truck ADTT % : 4 19 Bypass Length: 2.00 mi Detour Speed: 30 mph</p>
<p>HWY NETWORKS AND SERVICE CLASSIFICATION</p> <p>11 Milepost: 1.310 mi 12 Base Hwy Network: 0 20 Toll Facility: 3 13A LRS Inventory Route Subroute No.: 26 Functional Class: 16 102 TraffDirection: 2</p>	<p>CLEARANCES</p> <p>10 Vertical Clearance: 99.99 ft 47 Invent Route Horiz Clear: 28.80 ft (DJ) Min Vert Undrcir: 00.00 ft</p>
<p>ALTERNATE CLASSIFICATION</p> <p>100 Strahnet Hwy: 0 Transit Rt: 0 104 NHS System: 0 Emergency 0 105 Fed. Lands Hwy: 0 110 Truck Hwy Net: 0 School Bus: 0</p>	<p>WIDTHS</p> <p>32 Appr Rdwy Width: 28 ft 51 Brdg Rdwy With Curb-Curb: 28.8 ft</p>

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
County Route 631 (Landing Road) over
Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

LOAD RATING SUMMARY SHEET (LRSS)

(Form NJ-BI-101 Created 1/25/2011)

Project Information:

Group: -- Agreement No.: -- Contract ID: -- Agree/Mod No.: --

Rating Information:

Method: LRFR: No LFR: Yes ASR: No Other (Specify): N/A

Rating Date: 05/05/95 Computer Software Used: STAAD-III Version: Rev. 20.0W

Load Testing: No Cycle when Rating Performed: 6th Design Load: Unknown

Structure Information:

Plans Available? No Contract Designation: Unknown

Overlay? Yes Considered in Rating? Yes Type/Thickness: Bit. Conc./Unknown

Section Losses? No Considered in Rating? N/A Item 59 Cond.: Fair

For LRFR Use Only:

Surface Roughness Factor: -- Condition Factor: -- System Factor: --

ADTT (one direction): -- Resistance Factor: -- FCM: Yes/No

Load Rating Engineer (LRE):

Name: Unknown Firm: Unknown Initial: Unknown

Load Rating Reviewer (LRR) certification as per the NBIS Title 23 CFR Section 650.309(c):

Name: T.J. N.J. P.E. No.: Unknown

Firm: Unknown

I certify that this rating is an accurate representation of the subject structure, considering all deterioration and/or changes to loading conditions, to the extent determinable by research and visual inspection and testing performed. I am charged with the overall responsibility for bridge capacity evaluation for the above mentioned structure.

No Load Rating
performed this
Cycle

-- --
Sign Date

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over
 Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

LOAD RATING SUMMARY SHEET (LRSS) (cont.)

Rating Comments:

None.

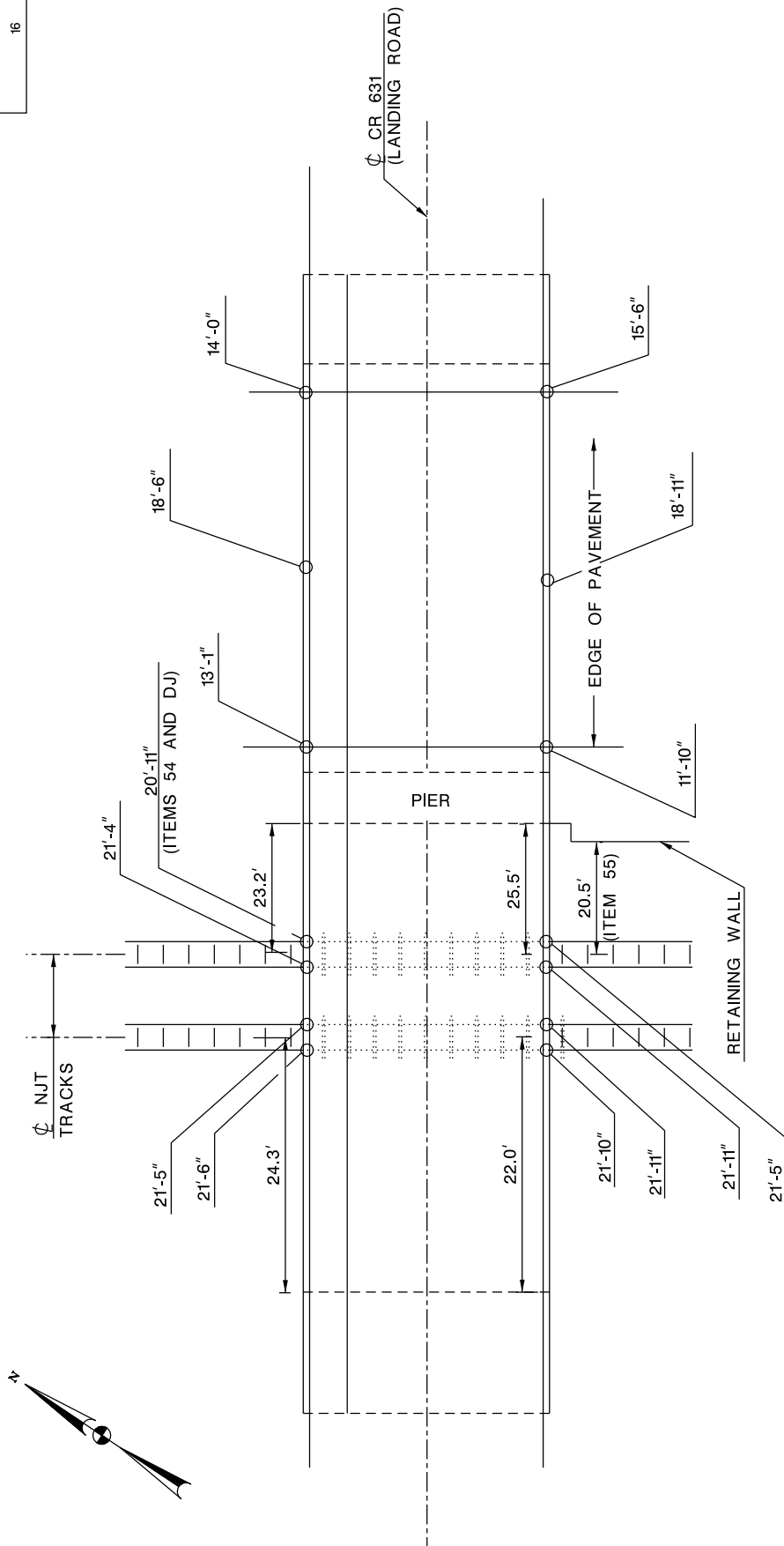
The Load Factor ratings, computed in the 6th Cycle report in accordance with the FHWA directive dated November 1993 and AASHTO Manual for Condition Evaluation of Bridges, 1994, as modified by Division 4 of the New Jersey Department of Transportation Design Manual, Bridges and Structures, are as follows:

<u>Material</u>	<u>Compressive Strength f'c</u>	<u>Allowable Stresses (Psi)</u>		
		<u>Yield</u>	<u>Inventory</u>	<u>Operating</u>
Concrete	2,500	N/A	800	1,200
Reinforcing Steel	N/A	33,000	18,000	25,000

<u>Member</u>	<u>Truck Type (Tons)</u>		<u>Rating (Tons)</u>							
			<u>LFR</u>				<u>LRFR</u>			
			<u>As-Built</u>		<u>As-Insp.</u>		<u>As-Built</u>		<u>As-Insp.</u>	
			<u>Inv.</u>	<u>Op.</u>	<u>Inv.</u>	<u>Op.</u>	<u>Inv.</u>	<u>Op.</u>	<u>Inv.</u>	<u>Op.</u>
Arch at Crown Cond. Rating = 5	H15	(15T)	46	77	---	---	---	---	---	---
	HL-93	(NL)	---	---	---	---	---	---	---	---
	HS-20	(36T)	65	109	---	---	---	---	---	---
	3	(25T)	58	98	---	---	---	---	---	---
	3S2	(40T)	37	63	---	---	---	---	---	---
	3-3	(40T)	59	99	---	---	---	---	---	---
	SU4	(27T)	---	---	---	---	---	---	---	---
	SU5	(31T)	---	---	---	---	---	---	---	---
	SU6	(35T)	---	---	---	---	---	---	---	---
	SU7	(39T)	---	---	---	---	---	---	---	---

(NL) = Notional Load

ROUTE 9014	STRUCTURE NO. 1400-073	DATE OF INSP. 9/13/2013
		CYCLE NO. 16



HORIZONTAL & VERTICAL UNDERCLEARANCE PLAN

N.T.S.

NEW JERSEY DEPARTMENT OF TRANSPORTATION
CR 631 (LANDING ROAD) OVER NJ TRANSIT-MORRISTOWN LINE MUNICIPALITY: TOWNSHIP OF ROXBURY COUNTY: MORRIS
CLEARANCE DIAGRAM
STANTEC CONSULTING SERVICES INC. 365 WEST PASSAIC STREET ROCHELLE PARK, NJ 07662

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013



Photo No: 16-01

Location:	North elevation, looking south
Description:	General view. Note the leaning utility pole on the spandrel wall between the two spans.



Photo No: 16-02

Location:	South elevation, looking north
Description:	General view.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013



Photo No: 16-03

Location:	Roadway, looking east
Description:	General view.



Photo No: 16-04

Location:	Roadway, looking west
Description:	General view. Note speed advisory posting.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013



Photo No: 16-05

Location: Underside of intrados, west span, looking west

Description: General view. Note the light to moderate scaling with fine to medium cracks, efflorescence and numerous large concrete patches throughout.



Photo No: 16-06

Location: South pipe railing, 3rd section from west, west span, looking southeast

Description: Moderate rusting with holes in upper and lower rails.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over 09/13/2013 &
 Name: NJ Transit-Morristown Line Insp. Date: 11/08/2013



Photo No: 16-07

Location:	South pipe railing, east end of the east span, looking northwest
Description:	Damaged pipe railing with fractured middle rail and middle post is now protected by bridge-mounted guide rail. Note the spall along the cold joint in the outside face of the southwest approach parapet.



Photo No: 16-08

Location:	South spandrel wall, east end of east span, looking northwest
Description:	Fine map cracking with light efflorescence. Note the broken section of conduit along the east face of the pier.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013



Photo No: 16-09

Location:	East skewback, north end, looking southeast
Description:	Fine vertical cracks with efflorescence.



Photo No: 16-10

Location:	Northeast wingwall, east end, looking south
Description:	Full height wide vertical crack with spall in the gunite coating.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013



Location:	Southwest retaining wall, looking west
Description:	Spalls with active leakage at bottom.



Location:	Southeast retaining wall, looking east
Description:	Voids and missing pointing throughout.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013



Photo No: 16-13

Location:	West face of pier, looking northeast
Description:	Efflorescence and seepage stain at middle below coping.



Photo No: 16-14

Location:	Southwest wingwall, looking northeast
Description:	Fine map cracking with efflorescence.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

**NEW JERSEY DEPARTMENT OF TRANSPORTATION
 STRUCTURAL EVALUATION AND BRIDGE MANAGEMENT
 FIELD NOTES
 MORRIS COUNTY**

Inspectors: Mark V. Ramirez, P.E. Name: County Route 631 (Landing Road) over NJ Transit-Morristown
A. La Bianca Line

Crew Chief: Mark V. Ramirez, P.E. Weather: Clear

Temperature: 40°F Special Equipment Used: None

RATINGS:

- N Not applicable.
- 9 Excellent Condition.
- 8 Very Good Condition – no problems noted.
- 7 Good Condition – some minor problems.
- 6 Satisfactory Condition – some minor deterioration of structural elements.
- 5 Fair Condition – minor section loss to primary structural elements.**
- 4 Poor Condition – advanced section loss to primary structural elements.
- 3 Serious Condition – seriously deteriorated primary structural elements.
- 2 Critical Condition – facility should be closed until repairs are made.
- 1 Imminent Failure Condition – facility closed. Study of repairs is feasible.
- 0 Failed Condition – facility is closed and beyond repair.

GPS COORDINATES			
@ Southwest corner			
N	40°	54' 17.00"	Lat.
W	74°	39' 57.42"	Long.

GENERAL

Type of Bridge: Two Span, Closed Spandrel, Reinforced Concrete Arch.

Year Built: 1907 Year of Widening / Major Repairs: 1972

No. of Lanes: On 2 Under None

Vertical Clearances: Over Deck: Unlimited

Minimum Under: 20.91' from the top of east rail of the east track under north fascia.

Maximum Under (Item 10): N/A

Horizontal Underclearance: Total Horizontal Clearance: N/A

Right 20.50' from the centerline of the east track to the pier retaining wall at the south fascia.

Left N/A

Overall Physical Condition of Structure: Fair due to the superstructure and substructure.

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

DECK

SI&A Item 58 Condition Rating: 7

SPAN # West

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	No significant defects.
N	Underside of Deck	Not visible.
N	Median	
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. No apparent defects.
8	Sidewalks/ Safetywalk <i>Concrete @ North only</i>	No apparent defects.
7	Parapets/ Balustrades/ Pedestals	No significant defects.
7	Railings <i>W-beam guide rail and pipe railing with concrete pylons</i>	The original deteriorated pipe rails on both sides are protected by bridge mounted w-beam guide rail (Photo 16-06). No significant defects.
N	Deck Joints / Filler Material	
N	Drains and Scuppers	
N	Light Stands	
N	Utilities	
N	Others	

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

DECK

SI&A Item 58 Condition Rating: 7

SPAN # East

RATING	COMPONENT	REMARKS
7	Wearing Surface / Top of Deck	Medium to wide longitudinal crack in the centerline of the westbound lane over the pier. No other significant defects.
N	Underside of Deck	Not visible.
N	Median	
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. No apparent defects.
8	Sidewalks/ Safetywalk <i>Concrete @ North only</i>	No apparent defects.
7	Parapets/ Balustrades	No significant defects.
7	Railings <i>W-beam guide rail and pipe railing with concrete pylons</i>	The original deteriorated pipe rails on both sides are protected by bridge mounted w-beam guide rail (Photo 16-07). No significant defects.
N	Deck Joints / Filler Material	
N	Drains and Scuppers	
N	Light Stands	
N	Utilities	
N	Others	

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over
 Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

APPROACHES

SI&A Item BA Rating: 7

SI&A Item 72 Rating: 5

APPROACH West

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement <i>Bituminous Concrete</i>	No significant defects.
7	Approach Shoulder	No significant defects.
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from the bridge. Horizontal: Sharp (90°) turn south.
N	Guide Rail Condition	
8	Sidewalks <i>Concrete @ North only</i>	Continuous. No apparent defects.
8	Curbs <i>Concrete</i>	Integral with sidewalk at north side. Continuous. No apparent defects.
N	Utilities	
7	Approach Roadway Embankment	South and north: Moderate slope, well vegetated; stable.
7	Others/ Parapet	No significant defects.

Additional Remarks:

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over
 Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

APPROACHES

SI&A Item BA Rating: 7

SI&A Item 72 Rating: 5

APPROACH East

RATING	COMPONENT	REMARKS
7	Approach Slab / Pavement <i>Bituminous Concrete</i>	No significant defects.
7	Approach Shoulder	No significant defects.
	Approach Roadway Vertical and Horizontal Alignment	Vertical: Downgrade away from bridge. Horizontal: T-intersection 100' to east. Speed reduction to 20 MPH.
7	Guide Rail Condition <i>W-beam @ NE only</i>	No significant defects.
8	Sidewalks <i>Concrete @ North only</i>	Continuous. No apparent defects.
7	Curbs <i>Concrete</i>	Integral with sidewalk at north side. Continuous. No significant defects.
N	Utilities	
7	Approach Roadway Embankment	South: Flat, parking lot. North: Moderate slope, vegetated, entrance to parking lot.
6	Others/ Parapet	Small spall along cold joint in southwest approach parapet outside face near bottom west end (Photo 16-07).

Additional Remarks:

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over
 Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

SUPERSTRUCTURE
(ARCH)

SI&A Item 59 Condition Rating: 5

SPAN # West

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Light to moderate scaling with fine to medium random cracks with efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 16-05).
N	Spandrel Columns/ Extradados	
5	Spandrel Walls	North: Fine map cracks with efflorescence and moisture staining (typical Photo 16-08) (60 SF total). South: Scattered fine map cracks and random cracks.
N	Others	

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

SUPERSTRUCTURE
(ARCH)

SI&A Item 59 Condition Rating: 5

SPAN # East

RATING	COMPONENT	REMARKS
5	Intrados of Arch (Soffit) Arch Ribs	Moderate scaling and efflorescence throughout. Numerous large concrete patches throughout 60% area (Photo 16-05). Medium longitudinal and transverse cracks with light to moderate efflorescence (180 LF total), mostly near the north and south ends of arch intrados.
N	Spandrel Columns/ Extradados	
5	Spandrel Walls	Fine map cracks with efflorescence throughout north wall and east end of south wall (Photo 16-08) (20 SF).
5	Others	Utility pole is leaning onto north spandrel wall between the two spans (wires are disconnected) (Photo 16-01).

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

SUBSTRUCTURE
(Arch/Frame)

SI&A Item 60 Condition Rating: 5

ABUTMENT West

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Bottom Portion-Crashwall: Fine vertical and map cracks with efflorescence and spalls/incipient spalls along the full length of the horizontal face (shotcrete) (typical Photo 16-09).
5	Wingwalls/ Retaining Walls	South: Fine map cracks and large concrete patch at east end (Photo 16-14). North: Medium cracks (50 LF) with efflorescence.
4	Others <i>Retaining Walls</i>	Spalls (40 SF) with active leakage along bottom at southwest wall (Photo 16-11).

**Additional
Remarks:**

ABUTMENT East

RATING	COMPONENT	REMARKS
6	Footings/ Skewbacks	Few fine vertical cracks with moderate efflorescence at few locations (16 LF total) (Photo 16-09). Hollow (5%) areas of gunite throughout.
5	Wingwalls/ Retaining Walls	South: Scattered fine map cracks with efflorescence. North: Full height wide vertical crack with a spall (2 SF) in the gunite coating (Photo 16-10). Medium horizontal cracks (30 LF) with efflorescence.
5	Others <i>Retaining Walls for Parking Area</i>	North: Several loose stones protruding up to 6"; missing and loose mortar (80 LF). South: Half covered with gunite; scattered areas of discoloration; remaining half exhibits voids up to 1'-4" deep (6 SF) and missing mortar pointing (150 SF) (Photo 16-12).

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over
 Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

SUBSTRUCTURE

SI&A Item 60 Condition Rating: 5

PIER Center

RATING	COMPONENT	REMARKS
5	Columns/ Stem Crashwall <i>Gunite cover</i>	West Face: Spalled and delaminated bottom along exposed footing (20 SF) full width. Heavy efflorescence and water seepage stain at middle below coping (Photo 16-13). Hollow (30%) areas throughout gunite cover. East Face: Heavy efflorescence and stains on delaminated gunite cover at north end (2' x 8' high). Incipient spalled coping top at several locations (5 SF). Hollow (30%) areas throughout gunite cover. North Face: Heavy rust and water stains on delaminated gunite (20 SF). South Face: Fine to medium horizontal and vertical cracks throughout face.
N	Pier Cap	
N	Bridge Seat	
5	Others/Fender	Broken section of conduit hanging from cable at south end of pier along east face (Photo 16-08).

**Additional
Remarks:**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over 09/13/2013 &
 Name: NJ Transit-Morristown Line Insp. Date: 11/08/2013

HIGHWAY SAFETY

Coding of SI&A Item 36: 0000
 1: Good
 0: Not Good
 N: Not Applicable

RATING		COMPONENT	REMARKS
0		Bridge Railing	W-beam guide rail mounted on top of short concrete parapet/headwall (2" to 8" high from grade). There is a pipe railing behind guide rail (3'-4" high). Chain link fence between guide rail and steel railing.
0	0	Transition to Bridge Railing	Northeast corner: Leading end within clear zone. Substandard single rail w-beam, posting 6'-3" spacing, no spacer blocks. No guide rails on southeast, southwest and northwest due to urban built-up areas.
	1	Curb / Sidewalk Terminations	Continuous sidewalks and curbs. No sidewalk at southeast and southwest.
0		Approach Guide Rails	Northeast only: Very short and no spacer blocks. 6'-3" post spacing. No guide rail at other three corners.
0		Approach Guide Rail End Terminals	Northeast only: Buffer end treatment only.

DECK GEOMETRY

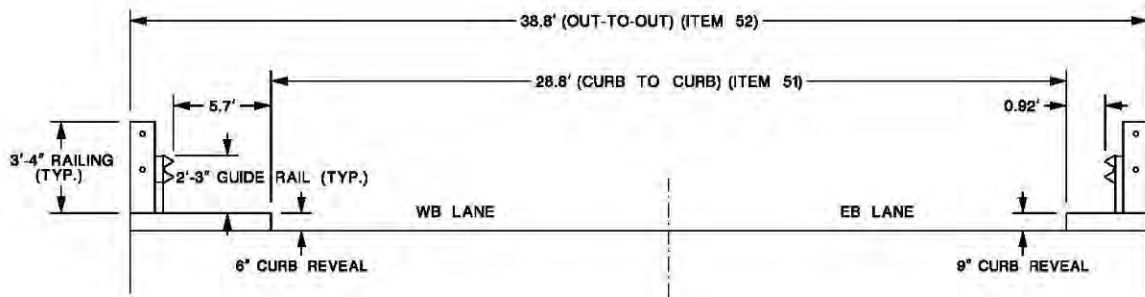
SI&A Item 68 Rating: 2

COMPONENT	REMARKS
Bridge Cross Section	Approaches are wider than bridge cross section. See sketch on following page.
Adequacy of Lane / Shoulder Widths	Two-way traffic, 2 lanes, 1' wide shoulders (typical). ADT = 23,061 (2013 projection from Traffic Count Summary by Morris County). Curb to curb width = 28.8', Table 2A.
Vertical Clearance over Deck	Unlimited.

*Posting for Load / Speed / Clearance Restrictions	20 MPH speed posting (Photo 16-04).
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Structure No.: 1400-073 Route: 9014 Cycle No.: 15
County Route 631 (Landing Road) over 09/13/2013 &
Name: NJ Transit-Morristown Line Insp. Date: 11/08/2013

DECK CROSS SECTION



Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 Name: County Route 631 (Landing Road) over 09/13/2013 &
NJ Transit-Morristown Line Insp. Date: 11/08/2013

CLEARANCES

FEATURE ON STRUCTURE: County Route 631 (Landing Road) SI&A SHEET 1

Minimum Vertical Clearance (SI&A Item 10)	Unlimited.
Total Horizontal Clearances (SI&A Item 47)	28.80' curb to curb.

CONTROLLING UNDERCLEARANCE DATA:	
Minimum Vertical Underclearance (SI&A Item 54)	20.92' from the top of east rail of the east track at the north fascia.
Minimum Vertical Underclearance (incl. shoulders) (SI&A Item DJ)	N/A
Lateral Right (SI&A Item 55)	20.50' from the centerline of the east track to the pier retaining wall at the south fascia.
Lateral Left (SI&A Item 56)	N/A

FEATURE UNDER STRUCTURE: N/A SI&A SHEET 2 or A

*Minimum Vertical Clearance (SI&A Item 10)	N/A
Total Horizontal Clearance (SI&A Item 47)	N/A
Minimum Vertical Underclearance (incl. shoulders) (SI&A Item DJ)	N/A

FEATURE UNDER STRUCTURE: N/A SI&A SHEET B

*Minimum Vertical Clearance (SI&A Item 10)	N/A
Total Horizontal Clearance (SI&A Item 47)	N/A
Minimum Vertical Underclearance (incl. shoulders) (SI&A Item DJ)	N/A

*** Minimum clearance for a 10 foot width of the pavement or traveled part of the roadway where the clearance is greatest shall be coded in feet and inches.**

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over
 Name: NJ Transit-Morristown Line Insp. Date: 09/13/2013 & 11/08/2013

FENCING

Coding of SI&A Item FN: Y
 Coding of SI&A Item FO: 0
 Coding of SI&A Item FP (in thousands): 20

Warranted (Per Design Manual Section 23):	Yes	
If Yes: (#1) Description: Highway carrying, grade separation or high level bridges with facility for pedestrian traffic.		
<u>Current Status of Fence & Sidewalk:</u>	<u>Left Side</u>	<u>Right Side</u>
a. Fence:	No	No
b. Sidewalk Width:	5.67 FT	0.00 FT
c. Total Height of fence above curb/sidewalk:	N/A	N/A
d. Type of Fence (per Design Manual Section 23):	N/A	N/A
Action Recommended: Install a curved top, chain link fence at the north fascia.		
Estimated Cost: \$19,176		

Structure No.: 1400-073 Route: 9014 Cycle No.: 15
 County Route 631 (Landing Road) over 09/13/2013 &
 Name: NJ Transit-Morristown Line Insp. Date: 11/08/2013

WORK DONE HISTORICAL DATA

CYCLE NO.	YEAR	WORK DONE SUMMARY
16	2013	None.
15	2011	New deck waterproofing with new asphalt overlay on top that extends over both approaches. New concrete sidewalk along north side. New concrete curbs and guide rails along both sides. Concrete repairs throughout both arch intrados. Concrete repairs in the north parapet/headwall along the west span. Concrete repairs in the spandrel wall and coping at both spandrel walls of both spans. Concrete repairs at the south wingwall and in the coping and vertical portion below the arch at north end of the west skewback. Concrete repairs in coping and wall at the northwest edge and the south face of the pier stem.
14	2009	Concrete repair to the southwest abutment wingwall. Concrete repair patches to both intrados.
13	2007	None.

APPENDIX – C

COST ESTIMATES

Class 1 - New Construction

SUMMARY

Route	Landing Road Bridge	Section/Contract #	Alternative #1
PM	0	UPC No.	0

Work Type			Totals from other pages
Earthwork			40000
Pavement			518520
Context Sensitive Design			200000
Culverts			0
Bridges			4175671.5
Drainage			219830
Incidental Items			186400
Landscape			71565
Noise Abatement			0
General Items			55575
Retaining Walls	582.8	1000	582800
PROJECT SUBTOTAL			6050361.5

Other Items	Proj. Subtotal Range	Choice	Amount	
Lighting, Traffic Stripes, Signs and Delineators		3% of Proj. Subtotal	181510.845	
Maintenance of Traffic		1.5% of Proj. Subtotal	90755	
Training		1% of Proj. Subtotal	60503.615	
Mobilization			605036.15	
	Project Cost < 5.0 (Mil.)	9% of Proj. Subtotal		0
	Project Cost 5.0 & above	10% of Proj. Subtotal		605036
Progress Schedule	Project Cost(Mil.)	\$	8000	
	Less than 2.0	0		0
	2.0 to 5.0	6,000		0
	5.0 to 10.0	8,000		8000
	10.0 to 20.0	15,000		0
	20.0 to 30.0	30,000		0
	30.0 to 40.0	40,000		0
	40.0 & above	58,000		0
Clearing Site	Project Cost (Mil.)	\$	115000	
	Less than 1.0	15,000		0
	1.0 to 2.0	30,000		0
	2.0 to 5.0	45,000		0
	5.0 to 10.0	115,000		115000
	10.0 to 20.0	220,000		0
	20.0 to 30.0	240,000		0
	30.0 to 40.0	250,000		0
	40.0 & above	490,000		0
Construction Layout	Project Cost(Mil.)	\$	87000	
	Less than 1.0	7,000		0
	1.0 to 2.0	20,000		0
	2.0 to 5.0	42,000		0
	5.0 to 10.0	87,000		87000
	10.0 to 20.0	160,000		0
	20.0 to 30.0	220,000		0
	30.0 to 40.0	490,000		0
	40.0 & above	890,000		0
PROJECT TOTAL			7198168	

Class 1 - New Construction

CONTINGENCIES & ESCALATION

Y

3.00

Y = Number of Years until midpoint of construction duration plus number of years until construction start. If midpoint is less than 2 years from the date of this estimate, no escalation is required. Maximum value = 10%

3.00

7198167.533	1.030	1.04	7710677
Project Total Contingencies (1+C)		1 + [0.01 (Y+1) (Y-2)]	Construction Estimate for Initial

Project Cost(Mil.)	Contingencies (C) Percent	Average Construction Duration in Years
0-10	3%	1
10-20	2.50%	2
20-50	2%	3
Over 50	1.50%	4

0.030
0.000
0.000
0.000

CONSTRUCTION CHANGE ORDER CONTINGENCIES

Total Federal Participating Items

in Millions of \$

Construction Change Order Contingency Amount

\$0 to 0.1	\$6,000	0
0.1 to 0.5	25,000	0
0.5 to 5.0	25,000 + 4% of amount in excess of \$500,000	0
5.0 to 10.0	205,000 + 3% of amount in excess of \$5,000,000	286300
10.0 to 15.0	355,000 + 2% of amount in excess of \$10,000,000	0
15.0 and above	455,000 + 1.5% of amount in excess of \$15,000,000 - \$500,000 max	0

For State Funded Projects, Contingencies for Change orders = 0

CHANGE ORDER CONTINGENC = 286300

UTILITIES RELOCATIONS BY COMPANIES/OWNERS

7710677.061	0	0
	x % or + Estimate	=
Construction Cost for Initial Estimate	Use % or utilities detailed estimate	Utility Relocation Cost for Initial Estimate

If there are no utility relocations on the project indicate "No Utilities" in the box above.

RIGHT OF WAY COST

If there is no ROW cost on the project indicate "No ROW" the box

SUMMARY

Construction Estimate for Initial	7,710,677
Construction Engineering (CE)	0
Contingencies	286,300
Utilities Relocations	0
Total Construction Cost	7,996,977
Right of Way Cost	0

Project: Landing Bridge Road
 Utility: Summary Estimate
 Alternative: 1

Date: 1/14/03
 AmerCom Job #2003

UTILITY		DESCRIPTION	WORK SUMMARY	TOTAL
Electric	GPU	Preliminary Estimated Utility Impacts	5 Relocated Poles, wire	\$808,573
Telephone	Verizon/BA	Preliminary Estimated Utility Impacts	1 Additional Pole, wire	\$433,550
Cable	Comcast	Preliminary Estimated Utility Impacts	415 LM of cable wire	\$433,550
Water	Roxbury Water Co.	Preliminary Estimated Utility Impacts	417 LM of new line	\$150,980

Total	\$1,826,653
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Project: Landing Bridge Road
 Utility: Electric
 Alternative: 1

Date: 4/04/03
 AmerCom Job#1004

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
E1	Electric	Remove old wooden pole	16	UNIT(S)	\$598	\$9,568
E2	Electric	New Pole, 12.2 m high	16	UNIT(S)	\$1,093	\$17,488
E3	Electric	Foundation for line poles, excavation in earth	16	UNIT(S)	\$59	\$944
E4	Electric	Guys, anchors and hardware for pole, in earth	16	UNIT(S)	\$1,323	\$21,168
E5	Electric	Cross arms with hardware and insulators, 1.5 m long	16	UNIT(S)	\$380	\$6,080
E6	Electric	Transformers, Oil Filled, 1000kVA	3	UNIT(S)	\$25,070	\$75,210
E7	Electric	Light Braket Arm	3	UNIT(S)	\$150	\$450
E8	Electric	Manhole	0	UNIT(S)	\$23,000	\$0
E9	Electric	3 Phase , Distrib. 34kV High Line, Primary	769	L.M.	\$566	\$435,254
E10	Electric	3 Phase , Distrib. 12kV	0	L.M.	\$566	\$0
E11	Electric	2 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E12	Electric	1 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E13	Electric	120/240V Secondary Electrical Overhead	643	L.M.	\$377	\$242,411

Total	\$808,573
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Project: Landing Road Bridge
 Utility: Telephone
 Alternative: 1

Date: 4/04/03
 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
T1	Telephone	Remove old wooden pole	0	UNIT(S)	\$598	\$0
T2	Telephone	New Pole, 12.2 m high	0	UNIT(S)	\$1,093	\$0
T3	Telephone	Foundation for line poles, excavation in earth	0	UNIT(S)	\$59	\$0
T4	Telephone	Guys, anchors and hardware for pole, in earth	0	UNIT(S)	\$1,323	\$0
T5	Telephone	Cross arms with hardware and insulators, 1.5 m long	0	UNIT(S)	\$380	\$0
T6	Telephone	Telephone Wire, 300 Pair	754	L.M.	\$575	\$433,550
T7	Telephone	Fiber Optic Cable	0	L.F.	\$575	\$0
T8	Telephone	Manhole	0	UNIT(S)	\$23,000	\$0

Total	\$433,550
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Project: Landing Road Bridge
 Utility: Telephone
 Alternative: 1

Date: 4/04/03
 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
C1	Cable	Cable Wire	754	L.M.	\$575	\$433,550

Total	\$433,550
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Project: Landing Road Bridge
 Utility: Water
 Alternative: 1

Date: 4/04/03
 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
W1	Water	300MM Water Main	242	L.M.	\$426	\$103,092
W2	Water	Reset Water Valve Box	4	UNIT(S)	\$472	\$1,888
W3	Water	300 MM Gate Valves and Boxes	10	UNIT(S)	\$4,600	\$46,000

Total	\$150,980
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LANDING ROAD BRIDGE - AMERCOM #1011

Bypass Alternative (Alternative 1)

Right-Of-Way Acquisitions

TABLE 2 - SUMMARY MATRIX

PARCEL #	LOCATION / OWNER NAME	STREET	CITY, NJ	ZIP	LOT	BLOCK	RATABLE \$ / Ac.	ROW TAKING AREA (appx.)			\$ BASED ON ACRE	BLDG. (TYPE)
								SQ. M	SQ. FT.	ACRE		
1	DARYL D. & KIM W. REIGEL	120 LANDING RD.	ROXBURY TWP., NJ	07850	5	10020	\$ 930,000.00	44.00	473.61	0.011	\$ 10,111.55	Y (COMMERCIAL)
2	MARZANO ASSOCIATES	101 KINGS HIGHWAY	ROXBURY TWP., NJ	07850	7	10020	\$ 930,000.00	26.00	279.86	0.006	\$ 5,975.01	Y (COMMERCIAL)
3	D & B SHICKEN SHACK	118 LANDING RD.	ROXBURY TWP., NJ	07850	8	10020	\$ 930,000.00	28.40	305.70	0.007	\$ 6,526.55	Y (COMMERCIAL)
4	CUMBERLAND FARMS, INC.	124 LANDING RD.	ROXBURY TWP., NJ	07850	9	10020	\$ 930,000.00	140.70	1,514.48	0.035	\$ 32,333.99	Y (COMMERCIAL)
5	130 LANDING CORP., & RUMORS, INC.	130 LANDING RD.	ROXBURY TWP., NJ	07850	10	10020	\$ 930,000.00	23.50	252.95	0.006	\$ 5,400.49	Y (COMMERCIAL)
6	JOHN R. & JANICE CHAPLIN	132 LANDING RD.	ROXBURY TWP., NJ	07850	11	10020	\$ 930,000.00	21.90	235.73	0.005	\$ 5,032.80	Y (COMMERCIAL)
7	DEAN & RICHARD CURTIS	134 LANDING RD.	ROXBURY TWP., NJ	07850	12	10020	\$ 930,000.00	8.30	89.34	0.002	\$ 1,907.41	Y (COMMERCIAL)
8	RICHARD B. TREACY	146 LANDING RD.	ROXBURY TWP., NJ	07850	13	10020	\$ 930,000.00	19.40	208.82	0.005	\$ 4,458.28	Y (COMMERCIAL)
9	LAKESIDE REALTY ASSOC. C/O J. CINOT	150 LANDING RD.	ROXBURY TWP., NJ	07850	16	10020	\$ 930,000.00	311.70	3,355.11	0.077	\$ 71,631.15	Y (COMMERCIAL)
10	MATTHEW & EDNA VESEL	147 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	25	10101	\$ 460,000.00	230.30	2,478.93	0.057	\$ 26,177.85	Y (RESIDENTIAL)
11	DOMINICK DELEFAVE III	143 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	26	10101	\$ 930,000.00	739.80	7,963.14	0.183	\$ 170,011.96	Y (COMMERCIAL)
12	CHARLES T & HILDE KLINE	139 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	27*	10101	\$ 460,000.00	1,156.10	12,444.16	0.286	\$ 158,600.00	Y (RESIDENTIAL)
13	ERIC FEIGELSON	133 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	28*	10101	\$ 460,000.00	1,396.90	15,036.11	0.345	\$ 405,000.00	Y (RESIDENTIAL)
14	LANDING INVESTMENTS LLC	131 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	29	10101	\$ 930,000.00	120.20	1,293.82	0.030	\$ 27,622.92	Y (COMMERCIAL)
15	THEODORE W. & JOAN R. MILLER	125 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	30	10101	\$ 930,000.00	71.90	773.93	0.018	\$ 16,523.19	Y (COMMERCIAL)
16	NJ TRANSIT		ROXBURY TWP., NJ	07850	31	10101	\$ 930,000.00	4,598.30	49,495.69	1.136	\$ 1,056,726.11	N (RAILROAD)
17	THOMAS VALIANTE	CANAL STREET	ROXBURY TWP., NJ	07850	47*	10101	\$ 930,000.00	1,048.40	11,284.88	0.259	\$ 225,000.00	Y
18	RAYMOND & LINDA VAN HOREBEKE	LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	48	10101	\$ 250,000.00	2,037.40	21,930.39	0.503	\$ 125,863.12	N
19	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49*	10101	\$ 930,000.00	1,427.60	15,366.56	0.353	\$ 390,000.00	Y (COMMERCIAL)
20	LILLIAN DELASANDRO	119 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	2*	10903	\$ 930,000.00	332.95	3,583.83	0.082	\$ 114,200.00	Y
21	THOMAS & JUNE VALIANTE	117 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3	10903	\$ 930,000.00	208.30	2,242.12	0.051	\$ 47,869.01	Y
22	ABRAM LOYD III SNYDER	6 DOVER ST.	ROXBURY TWP., NJ	07850	4	10903	\$ 930,000.00	70.80	762.08	0.017	\$ 16,270.41	Y
23	CYNTHIA & BIAGIO BERDARDO	127 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	6	10903	\$ 930,000.00	27.50	296.01	0.007	\$ 6,319.72	Y
24	B & E LEE ASSOCIATES	109 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1*	10904	\$ 930,000.00	836.72	9,006.41	0.207	\$ 280,000.00	Y
25	DIMDAN ASSOCIATES	105 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3*	10904	\$ 930,000.00	2,419.10	26,038.97	0.598	\$ 616,600.00	Y
26	KIMDAN ASSOICATES C/O SHORTINO	104 LANDING RD.	ROXBURY TWP., NJ	07850	1	10905	\$ 250,000.00	128.80	1,386.39	0.032	\$ 7,956.79	N
27	FIRST FIDELITYc/o FIRST UNION CRE281094	118 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1	11002	\$ 930,000.00	305.90	3,292.68	0.076	\$ 70,298.27	Y (COMMERCIAL)
28	STATE OF NEW JERSEY		ROXBURY TWP., NJ	07850	1	11101	\$ 250,000.00	268.50	2,890.11	0.066	\$ 16,586.95	N
29	NEW JERSEY, STATE OF LABOR/INDUSTRY BLDG.		ROXBURY TWP., NJ	07850	2	11101	\$ 930,000.00	58.50	629.69	0.014	\$ 13,443.77	Y (COMMERCIAL)
								TOTAL R.O.W. TAKING COST =		\$	3,947,188.43	

* DENOTES ENTIRE PARCEL TAKING OR BUILDING DEMOLITION (\$ BASED ON TAX RECORD ASSESSMENT)

Class 1 - New Construction

SUMMARY

Route	Landing Road Bridge	Section/Contract #	ALTERNATIVE NO. 7-C
PM	0	UPC No.	0

Work Type	Totals from other pages
Earthwork	35000
Pavement	371320
Context Sensitive Design	150000
Culverts	0
Bridges	2577708.9
Drainage	161190
Incidental Items	142900
Landscape	52095
Noise Abatement	0
General Items	38475
PROJECT SUBTOTAL	3528688.9

Other Items	Proj. Subtotal Range	Choice	Amount
Lighting, Traffic Stripes, Signs and Delineators		3% of Proj. Subtotal	105860.667
Maintenance of Traffic		1.5% of Proj. Subtotal	52930
Training		1% of Proj. Subtotal	35286.889
Mobilization			317582.001
	Project Cost < 5.0 (Mil.)	9% of Proj. Subtotal	317582
	Project Cost 5.0 & above	10% of Proj. Subtotal	0
Progress Schedule	Project Cost(Mil.)	\$	6000
	Less than 2.0	0	0
	2.0 to 5.0	6,000	6000
	5.0 to 10.0	8,000	0
	10.0 to 20.0	15,000	0
	20.0 to 30.0	30,000	0
	30.0 to 40.0	40,000	0
	40.0 & above	58,000	0
Clearing Site	Project Cost (Mil.)	\$	45000
	Less than 1.0	15,000	0
	1.0 to 2.0	30,000	0
	2.0 to 5.0	45,000	45000
	5.0 to 10.0	115,000	0
	10.0 to 20.0	220,000	0
	20.0 to 30.0	240,000	0
	30.0 to 40.0	250,000	0
	40.0 & above	490,000	0
Construction Layout	Project Cost(Mil.)	\$	42000
	Less than 1.0	7,000	0
	1.0 to 2.0	20,000	0
	2.0 to 5.0	42,000	42000
	5.0 to 10.0	87,000	0
	10.0 to 20.0	160,000	0
	20.0 to 30.0	220,000	0
	30.0 to 40.0	490,000	0
	40.0 & above	890,000	0
PROJECT TOTAL			4133349

Class 1 - New Construction

CONTINGENCIES & ESCALATION

Y

3.00

Y = Number of Years until midpoint of construction duration plus number of years until construction start. If midpoint is less than 2 years from the date of this estimate, no escalation is required. Maximum value = 10%

3.00

4133348.791	1.030	1.04	4427643
Project Total Contingencies (1+C)		1 + [0.01 (Y+1) (Y-2)]	Construction Estimate for Initial

Project Cost(Mil.)	Contingencies (C) Percent	Average Construction Duration in Years
0-10	3%	1
10-20	2.50%	2
20-50	2%	3
Over 50	1.50%	4

0.030
0.000
0.000
0.000

CONSTRUCTION CHANGE ORDER CONTINGENCIES

Total Federal Participating Items in Millions of \$

Construction Change Order Contingency Amount

\$0 to 0.1	\$6,000	0
0.1 to 0.5	25,000	0
0.5 to 5.0	25,000 + 4% of amount in excess of \$500,000	182100
5.0 to 10.0	205,000 + 3% of amount in excess of \$5,000,000	0
10.0 to 15.0	355,000 + 2% of amount in excess of \$10,000,000	0
15.0 and above	455,000 + 1.5% of amount in excess of \$15,000,000 - \$500,000 max	0

For State Funded Projects, Contingencies for Change orders = 0

CHANGE ORDER CONTINGENC = 182100

UTILITIES RELOCATIONS BY COMPANIES/OWNERS

4427643.224	0.09	0
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x % or + Estimate = Utility Relocation Cost for Initial Estimate

Construction Cost for Initial Estimate Use % or utilities detailed estimate

If there are no utility relocations on the project indicate "No Utilities" in the box above.

RIGHT OF WAY COST

If there is no ROW cost on the project indicate "No ROW" the box

SUMMARY

Construction Estimate for Initial	4,427,643
Construction Engineering (CE)	0
Contingencies	182,100
Utilities Relocations	0
Total Construction Cost	4,609,743
Right of Way Cost	0

Project: Landing Bridge Road
 Utility: Summary Estimate
 Alternative: 7-C

Date: 1/14/03
 AmerCom Job #2003

UTILITY	COMPANY	DESCRIPTION	WORK SUMMARY	TOTAL
Electric	GPU	Preliminary Estimated Utility Impacts	5 Relocated Poles, wire	\$230,294
Telephone	Verizon/BA	Preliminary Estimated Utility Impacts	1 Additional Pole, wire	\$265,078
Cable	Comcast	Preliminary Estimated Utility Impacts	415 LM of cable wire	\$238,625
Water	Roxbury Water Co.	Preliminary Estimated Utility Impacts	417 LM of new line	\$202,530

Total	\$936,527
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Project: Landing Bridge Road
 Utility: Electric
 Alternative: 7-C

Date: 4/04/03
 AmerCom Job#1004

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
E1	Electric	Remove old wooden pole	5	UNIT(S)	\$598	\$2,990
E2	Electric	New Pole, 12.2 m high	5	UNIT(S)	\$1,093	\$5,465
E3	Electric	Foundation for line poles, excavation in earth	5	UNIT(S)	\$59	\$295
E4	Electric	Guys, anchors and hardware for pole, in earth	5	UNIT(S)	\$1,323	\$6,615
E5	Electric	Cross arms with hardware and insulators, 1.5 m long	5	UNIT(S)	\$380	\$1,900
E6	Electric	Transformers, Oil Filled, 1000kVA	2	UNIT(S)	\$25,070	\$50,140
E7	Electric	Light Braket Arm	2	UNIT(S)	\$150	\$300
E8	Electric	Manhole	0	UNIT(S)	\$23,000	\$0
E9	Electric	3 Phase , Distrib. 34kV High Line, Primary	204	L.M.	\$566	\$115,464
E10	Electric	3 Phase , Distrib. 12kV	0	L.M.	\$566	\$0
E11	Electric	2 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E12	Electric	1 Phase , Distrib. 12kV	0	L.M.	\$189	\$0
E13	Electric	120/240V Secondary Electrical Overhead	125	L.M.	\$377	\$47,125

Total	\$230,294
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Project: Landing Road Bridge
 Utility: Telephone
 Alternative: 7-C

Date: 4/04/03
 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
T1	Telephone	Remove old wooden pole	1	UNIT(S)	\$598	\$598
T2	Telephone	New Pole, 12.2 m high	1	UNIT(S)	\$1,093	\$1,093
T3	Telephone	Foundation for line poles, excavation in earth	1	UNIT(S)	\$59	\$59
T4	Telephone	Guys, anchors and hardware for pole, in earth	1	UNIT(S)	\$1,323	\$1,323
T5	Telephone	Cross arms with hardware and insulators, 1.5 m long	1	UNIT(S)	\$380	\$380
T6	Telephone	Telephone Wire, 300 Pair	415	L.M.	\$575	\$238,625
T7	Telephone	Fiber Optic Cable	0	L.F.	\$575	\$0
T8	Telephone	Manhole	1	UNIT(S)	\$23,000	\$23,000

Total	\$265,078
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Project: Landing Road Bridge
 Utility: Telephone
 Alternative: 7-C

Date: 4/04/03
 AmerCom Job#1011

PAY ITEM NO.	UTILITY	DESCRIPTION	QUANTITY	UNITS	UNIT PRICE	TOTAL
C1	Cable	Cable Wire	415	L.M.	\$575	\$238,625

Total	\$238,625
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LANDING ROAD BRIDGE - AMERCOM #1011

Replacement Alternative (Alternative 7-C)

Right-Of-Way Acquisitions

TABLE 1 - SUMMARY MATRIX

PARCEL #	LOCATION / OWNER NAME	STREET	CITY, NJ	ZIP	LOT	BLOCK	RATABLE \$ / Ac.	ROW TAKING AREA (appx.)			\$ BASED ON		BLDG. (TYPE)
								SQ. M	SQ. FT.	ACRE	ACRE		
A	MARZANO ASSOCIATES	101 KINGS HIGHWAY	ROXBURY TWP., NJ	07850	7	10020	\$ 930,000.00	19.00	204.51	0.005	\$ 4,366.35	Y (COMMERCIAL)	
B	D & B SHICKEN SHACK	118 LANDING RD.	ROXBURY TWP., NJ	07850	8	10020	\$ 930,000.00	3.10	33.37	0.001	\$ 712.40	Y (COMMERCIAL)	
C	CUMBERLAND FARMS, INC.	124 LANDING RD.	ROXBURY TWP., NJ	07850	9	10020	\$ 930,000.00	173.30	1,865.39	0.043	\$ 39,825.73	Y (COMMERCIAL)	
D	EMEDIN & MARY RIVERA	100 KINGS HIGHWAY	ROXBURY TWP., NJ	07850	6	10021	\$ 930,000.00	10.80	116.25	0.003	\$ 2,481.93	Y (COMMERCIAL)	
E	THEODORE W. & JOAN R. MILLER	125 LEDGE-LANDING RD.	ROXBURY TWP., NJ	07850	30	10101	\$ 460,000.00	924.70	9,953.39	0.228	\$ 105,109.24	Y (VACANT)	
F	NJ TRANSIT		ROXBURY TWP., NJ	07850	31	10101	\$ 930,000.00	488.00	5,252.79	0.121	\$ 112,146.30	N (RAILROAD)	
G	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49*	10101	\$ 930,000.00	211.80	2,279.80	0.052	\$ 48,673.33	Y (COMMERCIAL)	
H	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49	10101	\$ 250,000.00	0.00	0.00	0.000	\$ -	N	
I	WEICHERT REALTORS	101 LANDING RD.	ROXBURY TWP., NJ	07850	49	10101	\$ 250,000.00	0.00	0.00	0.000	\$ -	N	
J	CYNTHIA & BIAGIO BERDARDO	127 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	6	10902	\$ 460,000.00	27.50	296.01	0.007	\$ 3,125.88	Y (RESIDENTIAL)	
K	LILLIAN DELASANDRO	119 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	2*	10903	\$ 230,000.00	332.95	3,583.83	0.082	\$ 114,200.00	Y (RESIDENTIAL)	
L	THOMAS & JUNE VALIANTE	117 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3	10903	\$ 460,000.00	206.70	2,224.90	0.051	\$ 23,495.27	Y (RESIDENTIAL)	
M	ABRAM LOYD III SNYDER	6 DOVER ST.	ROXBURY TWP., NJ	07850	4	10903	\$ 230,000.00	70.80	762.08	0.017	\$ 4,023.86	Y (RESIDENTIAL)	
N	B & E LEE ASSOCIATES	109 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1*	10904	\$ 930,000.00	836.72	9,006.41	0.207	\$ 280,000.00	Y (COMMERCIAL)	
O	KIMDAN ASSOCIATES	105 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	3*	10904	\$ 930,000.00	2419.10	26,038.97	0.598	\$ 616,600.00	Y (COMMERCIAL)	
P	KIMDAN ASSOCIATES	104 LANDING RD.	ROXBURY TWP., NJ	07850	1	10905	\$ 250,000.00	73.50	791.15	0.018	\$ 4,540.56	N	
Q	BELL ATLANTIC CORP/PROPERTY TAX # 3137	128 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	7	11001	\$ 930,000.00	43.60	469.31	0.011	\$ 10,019.63	Y (COMMERCIAL)	
R	FIRST FIDELITYc/o FIRST UNION CRE281094	118 LAKESIDE BLVD.	ROXBURY TWP., NJ	07850	1	11002	\$ 930,000.00	277.50	2,986.99	0.069	\$ 63,771.72	Y (COMMERCIAL)	
S	STATE OF NEW JERSEY		ROXBURY TWP., NJ	07850	1	11101	\$ 250,000.00	246.80	2,656.53	0.061	\$ 15,246.40	N	
T	NEW JERSEY, STATE OF LABOR/INDUSTRY BLDG.		ROXBURY TWP., NJ	07850	2	11101	\$ 930,000.00	20.10	216.35	0.005	\$ 4,619.14	Y (COMMERCIAL)	
* DENOTES ENTIRE PARCEL TAKING (\$ BASED ON TAX RECORD ASSESSMENT)								TOTAL R.O.W. TAKING COST =		\$ 1,461,288.99			

Information 973-448-2000
 Construction 973-448-2009
 Building 973-448-2034
 Court 973-448-2018
 Engineer 973-448-2012
 Fire Official 973-448-2028
 Health 973-448-2002
 Manager 973-448-2001
 Mayor & Council 973-448-2100
 Police

Township of Roxbury
 1715 Route 46
 Ledgewood, New Jersey 07852

Planning/Zoning 973-448-2008
 Public Works 973-448-2069
 Recreation 973-448-2015
 Tax Assessor 973-448-2021
 Tax Collector 973-448-2022
 Technology 973-448-2099
 Township Clerk 973-448-2001
 Treasurer 973-448-2006
 Wastewater Treatment Plant 973-398-2818
 Zoning Officer 973-448-2013

Number-15-6
 Page 1 of 4
 Property Location:
 Route 631/Landing Rd

Date: February 6, 2015
 To: RBA Group Inc
 7 Campus Dr., Suite 300
 Parsippany, NJ 07054-4495


NOTICE: Every property owner listed, as well as all utility companies listed (see attached) must be served either by personal service, registered or certified mail. **If an adjoining municipality is included within the search area, that Township Clerk must be notified.** These notices must be mailed or delivered at least ten days before the meeting. Affidavit of Service must be notarized by a Notary Public of New Jersey and submitted to the Clerk of the Board at least five days before the meeting.

Block/Lot Name/Address (as indicated on accompanying lists)

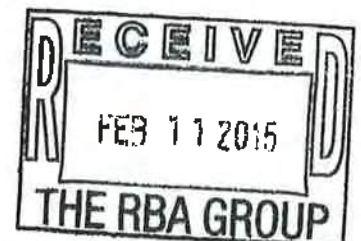
In addition, the agencies checked (X) below must also be served by personal service, registered or certified mail:

- X Morris County Planning Board, PO Box 900, Morristown, NJ 07963-0900
- X NJ Dept. of Transportation, 200 Stierli Ct, Mt Arlington, NJ 07856
- X NJDEP, Division of Land Use Regulations, MC501-02A, PO Box 420, Trenton NJ 08625-0420
- X NJ Transit, c/o Energy Solve-UBAR-14, PO Box 6077, Somerset, NJ 08873-6077
- X Morris County DOT, PO Box 900, Morristown, NJ 07963-0900
- X Chester Branch Co., PO Box 404, Succasunna, NJ 07876
- X Conrail, 6 Penn Center Plaza, Philadelphia, PA 19103
- X Adjacent Municipality of

I hereby certify that I have compiled the above list from current tax duplicates of the Township of Roxbury to the best of my ability and that said list complies with 13-2.507-7 (c) of the Land Use Ordinance.



 Joseph J. McKeon, CTA
 Tax Assessor
 Township of Roxbury
 Received: Check#241



OWNER & ADDRESS REPORT

ROXBURY TOWNSHIP

02/06/15 Page 1 of 2

BLOCK	LOT	QUAL	CLA	PROPERTY OWNER	PROPERTY LOCATION	Add'l Lots
10020	6		2	RIVERA, EMEDIN/MARY 111 KINGS HWY LANDING, NJ 07850	111 KINGS HWY	
10020	7.1010		4A	MARZANO ASSOCIATES 25 SHERWOOD DR MOUNTAIN LAKES, NJ 07046	101A KINGS HWY	
10020	7.2010		4A	LESSIG, DOUGLAS 25 SUNSET LN LANDING, NJ 07850	101B KINGS HWY	
10020	7.3010		4A	LESSIG, DOUGLAS 25 SUNSET LN LANDING, NJ 07850	201C KINGS HWY	
10020	8		4A	D & B CHICKEN SHACK 118 LANDING RD LANDING, NJ 07850	118 LEDGE-LANDING RD	
10020	5		4A	LANDING PROPERTIES LLC 120 LANDING RD LANDING, NJ 07850	120 LEDGE-LANDING RD	
10020	9		4A	TJ PROPERTIES LLC 124 LANDING RD LANDING, NJ 07850	124 LEDGE-LANDING RD	
10020	10		4A	MEGAMAC, LLC 130 LANDING RD LANDING, NJ 07850	130 LEDGE-LANDING RD	
10020	11		4A	GATES REAL ESTATE HOLDING, LLC 132 LANDING RD LANDING, NJ 07850	132 LEDGE-LANDING RD	
10020	14		1	MEGAMAC, LLC 130 LANDING RD LANDING, NJ 07850	RIGGS AVE, LAND	
10021	5		4A	SCANTLEBURY, ARTHUR B 112 KINGS HWY LANDING, NJ 07850	112 KINGS HWY	
10021	6		4A	SHIN, YOUNG S/KYOUNG I 102 LANDING RD LANDING, NJ 07850	100 KINGS HWY	
10101	28		4A	133 LANDING ROXBURY TWP LLC 6 DUMONT PL, 3RD FLOOR MORRISTOWN, NJ 07960	133 LEDGE-LANDING RD	
10101	29		4A	LANDING INVESTMENTS LLC 131 LANDING RD LANDING, NJ 07850	131 LEDGE-LANDING RD	
10101	29	B01	4A	LANDING INVESTMENTS LLC 131 LANDING RD LANDING, NJ 07850	131 LEDGE-LANDING RD	
10101	30		4A	LAKE HOPATCONG FOUNDATION 37 NOLAN'S POINT PARK RD LAKE HOPATCONG, NJ 07849	125 LEDGE-LANDING RD	
10101	48		1	GEDICKE'S MARINE INC 91 MT ARLINGTON BLVD LANDING, NJ 07850	MT ARLINGTON BLVD	
10101	49		4A	101 LANDING COMPANY, LLC 74 COMANCHE AVE ROCKAWAY, NJ 07866	101 LEDGE-LANDING RD	
10905	1		1	4C'S LAKEFRONT LODGING, LLC 107 TULSA TRL HOPATCONG, NJ 07843	104 LEDGE-LANDING RD	

OWNER & ADDRESS REPORT

ROXBURY TOWNSHIP

02/06/15 Page 2 of 2

BLOCK	LOT	QUAL	CLA	PROPERTY OWNER	PROPERTY LOCATION	Add'l Lots
10904	1		4A	N R & R REALTY LLC 334 MEADOWBROOK RD WYCKOFF, NJ 07481	109 LAKESIDE BLVD	
10904	2		2	ENGLISH, ELLEN P 111 DOVER ST LANDING, NJ 07850	111 DOVER ST	
10904	3		4A	4C'S LAKEFRONT LODGING, LLC 107 TULSA TRL HOPATCONG, NJ 07843	105 LAKESIDE BLVD	
10903	1		2	BROWN/MCCOY, ANTIONETTE N/RANDOLPH 121 LAKESIDE BLVD LANDING, NJ 07850	121 LAKESIDE BLVD	
10903	2		2	DIBLASI, THOMAS 174 MT ARLINGTON BLVD LANDING, NJ 07850	119 LAKESIDE BLVD	
10903	3		4A	LAKESIDE REALTY, LLC 117 LAKESIDE BLVD LANDING, NJ 07850	117 LAKESIDE BLVD	
10903	4		2	SNYDER, ABRAM LLOYD III 6 DOVER ST LANDING, NJ 07850	6 DOVER ST	
10902	5		4A	BAYVIEW LOAN SERVICING LLC 4425 PONCE DE LEON BLVD CORAL GABLE, FL 33146	131 LAKESIDE BLVD	
10902	6		2	MILLER/MEADE, EDWARD J/ANDREIA S 125-127 LAKESIDE BLVD LANDING, NJ 07850	127 LAKESIDE BLVD	
10902	7		15F	BEARS, WAHNETA 333 BOONTON ST LANDING, NJ 07850	333 BOONTON ST	
11101	1		15F	NEW JERSEY, STATE OF LABOR/INDUSTRY BLDG, CN229 TRENTON, NJ 08625	LAKESIDE BLVD	
11101	2		4A	GEDICKE'S MARINE INC 91 MT ARLINGTON BLVD LANDING, NJ 07850	91 MT ARLINGTON BLVD	
11002	1		4A	FIRST STATES INVESTORS 3108, LLC PO BOX 167129 IRVINGTON, TX 75016	118 LAKESIDE BLVD	
11001	5		4A	RF-HOPATCONGPROPERTY MANAGEMENT, LLC 136 LAKESIDE BLVD LANDING, NJ 07850	136 LAKESIDE BLVD	
11001	6		4A	MEADES/HELLER, ALAN R/JOHN PO BOX 700 MADISON, NJ 07940	130 LAKESIDE BLVD	
11001	7		4A	BELL ATLANTIC CORP/PROPERTYTAX#3137 PO BOX 152206 IRVING, TX 75015	128 LAKESIDE BLVD	
10901	12		4B	ABE SNYDER CO INC 6 DOVER ST LANDING, NJ 07850	LAKESIDE BLVD, REAR	
10101	27		2	KLINE, HILDE W/ROBIN R 139 LANDING RD LANDING, NJ 07850	139 LEDGE-LANDING RD	
10020	7			Riggs Lake		
10101	31			Railroad		
9801	4			Railroad		
11501	1			Lake Hopatcong		

UTILITIES SERVING THE TOWNSHIP OF ROXBURY
ALL UTILITIES MUST BE NOTIFIED

AT&T Legal Department
Attn: Network Counsel
340 Mount Kemble Ave
Morristown, NJ 07960
(Re: 10110944)

Cablevision of Morris
Attn: Bruce Hickson or John Bartels
683 Route 10 E
Randolph, NJ 07869
(973 659-2470)

Columbia Gas Transmission Corp.
Attn: Robert Schini
1470 Poorhouse Rd
Dowington, Pa 19335
(610 518-3547)
(800 835-7191) Emergency only

JCP&L
13 Richboynton Rd
Dover, NJ 07801
(973 537-2600) Vicki Thiel

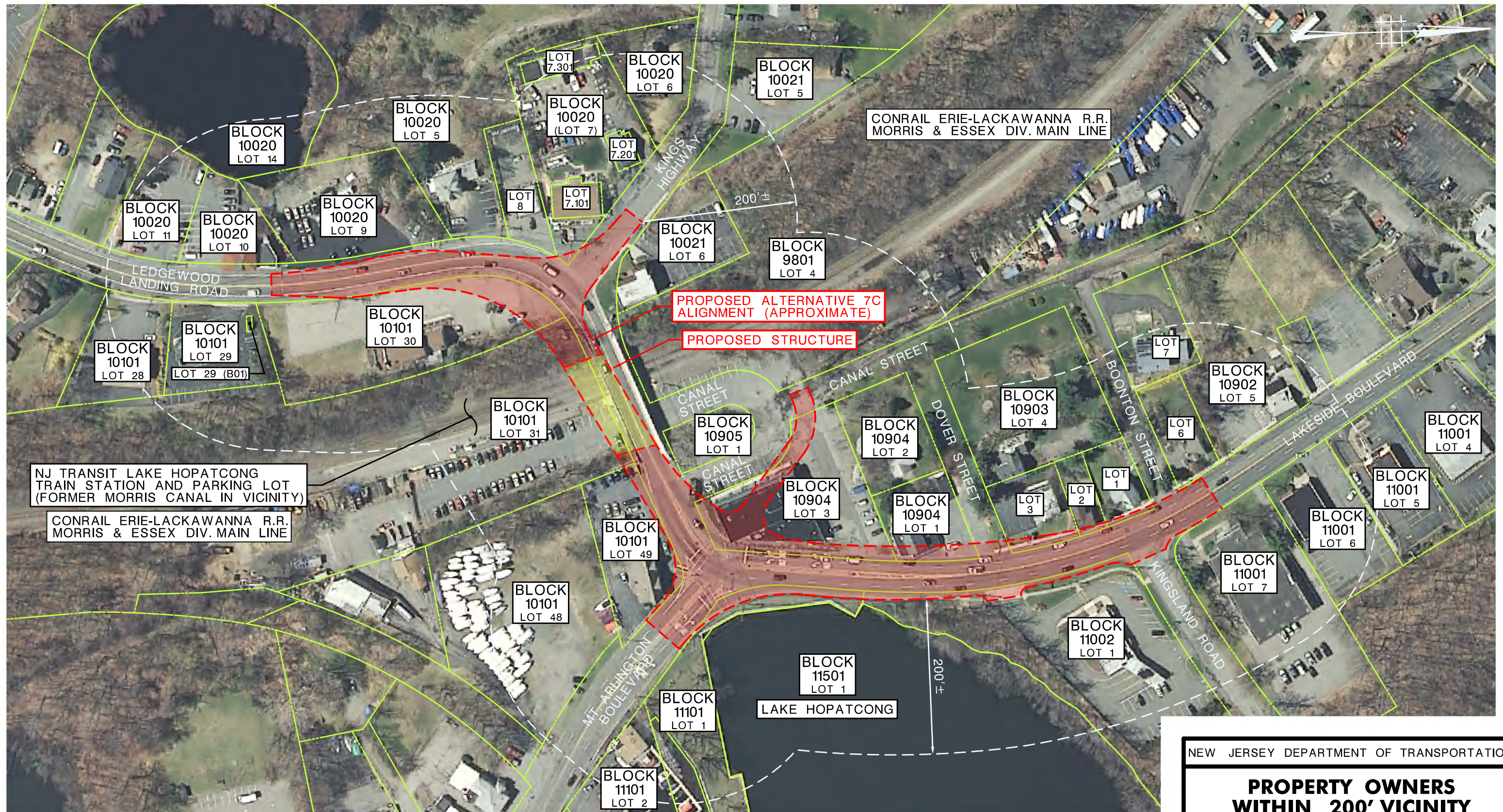
Musconetcong Sewerage Authority
110 Continental Dr
Budd Lake, NJ 07828
(973 347-1525) James Schilling
jschilling@msa-nj.org

NJ Natural Gas Co
Attn: Matthew Puzio
1415 Wyckoff Rd
PO Box 1464
Wall, NJ 07719
(732 938-1030)

Roxbury Water Co
Attn: John Hosking
79 Sunset Strip
PO Box 560
Succasunna, NJ 07876
(973 584-4118)

Township of Roxbury
1715 Route 46
Water & Sewer Dept.
c/o Engineering Dept
Ledgewood, NJ 07852
(973 448-2018)

Verizon Communications
Attn: Richard Flint
290 W Mt Pleasant Ave
Bldg 4-Ground Floor
Livingston, NJ 07039
(973 422-5132)



NJ TRANSIT LAKE HOPATCONG TRAIN STATION AND PARKING LOT (FORMER MORRIS CANAL IN VICINITY)

CONRAIL ERIE-LACKAWANNA R.R. MORRIS & ESSEX DIV. MAIN LINE

- NOTES:
1. SOURCE OF PROPOSED ALTERNATIVE DATA:
REPLACEMENT ALTERNATIVE 7-C: LANDING ROAD BRIDGE NO. 1400-073 OVER NEW JERSEY TRANSIT & THE MORRIS CANAL (PARSON - 2006)
- ALIGNMENT DATA PROVIDED BY GRAPHIC, LOCATION ON PLAN APPROXIMATE
 2. SOURCE OF AERIAL IMAGERY AND TAX PARCELS:
NEW JERSEY OFFICE OF INFORMATION TECHNOLOGY - 2012 (NJOIT)
 3. BLOCK AND LOT NUMBERS PROVIDED BY ROXBURY TOWNSHIP TAX MAPS

NEW JERSEY DEPARTMENT OF TRANSPORTATION

**PROPERTY OWNERS
WITHIN 200' VICINITY**

MORRIS COUNTY ROUTE 631 LANDING ROAD
BRIDGE REPLACEMENT
STRUCTURE NO. 1400-073
ROXBURY TOWNSHIP, MORRIS COUNTY



DRAFT : 02-03-2015

Classification No. 5 - BRIDGE REPAIR

Route	Landing Road Bridge	Section/Contract #	Rehabilitation Alternative
PM		UPC No.	

PAVEMENT

3.6 M WIDE LANE (from subgrade up)

Pav't. Type	Description of Pavement	Cost/Linear Meter
A	250 mm R.C. Pavement	510
B	50 mm HMA Surf. Crs. & 200 mm HMA Base	200
C	75 mm HMA Surf. Crs. & 100 mm HMA Base	150
D	50 mm HMA Surf. Crs. & 50 mm HMA Base	70
E	Bridge Approach & Transition Slabs	510
	(Resurfacing Portion only F & G)	
F	50 mm HMA Surface Course	27
G	75 mm HMA Surface Course	40
H	Milling 50 mm	10

Computation Table for Pavement. Cost

Type	Cost from table above	x Length	x Pavement *W.F.	= Amount
				0
A	510	41	2	41820
E	510	35	2	35700
H	10	40	2	800
F	27	40	2	2160
				0
				0
				0
				0
PAVEMENT TOTAL			=	80480

*Width Factors = Ratio of 3.6 meter wide lane to actual pavement width.

Example = actual pavement width = 7.5 meters = 7.5/3.6 = 2.05 W.F.

INCIDENTAL ITEMS

Item	Cost / L.M.	x Quantity	= Amount
Beam Guide Rail	55	80	4400
Fence 1.8 Meter High	60	80	4800
225 mm X 400 mm Conc. Vertical	45	160	7200
375mm X 1040 mm Conc. Barrier	165	0	0
600mm X 1040 mm Conc. Barrier	240	0	0
600mm X Variable Conc. Barrier	150	0	0
INCIDENTAL ITEMS TOTAL	=		16400

BRIDGES

Cost to be provided by the Bureau of Structural Engineering

TOTAL = 750000

Class 5 - Bridge Repair

SUMMARY

Route	Landing Road Bridge	Section/Proj. Id. #	Rehabilitation Alternative
PM		0 UPC No.	0

Work Type	Totals from other pages
Pavement	80480
Incidental Items	16400
Bridges	750000
PROJECT SUBTOTAL	846880

Other Items	Proj. Subtotal Range	Choice	Amount
Lighting, Traffic Stripes, Signs and Delineators		1% of Proj. Subtotal	8468.8
Maintenance of Traffic		7% of Proj. Subtotal	59282
Training		1% of Proj. Subtotal	8468.8
Mobilization	Project Cost (Mil.)	% of Proj. Subtotal	67750
	Less than 1.0	8.00%	67750
	1.0 to 5.0	5.00%	0
	5.0 & above	5.00%	0
Clearing Site	Project Cost (Mil.)	\$	2000
	Less than 1.0	2,000	2000
	1.0 & above	3,000	0
Construction Layout	Project Cost (Mil.)	\$	4000
	Less than 1.0	4,000	4000
	1.0 & above	6,000	0
PROJECT TOTAL			996850

CONTINGENCIES & ESCALATION

Y

Y = Number of Years until midpoint of construction duration plus number of years until construction start. If midpoint is less than 2 years from the date of this estimate, no escalation is required. Maximum value = 10%

3.00

3.00

996849.6	1.030	1.04	1067825
Project Total Contingencies (1+C)		1 + [0.01 (Y+1) (Y-2)]	Construction Estimate for Initial

Project Cost (Mil.)	Contingencies (C) Percent	Average Construction Duration in Years
0-20	3%	1
Over 20	2.0%	2

0.030
0.000

CONSTRUCTION ENGINEERING (CE)

Project Cost (Mil.)	% of Construction Cost
Less than 1.0	14.90%
1.0 to 5.0	12.20%
5.0 to 10.0	10.80%
10.0 & above	9.50%

0
130275
0
0

CONSTRUCTION ENGINEERING AMOUNT \$130,274.69

Class 5 - Bridge Repair

CONSTRUCTION CHANGE ORDER CONTINGENCIES

Total Federal Participating Items in Millions of \$	Construction Change Order Contingency Amount	
\$0 to 0.1	\$6,000	0
0.1 to 0.5	25,000	0
0.5 to 5.0	25,000 + 4% of amount in excess of \$500,000	47700
5.0 to 10.0	205,000 + 3% of amount in excess of \$5,000,000	0
10.0 to 15.0	355,000 + 2% of amount in excess of \$10,000,000	0
15.0 and above	455,000 + 1.5% of amount in excess of \$15,000,000 - max \$500,000	0
		0
For State Funded Projects, Contingencies for Change orders = 0		
<u>CHANGE ORDER CONTINGENC</u>	=	47700

UTILITIES RELOCATIONS BY COMPANIES/OWNERS

1067825	0.085	90765
Construction Cost for Initial Estimate	Use 8.5% or utilities detailed estimate	Utility Relocation Cost for Initial Estimate

If there are no utility relocations on the project indicate "No Utilities" in the box above.

RIGHT OF WAY COST

If there is no ROW cost on the project indicate "No ROW" the box

SUMMARY

Construction Estimate for Initial	1067825
Construction Engineering (CE)	130275
Contingencies	47700
Utilities Relocations	90765
Total Construction Cost	1336565
Right of Way Cost	0

APPENDIX – D

RESOLUTION OF SUPPORT

Township of Roxbury
1715 Route 46
Ledgewood, New Jersey 07852

INFORMATION 448-2000
CONSTRUCTION/BLOG 448-2009
COURT 448-2034
ENGINEER 448-2018
FIRE OFFICIAL 448-2012
HEALTH 448-2028
MANAGER 448-2002
MAYOR & COUNCIL 448-2001
POLICE 448-2100

PLANNING/ZONING 448-2008
PUBLIC WORKS 448-2089
RECREATION 448-2015
TAX ASSESSOR 448-2021
TAX COLLECTOR 448-2022
TOWNSHIP CLERK 448-2001
TREASURER 448-2006
WASTEWATER TREATMENT PLANT 564-5380
ZONING OFFICER 448-2013

April 16, 2003

Handwritten signature/initials

Surinder K. Thapar, P.E., P.P.
Assistant County Engineer
Morris County Department of Public Works
P.O. Box 900
Morristown, NJ 07963

Dear Mr. Thapar:

Enclosed please find a certified copy of Resolution 03-131 "A RESOLUTION SUPPORTING THE REPLACEMENT OF LANDING ROAD BRIDGE (ALTERNATIVE 7-C)", which was adopted by the Township Council on April 14, 2003.

If you have any questions, please feel free to contact my office.

Very truly yours,

TOWNSHIP OF ROXBURY

Janet Rieth
BettyLou DeCroce *for BLD*
Township Clerk

/jr

Enclosure

clerk\janet\reslcty

RECEIVED
COUNTY OF MORRIS
APR 21 2003
ENGINEERING DEPARTMENT

RESOLUTION NO. 03 - 131**A RESOLUTION SUPPORTING THE REPLACEMENT OF
LANDING ROAD BRIDGE (ALTERNATIVE 7-C)**

WHEREAS, the Morris County Department of Public Works studied the feasible options to improve the structural capacity and traffic safety of the Landing Road Bridge over New Jersey Transit and the Morris Canal in the Township of Roxbury; and

WHEREAS, the Township Council previously adopted Resolution No. 98-314 on October 6, 1998 which supported the proposed alignment #7, which specified the replacement of the existing bridge at its current alignment subject to conditions as specified in said resolution; and

WHEREAS, Morris County and their Engineering Consultant, Parsons Transportation Group, Inc. have developed alternatives to address these conditions; and

WHEREAS, the Engineering/Public Works Department has met with Morris County and Parsons to review the alternatives; and

WHEREAS, this department agrees with the preferred alternative 7-C as contained in their April 14, 2003 presentation package; and

WHEREAS, the Township Engineering/Public Works Department recommends adoption of a resolution supporting alternative 7-C as the preferred alternative for the Landing Road Bridge Replacement Project.

NOW, THEREFORE, BE IT RESOLVED, by the Township Council, of the Township of Roxbury, in the County of Morris and State of New Jersey that support is hereby established for the replacement of the Landing Road Bridge (Alignment 7-C).

BE IT FURTHER RESOLVED, that a signed copy of this resolution be provided to the Township Engineer/Public Works Director, Police Chief, Historical Advisory Committee, and Morris County Department of Public Works Administrative & Records Bldg., P.O. Box 900, Morristown, New Jersey 07963, Attention: Surinder K. Thapar, P.E., P.P., Assistant County Engineer.

ADOPTED: Apr 11 14, 2003


Betty Lou DeCroce
Township Clerk

MAK.bja.ResReplacLandingRdBridgeAlter7-C.41003

RESOLUTION NO. 2015 – 310

**A RESOLUTION SUPPORTING THE REPLACEMENT OF
LANDING ROAD BRIDGE (ALTERNATIVE 7-C)**

WHEREAS, the Morris County Department of Public Works studied the feasible options to improve the structural capacity and traffic safety of the Landing Road Bridge over the New Jersey Transit and the Morris Canal in the Township of Roxbury; and

WHEREAS, the Township Council previously adopted Resolution No. 03-131 on April 14, 2003 which supported the proposed alignment #7-3, which specified the replacement of the existing bridge at its current alignment subject to conditions as specified in said resolution; and

WHEREAS, the Engineering/Public Works Department has met with Morris County to review the alternatives; and

WHEREAS, this department agrees with the preferred alternative 7-C as contained in their June 9, 2015 presentation package; and

WHEREAS, the Township Engineering/Public Works Department recommends adoption of a resolution supporting alternative 7-C as the preferred alternative for the Landing Road Bridge Replacement Project; and

WHEREAS, the Alternative was presented to Mayor and Council on June 9, 2015; and

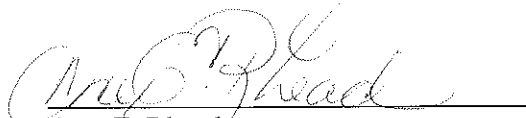
WHEREAS, the alternative was presented to the public by the Morris County Engineering Department on August 12, 2015.

NOW, THEREFORE, BE IT RESOLVED by the Township Council, of the Township of Roxbury, in the County of Morris and State of New Jersey that support is hereby established for the replacement of the Landing Road Bridge (Alignment 7-C).

BE IT FURTHER RESOLVED, that a certified copy of this resolution be provided to County of Morris Department of Planning & Public Works, Division of Engineering & Transportation, Attention: Roslyn Khurdan, PO Box 900, Morristown, NJ 07960-0900 and a copy be provided to Michael Kobylarz, Township Engineer/Director of Utilities; Richard Blood, Director of Public Works; Chief Simonetti, Police Department; Historical Advisory Committee.

ADOPTED: September 29, 2015

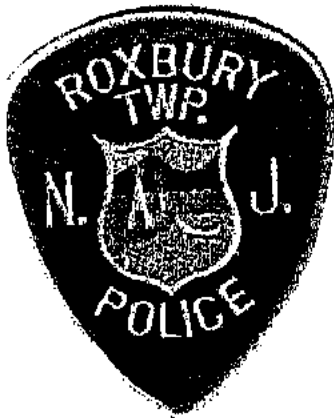
Attest:



Amy E. Rhead
Township Clerk

APPENDIX – E

ACCIDENT DATA (2011)
CEEKGPV'FCVC'*4236+'



Roxbury Township Police Department

1715 State Highway Route 46
 Ledgewood, New Jersey, 07852
 (973) 448-2100 Main
 (973) 448-2081 Fax

FAX TRANSMITTAL

To: Augusto Molina

From: Sgt. Dan Murray RTPD

Date: 2/14/12

Ref: LANDING RD MVC STATS 2005-2011

Pages: 2 (Including cover page)

Comments:
SEND E-MAIL RECEIPT CONFIRMATION.
THANKS
DMURRAY@ROXBURYPD.COM

Sgt. Daniel Murray
 Roxbury Twp. Police Dept.
 1715 State Hwy. Rt. 46
 Ledgewood, NJ 07852

01/16/2012

Augusto V. Molino
 100 Broadway
 New York, NY 10005

Mr. Molino,

At your request, motor vehicle crash statistics have been compiled for Landing Road, specifically in the area of the Landing Road Bridge. Some of the data is unavailable due to non-specific recordation of those statistics for the location you requested. The statistics that are available for this location give a reasonable idea of the amount of motor vehicle crashes that occur in this area on a yearly basis.

Total MVC's on Landing Road (from Rt. 46 to Lakeside Blvd.)

<u>2005</u>	<u>2006</u>	<u>2007</u>	<u>2008</u>	<u>2009</u>	<u>2010</u>	<u>2011</u>
53	45	54	34	54	32	48

Number of those MVC's occurring on Landing Rd. between Hart St. and intersection with Lakeside Blvd. and Mt Arlington Blvd. (including the Landing Rd. Bridge)

<u>2005</u>	<u>2006</u>	<u>2007</u>	<u>2008</u>	<u>2009</u>	<u>2010</u>	<u>2011</u>
17	*	*	16	*	12	24

(* - unable to locate specific data for this area of Landing Rd.)

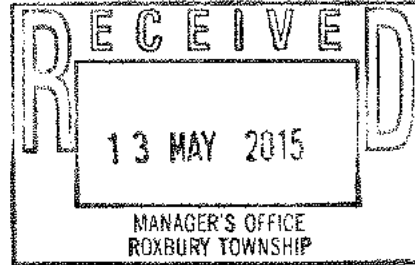
Should I be of any further assistance with your project feel free to contact me via e-mail or phone at (973) 946-5016.

Regards,

Sgt. Daniel Murray
 Roxbury Twp. Police Dept.

Memo

To: Sgt. D. Murray
From: Ptl. M Fournier
Date: May 13, 2015
Re: Breakdown of Landing Rd crashes



Sgt Murray,

As you requested, here is a list of crashes that occurred in 2014 on Landing Rd in the particular areas you asked:

Landing Rd between Hart St and the Landing Intersection: 16

Landing Rd at Hart St intersection: 2

Landing Rd at Kings Hwy intersection: 0

Landing Rd between Hart St and Kings Hwy: 12

Landing Intersection (Lakeside Blvd, Mt Arlington Blvd, and Landing Rd): 3

Landing Rd between Kings Hwy and Landing Intersection: 4

This information is being forwarded to the town manager along with copies of all crash reports from Landing Rd in 2014, including crashes that occurred south of Hart St.

Respectfully,

Ptl. M. Fournier #193

1 Case Number 2014-020750	10 Crash Occurred On: LAKESIDE BLVD	11 Speed Limit 35	118a 29
2 Police Dept of Code ROXBURY TWP 01	30 Road Name LAKESIDE BLVD	12 Route No. Suffix N 0631 A	118b --
3 Station/Precinct	14 <input type="checkbox"/> At Intersection with <input checked="" type="checkbox"/> N <input type="checkbox"/> E <input checked="" type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles <input type="checkbox"/> 16	13 Milepost 35	119a --
4 Date of Crash mm dd yy 09/07/14	5 Day of Week SUN	17 Cross Road Name LANDING ROAD	119b --
6 Time (use 2400 hrs) 1338	7 Municipality Code 1436	18 Speed Limit 35	120 --
8 Total Killed 00	9 Total Injured 00	19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB	121 --
23 Veh No 01	24 Policy No. F1360505	25 Ins Code 008	122 --
26 Driver's First Name Initial Last Name ANISSA D NAVARRO F			56 Driver's First Name Initial Last Name ANISSA D NAVARRO F
27 Number and Street 610 DURBAN AVENUE			57 Number and Street 610 DURBAN AVENUE
28 City State Zip HOPATCONG NJ 07843-1017			58 City State Zip HOPATCONG NJ 07843-1017
31 State NJ	32 Driver's License No N09070456452954	33 DOB mm dd yy 02/02/95	34 Expires mm yy 08/16
35 Owner's First Name Initial Last Name <input type="checkbox"/> Same As Driver JORGE L NAVARRO			65 Owner's First Name Initial Last Name <input type="checkbox"/> Same As Driver JORGE L NAVARRO
36 Number and Street 610 DURBAN AVE			66 Number and Street 610 DURBAN AVE
37 City State Zip HOPATCONG NJ 07843-1017			67 City State Zip HOPATCONG NJ 07843-1017
38 Make HON	39 Model CIV	40 Color BG	41 Year 2004
42 Plate No. REV33H		43 State NJ	
44 VIN 2HGES16554H576050			45 Expires 04/15
46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed DRIVER			47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police
48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0.00 % <input type="checkbox"/> Pending			78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending
49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.			79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.
50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other			80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other
51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs			81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs
52 Carrier name			82 Carrier name
135 Crash Description Veh. 1 was negotiating a left hand turn from Landing Road onto Lakeside Blvd. Driver 1 stated she was in the right hand lane and a vehicle bearing NJ Reg. S74EFF was positioned in the left. As the two vehicles negotiated the turn, the vehicle from the left lane, crossed into the right lane, causing Veh. 1 to run off the roadway and strike the curb to avoid collision. The Veh. bearing NJ Reg. S74EFF left the scene. The registered owner of the vehicle leaving the scene is Bernice J. Ryan, 120 W. 27th St. Bayonne NJ 07002. The only known phone number for Ryan, (201) 438-3957, is disconnected. The driver of Veh. 1 sustained a flat front right tire and the tire was changed at the scene. Driver 1 and the vehicle in question never made contact. The occupants in Veh. 1 refused medical attention.			

136 Damage To Other Property NONE			
137 Charge <input type="checkbox"/> Multiple Charges	138 Summons No.	139 Charge <input type="checkbox"/> Multiple Charges	140 Summons No.
141 Officer's Signature WEAVER, SCOTT <i>[Signature]</i>	142 Badge No. 225	143 Reviewed By PALANCHI, MARC <i>[Signature]</i>	144 Case Status OPEN

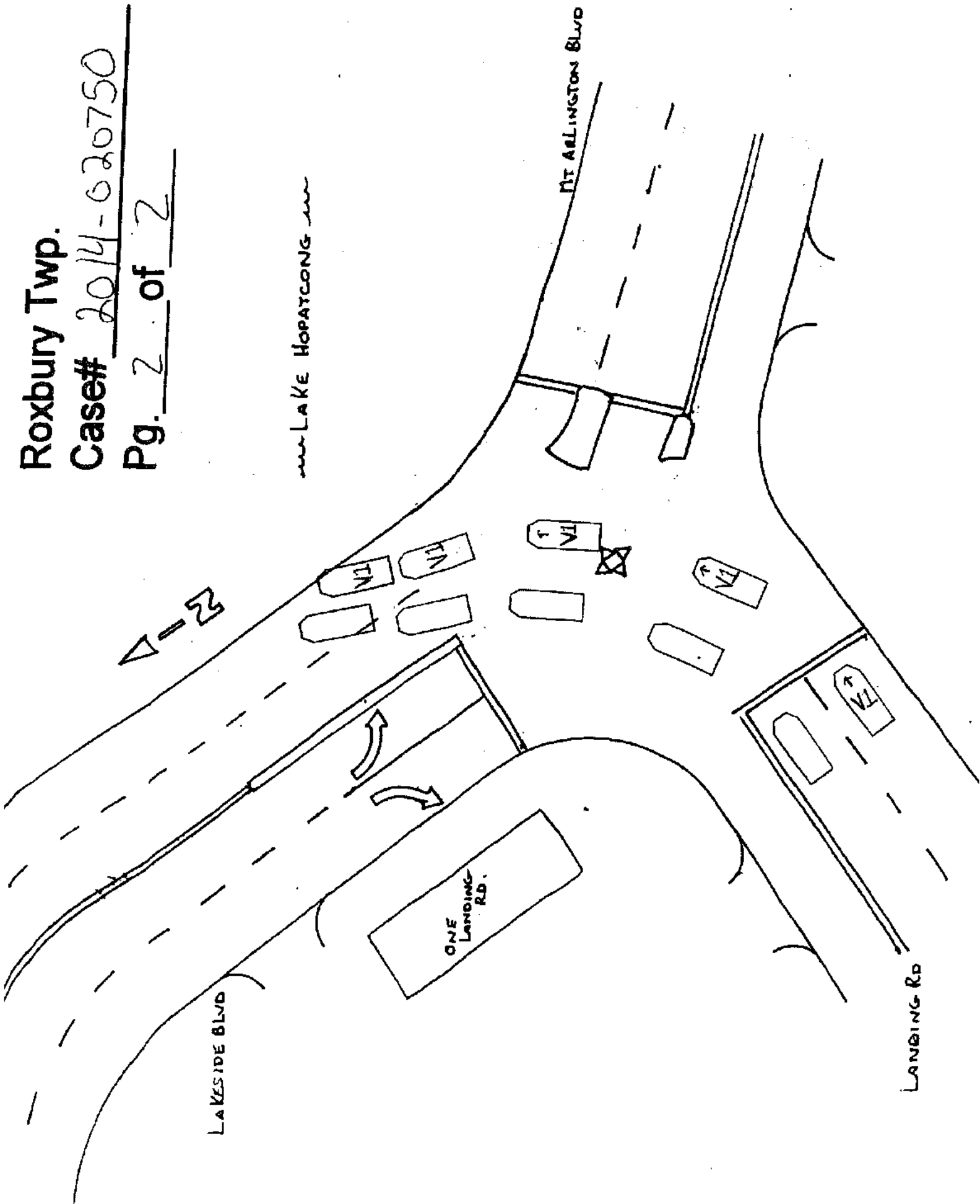
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	19	F	--	--	01	09	04	--	--	NAVARRO, ANISSA D, 610 DURBAN AVENUE, HOPATCONG, NJ 07843-1017
B	1	03	01	--	22	M	--	--	01	09	04	--	--	HERNANDEZ, RICARDO A, 275 PAULISON AVE APT-A4, PASSAIC, NJ 07055-3823
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

Roxbury Twp.

Case# 2014-020750

Pg. 2 of 2

Lake Hopatcong



New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

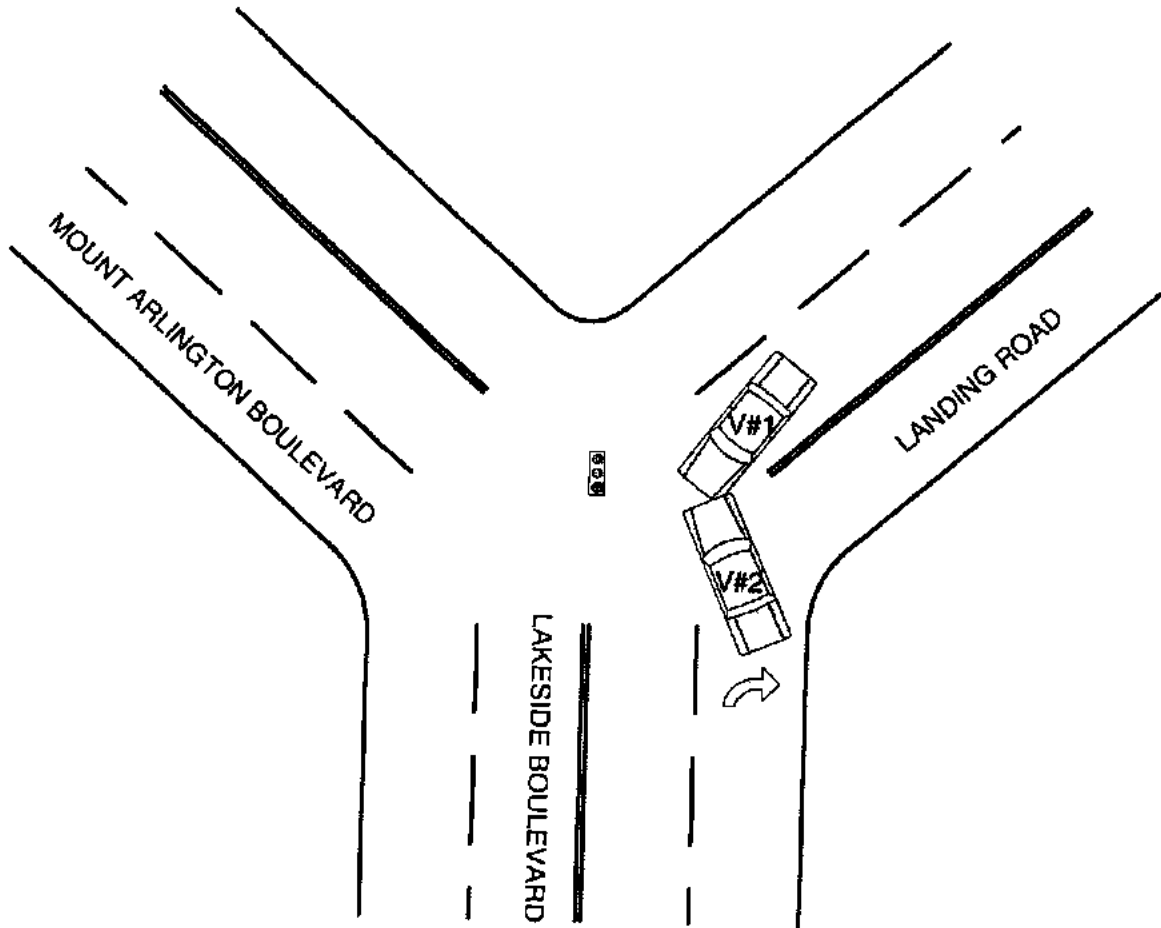
05	Page 1 of 1 <input type="checkbox"/> Fatal		New Jersey Police Crash Investigation Report										<input checked="" type="checkbox"/> Reportable <input type="checkbox"/> Non-Reportable <input type="checkbox"/> Change Report																
01	1 Case Number 2014-028801			10 Crash Occurred On: LAKESIDE BOULEVARD					11 Speed Limit S 35 0831 A 1.30			118a 25																	
02	2 Police Dept of ROXBURY TWP			14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles <input type="checkbox"/> 16					12 Route No. 18 Milepost 36			118b																	
07	3 Station/Princt ---			15					17 Cross Road Name LANDING ROAD			119a 03																	
06	4 Date of Crash mm dd yy 12/09/14		5 Day of Week TUE		6 Time (use 2400 hrs) 1942		7 Municipality Code 1436		8 Total Killed ---		9 Total Injured ---		19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB		119b 08														
02	23 Veh No 01		24 Policy No. 16926043			25 Ins Code 135		53 Veh No 02		54 Policy No. F2381977			55 Ins Code 426		120 01														
02	26 Driver's First Name WAYNE			Initial E		Last Name HARRIS		29 Sex M		56 Driver's First Name BRIAN			Initial W		Last Name KINCAID		59 Sex M		121 01										
02	27 Number and Street 204 IDALROY TRL			State NJ		Zip 07843-1243		30 Eyes 2		57 Number and Street 16 MAPLE LANE			State NJ		Zip 07856-1387		60 Eyes 4		122 00										
02	28 City HOPATCONG			State NJ		Zip 07843-1243		58 City MT ARLINGTON		State NJ		Zip 07856-1387		61 State NJ		62 Driver's License No K44560968607614		63 DOB mm dd yy 07/29/61		64 Expires mm yy 05/16		123 00							
03	31 State NJ			32 Driver's License No H06707795601512			33 DOB mm dd yy 01/11/51		34 Expires mm yy 04/17		61 State NJ			62 Driver's License No K44560968607614			63 DOB mm dd yy 07/29/61		64 Expires mm yy 05/16		124 01								
02	35 Owner's First Name WAYNE			Initial E		Last Name HARRIS		65 Owner's First Name BRIAN		Initial W		Last Name KINCAID		66 Number and Street 204 IDALROY TRL		67 City HOPATCONG		State NJ		Zip 07843-1243		125 03							
02	36 Number and Street 204 IDALROY TRL			State NJ		Zip 07843-1243		66 Number and Street 16 MAPLE LANE		67 City MT ARLINGTON		State NJ		Zip 07856-1387		68 Make SUB		69 Model LEG		70 Color SLV		71 Year 2002		72 Plate No. R31BLW		73 State NJ		126 03	
01	38 Make SUB			39 Model LEG		40 Color SLV		41 Year 2002		42 Plate No. R31BLW		43 State NJ		68 Make TOY		69 Model RAV		70 Color BK		71 Year 2014		72 Plate No. U72EUB		73 State NJ		127 26			
04	44 VIN 4S3BE636426207172			45 Expires 10/15		46 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input checked="" type="checkbox"/> Towed		47 Authority <input type="checkbox"/> Impound <input type="checkbox"/> Disabled <input checked="" type="checkbox"/> Police		74 VIN 2T3RFREV7E212363			75 Expires 10/17		76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input checked="" type="checkbox"/> Towed		77 Authority <input type="checkbox"/> Impound <input type="checkbox"/> Disabled <input checked="" type="checkbox"/> Police		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		128a 26				
01	46 Vehicle Removed To RON AND SONS TOWING			47 Authority <input checked="" type="checkbox"/> Police		76 Vehicle Removed To ATLANTIC TOWING			77 Authority <input checked="" type="checkbox"/> Police		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other'		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		128b ---						
01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending			49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>			Name or Placard No.		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other'		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		128c ---														
01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other'			51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs			52 Carrier name		136 Damage To Other Property		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.		128d ---										
01	141 Officer's Signature SYLVESTER, JOHN			142 Badge No. 205		143 Reviewed By PALANCHI, MARC			Badge No. 138		144 Case Status OPEN		128e ---																

D#1 stated he turned left from Landing Road north onto Lakeside Boulevard north after observing a solid green light when V#2 turned in front of him striking his vehicle in the front. D#2 stated he was in the right turn only lane on Lakeside Boulevard heading south and decided to suddenly turn left onto Mount Arlington Boulevard. D#2 advised he did not check the traffic signal and believes he went through a red light. In effect, D#2 did not obey the traffic signal and completed an improper left turn from a right turn only lane. Both vehicles were left disabled and were towed from the scene. No injuries were reported.

136	Damage To Other Property				
Oper.	137 Charge <input type="checkbox"/> Multiple Charges	138 Summons No.	Oper.	139 Charge <input type="checkbox"/> Multiple Charges	140 Summons No.
141	Officer's Signature SYLVESTER, JOHN		142	Badge No. 205	
			143	Reviewed By PALANCHI, MARC	
				Badge No. 138	
				144 Case Status OPEN	

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death	
A	1	01	01	--	63	---	--	--	01	04	04	--	--	HARRIS, WAYNE E. 204 IDALROY TRL, HOPATCONG, NJ 07843-1243	
B	2	01	01	--	63	---	--	--	01	04	04	--	--	KINCAID, BRIAN W. 16 MAPLE LANE, MT ARLINGTON, NJ 07856-1387	
C	--	--	--	--	--	--	--	--	--	--	--	--	--		
D	--	--	--	--	--	--	--	--	--	--	--	--	--		
E	--	--	--	--	--	--	--	--	--	--	--	--	--		

ROXBURY TOWNSHIP
POLICE DEPARTMENT
PTL J. SYLVESTER #205
CASE #2014-028801
DECEMBER 9, 2014



01	1 Case Number 2014-003047	10 Crash Occurred On: LANDING RD	11 Speed Limit 38	12 Route No. 0631	13 Milepost A	118a	26		
01	2 Police Dept of ROXBURY TWP	Code 01	Road Name NORTH	Dir 38	Suffix A	118b	--		
01	3 Station/Precinct	14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles <input type="checkbox"/> 16	15	16	17 Cross Road Name LAKESIDE BLVD	18 Speed Limit 36	118c	--	
04	4 Date of Crash mm dd yy 02/10/14	5 Day of Week MON	6 Time (use 2400 hrs) 1447	7 Municipality Code 1436	8 Total Killed 00	9 Total Injured 00	119c	--	
01	23 Veh No 01	24 Policy No. 4243-49-00-02	25 Ins Code 148	53 Veh No	54 Policy No.	55 Ins Code	120	07	
01	26 Driver's First Name DOONNA	Initial L	Last Name SPYCHAJ	26 Sex F	56 Driver's First Name	Initial	Last Name	56 Sex	
01	27 Number and Street 6 GROVE STREET	30 Eyes 4	57 Number and Street	60 Eyes					
01	28 City STANHOPE	State NJ	Zip 07874-2101	58 City	State	Zip	122	--	
01	31 State NJ	32 Driver's License No S71171747382694	33 DOB mm dd yy 12/21/69	34 Expires mm yy 09/14	61 State	62 Driver's License No	63 DOB mm dd yy	64 Expires mm yy	
16	35 Owner's First Name <input type="checkbox"/> Same As RICHARD	Initial	Last Name SPYCHAJ	65 Owner's First Name <input type="checkbox"/> Same As Driver	Initial	Last Name	125	--	
01	36 Number and Street 6 GROVE STREET	66 Number and Street							
01	37 City STANHOPE	State NJ	Zip 07874-2101	67 City	State	Zip	126	03	
01	38 Make MER	39 Model VIL	40 Color MN	41 Year 2006	42 Plate No. E16CLV	43 State NJ	68 Make	69 Model	
01	44 VIN 4M2XV14T3YDJ22893	45 Expires 02/14	74 VIN	75 Expires					
01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police	76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police					
01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending					78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending	128d	--	
01	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	Name or Placard No.					79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	Name or Placard No.	
01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other					80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	129c	--	
13	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs					81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	129d	--	
01	52 Carrier name					82 Carrier name	130	03	
01	135 Crash Description							131	02

D1 STATES SHE WAS TRAVELING NORTH ON LANDING RD AND AS SHE APPROACHED LAKESIDE BLVD SHE BLACKED OUT AND HIT A DOT SIGN WITH HER PASSENGER SIDE MIRROR AND STOPPED JUST BEFORE THE CONCRETE WALL AT LAKE HOPATCONG. D1 STATED THAT SHE LEFT WORK EARLY BECAUSE SHE WAS SICK AND DID VOMIT TWICE WHILE I WAS ON SCENE. D1 REFUSED MEDICAL ATTENTION AND WAS NOT INJURED.

136 Damage To Other Property
NONE (DOT SIGN NOT DAMAGED)

137 Charge Multiple Charges 138 Summons No. Oper. 139 Change Multiple Charges 140 Summons No.

141 Officer's Signature
DENICOLA, THOMAS

142 Badge No.
207

143 Reviewed By
MURRAY, DANIEL

Badge No.
159

144 Case Status
CLOSED

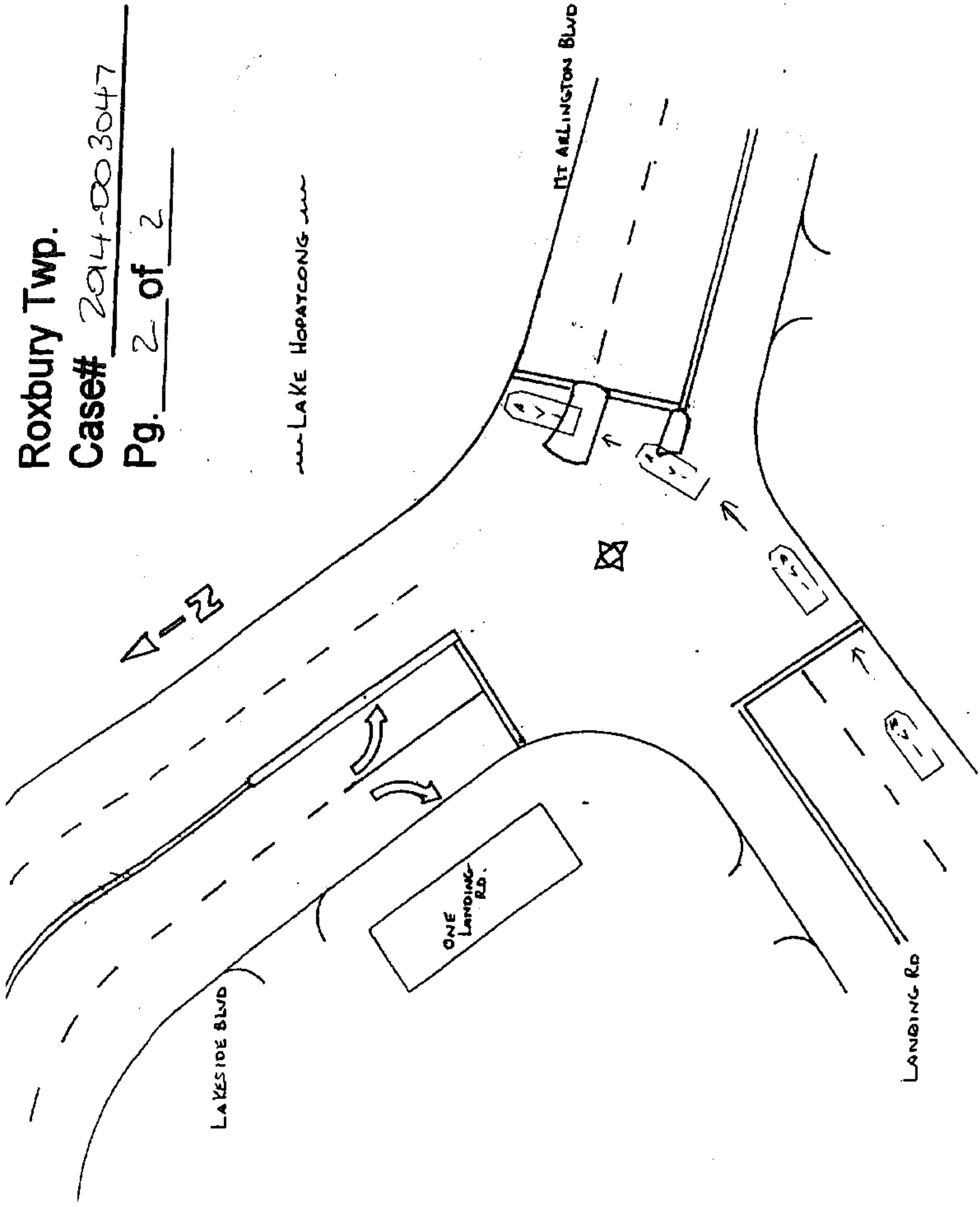
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	64	F	--	--	01	09	04	--	--	SPYCHAJ, DONNA L, 6 GROVE STREET, STANHOPE, NJ 07874-2101
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

Roxbury Twp.

Case# 2014-003047

Pg. 2 of 2

LAKE HOPATCONG



07 1 Case Number **2014-010221** 10 Crash Occurred On: **LANDING RD** NORTH **36** 11 Speed Limit **0631 A .90** 118a **25**
 08 2 Police Dept of **ROXBURY TWP** Code **01** 14 At Intersection with N E S W of: **MITCHELL ROAD** 12 Route No. Suffix 13 Milepost **25** 118b **-**
 09 3 Station/Precinct 15 Feet Miles 16 17 Cross Road Name 18 Speed Limit **25** 119a **02**
 100 4 Date of Crash **06/07/14** 5 Day of Week **WED** 6 Time (use 2400 hrs) **1716** 7 Municipality Code **1436** 8 Total Killed **0** 9 Total Injured **0** 19 Ramp To: From: 20 Route/Name NB EB SB WB 119b **-**
 04 21 Latitude 22 Longitude 120 **-**

101 23 Veh No **01** 24 Policy No. **31383721-2** 25 Ins Code **134** 53 Veh No **02** 54 Policy No. **PAA00001412907** 55 Ins Code **963** 120 **01**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY DNT & RUN PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY DNT & RUN 121 **01**

102 26 Driver's First Name Initial Last Name 28 Sex 56 Driver's First Name Initial Last Name 58 Sex 102 **01**
JOSEPH R FEOLA M **MICHAEL J NICOTRA M** 122 **-**
 27 Number and Street **105 PAKATING RD** 29 Eyes **2** 57 Number and Street **104 HUDSON AVE** 60 Eyes **5**

103 28 City State Zip 58 City State Zip 123 **-**
HOPATCONG NJ 07843 **HOPATCONG NJ 07843-1708**

104 31 State 32 Driver's License No 33 DOB 34 Expires 61 State 62 Driver's License No 63 DOB 64 Expires 124 **06**
NJ F26064107901832 01/26/83 03/16 NJ N41226447108725 06/10/72 06/17

105 35 Owner's First Name Initial Last Name 65 Owner's First Name Initial Last Name 125 **07**
 Same As Driver **JOSEPH R FEOLA** Same As Driver **MICHAEL J NICOTRA**

106 36 Number and Street 66 Number and Street 126 **04**
105 PAKATING RD 104 HUDSON AVE

107 37 City State Zip 67 City State Zip 127 **04**
HOPATCONG NJ 07843-1444 HOPATCONG NJ 07843-1708

108 38 Make 39 Model 40 Color 41 Year 42 Plate No. 43 State 68 Make 69 Model 70 Color 71 Year 72 Plate No. 73 State 128a **26**
HYU SON RD 2011 YXB88Z NJ FOR ESC GY 2008 RED91D NJ

109 44 VIN 45 Expires 74 VIN 75 Expires 128b **-**
6NPEC4AC48H306603 06/14 1FMCU93Z98KB66089 06/14

110 46 Vehicle Removed To Driven Left at Scene Towed 47 Authority Owner Driver Police 76 Vehicle Removed To Driven Left at Scene Towed 77 Authority Owner Driver Police 128c **-**
 Impound Disabled Impound Disabled

111 48 Alcohol/Drug Test Given: No Yes Refused 78 Alcohol/Drug Test Given: No Yes Refused 128d **-**
 Type: Breath Blood Urine Type: Breath Blood Urine 129a **26**
 Results: 0. % Pending Results: 0. % Pending

112 49 Hazardous Material On Board Spill Name or Placard No. 79 Hazardous Material On Board Spill Name or Placard No. 129b **-**
 129c **-**

113 50 Carrier No. USDOT Other' 80 Carrier No. USDOT Other' 129d **-**

114 51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 130 **06**
 52 Carrier name 82 Carrier name 131 **06**

117 135 Crash Description 132 **12**
Investigation revealed that both vehicle #1 and #2 were traveling Landing Road north bound in stop and go traffic. Driver #2 was at fault as he admitted to striking the rear of vehicle #1. Vehicle #1 received minor damage as vehicle #2 reported no damage. No injuries were reported.
Patrol cleared. 133 **12**

136 Damage To Other Property **NONE STATED NONE OBSERVED**
 137 Charge Multiple Charges 138 Summons No. Oper. 139 Charge Multiple Charges 140 Summons No.

141 Officer's Signature **BECK, WILLIAM** 142 Badge No. **134** 143 Reviewed By **PALANCHI, MARC** Badge No. **138** 144 Case Status **CLOSED**

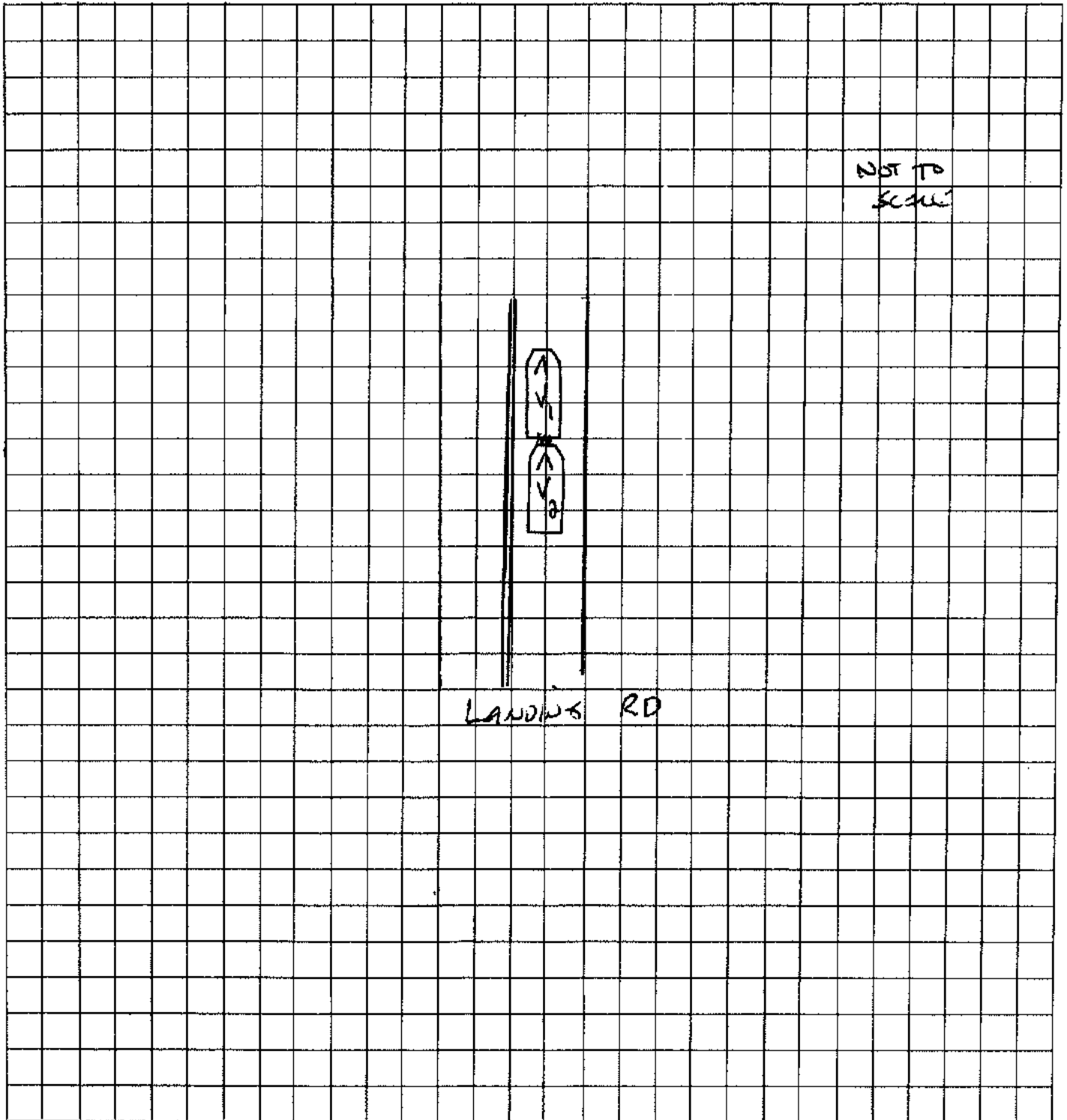
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	-	31	M	-	-	-	09	04	-	-	FEOLA, JOSEPH R, 105 PAKATING RD, HOPATCONG, NJ 07843
B	1	01	01	-	29	F	-	-	-	09	04	-	-	REIS, RACHAL R, 105 PAKATING RD, HOPATCONG, NJ 07843
C	1	06	01	-	1	M	-	-	-	05	05	-	-	FEOLA, AIDEN, 105 PAKATING RD, HOPATCONG, NJ 07843
D	2	01	01	-	41	M	-	-	-	09	04	-	-	NICOTRA, MICHAEL J, 104 HUDSON AVE, HOPATCONG, NJ 07843-1708
E	-	-	-	-	-	-	-	-	-	-	-	-	-	

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: Roxbury PD Code: 1436
Station: _____ Case No: 14-010221

134 Crash Diagram (NOT TO SCALE)



[Signature]
Officer's Signature

134
Badge Number

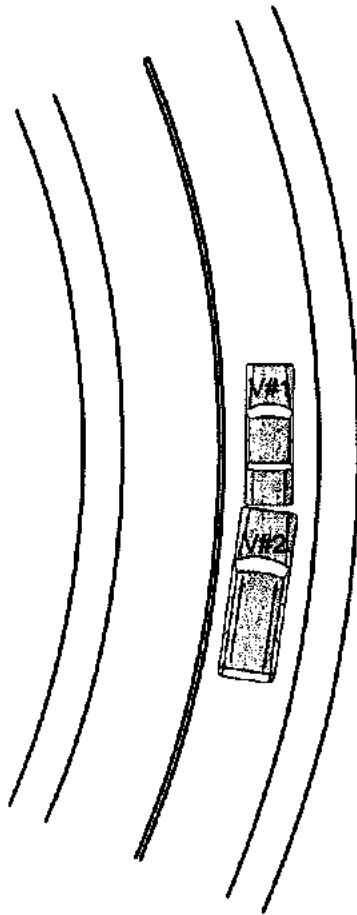
New Jersey Police Crash Investigation Report

Reportable
 Non-Reportable
 Change Report

96	Page 1 of 1 <input type="checkbox"/> Fatal										
97	1 Case Number 2014-011550		10 Crash Occurred On: LANDING ROAD				11 Speed Limit N 36 0631 A		13 Milepost 25		118a
98	2 Police Dept of ROXBURY TWP		Code 01		200 Road Name MITCHELL AVENUE		12 Route No. Suffix 18		13 Milepost 25		118b
99	3 Station/Precinct -		14 <input type="checkbox"/> At Intersection with <input checked="" type="checkbox"/> N <input type="checkbox"/> E		15 <input checked="" type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W		16 <input type="checkbox"/> Miles		17 Cross Road Name		119a
100	4 Date of Crash mm dd yy 05/23/14		5 Day of Week FRI		6 Time (use 2400 hrs) 1612		7 Municipality Code 1436		8 Total Killed -		119b
101	9 Total Injured -		19 Ramp <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB		20 Route/Name		21 Latitude		22 Longitude		119c
102	23 Veh No 01		24 Policy No. F0218265		25 Ins Code 426		53 Veh No 02		54 Policy No. Y464339D1030M		120
103	26 Driver's First Name FRANCISCO		Initial K		Last Name ESTEVEZ		29 Sex M		56 Driver's First Name SUSAN		121
104	27 Number and Street 4 TEMPLE TRAIL		30 Eyes Z		57 Number and Street 141 LAKESIDE BLVD		59 Sex F		60 Eyes 4		122
105	28 City HOPATCONG		State NJ		Zip 07843-1738		58 City HOPATCONG		State NJ		123
106	31 State NJ		32 Driver's License No E80672670011832		33 DOB mm dd yy 11/05/63		34 Expires mm yy 03/17		61 State NJ		124
107	35 Owner's First Name <input type="checkbox"/> Same As JOVANKA		Initial K		Last Name ESTEVEZ		65 Owner's First Name <input type="checkbox"/> Same As WAYNE		Initial W		125
108	36 Number and Street 4 TEMPLE TRAIL		66 Number and Street 141 LAKESIDE BLVD		67 City HOPATCONG		State NJ		Zip 07843-1423		126
109	37 City HOPATCONG		State NJ		Zip 07843-1738		68 Make HYU		69 Model ELA		127
110	38 Model TOY		39 Color BK		41 Year 2007		42 Plate No. YJS60K		43 State NJ		128a
111	44 VIN JTMBD33V676029832		45 Expires 11/14		74 VIN KMH4U46D47U137772		75 Expires 10/14				128b
112	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		77 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police				128c
113	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		128d
114	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.				129a
115	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other						129b
116	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs						129c
117	52 Carrier name				82 Carrier name						129d
118	135 Crash Description										132
119	<p>D#1 stated he was stopped in traffic when V#2 struck his vehicle from behind. D#2 stated she was stopped in traffic behind V#1 when her foot slipped off of the brake. D#2 advised her vehicle accelerated forward and struck V#1 in the rear. D#1 advised he had back pain and was evaluated by Roxbury Company #2 First Aid. D#1 signed an RMA refusing further medical attention after being evaluated. Both vehicles sustained minor damage and were driven from the scene.</p>										133
120	136 Damage To Other Property										134
121	Oper.		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		Oper.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.
122	141 Officer's Signature SYLVESTER, JOHN				142 Badge No. 205		143 Reviewed By PALANCHI, MARC		Badge No. 138		144 Case Status CLOSED

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	04	60	M	06	08	01	04	04	--	--	ESTEVEZ, FRANCISCO, 4 TEMPLE TRAIL, HOPATCONG, NJ 07843-1738
B	2	01	01	--	63	F	--	--	01	04	04	--	--	SHAW, SUSAN, 141 LAKESIDE BLVD, HOPATCONG, NJ 07843-1423
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

ROXBURY TWP POLICE
DEPT
CASE #2014-011550
PTL J.SYLVESTER #205
05/23/2014



LANDING ROAD



1 Case Number: **2014-000364** 10 Crash Occurred On: **LANDING ROAD** 11 Speed Limit: **35** 12 Route No.: **0631** 13 Milepost: **1.10**
 2 Police Dept of: **ROXBURY TWP** Code: **01** 14 At Intersection with N E S W of: **KINGS HIGHWAY** 16 Speed Limit: **35**
 3 Station/Princt: **ROXBURY** 15 Feet Miles 16 Ramp Front: To: NB EB SB WB
 4 Date of Crash: **01/06/14** 5 Day of Week: **MON** 6 Time (use 2400 hrs): **1234** 7 Municipality Code: **1436** 8 Total Killed: **--** 9 Total Injured: **--** 17 Cross Road Name: **---** 18 NB EB SB WB
 20 Roll#/Name: **---** 21 Latitude: **---** 22 Longitude: **---**

23 Veh No: **01** 24 Policy No.: **909716482 04/04** 25 Ins Code: **064** 53 Veh No: **02** 54 Policy No.: **S1767504** 55 Ins Code: **205**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN HIT & RUN

26 Driver's First Name: **HERMAN** Initial: **H** Last Name: **DAVIDOVIC** 28 Sex: **M** 56 Driver's First Name: **RONALD** Initial: **R** Last Name: **RIEDEL** 59 Sex: **M**
 27 Number and Street: **134 LAKESIDE BLVD APT 12** 30 Eyes: **B** 57 Number and Street: **10 BROADWAY** 60 Eyes: **2**

28 City: **HOPATCONG** State: **NJ** Zip: **07843-1461** 58 City: **HOPATCONG** State: **NJ** Zip: **07843-1239**

31 State: **NJ** 32 Driver's License No.: **D09133270003495** 33 DOB: **03/26/48** 34 Expires: **01/13** 61 State: **NJ** 62 Driver's License No.: **R41686687104612** 63 DOB: **04/11/81** 64 Expires: **04/14**

35 Owner's First Name: **HERMAN** Initial: **H** Last Name: **DAVIDOVIC** 65 Owner's First Name: **RJ RIEDEL LANDSCAPE** Initial: **R** Last Name: **RIEDEL LANDSCAPE LLC**

36 Number and Street: **134 LAKESIDE BLVD APT 12** 66 Number and Street: **---**

37 City: **HOPATCONG** State: **NJ** Zip: **07843-1461** 67 City: **---** State: **---** Zip: **---**

38 Make: **CHE** 39 Model: **COL** 40 Color: **WT** 41 Year: **2004** 42 Plate No.: **LE67RH** 43 State: **NJ** 68 Make: **FOR** 69 Model: **F25** 70 Color: **WT** 71 Year: **2005** 72 Plate No.: **XD212L** 73 State: **NJ**

44 VIN: **1GCCS148748132161** 45 Expires: **09/14** 74 VIN: **1FTSX21696EC26866** 75 Expires: **06/14**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending

49 Hazardous Material On Board: Spill: Name or Plicard No.: **---** 79 Hazardous Material On Board: Spill: Name or Plicard No.: **---**

50 Carrier No.: USDOT Other 60 Carrier No.: USDOT Other

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 25,000 lbs > 25,001 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 25,000 lbs > 25,001 lbs

52 Carrier name: **---** 82 Carrier name: **---**

135 Crash Description: **V1 was traveling South on Landing road and began to slow for traffic in the area of Landing Road / Kings Highway. V2 was directly behind V1 also traveling South. V2 hit V1 from behind as traffic slowed. D2 stated he was not paying attention to the traffic in front of him. V2 at fault for the accident.**

136 Damage To Other Property: **---**

Oper. **---** 137 Charge: Multiple Charges 138 Summons No.: **---** Oper. **---** 139 Charge: Multiple Charges 140 Summons No.: **---**

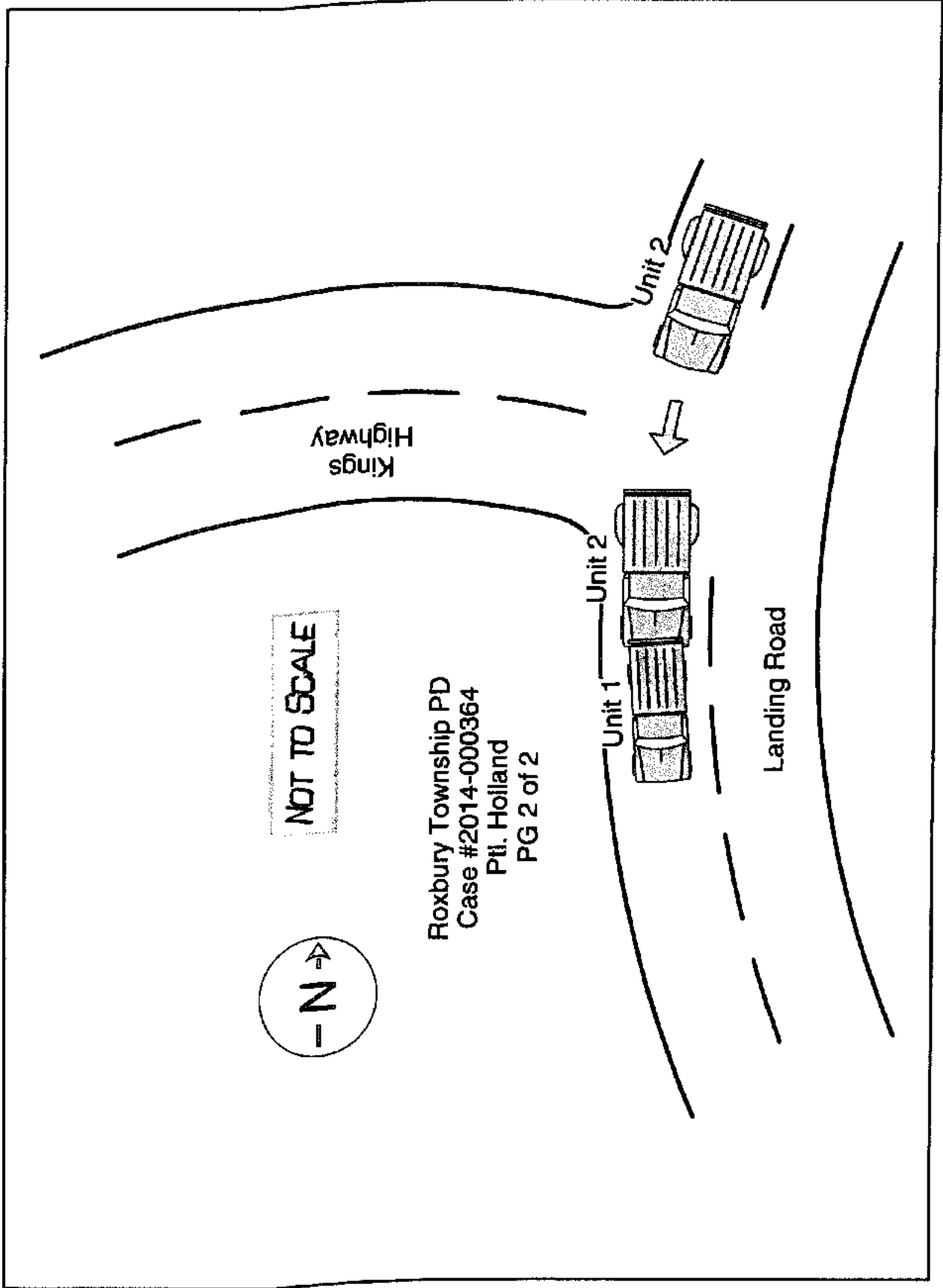
141 Officer's Signature: **HOLLAND, MATTHEW** 142 Badge No.: **203** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	64	M	--	--	--	09	04	--	--	DAVIDOVIC, HERMAN, 134 LAKESIDE BLVD APT 12, HOPATCONG, NJ 07843-1461
B	2	01	01	--	52	M	--	--	--	09	04	--	--	RIEDEL, RONALD J, 10 BROADWAY, HOPATCONG, NJ 07843-1239
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	



NOT TO SCALE

Roxbury Township PD
Case #2014-000364
Pti. Holland
PG 2 of 2



New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

04	Page 1 of 1 <input type="checkbox"/> Fatal		New Jersey Police Crash Investigation Report										<input checked="" type="checkbox"/> Reportable <input type="checkbox"/> Non-Reportable <input type="checkbox"/> Change Report													
01	1 Case Number 2014-011260		10 Crash Occurred On: 208 LANDING RD				11 Speed Limit 36				0631		A		70		118a	26								
01	2 Police Dept of ROXBURY TWP		Code 01		Road Name 100				Dir 12		Routes No. 18		Suffix 26		13 Milepost 26		118b	--								
01	3 Station/Precinct 14		<input checked="" type="checkbox"/> Feet		<input type="checkbox"/> N <input type="checkbox"/> E		<input type="checkbox"/> S <input type="checkbox"/> W		of HART STREET				17 Cross Road Name		<input type="checkbox"/> NB <input type="checkbox"/> EB		119a	02								
01	4 Date of Crash 05/20/14		5 Day of Week TUE		6 Time (use 2400 hrs) 1827		7 Municipality Code 1436		8 Total Killed 0		9 Total Injured 0		19 Ramp To: <input type="checkbox"/> From: 20		20 Route/Name <input type="checkbox"/> SB <input type="checkbox"/> WB		119b	--								
01	23 Veh No 01		24 Policy No. 4082-78-60-56		25 Ins Code 148		53 Veh No 02		54 Policy No. AOJ-238-284935-4030		55 Ins Code 090		<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN		<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN		120	01								
01	26 Driver's First Name DEBRA		Initial J		Last Name PRELL		29 Sex F		56 Driver's First Name DEBRA		Initial L		Last Name GINSBERG		58 Sex F		121	01								
01	27 Number and Street 114 WINDSOR AVE		State NJ		Zip 07843-1291		30 Eyes 4		57 Number and Street 8201 HAMPTON COURT		State NJ		Zip 07843-1545		60 Eyes 2		122	--								
01	28 City HOPATCONG		State NJ		Zip 07843-1291		58 City RANDOLPH		State NJ		Zip 07843-1545		61 State NJ		62 Driver's License No. G46181697359662		63 DOB 09/07/60		64 Expires 10/17		123	--				
01	31 State NJ		32 Driver's License No. P73321697164704		33 DOB 04/21/70		34 Expires 06/17		61 State NJ		62 Driver's License No. G46181697359662		63 DOB 09/07/60		64 Expires 10/17		124	01								
01	35 Owner's First Name DEBRA		Initial J		Last Name PRELL		65 Owner's First Name DEBRA		Initial L		Last Name GINSBERG		<input checked="" type="checkbox"/> Same As Driver		<input checked="" type="checkbox"/> Same As Driver		125	01								
01	36 Number and Street 114 WINDSOR AVE		State NJ		Zip 07843-1291		66 Number and Street 132 SHARP AVE		State NJ		Zip 07843-1545		67 City HOPATCONG		State NJ		Zip 07843-1545		126	--						
01	38 Make HYU		39 Model VER		40 Color BL		41 Year 2010		42 Plate No. PPA26R		43 State NJ		68 Make HON		69 Model CIV		70 Color GY		71 Year 2009		72 Plate No. E96CAG		73 State NJ		128a	26
01	44 VIN KMBNU4CC8AU115536		45 Expires 09/14		74 VIN 2HGFA16589H527732		75 Expires 06/16		46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		77 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		128b	--								
01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name of Placard No.		128c	--								
01	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name of Placard No.		50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,000 lbs		82 Carrier name		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,000 lbs		82 Carrier name		129a	26								
01	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,000 lbs		82 Carrier name		136 Damage To Other Property		Oper.		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		Oper.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.		129b	--						
01	141 Officer's Signature PRUDEN, SEAN <i>[Signature]</i>		142 Badge No.		143 Reviewed By PALANCHI, MARC <i>[Signature]</i>		Badge No. 138		144 Case Status OPEN		136 Damage To Other Property		136 Damage To Other Property		136 Damage To Other Property		136 Damage To Other Property		129c	--						

M.V. #1 and M.V. #2 were in stop and go traffic on Landing Road. Driver of M.V. #2 thought M.V. #1 began to move at which time the driver of M.V. #2 accelerated and struck M.V. #1 in the rear bumper. M.V. #2 had damage to the front bumper and hood of the vehicle. All parties declined medical attention. Patrol cleared.

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	44	F	--	--	--	09	04	--	--	PRELL, DEBRA J, 114 WINDSOR AVE, HOPATCONG, NJ 07843-1291
B	2	01	01	--	53	F	--	--	--	09	04	--	--	GINSBERG, DEBRA L, 8201 HAMPTON COURT, RANDOLPH, NJ
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

Roxbury Township Police Department
Ptl. Pruden
2014-011260
Pg. 2 of 2



NOT TO SCALE

Landing Road

Unit 1



Unit 2

New Jersey Police Crash Investigation Report

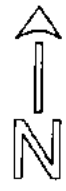
Reportable
 Non-Reportable
 Change Report

96	05	Page 1 of 1 <input type="checkbox"/> Fatal									
97	01	1 Case Number 2014-009764		10 Crash Occurred On: LANDING RD				11 Speed Limit S 36		118a	
98	01	2 Police Dept of ROXBURY TWP Code 01		14 Road Name <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles 15				12 Route No. Suffix		13 Milepost 18 Speed Limit	
99	06	3 Station/Precinct		16 of: 134 LANDING RD				17 Cross Road Name		119a	
100	04	4 Date of Crash mm dd yy 05/02/14		5 Day of Week FRI		6 Time (use 2400 hrs) 1834		7 Municipality Code 1436		8 Total Killed 0	
101	02	23 Veh No 01		24 Policy No. 60990404		25 Ins Code 134		53 Veh No 02		54 Policy No. 50482046	
102	01	26 Driver's First Name JAMIE Initial M		Last Name FIGLIOLINO		29 Sex F		56 Driver's First Name RAYMOND Initial C		59 Sex M	
103	01	27 Number and Street 647 WYCKOFF AVE		30 Eyes 6		57 Number and Street 44 DARTMOUTH TRL HOPATCONG		60 Eyes 2		119b	
104	02	31 State NJ		32 Driver's License No F42663847454825		33 DOB mm dd yy 04/16/82		34 Expires mm yy 04/15		55 Ins Code 135	
105	01	35 Owner's First Name <input checked="" type="checkbox"/> Same As JAMIE Initial M		Last Name FIGLIOLINO		65 Owner's First Name <input checked="" type="checkbox"/> Same As RAYMOND Initial C		Last Name VREELAND		119c	
106	01	38 Number and Street 647 WYCKOFF AVE		39 City MAHWAH		40 State NJ		41 Zip 07430-3011		120	
107	01	37 City MAHWAH		State NJ		Zip 07430-3011		67 City HOPATCONG		State NJ	
108	01	38 Make TOY		39 Model CAM		40 Color GY		41 Year 2004		42 Plate No. V74AKB	
109	04	44 VIN 4T1BE32K24U921320		45 Expires 11/14		74 VIN 1FMYU24E3XUB98963		75 Expires 06/14		121	
110	01	46 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input checked="" type="checkbox"/> Towed		47 Authority <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		77 Authority <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		122	
111	01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0 % <input type="checkbox"/> Pending		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine	
112	01	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		123	
113	01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				124	
114	01	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				125	
115	03	52 Carrier name				82 Carrier name				126	
116	03	135 Crash Description									
117	03	<p>D2 stated that as he was slowing down to make a right hand turn into the parking lot of Affordable Taxi, his vehicle was hit from behind.</p> <p>D1 stated that she "had not eaten all day and as a result, had spaced out" while she was driving south on Landing Road. By the time she realized that V2 was turning right, her vehicle had drifted to the right and was now traveling on the right shoulder. D1 attempted to stop her vehicle but was unable to do so in time. The front drivers side of V1 subsequently collided into the rear passenger side of V2 causing damage to both vehicles.</p> <p>W1 stated that he saw V2 slowing down with its right blinker on, he stated it appeared that V1 was attempting to pass V2 on the right hand shoulder when the two vehicles collided.</p>									
118	06	136 Damage To Other Property									
119	06	Oper. 1		137 Charge <input type="checkbox"/> Multiple Charges 38:4-97		138 Summons No. R-062367		Oper.		139 Charge <input type="checkbox"/> Multiple Charges	
120	06	141 Officer's Signature PYCH III, STANLEY MATHEW <i>[Signature]</i>		142 Badge No. 224		143 Reviewed By PALANCHI, MARC <i>[Signature]</i>		Badge No. 138		144 Case Status CLOSED	

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	32	F	--	--	--	08	04	--	--	FIGLIOLINO, JAMIE M, 647 WYCKOFF AVE, MAHWAH, NJ 07430-3011
B	2	01	01	--	58	M	--	--	--	09	04	--	--	VREELAND, RAYMOND C, 44 DARTMOUTH TRL HOPATCONG, NJ 07843-1138
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

ROXBURY TWP. POLICE
PTL. PYCH
2014-009764
PAGE 2 OF 2.

134 LANDING
ROAD
AFFORDABLE TAXI



NOT TO SCALE

01 1 Case Number **2014-024868** 10 Crash Occurred On: **LANDING RD SOUTH 36 0631 B** 11 Speed Limit **25**

02 2 Police Dept of **ROXBURY TWP** Code **01** 6000 Road Name **KINGS HWY** 12 Route No. **25** 13 Milepost **---**

03 3 Station/Pracinct **14** At Intersection with N E Feet S W of **15** 16 **17 Cross Road Name** NB EB SB WB

04 4 Date of Crash **10/22/14** 5 Day of Week **WED** 6 Time (use 2400 hrs) **1936** 7 Municipality Code **1436** 8 Total Killed **---** 9 Total Injured **---** 19 Ramp To: From: 20 Route/Name **---**

21 Latitude **---** 22 Longitude **---**

01 23 Veh No **01** 24 Policy No. **PANJ00466842** 25 Ins Code **071** 53 Veh No **02** 54 Policy No. **NJA8232738** 55 Ins Code **810**

02 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

02 26 Driver's First Name **DANIEL** Initial **W** Last Name **MARINO** 29 Sex **M** 56 Driver's First Name **KEITH** Initial **R** Last Name **WITTY** 59 Sex **M**

02 27 Number and Street **28 HARVARD TRAIL** 30 Eyes **6** 57 Number and Street **21 WILLIAMS RD** 60 Eyes **2**

02 28 City **HOPATCONG** State **NJ** Zip **07843-1721** 58 City **LANDING** State **NJ** Zip **07850-1741**

02 31 State **NJ** 32 Driver's License No **M08011638612678** 33 DOB **12/09/67** 34 Expires **01/15** 61 State **NJ** 62 Driver's License No **W47264297909862** 63 DOB **09/21/88** 64 Expires **09/15**

03 35 Owner's First Name **DANIEL** Initial **W** Last Name **MARINO** 65 Owner's First Name **KEITH** Initial **R** Last Name **WITTY**

04 Same As Driver Same As Driver

03 36 Number and Street **28 HARVARD TRAIL** 66 Number and Street **21 WILLIAMS RD**

04 37 City **HOPATCONG** State **NJ** Zip **07843-1721** 67 City **LANDING** State **NJ** Zip **07850-1741**

01 38 Make **FOR** 39 Model **MUS** 40 Color **RD** 41 Year **1991** 42 Plate No. **P47CLE** 43 State **NJ** 68 Make **TOY** 69 Model **TAC** 70 Color **BL** 71 Year **2006** 72 Plate No. **L88AFJ** 73 State **NJ**

05 44 VIN **1FACP41M3MF102936** 45 Expires **09/16** 74 VIN **6TEUU42N66Z109867** 75 Expires **10/16**

05 46 Vehicle Removed To Driven Left at Scene Towed Impound Disabled Authority Owner Driver Police

76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled Authority Owner Driver Police

01 48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending

78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending

49 Hazardous Material On Board Spill Name or Placard No. **---**

79 Hazardous Material On Board Spill Name or Placard No. **---**

50 Carrier No. USDOT Other

80 Carrier No. USDOT Other

51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

52 Carrier name **---** 82 Carrier name **---**

02 136 Crash Description

Investigation revealed that vehicle #2 pulled out of Rumors parking lot on Landing Road south bound when it lost control and fish tailed around striking vehicle #1 which was also traveling Landing Road south bound. No injuries were reported and both vehicles received damage.

Driver #2 stated he lost control of his vehicle and acknowledged he was at fault in the accident.

136 Damage To Other Property **NONE STATED OR OBSERVED**

137 Charge Multiple Charges 138 Summons No. **---** 139 Charge Multiple Charges 140 Summons No. **---**

141 Officer's Signature **BECK, WILLIAM** 142 Badge No. **134** 143 Reviewed By **PALANCHI, MARC** 144 Case Status **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	---	46	M	---	---	---	09	04	---	---	MARINO, DANIEL W, 28 HARVARD TRAIL, HOPATCONG, NJ 07843-1721
B	2	01	01	---	28	M	---	---	---	09	04	---	---	WITTY, KEITH R, 21 WILLIAMS RD, LANDING, NJ 07850-1741
C	---	---	---	---	---	---	---	---	---	---	---	---	---	---
D	---	---	---	---	---	---	---	---	---	---	---	---	---	---
E	---	---	---	---	---	---	---	---	---	---	---	---	---	---

New Jersey Police Crash Investigation Report

Police Dept: Roxbury PD

Code: 1432

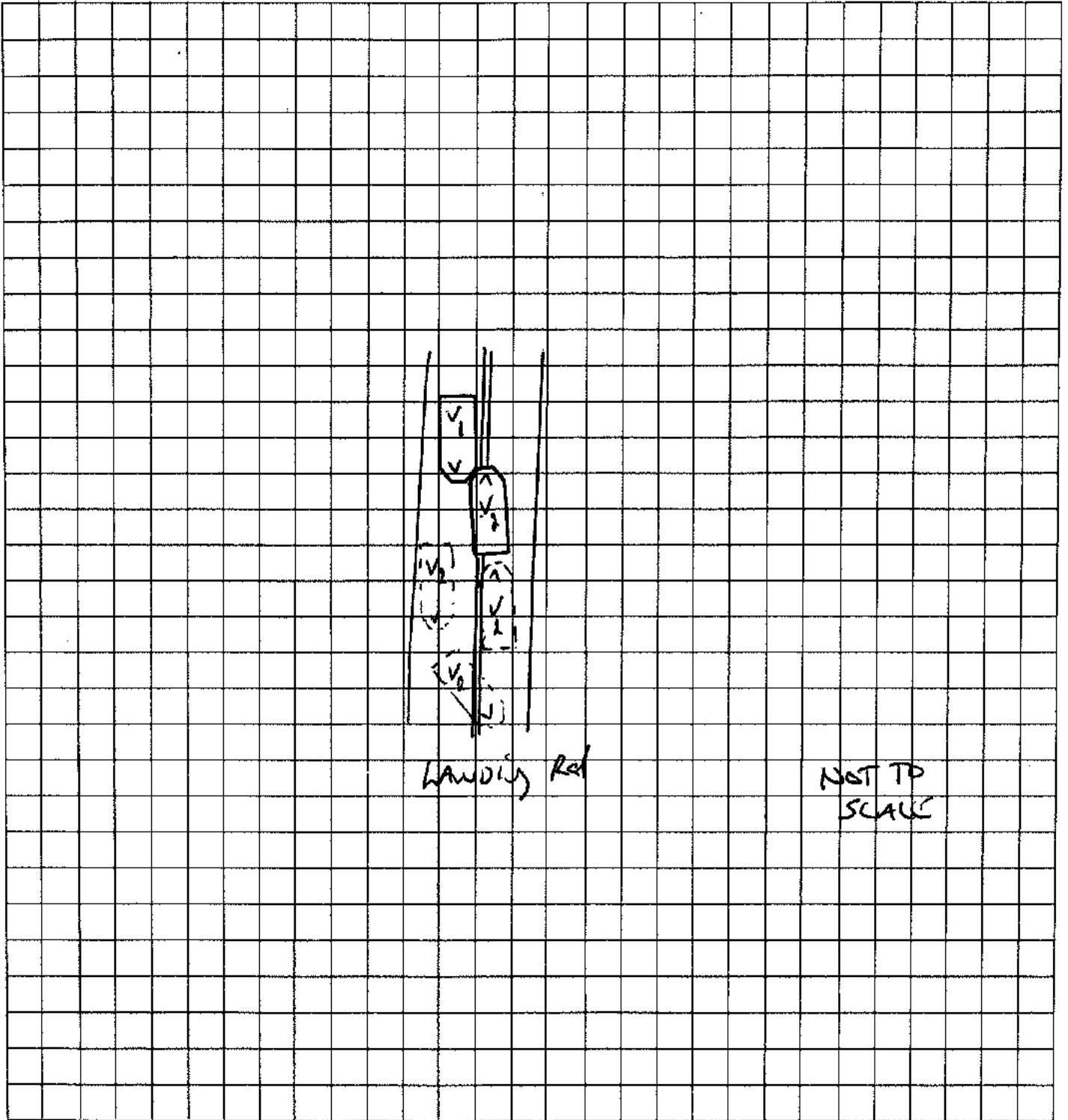
Motor Vehicle Crash Diagram

Station: _____

Case No: 14-024860

134 Crash Diagram (NOT TO SCALE)

↓ Indicate North

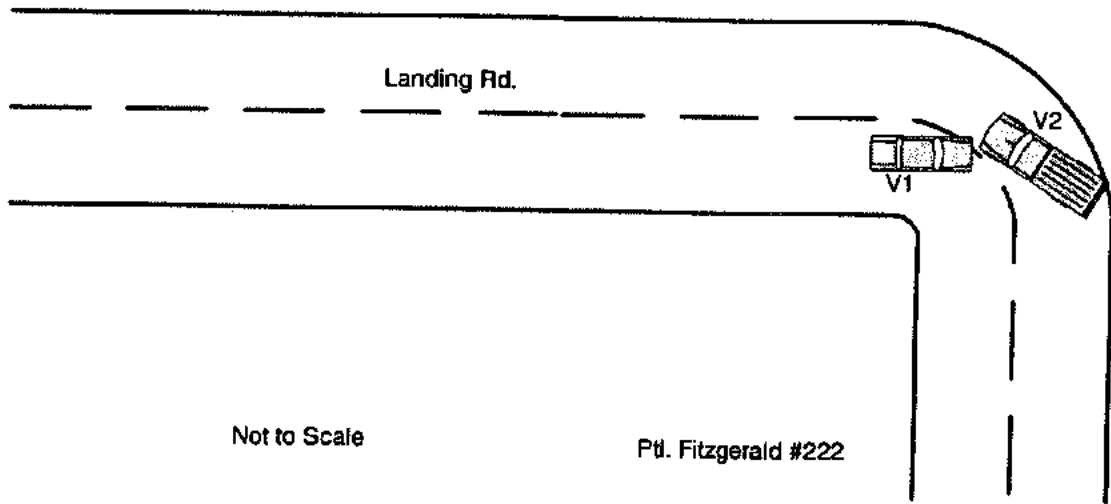


[Signature]
Officer's Signature

134
Badge Number

Case # 2014-028603

12/06/2014



-
-
-
Not to Scale

Ptl. Fitzgerald #222

Ptl. John Fitzgerald #222

01 1 Case Number **2014-022385** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit **35** 0631 **A** 1.10 118a 02

02 2 Police Dept of **ROXBURY TWP** Code **01** 1000 Road Name **KINGS HIGHWAY** Dir **S** 12 Route No. **26** 13 Milepost **01** 119b 01

03 3 Station/Precinct **14** 15 At Intersection with N E Feet S W of: **18 Speed Limit 26** 119a 25

04 4 Date of Crash **09/26/14** 5 Day of Week **THU** 6 Time (use 2400 hrs) **0756** 7 Municipality Code **1436** 8 Total Killed **-** 9 Total Injured **-** 10 Ramp To: From: NB EB SB WB 119b 25

05 23 Veh No **01** 24 Policy No. **9929523802081** 25 Ins Code **884** 53 Veh No **02** 54 Policy No. **BA8298143** 55 Ins Code **056** 120 01

06 26 Driver's First Name **SHANA** Initial **M** Last Name **GJELSVIK** 29 Sex **F** 56 Driver's First Name **BRUCE** Initial **W** Last Name **MAGEE** 59 Sex **M** 121 01

07 27 Number and Street **113 WEHRLI ROAD** 30 Eyes **2** 57 Number and Street **138 SHARP AVE** 60 Eyes **6** 122 -

08 28 City **LONG VALLEY** State **NJ** Zip **07863-3419** 58 City **HOPATCONG** State **NJ** Zip **07843-1569** 123 -

09 31 State **NJ** 32 Driver's License No **G47867037461742** 33 DOB **11/03/74** 34 Expires **11/16** 61 State **NJ** 62 Driver's License No **M01341008604616** 63 DOB **04/30/61** 64 Expires **12/17** 124 01

10 35 Owner's First Name **BRIAN** Initial **H** Last Name **GJELSVIK** 65 Owner's First Name **ACCURATE PEST CONTROL** Initial **INC** Last Name **INC** 125 08

11 36 Number and Street **5 CARRIAGE ROAD** 66 Number and Street **143 LANDING RD** 126 04

12 37 City **HACKETTSTOWN** State **NJ** Zip **07840-4691** 67 City **LANDING** State **NJ** Zip **07850** 127 04

13 38 Make **CHR** 39 Model **TOW** 40 Color **WT** 41 Year **2012** 42 Plate No. **YUD97W** 43 State **NJ** 68 Make **CHE** 69 Model **EXP** 70 Color **WT** 71 Year **2003** 72 Plate No. **X6149W** 73 State **NJ** 128a 26

14 44 VIN **2C4RC1CG7CR227208** 45 Expires **04/15** 74 VIN **1GCFG25T031190014** 75 Expires **05/15** 128b -

15 46 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 47 Authority Owner Driver Police 76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 77 Authority Owner Driver Police 128c -

16 48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 128d -

17 49 Hazardous Material On Board Spill Name or Placard No. 79 Hazardous Material On Board Spill Name or Placard No. 129a 26

18 50 Carrier No. USDOT Other 80 Carrier No. USDOT Other 129b -

19 51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 129c -

20 52 Carrier name 82 Carrier name **ACCURATE PEST CONTROL INC.** 129d -

21 136 Crash Description 132 06

03 Driver two stated that he was traveling south on Landing Rd. when he activated his left turn signal o turn into Accurate Pest Control (143 Landing Rd.). He then slowed down, stopped and waited to make the left turn. He was then struck from behind by vehicle one. Driver one stated that she was traveling south on Landing Rd. directly behind vehicle two and observed vehicle stopped. She then applied her vehicle's brakes and slid into the rear of vehicle two. Driver one was at fault for the accident. End of report. 133 06

136 Damage To Other Property **NONE**

Oper. 137 Charge Multiple Charges 138 Summons No. Oper. 139 Charge Multiple Charges 140 Summons No.

141 Officer's Signature **EDMUNDS, JONATHAN** 142 Badge No. **166** 143 Reviewed By **PALANCHI, MARC** Badge No. **138** 144 Case Status **CLOSED**

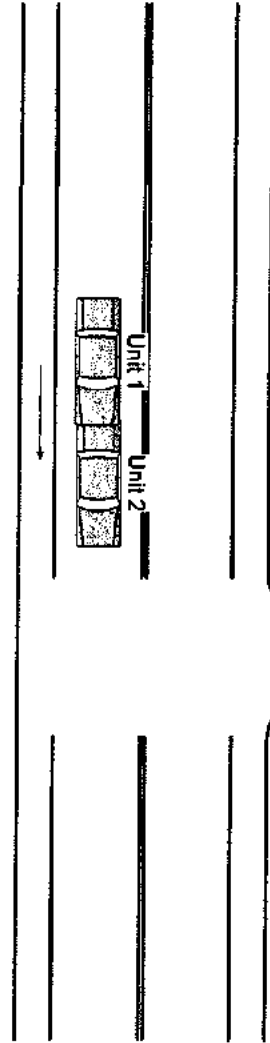
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	-	39	F	-	-	-	09	04	-	-	GJELSVIK, SHANA M, 113 WEHRLI ROAD, LONG VALLEY, NJ 07853-3419
B	2	01	01	-	53	M	-	-	-	09	04	-	-	MAGEE, BRUCE W, 138 SHARP AVE, HOPATCONG, NJ 07843-1569
C	2	03	01	-	52	M	-	-	-	09	04	-	-	VACANTE, ANTHONY M, 29 JUNCTION STREET APT 8, FRANKLIN, NJ 07416-1400
D	-	-	-	-	-	-	-	-	-	-	-	-	-	
E	-	-	-	-	-	-	-	-	-	-	-	-	-	

ROXBURY TWP. POLICE
2014-022385
PTL. EDMUNDS
PAGE 2 OF 2.

NOT TO SCALE



LANDING RD.



ENTRANCE TO 143 LANDING
RD.

New Jersey Police Crash Investigation Report

Reportable
 Non-Reportable
 Change Report

96	05	Page 1 of 1 <input type="checkbox"/> Fatal																																																																																																			
97	01	1 Case Number 2014-014668		10 Crash Occurred On: LANDING ROAD				11 Speed Limit SOUTH 35 0631 A 1.10				118a 26																																																																																									
98	01	2 Police Dept of ROXBURY TWP		Code 01		Road Name KINGS ROAD				12 Route No. Suffix 13 Milepost 35		118b -																																																																																									
99	06	3 Station/Precinct		10 <input type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E		14 <input checked="" type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W				17 Cross Road Name 35		119a 01																																																																																									
100	04	4 Date of Crash mm dd yy 07/03/14		5 Day of Week THU		6 Time (use 2400 hrs) 1727		7 Municipality Code 1436		8 Total Killed 0		119b 02																																																																																									
101	02	23 Veh No 01		24 Policy No. A02231-5202790041		25 Ins Code 182		53 Veh No 02		54 Policy No. 4227288786		120 01																																																																																									
102	02	26 Driver's First Name Initial Last Name KAREN A FALCONER				29 Sex F		56 Driver's First Name Initial Last Name BRIAN M O'DONNELL				121 01																																																																																									
103	06	27 Number and Street 30 WILLIAMS TRAIL				30 Eyes 2		57 Number and Street 15 VALLEY WAY				122 -																																																																																									
104	02	28 City HOPATCONG		State NJ		Zip 07843-1123		58 City MENDHAM		State NJ		123 -																																																																																									
105	04	31 State NJ		32 Driver's License No. F02614246166662		33 DOB mm dd yy 06/03/65		34 Expires mm yy 09/17		61 State NJ		124 01																																																																																									
106	-	35 Owner's First Name Initial Last Name <input type="checkbox"/> Same As KAREN A FALCONER				65 Owner's First Name Initial Last Name <input type="checkbox"/> Same As EDWARD P O'DONNELL				125 16																																																																																											
107	-	36 Number and Street 30 WILLIAMS TRAIL				66 Number and Street 15 VALLEY WAY				126 04																																																																																											
108	01	37 City HOPATCONG		State NJ		Zip 07843-1123		67 City MENDHAM		State NJ		127 04																																																																																									
109	01	38 Make DOD		39 Model DAK		40 Color BL SL		41 Year 2002		42 Plate No. S66DTT		128a 26																																																																																									
110	01	44 VIN 1B7HL48N02S520725				45 Expires 11/14		74 VIN JN1AZ34D76M609632				128b -																																																																																									
111	01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled				47 Authority <input checked="" type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled				77 Authority <input checked="" type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police																																																																																									
112	-	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused				78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused				128d -																																																																																											
113	-	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.				79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.				128e -																																																																																											
114	-	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				128f -																																																																																											
115	-	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				128g -																																																																																											
116	01	52 Carrier name				82 Carrier name				128h 07																																																																																											
117	03	135 Crash Description Driver #1 stated she was just starting in traffic in a stacked line traveling Landing Road north bound for the Landing intersection when vehicle #2 was traveling Landing Road south bound losing control (kings curve) and spinning into vehicle #1. Driver #2 stated he did not know what happened and that he slid on the wet road surface. No other vehicles were having any issues navigating the curve. Both vehicles received minor damage with no injuries being reported.										132 12																																																																																									
136 Damage To Other Property NONE STATED OR OBSERVED																																																																																																					
137 Charge 39:4-97		<input type="checkbox"/> Multiple Charges		138 Summons No. T2649		139 Charge		<input type="checkbox"/> Multiple Charges		140 Summons No.																																																																																											
141 Officer's Signature BECK, WILLIAM				142 Badge No. 134		143 Reviewed By PALANCHI, MARC				144 Case Status CLOSED																																																																																											
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th></th> <th>83</th> <th>84</th> <th>85</th> <th>86</th> <th>87</th> <th>88</th> <th>89</th> <th>90</th> <th>91</th> <th>92</th> <th>93</th> <th>94</th> <th>95</th> <th>Names & Addresses of Occupants - If Deceased, Date & Time of Death</th> </tr> </thead> <tbody> <tr> <td>A</td> <td>1</td> <td>01</td> <td>01</td> <td>-</td> <td>49</td> <td>F</td> <td>-</td> <td>-</td> <td>-</td> <td>08</td> <td>04</td> <td>-</td> <td>-</td> <td>FALCONER, KAREN A, 30 WILLIAMS TRAIL, HOPATCONG, NJ 07843-1123</td> </tr> <tr> <td>B</td> <td>2</td> <td>01</td> <td>01</td> <td>-</td> <td>22</td> <td>M</td> <td>-</td> <td>-</td> <td>-</td> <td>09</td> <td>04</td> <td>-</td> <td>-</td> <td>O'DONNELL, BRIAN M, 15 VALLEY WAY, MENDHAM, NJ 07945-1701</td> </tr> <tr> <td>C</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td></td> </tr> <tr> <td>D</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td></td> </tr> <tr> <td>E</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td>-</td> <td></td> </tr> </tbody> </table>													83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death	A	1	01	01	-	49	F	-	-	-	08	04	-	-	FALCONER, KAREN A, 30 WILLIAMS TRAIL, HOPATCONG, NJ 07843-1123	B	2	01	01	-	22	M	-	-	-	09	04	-	-	O'DONNELL, BRIAN M, 15 VALLEY WAY, MENDHAM, NJ 07945-1701	C	-	-	-	-	-	-	-	-	-	-	-	-	-		D	-	-	-	-	-	-	-	-	-	-	-	-	-		E	-	-	-	-	-	-	-	-	-	-	-	-	-	
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death																																																																																							
A	1	01	01	-	49	F	-	-	-	08	04	-	-	FALCONER, KAREN A, 30 WILLIAMS TRAIL, HOPATCONG, NJ 07843-1123																																																																																							
B	2	01	01	-	22	M	-	-	-	09	04	-	-	O'DONNELL, BRIAN M, 15 VALLEY WAY, MENDHAM, NJ 07945-1701																																																																																							
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D	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																								
E	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																								

New Jersey Police Crash Investigation Report

Police Dept: Roxbury

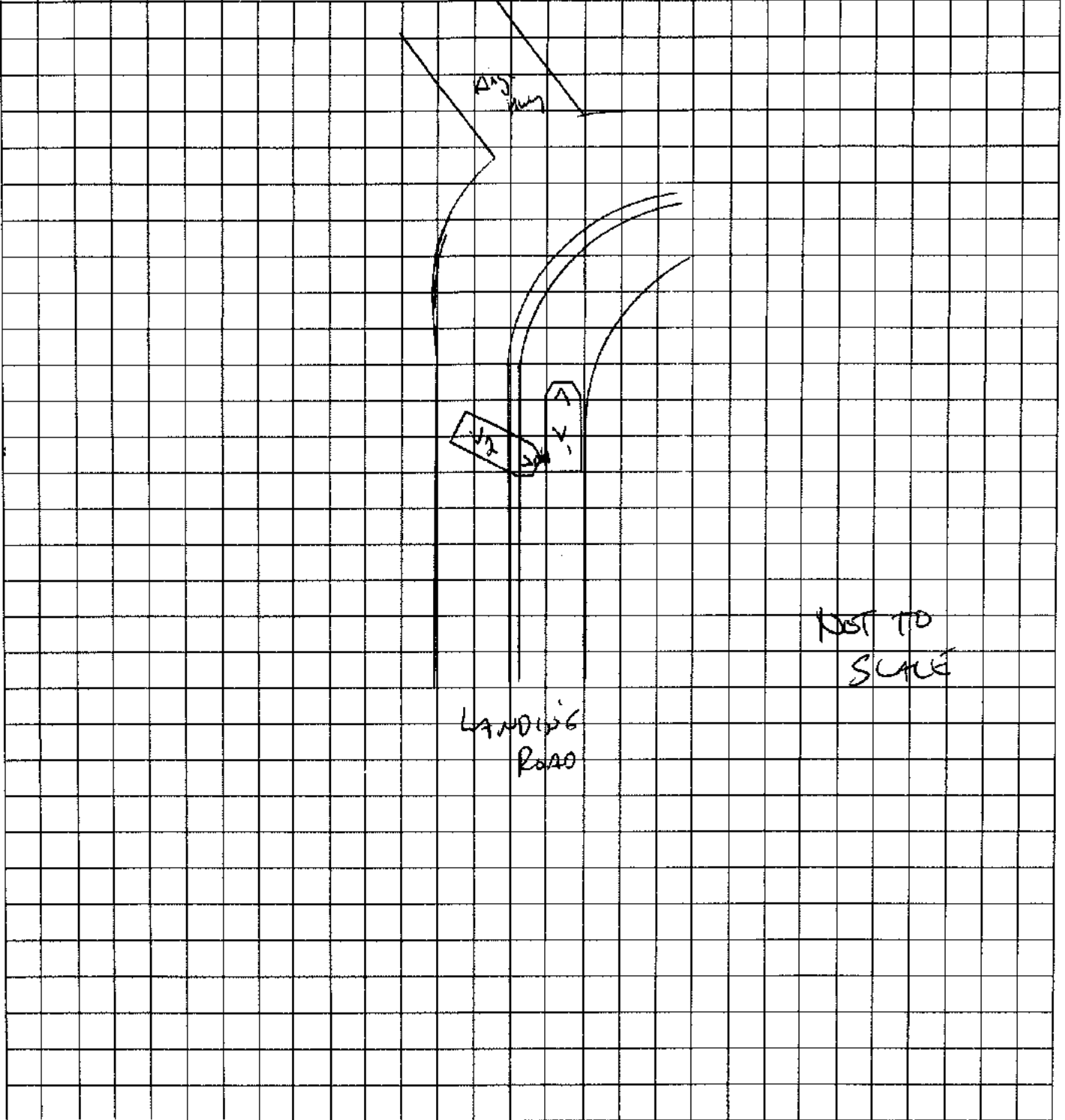
Code: 1436

Motor Vehicle Crash Diagram

Station: _____

Case No: 14-14668

134 Crash Diagram (NOT TO SCALE)



[Signature]
Officer's Signature

134
Badge Number

1 Case Number: **2014-013174** 10 Crash Occurred On: **LANDING RD NORTH 35 0631 A .70** 11 Speed Limit
 2 Police Dept of: **ROXBURY TWP 01** 14 At Intersection with N E S W of: **HART STREET** 12 Route No. Suffix 13 Milepost
 3 Station/Precinct: **01** 15 Feet Miles 16 NB EB SB WB 17 Cross Road Name 18 Speed Limit **25**

4 Date of Crash: **06/13/14** 5 Day of Week: **FRI** 6 Time (use 2400 hrs): **1547** 7 Municipality Code: **1435** 8 Total Killed: **0** 9 Total Injured: **0** 19 Ramp To: From: 20 Route/Name NB EB SB WB
 21 Latitude 22 Longitude

23 Veh No: **01** 24 Policy No: **902289342** 25 Ins Code: **134** 53 Veh No: **02** 54 Policy No: **4326-73-66-62** 55 Ins Code: **100**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY DMT & RDN

26 Driver's First Name: **STEPHEN** Initial: **R** Last Name: **CUPPOLETTI** 29 Sex: **M** 56 Driver's First Name: **JORGE** Initial: **W** Last Name: **TORRES** 59 Sex: **M**
 27 Number and Street: **114 EVERGREEN AVE** 30 Eyes: **5** 57 Number and Street: **77 ROBERT STREET APT. 1** 60 Eyes: **2**

28 City: **HOPATCONG** State: **NJ** Zip: **07843-1908** 58 City: **WHARTON** State: **NJ** Zip: **07885-1918**
 31 State: **NJ** 32 Driver's License No: **C93267227903926** 33 DOB: **03/11/92** 34 Expires: **12/16** 61 State: **NJ** 62 Driver's License No: **T86484108811872** 63 DOB: **11/25/67** 64 Expires: **12/17**

35 Owner's First Name: **STEPHEN** Initial: **R** Last Name: **CUPPOLETTI** 65 Owner's First Name: **JORGE** Initial: **W** Last Name: **TORRES**
 Same As Driver Same As Driver
 36 Number and Street: **114 EVERGREEN AVE** 66 Number and Street: **79 JAMES ST**
 37 City: **HOPATCONG** State: **NJ** Zip: **07843-1908** 67 City: **DOVER** State: **NJ** Zip: **07801-2617**

38 Make: **JEE** 39 Model: **GRC** 40 Color: **BK** 41 Year: **1998** 42 Plate No.: **Y88CYE** 43 State: **NJ** 68 Make: **TOYT** 69 Model: **CAM** 70 Color: **BK** 71 Year: **2010** 72 Plate No.: **A89CAA** 73 State: **NJ**
 44 VIN: **1J4GZ48SXWC184849** 45 Expires: **03/15** 74 VIN: **4T1BF3EK2AU024485** 75 Expires: **03/14**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police
RON AND SON'S 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending
 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending

49 Hazardous Material On Board: Spill: Name or Placard No.:
 50 Carrier No.: USDOT Other
 51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 52 Carrier name
 79 Hazardous Material On Board: Spill: Name or Placard No.:
 80 Carrier No.: USDOT Other
 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 82 Carrier name

136 Damage To Other Property: **NONE STATED OR OBSERVED**
 Oper: **2** 137 Charge: **3914-97** Multiple Charges 138 Summons No.: **E14-000522** Oper: Multiple Charges 139 Charge: Multiple Charges 140 Summons No.:

141 Officer's Signature: **BECK, WILLIAM** 142 Badge No.: **134** 143 Reviewed By: **PALANCHI, MARC** Badge No.: **138** 144 Case Status: **CLOSED**

	B3	B4	B5	B6	B7	B8	B9	B0	B1	B2	B3	B4	B5	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	22	M	--	--	--	09	04	--	--	CUPPOLETTI, STEPHEN R, 114 EVERGREEN AVE, HOPATCONG, N: 07843-1908
B	2	01	01	--	46	M	--	--	--	09	04	--	--	TORRES, JORGE W, 77 ROBERT STREET APT. 1, WHARTON, N: 07885-1918
C	2	03	01	--	39	F	--	--	--	04	04	--	--	TORRES, SANDRA L, 77 ROBERT STREET APT-1, WHARTON, N: 07885
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

2 of 2

New Jersey Police Crash Investigation Report

Police Dept: Rosbury 80

Code: 1436

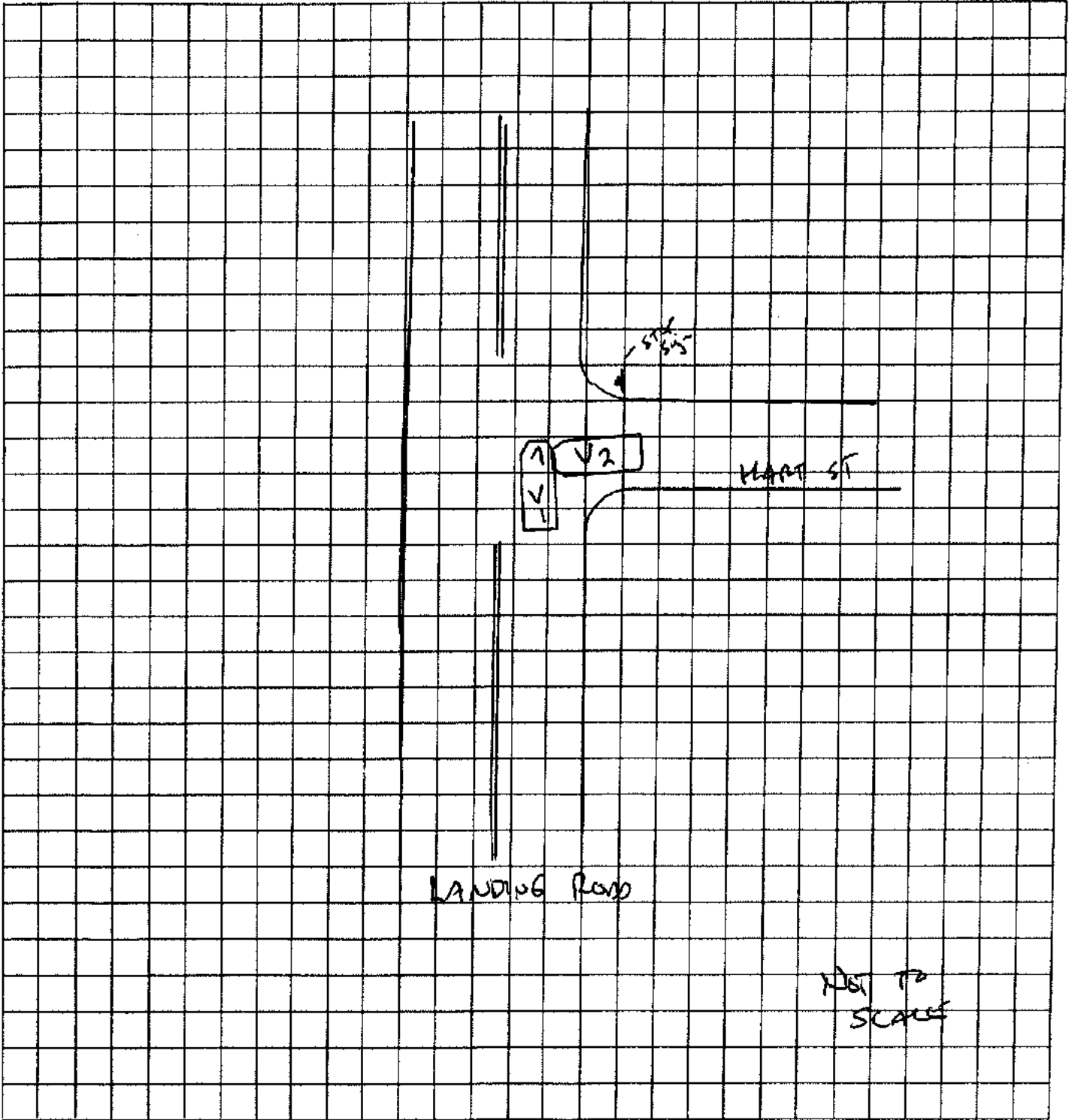
Motor Vehicle Crash Diagram

Station: _____

Case No: 14-013174

134 Crash Diagram (NOT TO SCALE)

④ Indicate North



New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

Page 1 of 2 Fatal

06	1 Case Number 2014-011772										118a														
01	10 Crash Occurred On: LANDING RD NORTH 35 0631 A 32.38										00														
06	2 Police Dept of ROXBURY TWP 01			Road Name HART STREET			12 Route No. Suffix 18 Speed Limit 25			13 Milepost 25		118b													
06	3 Station/Precinct			14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W			17 Cross Road Name			18 Speed Limit		00													
06	4 Date of Crash 06/26/14		5 Day of Week MON		6 Time (use 2400 hrs) 2166		7 Municipality Code 1436		8 Total Killed 0		9 Total Injured 0		119a												
04	23 Veh No 01		24 Policy No. 27067965-0			25 Ins Code 134		53 Veh No		54 Policy No.		55 Ins Code		119b											
02	<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN																								
01	26 Driver's First Name Initial Last Name THOMAS A KROZSER						29 Sex M						121												
01	27 Number and Street 121 BROOKLYN STANHOPE ROAD						30 Eyes S			57 Number and Street 60 Eyes			122												
01	28 City State Zip STANHOPE NJ 07874-2608						58 City State Zip						123												
1	31 State NJ		32 Driver's License No K76197406106956			33 DOB 08/16/96		34 Expires 07/15		61 State		62 Driver's License No		63 DOB		64 Expires		124							
15	35 Owner's First Name Initial Last Name <input type="checkbox"/> Same As THOMAS A KROZSER						65 Owner's First Name Initial Last Name <input type="checkbox"/> Same As						125												
16	36 Number and Street 121 BROOKLYN STANHOPE ROAD						66 Number and Street						126												
16	37 City State Zip STANHOPE NJ 07874-2608						67 City State Zip						127												
16	38 Make HON		39 Model CBR		40 Color BK YW		41 Year 2001		42 Plate No. 2MBH8		43 State NJ		68 Make		69 Model		70 Color		71 Year		72 Plate No.		73 State		128a
08	44 VIN JH2SC44031M106936						45 Expires 03/16		74 VIN						75 Expires		128b								
01	46 Vehicle Removed To <input type="checkbox"/> Driven <input checked="" type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled						47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled						77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		128c								
01	48 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0, % <input type="checkbox"/> Pending						78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0, % <input type="checkbox"/> Pending						128d												
01	49 Hazardous Material On Board Spill <input type="checkbox"/> <input type="checkbox"/>						79 Hazardous Material On Board Spill <input type="checkbox"/> <input type="checkbox"/>						128e												
01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other						80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other						128f												
01	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs						81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs						130												
01	52 Carrier name						82 Carrier name						131												
01	135 Crash Description Rider #1 reported that he lost control of his Motorcycle due to a vehicle making an illegal u turn in front of him or by pulling out of Landing Auto body. Rider #1 also stated that his motorcycle never made contact with the vehicle which turned out in front of him. Rider #1 complained of pain to his arms and back where he had "road rash" from dumping his motorcycle. Roxbury Company #2 responded to the scene and transported rider #1 to Dover Hospital.												133												
01	A short time after the incident I learned that the vehicle in question was stopped by NJSP on Route 80 EB at mile marker 35.6. I responded to the stop on Route 80.												133												
01	136 Damage To Other Property NONE STATED OR OBSERVED												132												
01	137 Charge <input type="checkbox"/> Multiple Charges			138 Summons No.			139 Charge <input type="checkbox"/> Multiple Charges			140 Summons No.			133												
01	141 Officer's Signature BECK, WILLIAM						142 Badge No. 134		143 Reviewed By PALANCHI, MARC				144 Case Status CLOSED												

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	02	04	17	M	12	02	02	06	06	--	6403	KROZSER, THOMAS A. 121 BROOKLYN STANHOPE ROAD, STANHOPE, NJ 07874-2508
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

New Jersey Police Crash Investigation Report

Police Dept: **ROXBURY TWP**

Code: **01**

Motor Vehicle Crash Description

Station:

Case No: **2014-011772**

(Refer to vehicle by number)

ALL INVOLVED	Veh Occ	Pos In/On	Eject	Phys Cond	Age	Sex	Loc Inj	Type Inj	Ref Med	Equip Avail	Equip Used	Baq Dept	Hosp Code	Names & Addresses of Occupants - If Deceased, Date & Time of Death
	F	83	84	85	86	87	88	89	90	91	92	93	94	
G	--	--	--	--	--	--	--	--	--	--	--	--	--	
H	--	--	--	--	--	--	--	--	--	--	--	--	--	
I	--	--	--	--	--	--	--	--	--	--	--	--	--	
J	--	--	--	--	--	--	--	--	--	--	--	--	--	

135 Crash Description

I spoke with Shawn Ryan O'Boyle of 15023 Ranson Road, Huntersville, NC 28078. I identified Shawn from his NC photo drivers license 23158267. O'Boyle stated he was lost and turned around in the parking lot of Landing Auto Body. He stated he started to pull out onto landing road when he heard then saw a motorcycle traveling at a high rate of speed. O'Boyle stated he and his family watched as the motorcycle lost control of his bike and dumped it in the oncoming lane of Landing Road.

He stated he then pulled out and parked on the side of the road and checked on the rider. He stated the rider became verbally abusive and then another 4 or 5 motorcycles arrived on scene and started getting hostile towards him and his wife so he left the scene after being there for five minutes. He knew he had nothing to do with the accident. and felt scared that these other motorcycle riders were blaming him when none of them had not witnessed the incident. O'Boyle advised the group he was leaving because he was not involved and he stated a short time later another motorcycle was following him on Route 80.

I inspected O'Boyle Enterprise rental (VA) VAU6903 black Chevy Tahoe which had no damage to the vehicle.

Due to the conflicting statements I am unable to determine fault for this incident.

No Diagram

97 01 1 Case Number **2014-019521** 10 Crash Occurred On: **LANDING ROAD NORTH 36 0631 A** 11 Speed Limit **26**

98 01 2 Police Dept of **ROXBURY TWP** Code **01** 14 Road Name **LANDING ROAD** Dir **NORTH** 12 Route No. **36** Suffix **A** 13 Milepost **26**

99 01 3 Station/Precinct **01** 15 At Intersection with N E S W of **KINGS HWY** 18 Speed Limit **25**

100 01 4 Date of Crash **08/26/14** 5 Day of Week **TUE** 6 Time (use 2400 hrs) **1033** 7 Municipality Code **1436** 8 Total Killed **0** 9 Total Injured **0** 19 Ramp To: NB EB SB WB

101 01 20 Route/Name **08/26/14** 21 Latitude **40.8119** 22 Longitude **-75.0819**

101 02 23 Veh No **01** 24 Policy No. **BA0361984** 25 Ins Code **116** 53 Veh No **02** 54 Policy No. **4313174155** 55 Ins Code **100**

PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

102 01 26 Driver's First Name **GASPAR** Initial **D** Last Name **PINTO** 29 Sex **M** 56 Driver's First Name **AMBER** Initial **M** Last Name **BLANCHARD** 59 Sex **F**

27 Number and Street **RDS BOX 8654** 30 Eyes **2** 57 Number and Street **817 RINGWOOD AVENUE #3 POMPTON LAKES** 60 Eyes **2**

103 01 28 City **E. STROUDSBURG** State **PA** Zip **18301** 58 City **POMPTON LAKES** State **NJ** Zip **07442-2233**

104 02 31 State **PA** 32 Driver's License No **25138167** 33 DOB **08/09/82** 34 Expires **08/16** 61 State **NJ** 62 Driver's License No **B60620377453922** 63 DOB **03/16/92** 64 Expires **12/17**

105 01 35 Owner's First Name **CAMPBELLS WELD & MACH** Initial **INC** Last Name **INC** 65 Owner's First Name **AMBER** Initial **M** Last Name **BLANCHARD**

Same As Driver

106 01 36 Number and Street **PO BOX 8656** 66 Number and Street **817 RINGWOOD AVENUE #3 POMPTON LAKES**

107 01 37 City **LANDING** State **NJ** Zip **07850** 67 City **POMPTON LAKES** State **NJ** Zip **07442-2233**

108 06 38 Make **DOD** 39 Model **RAM** 40 Color **BL** 41 Year **2002** 42 Plate No **UKI 45N** 43 State **NJ** 68 Make **LEX** 69 Model **ES3** 70 Color **BLK** 71 Year **1994** 72 Plate No **M27DLK** 73 State **NJ**

109 01 44 VIN **3B7KF26642M253966** 45 Expires **02/16** 74 VIN **JTBGK13T6R0020074** 75 Expires **09/14**

110 02 46 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 47 Authority Owner Driver Police 76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 77 Authority Driver Owner Police

111 01 48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending

112 01 49 Hazardous Material On Board Spill Name or Placard No. 79 Hazardous Material On Board Spill Name or Placard No.

113 01 50 Carrier No. USDOT Other 80 Carrier No. USDOT Other

114 01 51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

115 01 52 Carrier name 82 Carrier name

117 01 135 Crash Description
Investigation revealed that vehicle #1 was stopped in traffic making a left turn off of Landing Road onto Kings Hwy when it was struck from behind by Vehicle #2. I asked Driver #2 what happened and she replied it was her fault that she wasn't paying attention. There were no reports of any injuries.

Patrol cleared.

136 Damage To Other Property **NONE STATED OR OBSERVED**

137 Charge Multiple Charges 138 Summons No. Oper. 139 Charge Multiple Charges 140 Summons No.

141 Officer's Signature **BECK, WILLIAM** 142 Badge No. **134** 143 Reviewed By **PALANCHI, MARC** 144 Case Status **CLOSED**

		Names & Addresses of Occupants - If Deceased, Date & Time of Death																					
A	B	C	D	E	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	
					1	01	01	--	52	M	--	--	--	08	04	--	--						PINTO, GASPAR D, RDS BOX 8654, E. STROUDSBURG, PA 18301
					2	01	01	--	22	F	--	--	--	05	04	--	--						BLANCHARD, AMBER M, 817 RINGWOOD AVENUE #3 POMPTON LAKES, N. 07442-2233
					--	--	--	--	--	--	--	--	--	--	--	--	--						
					--	--	--	--	--	--	--	--	--	--	--	--	--						
					--	--	--	--	--	--	--	--	--	--	--	--	--						

New Jersey Police Crash Investigation Report

Police Dept: Roxbury NJ

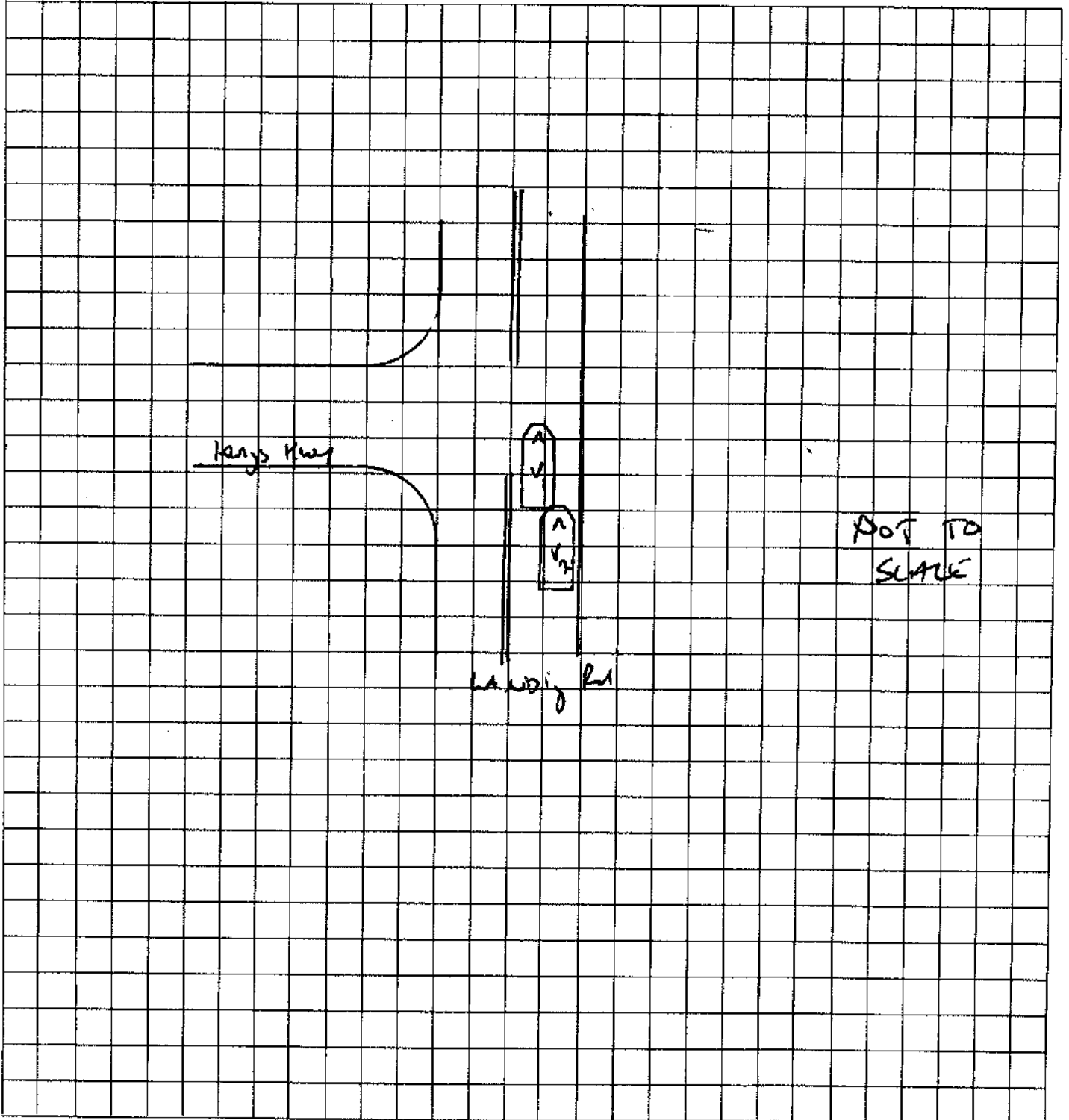
Code: 1435

Motor Vehicle Crash Diagram

Station: _____

Case No: 14-019521

134 Crash Diagram (NOT TO SCALE)



[Signature]
Officer's Signature

134
Badge Number

New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

Page 1 of 1 Fatal

1 Case Number: 2014-021185

2 Police Dept of: ROXBURY TWP

3 Station/Precinct: -

10 Crash Occurred On: LANDING RD

11 Speed Limit: 35

12 Route No: 0631

13 Milepost: 1.20

14: At Intersection with N E

15: Feet S W

16: LAKESIDE BLVD.

17 Cross Road Name

18: 35

19 Ramp: To: From: NB EB

20 Route/Name: SB WB

4 Date of Crash: 09/11/14

5 Day of Week: THU

6 Time (use 2400 hrs): 2147

7 Municipality Code: 1436

8 Total Killed: -

9 Total Injured: 1

21 Latitude: -

22 Longitude: -

23 Veh No: 01

24 Policy No: 93903260162/02

25 Ins Code: 054

53 Veh No: 02

54 Policy No: 0765824B1030

55 Ins Code: 982

26 Driver's First Name: JOSEPH

27 Number and Street: 104 OLD ALLAMUCHY RD APT 1

28 City: HACKETTSTOWN

29 Sex: M

30 Eyes: 2

56 Driver's First Name: GASTON

57 Number and Street: 120 WINDSOR AVENUE

58 City: HOPATCONG

59 Sex: M

60 Eyes: 2

31 State: NJ

32 Driver's License No: C40714107707892

33 DOB: 07/07/89

34 Expires: 07/18

61 State: NJ

62 Driver's License No: P45292766608962

63 DOB: 08/24/96

64 Expires: 07/18

35 Owner's First Name: JOSEPH

36 Number and Street: 104 OLD ALLAMUCHY RD APT 1

37 City: HACKETTSTOWN

38 Make: CHE

39 Model: SON

40 Color: BK

41 Year: 2012

42 Plate No: G70BVY

43 State: NJ

44 VIN: 1G1JD6SBXC4117875

45 Expires: 02/16

65 Owner's First Name: ELIZABETH

66 Number and Street: 120 WINDSOR AVE

67 City: HOPATCONG

68 Make: MIT

69 Model: ECL

70 Color: RED

71 Year: 1998

72 Plate No: M33DXS

73 State: NJ

74 VIN: 4A3AK34YXWE127833

75 Expires: 02/15

46 Vehicle Removed To: Driven Left at Scene Towed

47 Authority: Impound Owner Driver Police

76 Vehicle Removed To: Driven Left at Scene Towed

77 Authority: Owner Driver Police

48 Alcohol/Drug Test: Given: No Yes Refused

49 Results: 0.028 %

78 Alcohol/Drug Test: Given: No Yes Refused

79 Results: 0. %

49 Hazardous Material: On Board Spill

80 Carrier No. USDOT Other

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

52 Carrier name: -

81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

82 Carrier name: -

135 Crash Description:

On scene it was determined that driver one had consumed alcoholic type beverages. After the conclusion of Standardized Field Sobriety Tests, driver one was placed under arrest. Driver one was unable to provide an account of the accident. Driver two stated that he was traveling north on Landing Rd. while driver one just turned south onto Landing Rd. from Lakeside Blvd. While Driver one negotiated the turn his vehicle struck the center concrete curbing that divides the north & south bound lanes of Landing Rd. Vehicle one then veered into the North bound lane and struck his vehicle. Examining the evidence at the scene, it was determined that vehicle one was traveling south on Landing Rd. and struck the concrete curbing in the center of Landing Rd. The vehicle then veered into the north bound lane and struck vehicle two. Driver one is at fault for the accident. Additional summons R-63979 issued to driver one for 39:4-50. End of report.

136 Damage To Other Property: NONE

137 Charge: 39:4-97

138 Summons No: R0-63977

139 Charge: 39:4-88B

140 Summons No: R-063978

141 Officer's Signature: EDMUNDS, JONATHAN

142 Badge No: 166

143 Reviewed By: PALANCHI, MARC

144 Case Status: CLOSED

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	99	26	M	08	06	01	09	04	--	--	CICCONE, JOSEPH P, 104 OLD ALLAMUCHY RD APT 1, HACKETTSTOWN, NJ 07840-1548
B	2	01	01	--	18	M	--	--	--	09	04	--	--	PINTUELES, GASTON E, 120 WINDSOR AVENUE, HOPATCONG, NJ 07843-1222
C	2	03	01	--	18	()	--	--	--	09	04	--	--	PRUDEN, CARLEY E, 606 LAKESIDE AVE, ANDOVER, NJ 07821-2993
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

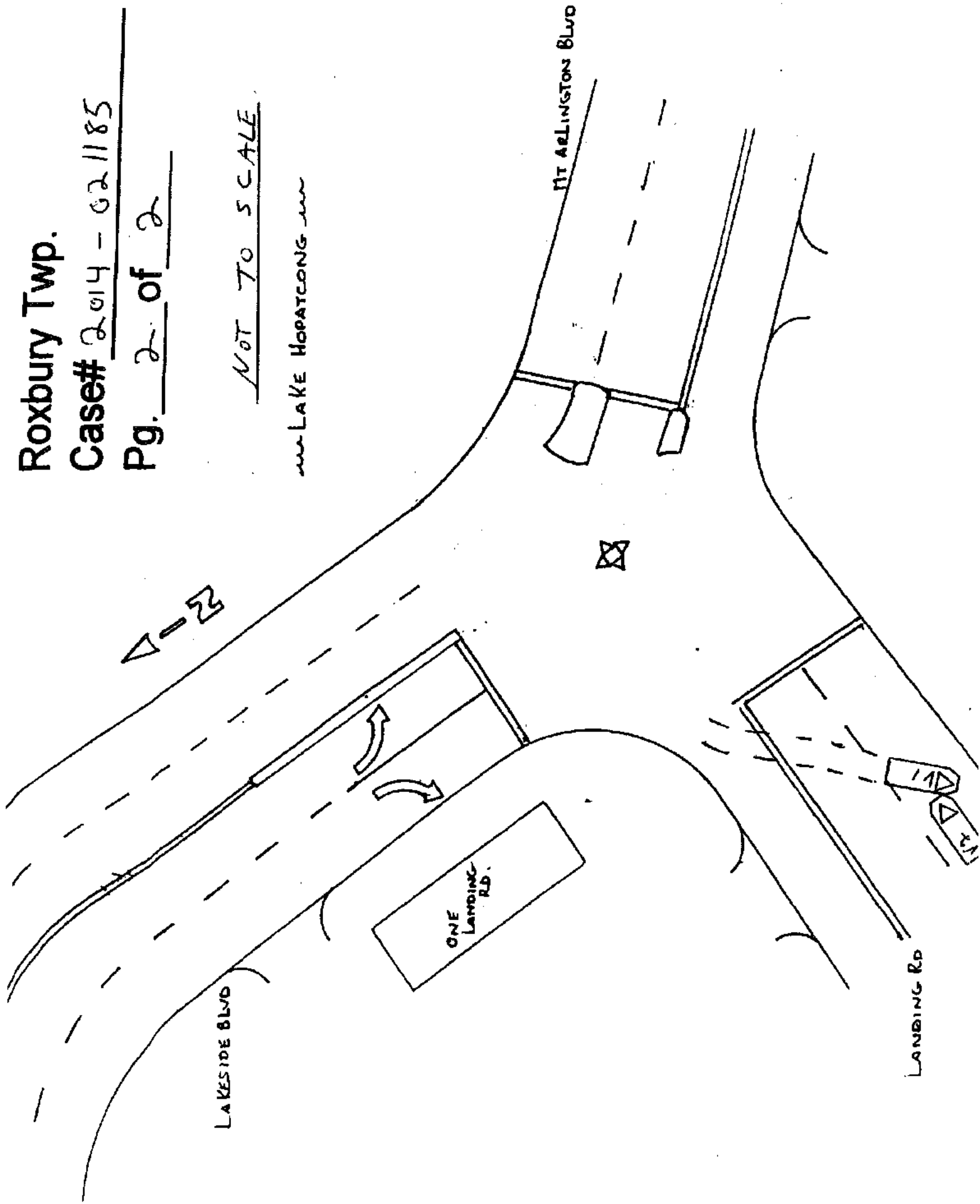
Roxbury Twp.

Case# 2014-021185

Pg. 2 of 2

NOT TO SCALE

LAKE HOPATCONG



1 Case Number: **2014-024212** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit: **35**
 2 Police Dept of: **ROXBURY TWP** Code: **01** Road Name: **NORTH** Dir: **35** 12 Route No. Suffix: **0631** A 13 Milepost: **--**
 3 Station/Precinct: **100** 14 At Intersection with N E Feet S W of: **LAKESIDE BLVD** 18 Speed Limit: **35**
 4 Date of Crash: **10/15/14** 5 Day of Week: **WED** 6 Time (use 2400 hrs): **1707** 7 Municipality Code: **1436** 8 Total Killed: **--** 9 Total Injured: **--** 17 Cross Road Name: **15** 19 Ramp To: NB EB SB WB
 20 Route/Name: **21** Latitude: **22** Longitude: **22**

23 Veh No: **01** 24 Policy No: **C10058750** 25 Ins Code: **945** 53 Veh No: **02** 54 Policy No: **9648295111051** 55 Ins Code: **129**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HJT & RUN

26 Driver's First Name: **KAREN** Initial: **M** Last Name: **HUGHES** 29 Sex: **F** 56 Driver's First Name: **KHELSAE** Initial: **A** Last Name: **ANDICAN** 59 Sex: **F**

27 Number and Street: **46-D WEST RIVERSTYX ROAD** 30 Eyes: **4** 57 Number and Street: **7 POWERVILLE RD** 60 Eyes: **2**

28 City: **HOPATCONG** State: **NJ** Zip: **07843-1868** 58 City: **MOUNTAIN LAKES** State: **NJ** Zip: **07046-1001**

31 State: **NJ** 32 Driver's License No: **H91384247457844** 33 DOB: **07/25/84** 34 Expires: **10/14** 61 State: **NJ** 62 Driver's License No: **A68764346154952** 63 DOB: **04/12/95** 64 Expires: **04/15**

35 Owner's First Name: **KAREN** Initial: **M** Last Name: **HUGHES** 65 Owner's First Name: **OSMAN** Initial: **A** Last Name: **ANDICAN**
 Same As Driver Same As Driver

36 Number and Street: **45-D WEST RIVERSTYX ROAD** 66 Number and Street: **1 CREST HILL DR**

37 City: **HOPATCONG** State: **NJ** Zip: **07843-1868** 67 City: **BOONTON** State: **NJ** Zip: **07005-9167**

38 Make: **HON** 39 Model: **CIV** 40 Color: **GRN** 41 Year: **2001** 42 Plate No.: **A13CAA** 43 State: **NJ** 68 Make: **MB** 69 Model: **E60** 70 Color: **BK** 71 Year: **2005** 72 Plate No.: **YNT14H** 73 State: **NJ**

44 VIN: **1HGEM22981L117831** 45 Expires: **04/15** 74 VIN: **WDBUF63J75K174342** 75 Expires: **01/15**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending

49 Hazardous Material On Board: Spill Name or Placard No. 79 Hazardous Material On Board: Spill Name or Placard No.

50 Carrier No. USDOT Other 80 Carrier No. USDOT Other

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 25,000 lbs > 25,000 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 25,000 lbs > 25,000 lbs

52 Carrier name 82 Carrier name

135 Crash Description: **D#1 STATED SHE WAS STOPPED IN TRAFFIC, NORTHBOUND ON LANDING RD, AND HER VEHICLE WAS STRUCK FROM BEHIND BY V#2. D#2 STATED SHE WAS TRAVELING NORTHBOUND ON LANDING RD AND BRIEFLY BECAME DISTRACTED AFTER MISSING A TURN. D#2 FURTHER STATED SHE THEN LOOKED AHEAD AND OBSERVED V#1 TO BE STOPPED, AND COULD NOT STOP IN TIME TO AVOID COLLISION. THERE WERE NO INJURIES REPORTED.**

136 Damage To Other Property: **NONE**

137 Charge: **--** Multiple Charges 138 Summons No.: **--** Oper. **--** 139 Charge: **--** Multiple Charges 140 Summons No.: **--**

141 Officer's Signature: **FOURNIER, MICHAEL** 142 Badge No.: **193** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

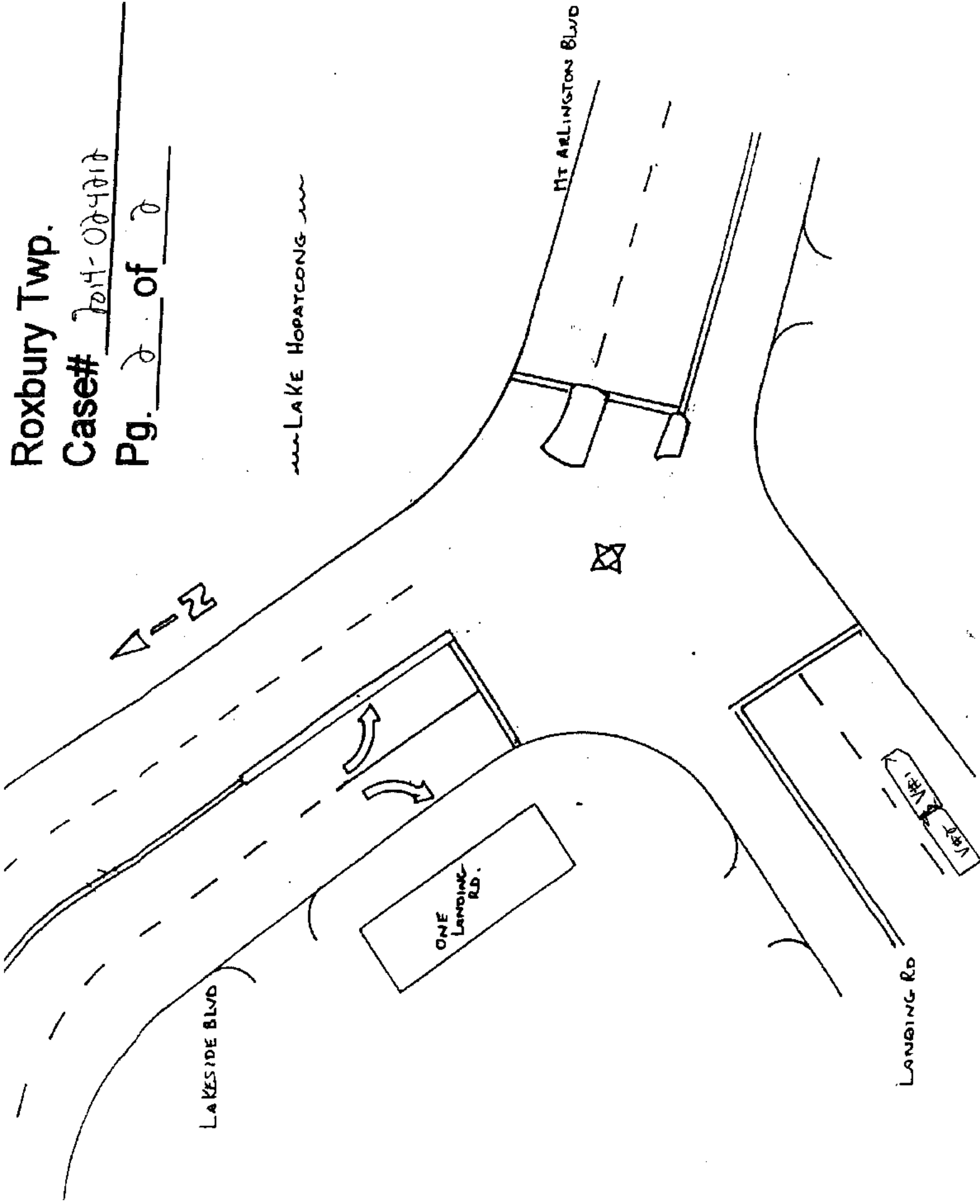
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	30	F	--	--	--	09	04	--	--	HUGHES, KAREN M, 45-D WEST RIVERSTYX ROAD, HOPATCONG, NJ 07843-1868
B	2	01	01	--	19	F	--	--	--	09	04	--	--	ANDICAN, KHELSAE A, 7 POWERVILLE RD, MOUNTAIN LAKES, NJ 07046-1001
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

Roxbury Twp.

Case# 7014-004010

Pg. 2 of 2

Lake Hopatcong



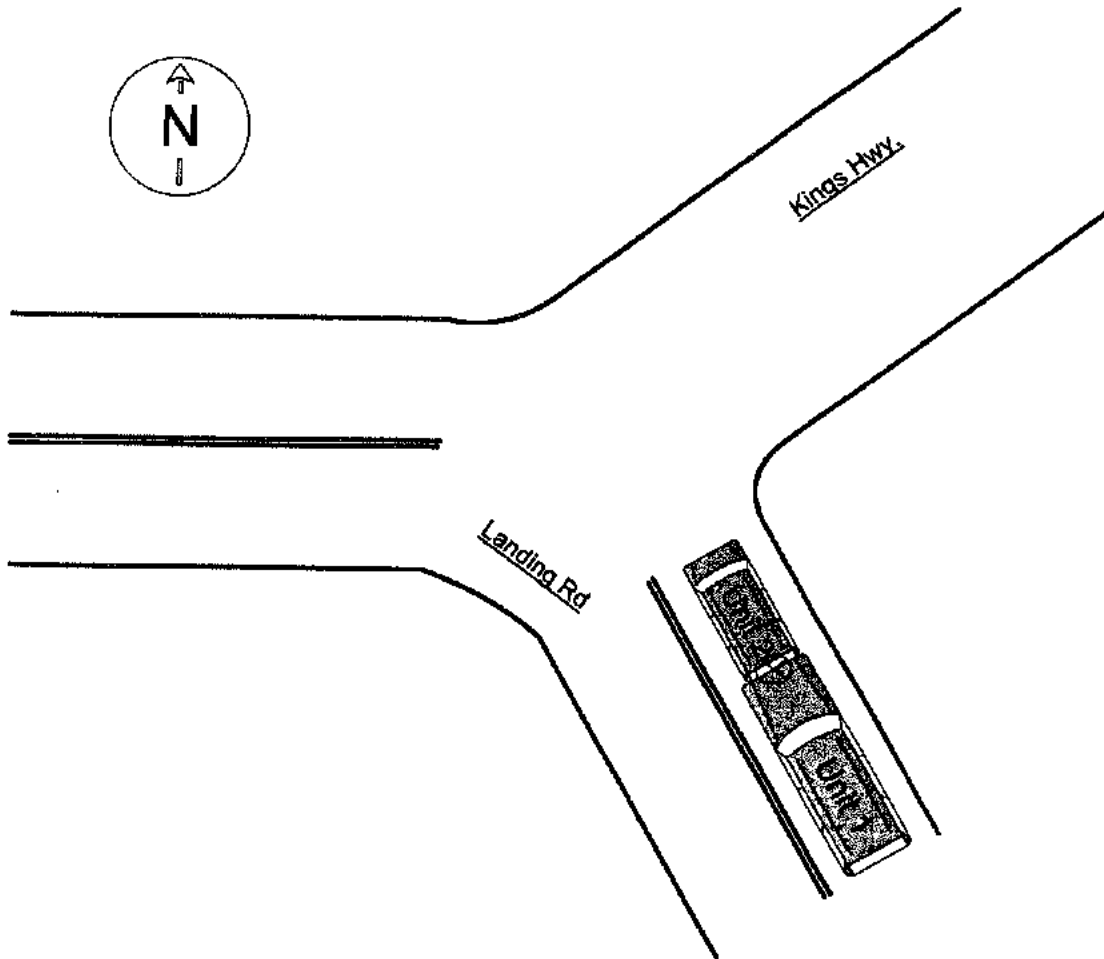
1 Case Number 2014-004400	10 Crash Occurred On: LANDING ROAD	11 Speed Limit S 35 0631 A 1.15	118a 09
2 Police Dept of ROXBURY TWP Code 01	20 Road Name LANDING ROAD	12 Route No. S 13 Milepost 0631 A 1.15	118b 09
3 Station/Precinct ---	<input type="checkbox"/> At Intersection with <input checked="" type="checkbox"/> N <input type="checkbox"/> E <input checked="" type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles <input type="checkbox"/> 16	17 Cross Road Name KINGS HWY.	118c 25
4 Date of Crash mm dd yy 02/27/14	5 Day of Week ---	6 Time (use 2400 hrs) 0744	7 Municipality Code 1436
8 Total Killed ---	9 Total Injured 0631	19 Ramp <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB	119b 01
23 Veh No 01	24 Policy No. F927302-0	25 Ins Code 426	53 Veh No 02
26 Driver's First Name Initial Last Name LINDA N HAMILTON		56 Driver's First Name Initial Last Name JOEL M KELLISH	
27 Number and Street 206 BROOKLYN MTN RD		57 Number and Street 5 NARITICONG AVE	
28 City State Zip HOPATCONG NJ		58 City State Zip HOPATCONG NJ	
31 State NJ	32 Driver's License No. H0354737552702	33 DOB mm dd yy 02/03/79	34 Expires mm yy 08/16
35 Owner's First Name Initial Last Name <input checked="" type="checkbox"/> Same As LINDA N HAMILTON		65 Owner's First Name Initial Last Name <input checked="" type="checkbox"/> Same As JOEL M KELLISH	
36 Number and Street 206 BROOKLYN MOUNTAIN RD		66 Number and Street 5 NARITICONG AVE	
37 City State Zip HOPATCONG NJ 07843-1227		67 City State Zip HOPATCONG NJ 07843-1408	
38 Make FOR	39 Model EXP	40 Color BLK	41 Year 2004
42 Plate No. VRM27K	43 State NJ	44 VIN 1FMFU18L64LA85707	45 Expires mm yy 04/14
46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		74 VIN JTLKT334X40183868	
47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		75 Expires mm yy 01/16	
48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending	
49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.	
60 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	
61 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 25,000 lbs <input type="checkbox"/> > 26,001 lbs	
52 Carrier name		82 Carrier name	

135 Crash Description
 Driver veh #1 stated that she was behind veh #2 on Landing Rd S/B. When veh #2 stopped in traffic she was unable to stop in time which resulted in the front end of her vehicle colliding with the rear end of veh #2. Driver veh #2 stated he had stopped on Landing Rd S/B when his vehicle was struck in the rear by the front end of veh #1. No injuries were reported or observed. Veh #1 had minor front end damage. Veh #2 had minor rear end damage. Driver veh #1 at fault for the crash.

136 Damage To Other Property ---	137 Charge <input type="checkbox"/> Multiple Charges	138 Summons No. ---	139 Charge <input type="checkbox"/> Multiple Charges	140 Summons No. ---
141 Officer's Signature MURRAY, DANIEL <i>[Signature]</i>		142 Badge No. 159	143 Reviewed By MURRAY, DANIEL <i>[Signature]</i>	144 Case Status CLOSED

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	44	F	--	--	--	09	04	--	--	HAMILTON, LINDA N, 206 BROOKLYN MTN RD, HOPATCONG, NJ
B	2	01	01	--	46	M	--	--	--	08	04	--	--	KELLISH, JOEL M, 5 NARITICONG AVE, HOPATCONG, NJ 07843-1408
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

Case No.: 2014-004400
Agency: Roxbury Twp.
Officer: Sgt. D. Murray #159



NOT TO SCALE

01 Case Number: **2014-015518** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit: **35** 0631 A 1.10 118a 02

02 Police Dept of Code: **ROXBURY TWP 01** 150 At Intersection with N E Dir: **N** 12 Route No. **0631** 13 Milepost: **1.10** 118b -

03 Station/Precinct: **150** Feet S W of: **LAKESIDE BLVD.** 18 Speed Limit: **36** 118c -

06 4 Date of Crash: **07/13/14** 5 Day of Week: **SUN** 6 Time (use 2400 hrs): **2013** 7 Municipality Code: **1438** 8 Total Killed: **-** 9 Total Injured: **1** 17 Cross Road Name: **LAKESIDE BLVD.** 19 Ramp: To: From: NB SB WB 118d 25

100 23 Veh No: **01** 24 Policy No: **X26423103030N** 25 Ins Code: **962** 53 Veh No: **02** 54 Policy No: **8804991** 55 Ins Code: **018** 119b -

101 26 Driver's First Name: **JUDITH** Initial: **J** Last Name: **ANTONELLI** 29 Sex: **F** 56 Driver's First Name: **JOHN** Initial: **S** Last Name: **GILL** 59 Sex: **M** 121 01

102 27 Number and Street: **26 GENEVA TRAIL** 30 Eyes: **4** 57 Number and Street: **16 ROOSEVELT TRL** 60 Eyes: **4** 122 -

103 28 City: **HOPATCONG** State: **NJ** Zip: **07843-1143** 58 City: **HOPATCONG** State: **NJ** Zip: **07843** 123 -

104 31 State: **NJ** 32 Driver's License No: **A59884170063474** 33 DOB: **03/14/47** 34 Expires: **05/15** 61 State: **NJ** 62 Driver's License No: **G43504078201494** 63 DOB: **01/13/49** 64 Expires: **05/16** 124 06

105 35 Owner's First Name: **JUDITH** Initial: **J** Last Name: **ANTONELLI** 65 Owner's First Name: **JOHN** Initial: **S** Last Name: **GILL** 125 08

106 36 Number and Street: **26 GENEVA TRAIL** 66 Number and Street: **16 ROOSEVELT TRAIL** 126 04

107 37 City: **HOPATCONG** State: **NJ** Zip: **07843-1143** 67 City: **HOPATCONG** State: **NJ** Zip: **07843-1631** 127 04

108 38 Make: **HON** 39 Model: **PL** 40 Color: **SL** 41 Year: **2011** 42 Plate No.: **V61ADT** 43 State: **NJ** 68 Make: **NYD** 69 Model: **ACB** 70 Color: **BK** 71 Year: **2010** 72 Plate No.: **2524BB** 73 State: **NJ** 128a 26

109 44 VIN: **8FNYP4H6XBB037760** 45 Expires: **09/14** 74 VIN: **KMHCM3AC4AU163466** 75 Expires: **08/16** 128b -

110 46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police 128c -

111 48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 128d -

112 49 Hazardous Material On Board: Spill: Name or Placard No.: **-** 79 Hazardous Material On Board: Spill: Name or Placard No.: **-** 129a 26

113 50 Carrier No. USDOT Other: **-** 60 Carrier No. USDOT Other: **-** 129b -

114 51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 129c -

115 52 Carrier name: **-** 62 Carrier name: **-** 130 12

116 135 Crash Description: **Driver one reported that she was stopped in traffic on Landing Rd. directly behind vehicle two when her foot accidentally came off the brake pedal resulting in her vehicle rolling forward and hitting the rear of vehicle two. Driver two stated that he was stopped in the right lane of Landing Rd. just before the intersection of Lakeside Blvd. when vehicle two struck the rear of his vehicle. Passenger two complained of neck pain and was transported to St. Clare's Hospital Dover Campus for treatment by Roxbury Company One Rescue Squad. No visible damage was observed to vehicle one. Damage to vehicle two consisted of an impression of vehicle one's license plate on the back bumper. Driver one was at fault for the accident. End of report.** 131 17

136 Damage To Other Property: **NONE** 132 06

137 Charge: Multiple Charges 138 Summons No.: **-** Oper.: **-** 139 Charge: Multiple Charges 140 Summons No.: **-** 133 06

141 Officer's Signature: **EDMUNDS, JONATHAN** 142 Badge No.: **166** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	-	67	F	-	-	-	09	04	-	-	ANTONELLI, JUDITH, 26 GENEVA TRAIL, HOPATCONG, NJ 07843-1143
B	2	01	01	-	65	M	-	-	-	09	04	-	-	GILL, JOHN S, 16 ROOSEVELT TRL, HOPATCONG, NJ 07843
C	2	03	01	04	60	F	04	08	02	09	04	-	6403	GILL, LUCIA, 16 ROOSEVELT TRL, HOPATCONG, NJ 07843
D	-	-	-	-	-	-	-	-	-	-	-	-	-	
E	-	-	-	-	-	-	-	-	-	-	-	-	-	

CHASMS SOUTH
OF ITHACA ST

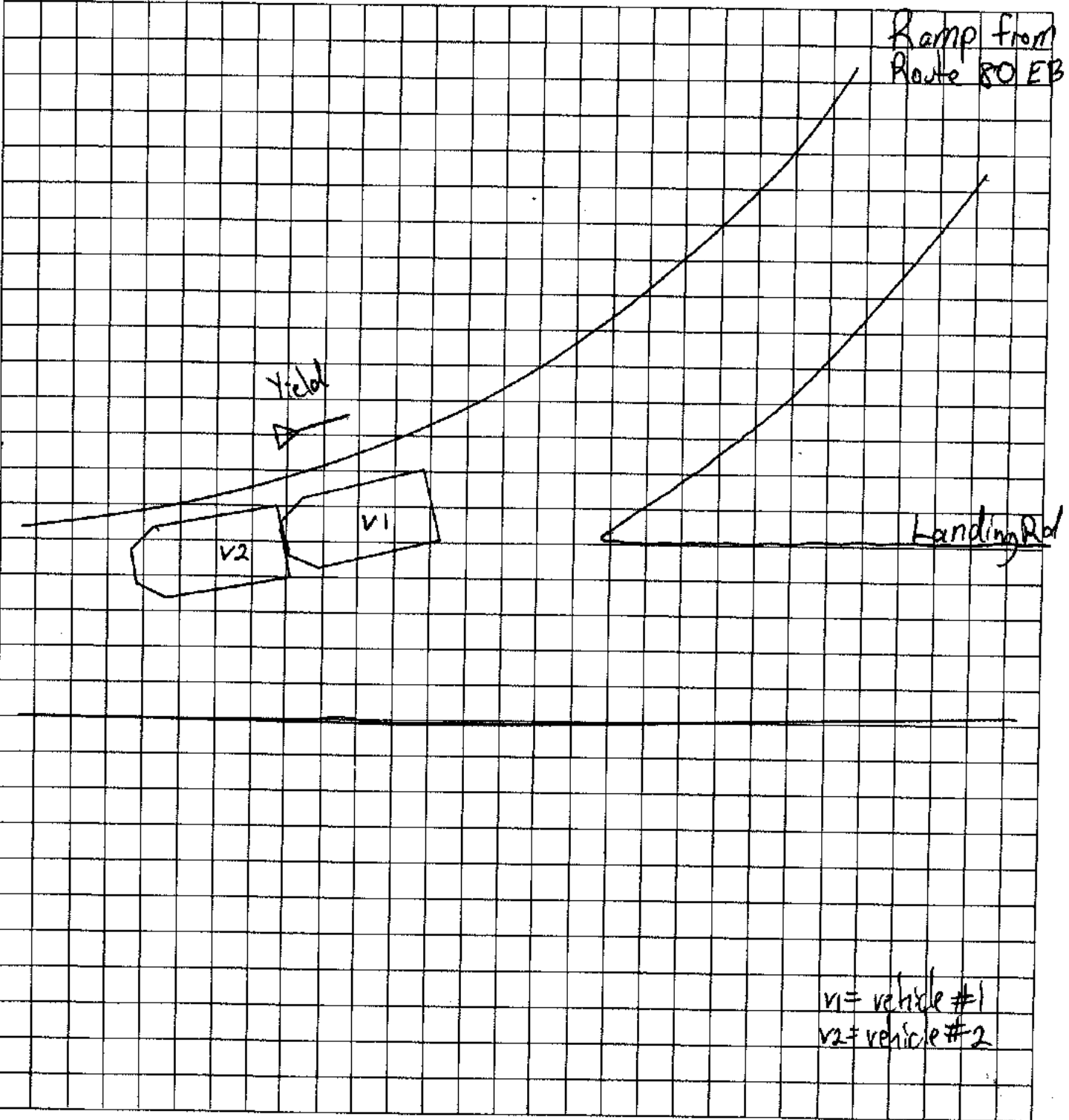
New Jersey Police Crash Investigation Report

Reportable
 Non-Reportable
 Change Report

96	01	Page 1 of 1 <input type="checkbox"/> Fatal																																																																																																																							
97	01	1 Case Number 2014-019427		10 Crash Occurred On: LANDING ROAD				11 Speed Limit S 46 0631 A .05				118a	05																																																																																																												
98	01	2 Police Dept of ROXBURY TWP		Code 01		100 Road Name 14		Dir S		12 Route No. Suffix RT. 80 E ON RAMP		13 Milepost 45		118b	--																																																																																																										
99	06	3 Station/Precinct		15		<input type="checkbox"/> At Intersection with <input checked="" type="checkbox"/> Feet <input type="checkbox"/> Miles		<input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> 16		17 Cross Road Name		<input type="checkbox"/> NB <input checked="" type="checkbox"/> EB <input type="checkbox"/> SB <input type="checkbox"/> WB		119a	25																																																																																																										
100	04	4 Date of Crash 08/26/14		5 Day of Week MON		6 Time (use 2400 hrs) 1344		7 Municipality Code 1435		8 Total Killed 00		9 Total Injured 00		119b	--																																																																																																										
101	02	23 Veh No 01		24 Policy No F087841-7		25 Ins Code 426		53 Veh No 92		54 Policy No 10784702		55 Ins Code 134		120	01																																																																																																										
102	01	26 Driver's First Name ADAM		Initial K		Last Name PAVLOV		29 Sex M		56 Driver's First Name NICOLE		Initial C		Last Name DIMAYO		121	01																																																																																																								
103	01	27 Number and Street 49 OAKWOOD AVE		30 Eyes 2		57 Number and Street P O BOX 203		31 State NJ		62 Driver's License No D44155915355892		63 DOB 08/27/85		64 Expires 02/17		122	00																																																																																																								
104	2	32 Driver's License No P09250087208872		33 DOB 06/22/97		34 Expires 07/18		35 Owner's First Name PETER		Initial P		Last Name PAVLOV		36 Owner's First Name NICOLE		Initial C		Last Name DIMAYO		123	00																																																																																																				
105	02	36 Number and Street 295 MT. ARLINGTON BLVD		37 City LANDING		State NJ		Zip 07850-1316		65 Owner's First Name NICOLE		Initial C		Last Name DIMAYO		124	11																																																																																																								
106	01	38 Make SUBA		39 Model LEG		40 Color GN		41 Year 2002		42 Plate No. SWD735		43 State NJ		66 Number and Street P O BOX 203		125	01																																																																																																								
107	01	44 VIN 4S3BE686927202433		45 Expires 08/14		74 VIN 6NPET46CX91643846		46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		77 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		126	04																																																																																																								
108	01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0.00 % <input type="checkbox"/> Pending		49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.		50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		52 Carrier name		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0.00 % <input type="checkbox"/> Pending		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		127	04																																																																																																						
109	01	53 Crash Description		54		55		56		57		58		59		128a	26																																																																																																								
110	03	Driver 1 stated he was traveling south bound on Landing Road in the left lane near the entrance ramp to Route 80 East. Driver 2 stated she was traveling south bound on Landing Road in the right lane near the entrance ramp to Route 80 East. Veh. 1 performed a right lane change and made contact with the driver side front door of Veh. 2. Veh. 1 sustained damage to the right passenger side door panels. On scene, Marisol Pavlov, a passenger in Veh. 1, stated the driver, Adam Pavlov, made a lane change near the intersection of the Route 80 East on ramp to avoid entrance. Both vehicles were able to be driven from the scene. Apparent cause of crash was unsafe lane change by Driver 1.														128b	--																																																																																																								
111	01	136 Damage To Other Property NONE		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.		141 Officer's Signature WEAVER, SCOTT		142 Badge No. 225		143 Reviewed By PALANCHI, MARC		144 Case Status CLOSED		128c	--																																																																																																				
112	01	81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		83		84		85		86		87		88		89		128d	--																																																																																																				
113	03	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th colspan="14">Names & Addresses of Occupants - if Deceased, Date & Time of Death</th> </tr> </thead> <tbody> <tr> <td>A</td> <td>1</td> <td>01</td> <td>01</td> <td>--</td> <td>17</td> <td>M</td> <td>--</td> <td>--</td> <td>01</td> <td>09</td> <td>04</td> <td>--</td> <td>--</td> <td colspan="4">PAVLOV, ADAM K, 49 OAKWOOD AVE, MINE HILL, NJ 07803-3227</td> </tr> <tr> <td>B</td> <td>1</td> <td>03</td> <td>01</td> <td>--</td> <td>40</td> <td>--</td> <td>--</td> <td>--</td> <td>01</td> <td>09</td> <td>04</td> <td>--</td> <td>--</td> <td colspan="4">PAVLOV, MARISOL, 122 DARTMOUTH TRAIL, HOPATCONG, NJ 07843-1153</td> </tr> <tr> <td>C</td> <td>1</td> <td>09</td> <td>01</td> <td>--</td> <td>12</td> <td>M</td> <td>--</td> <td>--</td> <td>01</td> <td>09</td> <td>09</td> <td>--</td> <td>--</td> <td colspan="4">PAVLOV, JOSHUA, 122 DARTMOUTH TRAIL, HOPATCONG, NJ 07843</td> </tr> <tr> <td>D</td> <td>2</td> <td>01</td> <td>01</td> <td>--</td> <td>24</td> <td>F</td> <td>--</td> <td>--</td> <td>01</td> <td>09</td> <td>04</td> <td>--</td> <td>--</td> <td colspan="4">DIMAYO, NICOLE C, P O BO. 203, LAYTON, NJ 07851-0203</td> </tr> <tr> <td>E</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td>--</td> <td colspan="4"></td> </tr> </tbody> </table>														Names & Addresses of Occupants - if Deceased, Date & Time of Death														A	1	01	01	--	17	M	--	--	01	09	04	--	--	PAVLOV, ADAM K, 49 OAKWOOD AVE, MINE HILL, NJ 07803-3227				B	1	03	01	--	40	--	--	--	01	09	04	--	--	PAVLOV, MARISOL, 122 DARTMOUTH TRAIL, HOPATCONG, NJ 07843-1153				C	1	09	01	--	12	M	--	--	01	09	09	--	--	PAVLOV, JOSHUA, 122 DARTMOUTH TRAIL, HOPATCONG, NJ 07843				D	2	01	01	--	24	F	--	--	01	09	04	--	--	DIMAYO, NICOLE C, P O BO. 203, LAYTON, NJ 07851-0203				E	--	--	--	--	--	--	--	--	--	--	--	--	--					129a	26
Names & Addresses of Occupants - if Deceased, Date & Time of Death																																																																																																																									
A	1	01	01	--	17	M	--	--	01	09	04	--	--	PAVLOV, ADAM K, 49 OAKWOOD AVE, MINE HILL, NJ 07803-3227																																																																																																											
B	1	03	01	--	40	--	--	--	01	09	04	--	--	PAVLOV, MARISOL, 122 DARTMOUTH TRAIL, HOPATCONG, NJ 07843-1153																																																																																																											
C	1	09	01	--	12	M	--	--	01	09	09	--	--	PAVLOV, JOSHUA, 122 DARTMOUTH TRAIL, HOPATCONG, NJ 07843																																																																																																											
D	2	01	01	--	24	F	--	--	01	09	04	--	--	DIMAYO, NICOLE C, P O BO. 203, LAYTON, NJ 07851-0203																																																																																																											
E	--	--	--	--	--	--	--	--	--	--	--	--	--																																																																																																												

96 05		New Jersey Police Crash Investigation Report										<input checked="" type="checkbox"/> Reportable		<input type="checkbox"/> Non-Reportable		<input type="checkbox"/> Change Report									
97 01	1 Case Number 2014-021101	10 Crash Occurred On: LANDING ROAD										11 Speed Limit 45		12 Route No. 0631		13 Milepost 0.04		118a 02							
98 01	2 Police Dept of ROXBURY TWP	14 Road Name LANDING ROAD										15 Dir S		16 Suffix A		17 Speed Limit 65		118b --							
99 05	3 Station/Precinct --	15 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles 16										19 Ramp <input type="checkbox"/> To: --		20 Route/Name ROUTE 80 RAMP		21 NB <input type="checkbox"/> EB <input type="checkbox"/> SB <input type="checkbox"/> WB		119a 25							
100 01	4 Date of Crash mm dd yy 09/11/14	5 Day of Week THU	6 Time (use 2400 hrs) 0640	7 Municipality Code 1438	8 Total Killed 0	9 Total Injured 1	21 Latitude --	22 Longitude --	23 Veh No 01		24 Policy No. PANJ-003576087		25 Ins Code 071		53 Veh No 02		54 Policy No. BAP644543624		55 Ins Code 379		119b --				
101 02	<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN										<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN										120 01				
102 01	26 Driver's First Name JENNIFER				Initial S		Last Name OLIVIER				28 Sex F		56 Driver's First Name JOHN				Initial A		Last Name CUBBERLY				59 Sex M		121 01
103 05	27 Number and Street 15 SPRUCE DRIVE				30 Eyes 4		57 Number and Street 21 BROAD AVE				60 Eyes 2		28 City ANDOVER		State NJ		Zip 07821-2235		58 City STANHOPE		State NJ		Zip 07874-3411		122 --
104 02	31 State NJ	32 Driver's License No 054013956257864			33 DOB mm dd yy 07/10/86		34 Expires mm yy 02/16		61 State NJ	62 Driver's License No C90194076107752			63 DOB mm dd yy 07/08/75		64 Expires mm yy 07/16		123 --								
105 01	35 Owner's First Name WILLIAM				Initial R		Last Name OLIVIER				65 Owner's First Name SCHINDLER ELEVATOR CORP				Initial --		Last Name --				124 07				
106 --	36 Number and Street 15 SPRUCE DRIVE				66 Number and Street 20 WHIPPANY RD				37 City ANDOVER				State NJ		Zip 07821-2235		67 City MORRISTOWN				State NJ		Zip 07960		125 08
108 01	38 Make SUB	39 Model LEG	40 Color GY	41 Year 2011	42 Plate No. J32ASH	43 State NJ	68 Make FOR	69 Model TRA	70 Color WT	71 Year 2013	72 Plate No. XARK91	73 State NJ	126 09												
109 03	44 VIN 4S3BMBK67B3246460				45 Expires mm yy 05/15		74 VIN NM0L57AN8DT148036				75 Expires mm yy 05/15		127 09												
110 01	46 Vehicle Removed To PARKING LOT				<input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police				76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed				77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		128a 28								
111 02	48 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending				78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending				128b --																
112 --	49 Hazardous Material On Board				Name or Placard No.				79 Hazardous Material On Board				Name or Placard No.				128c --								
113 --	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				129a 26																
114 --	51 Commercial Vehicle Weight				81 Commercial Vehicle Weight				129b --																
115 03	<input type="checkbox"/> < 10,000 lbs				<input type="checkbox"/> < 10,000 lbs				130 12																
116 03	<input type="checkbox"/> 10,001 - 26,000 lbs				<input type="checkbox"/> 10,001 - 26,000 lbs				131 12																
117 03	<input type="checkbox"/> > 26,001 lbs				82 Carrier name				132 06																
117 03	135 Crash Description Driver #1 said she was travelling south on Landing Rd. D1 said that she was slowing down and looked left to check traffic when her vehicle struck the rear of V2. Driver #2 said that he was stopped on Landing Rd south attempting to merge into traffic when his vehicle was struck in the rear by V1. D1 (Olivier) complained of neck pain. Roxbury ambulance 210 responded. D1 had her husband transport her for medical evaluation. Driver #1 (Olivier) at fault for the crash for striking the rear of V2.																				133 06				
136 Damage To Other Property																					--				
141 Officer's Signature HENRIKSEN, ERICK		137 Charge				138 Summons No.				139 Charge		140 Summons No.				142 Badge No. 188		143 Reviewed By PALANCHI, MARC				144 Case Status CLOSED			
141 Officer's Signature <i>[Signature]</i>		137 Charge				138 Summons No.				139 Charge		140 Summons No.				142 Badge No. 188		143 Reviewed By <i>[Signature]</i>				144 Case Status CLOSED			
A	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death											
A	1	01	01	04	28	F	04	06	01	09	04	--	00	OLIVIER, JENNIFER S. 15 SPRUCE DRIVE, ANDOVER, NJ 07821-2235											
B	2	01	01	--	38	M	--	--	--	09	04	--	--	CUBBERLY, JOHN A. 21 BROAD AVE. STANHOPE, NJ 07874-3411											
C	--	--	--	--	--	--	--	--	--	--	--	--	--												
D	--	--	--	--	--	--	--	--	--	--	--	--	--												
E	--	--	--	--	--	--	--	--	--	--	--	--	--												

134 Crash Diagram (NOT TO SCALE)



C. [Signature]

Officer's Signature

188

Badge Number

01 1 Case Number **2014-021680** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit **45** 118a 02
 01 2 Police Dept of **ROXBURY TWP** Code **01** 300 Road Name **LANDING RD** Dir **N** 45 0631 A 0.10 118b --
 01 3 Station/Precinct **---** 14 At Intersection with N E of: **SHIPPENPORT RD** 12 Route No. Suffix 13 Milepost 118c --
 05 15 Feet S W 17 Cross Road Name **35** 18 Speed Limit 119a 25
 15 Miles 16 19 Ramp To: NB EB 119b --
 From: SB WB

01 4 Date of Crash **09/17/14** 5 Day of Week **WED** 6 Time (use 2400 hrs) **0804** 7 Municipality Code **1436** 8 Total Killed **---** 9 Total Injured **---** 20 Route/Name **---** 21 Latitude **---** 22 Longitude **---**
 01 23 Veh No **01** 24 Policy No. **4037-28-11-61** 25 Ins Code **100** 53 Veh No **02** 54 Policy No. **X16 9471-F06-30V** 55 Ins Code **962**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

01 26 Driver's First Name **KATHLEEN** Initial **A** Last Name **VACCACIO** 29 Sex **F** 56 Driver's First Name **ASHLEY** Initial **D** Last Name **FINKELSTEIN** 59 Sex **F**
 27 Number and Street **ONE EXETER WAY** 30 Eyes **2** 57 Number and Street **43 RUSSLING RD** 60 Eyes **2**
 28 City **SUCCASUNNA** State **NJ** Zip **07876-1181** 58 City **HACKETTSTOWN** State **NJ** Zip **07840-4832**

02 31 State **NJ** 32 Driver's License No **V00294276162552** 33 DOB **12/11/55** 34 Expires **02/16** 61 State **NJ** 62 Driver's License No **F45030636463932** 63 DOB **03/30/93** 64 Expires **03/18**
 35 Owner's First Name **KATHLEEN** Initial **A** Last Name **VACCACIO** 65 Owner's First Name **JANICE** Initial **A** Last Name **ZITTEL**
 Same As Driver Same As Driver

01 36 Number and Street **ONE EXETER WAY** 66 Number and Street **43 RUSSLING RD**
 37 City **SUCCASUNNA** State **NJ** Zip **07876-1181** 67 City **HACKETTSTOWN** State **NJ** Zip **07840-4803**
 38 Make **HYD** 39 Model **SON** 40 Color **SL** 41 Year **2014** 42 Plate No. **E50EGW** 43 State **NJ** 68 Make **NIS** 69 Model **ROG** 70 Color **BK** 71 Year **2011** 72 Plate No. **T18DEY** 73 State **NJ**

01 44 VIN **5NPEB4AC2EH932999** 45 Expires **04/17** 74 VIN **JN8AS6MV0BWJ12714** 75 Expires **06/16**
 46 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 47 Authority Owner Driver Police 76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 77 Authority Owner Driver Police

01 48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending

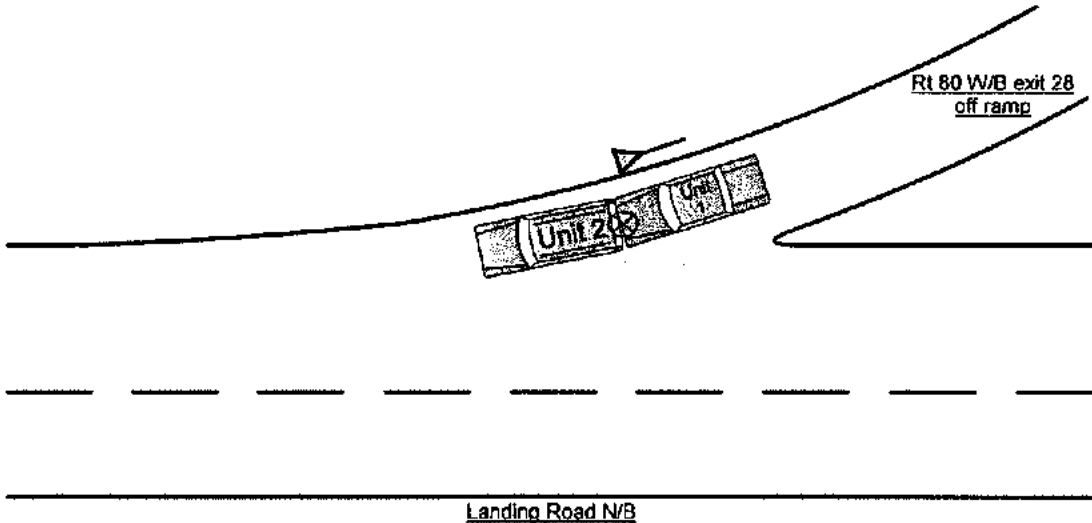
01 49 Hazardous Material On Board Spill Name or Placard No. **---** 79 Hazardous Material On Board Spill Name or Placard No. **---**
 50 Carrier No. USDOT Other 80 Carrier No. USDOT Other
 51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

01 52 Carrier name **---** 82 Carrier name **---**
 135 Crash Description
 01 Driver veh. #1 stated she was coming off the ramp for exit 28 from Rt. 80 w/b onto Landing Rd.. As she approached the end of the ramp veh. #2 was in front of her and stopped. Unable to stop in time the front end of her vehicle collided with the rear end of veh. #2. Driver veh. #2 stated that she was coming off of exit 28 from Rt. 80 w/b onto Landing Rd. and stopped at the end of the ramp to yield to traffic. While stopped her vehicle was struck in the rear by the front end of veh. #1. No injuries were reported or observed. Veh. #1 exhibited damage to the front bumper. Veh. #2 exhibited damage to the rear bumper and possibly to the driver side rear tire. Driver veh. #1 at fault for the crash.

136 Damage To Other Property **---**
 Oper. **---** 137 Charge Multiple Charges 138 Summons No. **---** Oper. **---** 139 Charge Multiple Charges 140 Summons No. **---**
 141 Officer's Signature **MURRAY, DANIEL** 142 Badge No. **159** 143 Reviewed By **MURRAY, DANIEL** 144 Case Status **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	58	F	--	--	--	09	04	--	--	VACCACIO, KATHLEEN A, ONE EXETER WAY, SUCCASUNNA, NJ 07876-1181
B	2	01	01	--	21	F	--	--	--	09	04	--	--	FINKELSTEIN, ASHLEY D, 43 RUSSLING RD, HACKETTSTOWN, NJ 07840-4832
C	2	03	01	--	20	M	--	--	--	09	04	--	--	JOHNSON, DARRIEN S, 11 CANFIELD STREET, BUDD LAKE, NJ 07828-2942
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

Case No.: 2014-021660
Agency: Roxbury Twp.
Officer: Sgt. D. Murray #159



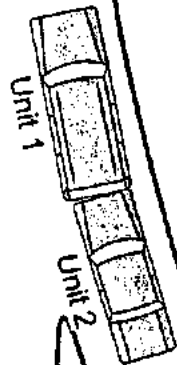
NOT TO SCALE

97	01	1 Case Number 2014-021333										118a	25
98	01	2 Police Dept of ROXBURY TWP Code 01										118b	--
99	05	3 Station/Precinct 100										119a	02
100	01	10 Crash Occurred On LANDING RD 11 Speed Limit 35										119b	--
101	02	12 Route No. 0831 A 13 Milepost 10										120	01
102	02	14 <input type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E Dir NORTH										121	01
103	02	15 <input checked="" type="checkbox"/> Feet <input checked="" type="checkbox"/> S <input type="checkbox"/> W of SHIPPENPORT RD 18 Speed Limit 35										122	--
104	02	16 <input type="checkbox"/> Miles 17 Cross Road Name										123	--
105	01	19 <input type="checkbox"/> To: <input type="checkbox"/> N6 <input type="checkbox"/> EB										124	08
106	01	20 Route/Name <input type="checkbox"/> SB <input type="checkbox"/> WB										125	07
107	01	4 Date of Crash mm dd yy 09/13/14										126	09
108	01	5 Day of Week SAT										127	09
109	01	6 Time (use 2400 hrs) 1809										128a	26
110	01	7 Municipality Code 1436										128b	--
111	01	8 Total Killed --										128c	--
112	01	9 Total Injured --										128d	--
113	01	21 Latitude										129a	26
114	01	22 Longitude										129b	--
115	01	23 Veh No 01 24 Policy No.										129c	--
116	01	25 Ins Code 02 53 Veh No 02 54 Policy No. 27722648 55 Ins Code 134										129d	--
117	01	<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN										130	06
118	01	26 Driver's First Name Initial Last Name VINCENT L RECASINO 29 Sex M 56 Driver's First Name Initial Last Name KERI N CANTWELL 59 Sex F										131	06
119	01	27 Number and Street 7818 NW WELCO AVE 30 Eyes 2 57 Number and Street 227 LAKESIDE BLVD APT-3 60 Eyes 4										132	12
120	01	28 City LAWTON State OK Zip 73506 58 City HOPATCONG State NJ Zip 07843-1619										133	--
121	01	31 State VA 32 Driver's License No T61788526 33 DOB 02/10/79 34 Expires 02/17 61 State NJ 62 Driver's License No C04934327662884 63 DOB 12/09/86 64 Expires 01/17										134	--
122	01	35 Owner's First Name Initial Last Name <input checked="" type="checkbox"/> Same As PATRICIA ROBERTS 65 Owner's First Name Initial Last Name <input type="checkbox"/> Same As DANIEL DIPPEL										135	--
123	01	36 Make FORD 39 Model ESCAPE 40 Color SILVER 41 Year 2010 42 Plate No. YASNA 43 State VA 66 Make HYD 69 Model ELA 70 Color BK 71 Year 2013 72 Plate No. WZ7DUZ 73 State NJ										136	--
124	01	37 City PORTSMOUTH State OK Zip 73703 67 City HOPATCONG State NJ Zip 07843-1619										137	--
125	01	38 Number and Street 4209 MEADOWVIEW RD 68 Number and Street 227 LAKESIDE BLVD APT 3										138	--
126	01	39 Model ESCAPE 40 Color SILVER 41 Year 2010 42 Plate No. YASNA 43 State VA 68 Make HYD 69 Model ELA 70 Color BK 71 Year 2013 72 Plate No. WZ7DUZ 73 State NJ										139	--
127	01	44 VIN 1FMCU4K39AKC47818 45 Expires 11/14 74 VIN KMHDH4AE2DU938653 75 Expires 01/17										140	--
128	01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed 47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police										141	--
129	01	76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed 77 Authority <input type="checkbox"/> Impound <input type="checkbox"/> Disabled <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police										142	--
130	01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending										143	--
131	01	78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending										144	--
132	01	49 Hazardous Material On Board Spill Name or Placard No.										145	--
133	01	79 Hazardous Material On Board Spill Name or Placard No.										146	--
134	01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other										147	--
135	01	80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other										148	--
136	01	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs										149	--
137	01	61 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs										150	--
138	01	52 Carrier name										151	--
139	01	82 Carrier name										152	--
140	01	135 Crash Description 24/25 - ALLSTATE PROPERTY AND CASUALTY INSURANCE CO., 4209 MEADOWVIEW RD, PORTSMOUTH VA 23703										153	12
141	01	136 Damage To Other Property NONE										154	--
142	01	137 Charge <input type="checkbox"/> Multiple Charges 138 Summons No. Oper. 139 Charge <input type="checkbox"/> Multiple Charges 140 Summons No.										155	--
143	01	141 Officer's Signature FOURNIER, MICHAEL 142 Badge No. 193 143 Reviewed By PALANCHI, MARC 144 Case Status CLOSED										156	--
144	01	145 Names & Addresses of Occupants - If Deceased, Date & Time of Death										157	--
145	01	A 1 01 01 -- 35 M -- -- -- 09 04 -- -- RECASINO, VINCENT L, 7818 NW WELCO AVE, LAWTON, OK 73505										158	--
146	01	B 1 03 01 -- -- F -- -- -- 09 04 -- -- RECASINO, BIANCA, 7818 NW WELCO AVE, LAWTON, OK 73505										159	--
147	01	C 1 07 01 -- 1 F -- -- -- 05 05 -- -- RECASINO, MADELINE, 7818 NW WELCO AVE, LAWTON, OK 73505										160	--
148	01	D 2 01 01 -- 27 F -- -- -- 09 04 -- -- CANTWELL, KERI N, 227 LAKESIDE BLVD APT-3, HOPATCONG, NJ 07843-1619										161	--
149	01	E -- -- -- -- -- -- -- -- -- -- -- -- -- -- -- --										162	--



NOT TO SCALE

LANDING RD



ROXBURY TOWNSHIP
2014-021333
PTL M. FOURNIER
PAGE 2 OF 2

ROUTE 80 OFF RAMP

03	1 Case Number 2014-022612		10 Crash Occurred On: LANDING RD		11 Speed Limit SOUTH 35 0631 A 0.01		118a	26
01	2 Police Dept of ROXBURY TWP		Code 01		Road Name Dir 12 Route No. Suffix 13 Milepost		118b	-
01	3 Station/Precinct 14		<input type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input checked="" type="checkbox"/> Feet <input checked="" type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles <input type="checkbox"/> 16		of N FRONTAGE RD		119a	02
05	4 Date of Crash 09/27/14		5 Day of Week SAT		6 Time (use 2400 hrs) 1701		7 Municipality Code 1438	
04	8 Total Killed 00		9 Total Injured 00		19 Ramp <input type="checkbox"/> To: <input type="checkbox"/> From: <input type="checkbox"/> 20 Route/Name		119b -	
01	23 Veh No 01		24 Policy No. A07-238-0202521-70-44		25 Ins Code 914		53 Veh No 02	
02	54 Policy No. 009943303		55 Ins Code 012		21 Latitude		22 Longitude	
01	26 Driver's First Name CHELSEA		Initial M		Last Name SWAYNOS		29 Sex F	
01	27 Number and Street 14 MUSCONETCONG AVE		28 City HOPATCONG		State NJ		Zip 07843-1610	
01	56 Driver's First Name MATTHEW		Initial J		Last Name AQUINA		59 Sex M	
01	57 Number and Street 33 CARLTON RD		58 City FLANDERS		State NJ		Zip 07836-4410	
02	31 State NJ		32 Driver's License No S95741227461962		33 DOB 01/04/86		34 Expires 01/17	
02	61 State NJ		62 Driver's License No A71196297106862		63 DOB 06/11/86		64 Expires 03/16	
02	35 Owner's First Name <input type="checkbox"/> Same As EDWARD		Initial J		Last Name SWAYNOS		65 Owner's First Name <input type="checkbox"/> Same As VICKI	
01	66 Owner's First Name AQUINA		Initial A		Last Name AQUINA		67 City FLANDERS	
01	36 Number and Street 14 MUSCONETCONG AVE		37 City HOPATCONG		State NJ		Zip 07843-1610	
01	66 Number and Street 33 CARLTON RD		67 City FLANDERS		State NJ		Zip 07836-4410	
01	38 Make MINI		39 Model ECL		40 Color BLACK		41 Year 2003	
01	42 Plate No. F46CTL		43 State NJ		68 Make FORD		69 Model FOC	
01	44 VIN 4A3AC44G03E121223		45 Expires 01/16		70 Color GY		71 Year 2010	
01	72 Plate No. V77BAL		73 State NJ		74 VIN 1FAHP3CN6AW216202		75 Expires 07/15	
01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		77 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police	

01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		128d	-
01	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine	
01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.	
13	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs		82 Carrier name		128e	
13	<input type="checkbox"/> 10,001 - 26,000 lbs		<input type="checkbox"/> 10,001 - 26,000 lbs				130	
13	<input type="checkbox"/> > 26,001 lbs		<input type="checkbox"/> > 26,001 lbs				131	
03	52 Carrier name						132	

135 Crash Description

D1 STATES SHE WAS TRAVELING SOUTH ON LANDING RD IN THE RIGHT LANE WHEN V2 DRIFTED INTO HER LANE. D1 FURTHER STATES SHE BEEPED AT V2 TO AVOID A CRASH AND V2 CRASHED INTO THE DRIVERS SIDE OF V1. AFTER THE ACCIDENT D1 SLOWED DOWN AND GOT THE LICENSE PLATE OF V2 (NJ REG#V77BAL) WHO LEFT THE SCENE AND NEVER STOPPED. INITIALLY AN OFFICER WAS SENT TO THE HOUSE OF D2 BUT HE WAS NOT HOME. I THEN LEFT A VOICE MESSAGE AND D2 CALLED ME BACK AND RESPONDED TO ROXBURY POLICE HQ. D2 STATED HE WAS TRAVELING SOUTH ON LANDING RD IN THE LEFT LANE AND DID NOT SEE V1 BECAUSE IT WAS IN HIS BLIND SPOT AND CRASHED INTO V1. D2 FURTHER STATED HE HAD NO PLACE TO PULL OVER AND JUMPED ON I-80 EAST AND NEVER STOPPED. I DID NOTICE DAMAGE TO THE PASSENGER SIDE FRONT OF V2. D2 AT FAULT AND WAS ISSUED A CARELESS

136 Damage To Other Property NONE				137 Charge <input type="checkbox"/> Multiple Charges				138 Summons No.				139 Charge <input type="checkbox"/> Multiple Charges CARELESS				140 Summons No. E1633			
141 Officer's Signature DENICOLA, THOMAS <i>Thomas Denicola</i>								142 Badge No. 207				143 Reviewed By PALANCHI, MARC <i>Marc Palanchi</i>				144 Case Status CLOSED			

	83	84	85	86	87	88	89	90	91	92	93	94	95	Name & Address of Occupants - If Deceased, Date & Time of Death			
A	1	01	01	-	18	F	-	-	01	09	04	-	-	SWAYNOS, CHELSEA M, 14 MUSCONETCONG AVE, HOPATCONG, NJ 07843-1610			
B	1	03	01	-	16	F	-	-	01	08	04	-	-	MIKAJLO, JACKIE, 403 DURBAN AVE, HOPATCONG, NJ 07843			
C	2	01	01	-	28	M	-	-	01	09	04	-	-	AQUINA, MATTHEW J, 33 CARLTON RD, FLANDERS, NJ 07836-4410			
D	-	-	-	-	-	-	-	-	-	-	-	-	-				
E	-	-	-	-	-	-	-	-	-	-	-	-	-				

New Jersey Police Crash Investigation Report

Motor Vehicle Crash Diagram

Police Dept: ROXBURY

Code: 01

Station: _____

Case No: 14-02262

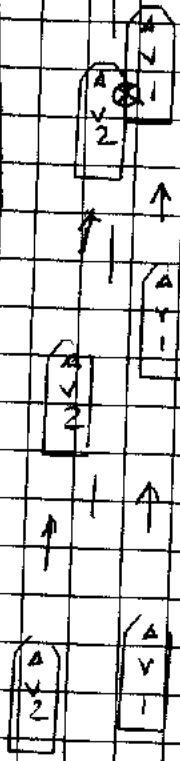
134 Crash Diagram (NOT TO SCALE)



I-80 E ONRAMP

NOT TO SCALE

ROAD



DM

Officer's Signature

207

Badge Number

New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

96	02	Page 1 of 2 <input type="checkbox"/> Fatal		New Jersey Police Crash Investigation Report																								
97	02	1 Case Number 2014-021334		10 Crash Occurred On: LAKESIDE BOULEVARD SOUTH 35 11 Speed Limit 0631 A 1.30										118a														
98	01	2 Police Dept of ROXBURY TWP Code 01		Road Name SOUTH 35 Dir A 12 Route No. 0631 Suffix A 13 Milepost 1.30										118b														
99	06	3 Station/Precinct		14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> S <input type="checkbox"/> W of: LANDING ROAD 18 Speed Limit 36										119a														
100	02	4 Date of Crash mm dd yy 09/13/14		5 Day of Week SAT		6 Time (use 2400 hrs) 1816		7 Municipality Code 1438		8 Total Killed 0		9 Total Injured 0		19 Ramp <input type="checkbox"/> To: <input type="checkbox"/> From: <input type="checkbox"/> 20 Route/Name <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> SB <input type="checkbox"/> WB		119b												
101	02	23 Veh No 01		24 Policy No. 0881637		25 Ins Code 896		53 Veh No 02		54 Policy No. 0470840A0830B		55 Ins Code 962		21 Latitude - 22 Longitude -		120												
102	02	26 Driver's First Name Initial Last Name JASON R TALLEY				29 Sex M		56 Driver's First Name Initial Last Name GLADYS M GIL				59 Sex F		27 Number and Street 4 BELL COURT		57 Number and Street 52 LAURIE RD		121										
103	02	28 City LANDING State NJ Zip 07850				30 Eyes 4		58 City LANDING State NJ Zip 07850-1666				60 Eyes 2		31 State NJ		32 Driver's License No T02963887909774		33 DOB mm dd yy 09/26/77		34 Expires mm yy 01/17		122						
104	01	35 Owner's First Name Initial Last Name <input checked="" type="checkbox"/> Same As JASON R TALLEY				36 Number and Street 4 BELL CT		65 Owner's First Name Initial Last Name <input checked="" type="checkbox"/> Same As GLADYS M GIL				66 Number and Street 52 LAURIE RD		63 DOB mm dd yy 12/21/64		64 Expires mm yy 11/15		37 City LANDING State NJ Zip 07850-1257		67 City LANDING State NJ Zip 07850-1666		123						
105	03	38 Make FOR		39 Model ECO		40 Color RED		41 Year 1996		42 Plate No. F94CUV		43 State NJ		68 Make HON		69 Model PH		70 Color WT		71 Year 2004		72 Plate No. WPR78C		73 State NJ		124		
106	01	44 VIN 1FTJE34H6THA46980		45 Expires 02/15		46 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		47 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		125
107	01	48 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input checked="" type="checkbox"/> Refused Type: <input checked="" type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		52 Carrier name		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		Name or Placard No.		126				
108	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		126a				
109	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		126b				
110	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		126c				
111	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		126d				
112	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		129a				
113	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		129b				
114	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		129c				
115	01	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		129d				
116	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		130				
117	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		131				
118	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		132				
119	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133				
120	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133a				
121	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133b				
122	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133c				
123	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133d				
124	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133e				
125	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133f				
126	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name		133g				
127	03	48 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input checked="" type="checkbox"/> Disabled		49 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 2HKYP186X4H612960		75 Expires 01/18		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input checked="" type="checkbox"/> Police		78 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,00								

New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

1 Case Number 2014-021334		10 Crash Occurred On: LAKESIDE BOULEVARD SOUTH 35 9631 A 1.30										118a 25							
2 Police Dept of ROXBURY TWP Code 01		Road Name: LAKESIDE BOULEVARD SOUTH 35 Dir: A 12 Route No. Suffix: 13 Milepost: 1.30										118b -							
3 Station/Precinct		14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Feet <input type="checkbox"/> Miles 16										118c -							
4 Date of Crash mm dd yy 09/13/14		5 Day of Week SAT		6 Time (use 2400 hrs) 1815		7 Municipality Code 1435		8 Total Killed 0		9 Total Injured 0		119a -							
23 Veh No 03		24 Policy No. 5845B1230				25 Ins Code 079		53 Veh No		54 Policy No.		55 Ins Code	119b -						
<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN												120 01							
26 Driver's First Name LOUIS Initial J Last Name SLOANE				28 Sex M		56 Driver's First Name				58 Sex		59 Sex	121 -						
27 Number and Street 45 SILVER SPRINGS DR				30 Eyes 4		57 Number and Street				60 Eyes		122 00							
28 City LANDING State NJ Zip 07850-1235				58 City				State				Zip	123 -						
31 State NJ		32 Driver's License No. S64974887106634		33 DOB mm dd yy 06/21/63		34 Expires mm yy 08/16		61 State		62 Driver's License No.		63 DOB mm dd yy	64 Expires mm yy	124 08					
35 Owner's First Name <input checked="" type="checkbox"/> Same As LOUIS Initial J Last Name SLOANE				65 Owner's First Name				67 Last Name				69 Sex	125 -						
36 Number and Street 45 SILVER SPRINGS DR				66 Number and Street				70 State				72 State	126 03						
37 City LANDING State NJ Zip 07850-1235				67 City				State				Zip	127 -						
38 Make DOD		39 Model RAM		40 Color PRP		41 Year 2006		42 Plate No. C98EPL		43 State NJ		68 Make		69 Model	70 Color	71 Year	72 Plate No.	73 State	128a 26
44 VIN 1D7HA18N55J639405				45 Expires 08/16				74 VIN				75 Expires				128b -			
46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed				47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police				76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed				77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police				128c -			
48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending				78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending				128d -											
49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>				79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>				129a -											
50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				129b -											
51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				129c -											
52 Carrier name				82 Carrier name				129d -											
135 Crash Description												130 06							
136 Damage To Other Property												131 06							
137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.		141 Officer's Signature FIELD, JOHN <i>John Field</i>				142 Badge No.		143 Reviewed By PALANCHI, MARC <i>Marc Palanchi</i>		144 Case Status CLOSED		132 -	

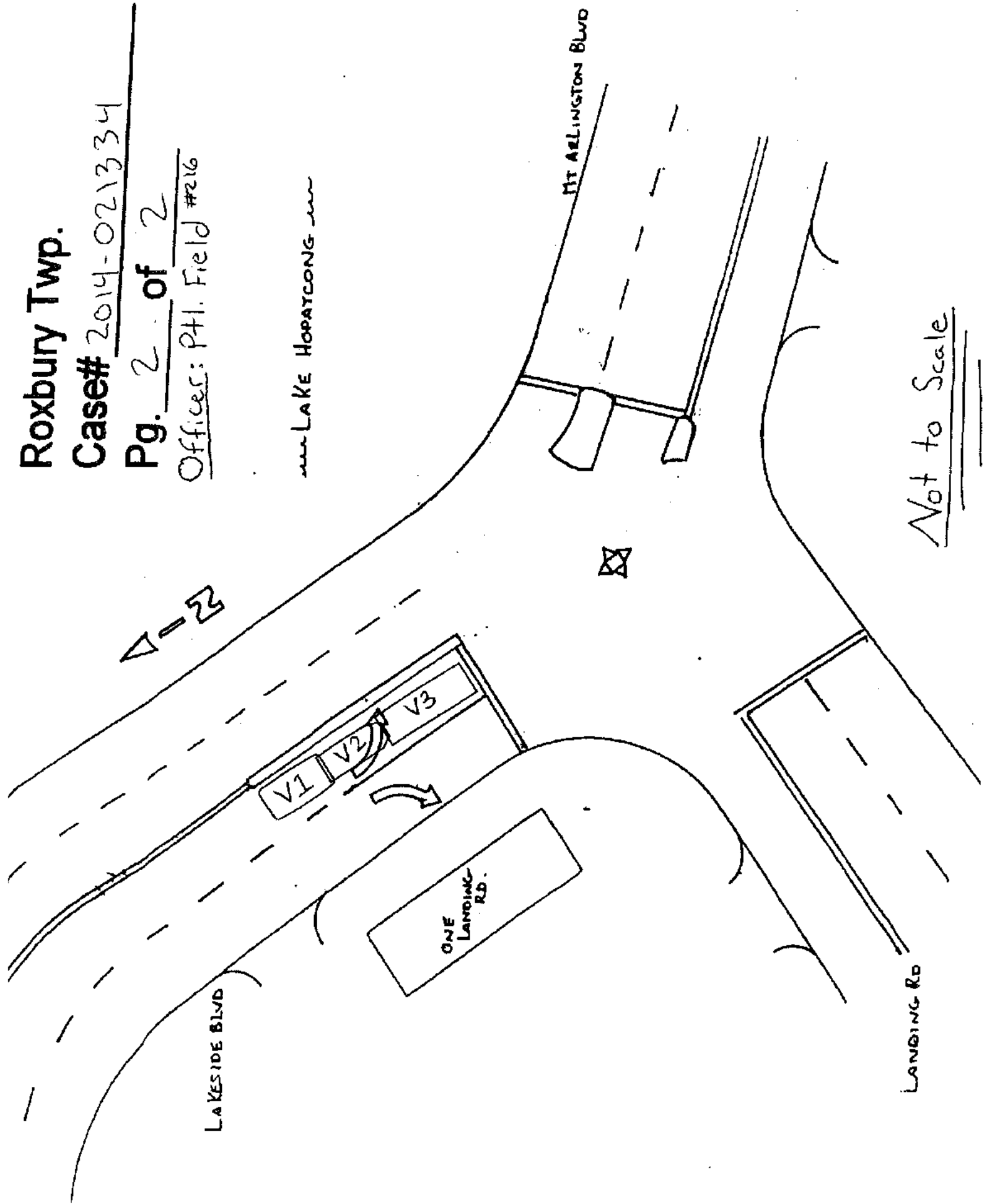
Roxbury Twp.

Case# 2014-021334

Pg. 2 of 2

Officer: Ptl. Field #216

LAKE HOPATCONG



Not to Scale

1 Case Number 2014-026130	10 Crash Occurred On: LANDING RD										11 Speed Limit S 35 0631 A .04		118a 02		
2 Police Dept of ROXBURY TWP	Code 01		14 Road Name RT. 80 EXIT RAMP										12 Route No. Suffix 18 Speed Limit 65		118b --
3 Station/Precinct 1436			15 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles 18										17 Cross Road Name		119a --
4 Date of Crash 11/06/14		5 Day of Week THU		6 Time (use 2400 hrs) 0820		7 Municipality Code 1436		8 Total Killed 0		9 Total Injured 0		19 Ramp <input type="checkbox"/> To: <input type="checkbox"/> From: <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> SB <input type="checkbox"/> WB		119b --	
23 Veh No 01		24 Policy No. 4244240158				25 Ins Code 148		53 Veh No 02		54 Policy No.		55 Ins Code		120 01	
26 Driver's First Name Initial Last Name JAMES D DUSSINGER														121 --	
27 Number and Street 21 PATERSON PLACE														122 --	
28 City State Zip NEWTON NJ 07860-2321														123 --	
31 State NJ		32 Driver's License No D94573836408962				33 DOB mm dd yy 08/27/98		34 Expires mm yy 08/17		61 State		62 Driver's License No		124 12	
35 Owner's First Name Initial Last Name <input type="checkbox"/> Same As DONNA S SORICELLI-DUSSING														125 --	
36 Number and Street 21 PATERSON PLACE														126 --	
37 City State Zip NEWTON NJ 07860-2321														127 --	
38 Make FORD		39 Model MUS		40 Color WT		41 Year 2012		42 Plate No. B57DJG		43 State NJ		68 Make FORD		128a 26	
44 VIN 1ZVBP8AM9C5287301		45 Expires 07/15		46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		47 Authority <input checked="" type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		128b --	
49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.														128c --	
50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other														128d --	
51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs														129a 26	
52 Carrier name														129b --	
135 Crash Description Driver 1 stated he was merging onto Landing Rd. off of Rt. 80. Driver 1 stated he did not pay attention to the merge and rear ended Vehicle 2. Driver 2 exited his vehicle and asked Driver 1 for money. When Driver 1 refused, Driver 2 fled the scene. Vehicle 2 is described as a maroon Ford pickup with unknown registration. I called the New Jersey State Police to see if they had a report of an accident on their ramp and they had not. Driver 1 is at fault for the accident for rear ending Driver 2.														129c --	
136 Damage To Other Property														129d --	
Oper. 1		137 Charge <input type="checkbox"/> Multiple Charges				138 Summons No.		Oper.		139 Charge <input type="checkbox"/> Multiple Charges				140 Summons No.	
141 Officer's Signature FITZGERALD, JOHN <i>PTC. John Fitzgerald</i>						142 Badge No.		143 Reviewed By PALANCHI, MARC <i>Marc Palanchi</i>				144 Case Status CLOSED		130 12	

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	18	M	--	--	--	09	04	--	--	DUSSINGER, JAMES D. 21 PATERSON PLACE, NEWTON, NJ 07860-2321
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

New Jersey Police Crash Investigation Report

Police Dept: Roxbury Township Code: 1436

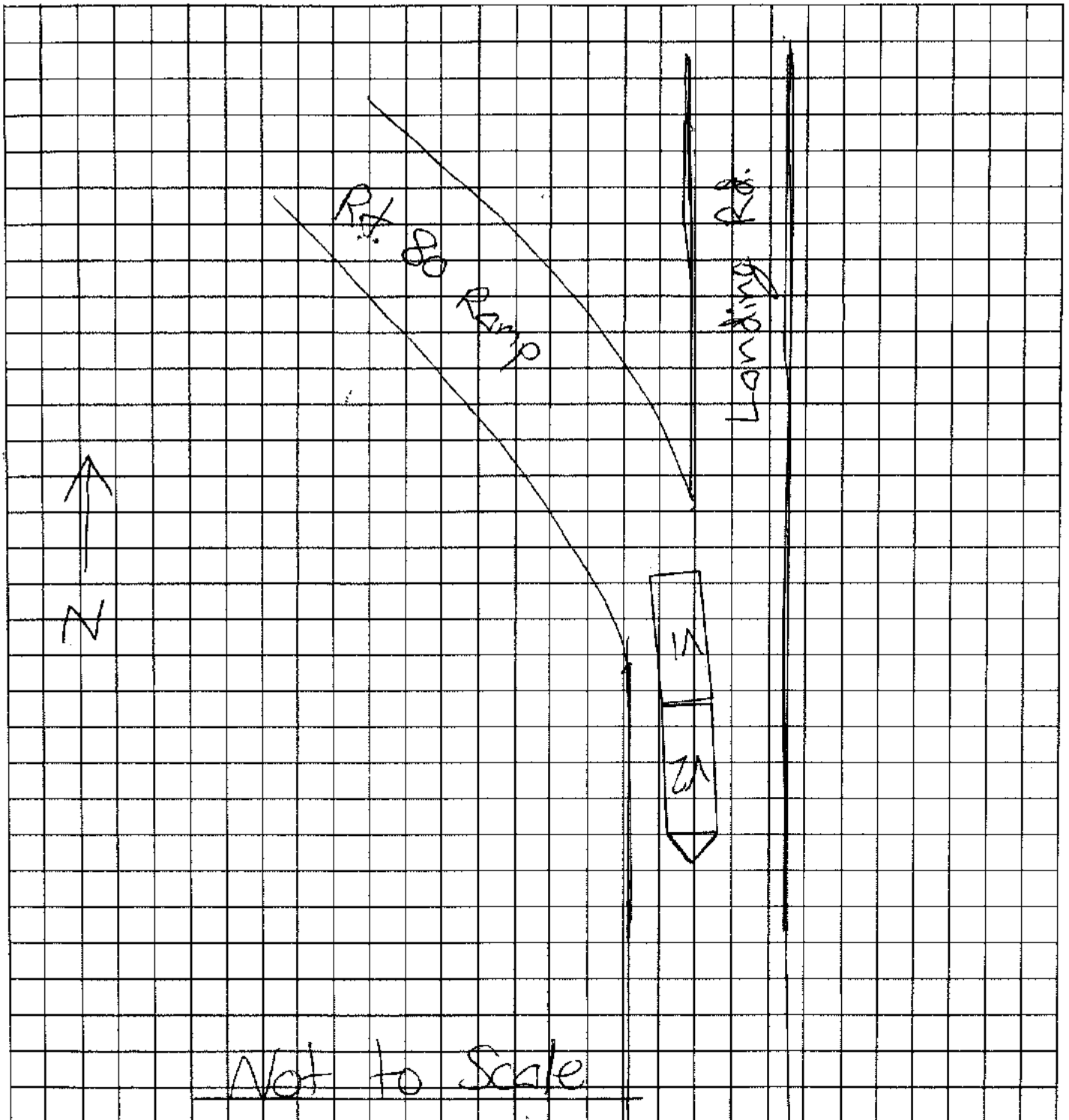
Motor Vehicle Crash Diagram

Station: 1436

Case No: 2014-026130

134 Crash Diagram (NOT TO SCALE)

○ Indicate North



Not to Scale

PH. John [Signature]
Officer's Signature

222

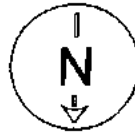
Badge Number

1 Case Number 2014-027482	10 Crash Occurred On LANDING RD	11 Speed Limit SOUTH 35 0631 A .60	
2 Police Dept of Code ROXBURY TWP 01	Road Name Dir	12 Route No.	13 Milepost 35
3 Station/Precinct	14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles 16	17 Cross Road Name SHIPPENPORT RD	
4 Date of Crash mm dd yy 11/22/14	5 Day of Week SAT	6 Time (use 2400 hrs) 1307	7 Municipality Code 1438
8 Total Killed	9 Total Injured	19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB	20 Route/Name
23 Veh No 01	24 Policy No. 031038791G71025	25 Ins Code 200	53 Veh No 02
26 Driver's First Name Initial ANDREW		29 Sex M	
27 Number and Street 24 MASON ST APT 4		30 Eyes 4	
28 City State Zip LK HOPATCONG NJ 07849-1287		58 Driver's First Name Initial Last Name JESSEROSE ANDERSON	
31 State NJ		32 Driver's License No C06680420009744	33 DOB mm dd yy 09/08/74
34 Expires mm yy 09/16		51 State NJ	52 Driver's License No A88744000063984
35 Owner's First Name Initial Last Name <input checked="" type="checkbox"/> Same As ANDREW CARR		65 Owner's First Name Initial Last Name <input type="checkbox"/> Same As ROBIN G ANDERSON	
36 Number and Street 24 MASON ST APT 4		66 Number and Street 54 ROGERENE WAY	
37 City State Zip LK HOPATCONG NJ 07849-1287		67 City State Zip LANDING NJ 07850-1845	
38 Make DD	39 Model DUR	40 Color BLACK	41 Year 2008
42 Plate No. G23CEV	43 State NJ	68 Make TOY	69 Model RAV
44 VIN 1B4HS28N6YP118604	45 Expires mm yy 06/15	70 Color SL	71 Year 2002
46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		72 Plate No. ZVD21F	
47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		73 State NJ	74 VIN JTEHH20V420179290
48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		75 Expires mm yy 06/15	
49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	
50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		77 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police	
51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 28,000 lbs <input type="checkbox"/> > 28,001 lbs		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending	
52 Carrier name		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	
135 Crash Description		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	
136 Damage To Other Property NONE		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 28,000 lbs <input type="checkbox"/> > 28,001 lbs	
137 Charge <input type="checkbox"/> Multiple Charges		82 Carrier name	
138 Summons No.		139 Charge <input type="checkbox"/> Multiple Charges	
140 Summons No.		141 Officer's Signature FOURNIER, MICHAEL	
142 Badge No. 193		143 Reviewed By PALANCHI, MARC	
144 Case Status CLOSED		145 Case Status	

D#1 STATED HE WAS TRAVELING SOUTHBOUND ON LANDING RD, AND HIS VEHICLE WAS STRUCK BY V#1 AS IT ENTERED LANDING RD FROM SHIPPENPORT RD. D#2 STATED SHE WAS TRAVELING SOUTHBOUND ON SHIPPENPORT RD. SHE FURTHER STATED THAT AS SHE APPROACHED LANDING RD MERGE, SHE OBSERVED AN AMBULANCE BEHIND HER WITH EMERGENCY LIGHTS ACTIVATED. D#2 SAID THERE WAS NOWHERE TO PULL OVER TO THE RIGHT, SO SHE STEERED LEFT WHERE V#2 COLLIDED WITH V#1 ON LANDING RD. THERE WERE NO INJURIES REPORTED. D#2 OPERATING V#2 WITH NJ LEARNER'S PERMIT. PASSENGER IN V#1 POSSESSED VALID NJ DRIVER LICENSE. BOX 64 - NO EXPIRATION PROVIDED.

137 Charge	<input type="checkbox"/> Multiple Charges	138 Summons No.	Oper.	139 Charge	<input type="checkbox"/> Multiple Charges	140 Summons No.
141 Officer's Signature FOURNIER, MICHAEL		142 Badge No. 193	143 Reviewed By PALANCHI, MARC		Badge No. 138	144 Case Status CLOSED

												Names & Addresses of Occupants - If Deceased, Date & Time of Death	
A	1	01	01	--	40	M	--	--	09	04	--	--	CARR, ANDREW, 24 MASON ST APT 4, LK HOPATCONG, NJ 07849-1267
B	2	01	01	--	16	F	--	--	09	04	--	--	ANDERSON, JESSEROSE, 54 ROGERENE WAY, LANDING, NJ 07850-1845
C	2	03	01	--	21	F	--	--	09	04	--	--	ANDERSON, EMILY, 54 ROGERENE WAY, LANDING, NJ 07850-1845
D	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	



NOT TO SCALE

LANDING RD

Unit 1

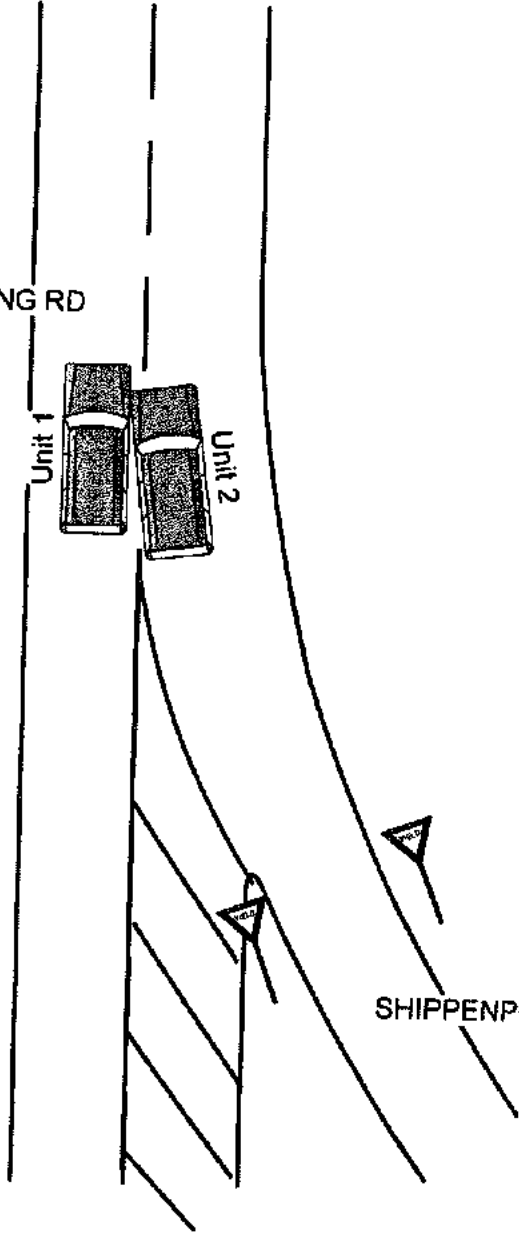


Unit 2



ROXBURY TOWNSHIP
2014-027482
PTL M. FOURNIER
PAGE 2 OF 2

SHIPPENPORT RD



01 Case Number **2014-029753** 10 Crash Occurred On: **LANDING ROAD** 11 Speed Limit **46** 0631 **A** .10
 02 Police Dept of Code **01** Road Name Dir 12 Route No. Suffix 13 Milepost
ROXBURY TWP 14 At Intersection with N E S W of **ROUTE 80 RAMP** 16 Speed Limit **66**
 03 Station/Precinct 15 Feet Miles 17 Cross Road Name NB EB
 04 Date of Crash mm dd yy 5 Day of Week **TUE** 6 Time (use 2400 hrs) **1630** 7 Municipality Code **1436** 8 Total Killed **0** 9 Total Injured **0** 19 Ramp To: From: 20 Route/Name SB WB
 21 Latitude **40.8810** 22 Longitude **-74.8783**

23 Veh No **01** 24 Policy No. **982280587 1052** 25 Ins Code **129** 53 Veh No **02** 54 Policy No. **809 890974 08/20** 55 Ins Code **054**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name Initial Last Name **LINDA SMITH** 26 Sex **F** 56 Driver's First Name Initial Last Name **PAOSHU CHI** 59 Sex **F**

27 Number and Street **149 RIGHTER RD** 30 Eyes **4** 57 Number and Street **14 YORKSHIRE DR** 60 Eyes **2**

28 City **SUCCASUNNA** State **NJ** Zip **07876-1645** 58 City **HACKETTSTOWN** State **NJ** Zip **07840-5604**

31 State **NJ** 32 Driver's License No **567784730051654** 33 DOB mm dd yy **01/19/85** 34 Expires mm yy **04/15** 61 State **NJ** 62 Driver's License No **C34166160062462** 63 DOB mm dd yy **12/18/46** 64 Expires mm yy **02/19**

35 Owner's First Name Initial Last Name Same As **LINDA SMITH** 65 Owner's First Name Initial Last Name Same As **CHINGHWA CHI**

36 Number and Street **149 RIGHTER RD** 66 Number and Street **14 YORKSHIRE DR.**

37 City **SUCCASUNNA** State **NJ** Zip **07876-1645** 67 City **HACKETTSTOWN** State **NJ** Zip **07840-5604**

38 Make **HOND** 39 Model **ODY** 40 Color **RD** 41 Year **2014** 42 Plate No. **S28EJP** 43 State **NJ** 68 Make **LEXS** 69 Model **RX** 70 Color **GD** 71 Year **2008** 72 Plate No. **UBF72D** 73 State **NJ**

44 VIN **6FNRL5H62EB073223** 45 Expires **06/17** 74 VIN **2T2HA31U26C085308** 75 Expires **11/18**

46 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 47 Authority Owner Driver Police 76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 77 Authority Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending

49 Hazardous Material On Board Spill Name or Placard No. 79 Hazardous Material On Board Spill Name or Placard No.

50 Carrier No. USDOT Other 80 Carrier No. USDOT Other

51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 25,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 25,000 lbs > 26,001 lbs

52 Carrier name 82 Carrier name

135 Crash Description
 Driver #1 said that she was travelling south on the Route 80 ramp to Landing Road south. D1 said that V2 stopped abruptly in front of her. D1 said that she applied the brakes but was unable to stop and struck the rear of V2. Driver #2 said that she was travelling south on the Route 80 ramp to Landing Road south. D2 said that she came to a stop when V1 struck her vehicle in the rear. No injuries reported. Driver #1 (Smith) at fault for the crash for striking the rear of V2.

136 Damage To Other Property

Oper. 1 137 Charge Multiple Charges 138 Summons No. Oper. 2 139 Charge Multiple Charges 140 Summons No.

141 Officer's Signature **HENRIKSEN, ERICK** 142 Badge No. **188** 143 Reviewed By **PALANCHI, MARC** 144 Case Status **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	49	F	--	--	--	09	04	--	--	SMITH, LINDA, 149 RIGHTER RD, SUCCASUNNA, NJ 07876-1645
B	2	01	01	--	68	F	--	--	--	09	04	--	--	CHI, PAOSHU, 14 YORKSHIRE DR, HACKETTSTOWN, NJ 07840-5604
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

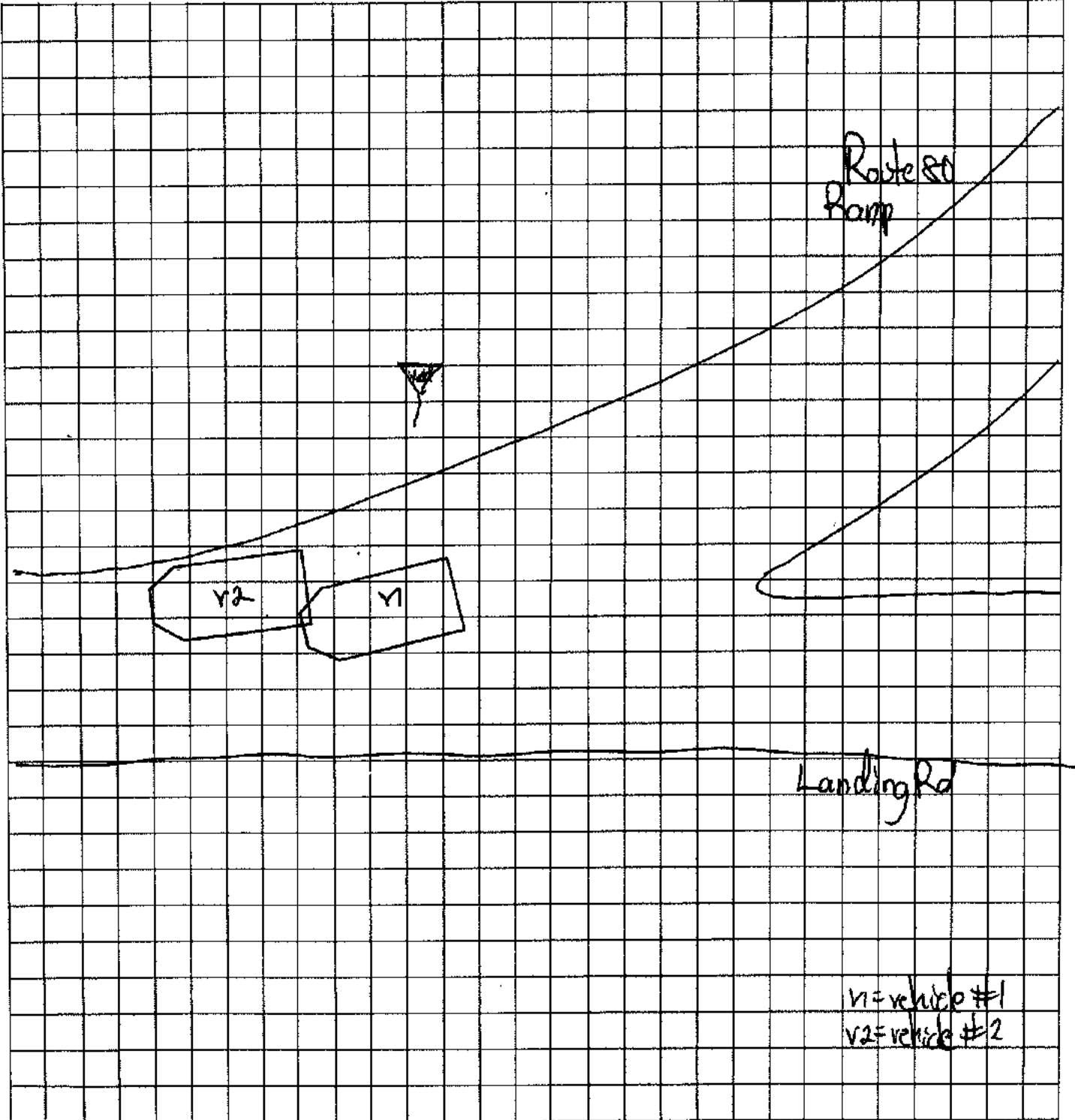
New Jersey Police Crash Investigation Report

Police Dept: Roxbury Twp Code: 01

Motor Vehicle Crash Diagram

Station: _____ Case No: 2014-029753

134 Crash Diagram (NOT TO SCALE)



C. [Signature]
Officer's Signature

188
Badge Number

07 1 Case Number **2014-000695** 10 Crash Occurred On **LANDING RD** 11 Speed Limit **36** **0631** **A** **.61** 118a **51**

08 2 Police Dept of **ROXBURY TWP** Code **01** 300 Road Name **HART STREET** Dir **S** 12 Route No. **19** Suffix **A** 13 Milepost **25** 118b **01**

09 3 Station/Precinct **14** At Intersection with N E Feet S W of **16** 119a **01**

06 4 Date of Crash **01/10/14** 5 Day of Week **FRI** 6 Time (use 2400 hrs) **0956** 7 Municipality Code **1436** 8 Total Killed **-** 9 Total Injured **-** 17 Cross Road Name **-** NB EB 119b **-**

100 20 Route Name **-** SB WB 119b **-**

101 21 Latitude **-** 22 Longitude **-** 120 **01**

102 23 Veh No **01** 24 Policy No. **4026026026** 25 Ins Code **148** 53 Veh No **-** 54 Policy No. **-** 55 Ins Code **-** 121 **-**

103 26 Driver's First Name **ISHNAHIR** Initial **I** Last Name **DIAZ** 28 Sex **F** 56 Driver's First Name **-** Initial **-** Last Name **-** 59 Sex **-** 122 **-**

104 27 Number and Street **208 TULSA TRL** 30 Eyes **2** 57 Number and Street **-** 60 Eyes **-** 123 **-**

105 28 City **HOPATCONG** State **NJ** Zip **07843-1257** 58 City **-** State **-** Zip **-** 124 **01**

106 31 State **NJ** 32 Driver's License No **D40263740064962** 33 DOB **04/09/86** 34 Expires **05/17** 61 State **-** 62 Driver's License No **-** 63 DOB **-** 64 Expires **-** 125 **-**

107 35 Owner's First Name **ISHNAHIR** Initial **I** Last Name **DIAZ** 65 Owner's First Name **-** Initial **-** Last Name **-** 126 **64**

108 36 Number and Street **208 TULSA TRL** 66 Number and Street **-** 127 **-**

109 37 City **HOPATCONG** State **NJ** Zip **07843-1267** 67 City **-** State **-** Zip **-** 128a **26**

110 38 Make **CHE** 39 Model **CAV** 40 Color **BL** 41 Year **2001** 42 Plate No. **S99DRK** 43 State **NJ** 68 Make **-** 69 Model **-** 70 Color **-** 71 Year **-** 72 Plate No. **-** 73 State **-** 128b **46**

111 44 VIN **1G1JF524017264188** 45 Expires **10/14** 74 VIN **-** 75 Expires **-** 128c **-**

112 46 Vehicle Removed To Driven Left at Scene Towed **ATLANTIC** 47 Authority Owner Driver Police Impound Disabled 76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 77 Authority Owner Driver Police Impound Disabled 128d **-**

113 48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 128e **-**

114 49 Hazardous Material On Board Spill Name or Placard No. **-** 79 Hazardous Material On Board Spill Name or Placard No. **-** 128f **-**

115 50 Carrier No. USDOT Other **-** 80 Carrier No. USDOT Other **-** 128g **-**

116 51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 128h **12**

117 52 Carrier name **-** 82 Carrier name **-** 131 **12**

135 Crash Description **Driver one was traveling south on Landing Rd. when her vehicle slid off the roadway and struck the guardrail. Driver one was driving too fast for the snow covered road which contributed to the accident. End of report.** 132 **-** 133 **-**

136 Damage To Other Property **NONE**

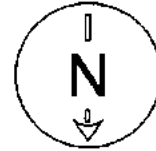
Oper. **-** 137 Charge Multiple Charges **-** 138 Summons No. **-** Oper. **-** 139 Charge Multiple Charges **-** 140 Summons No. **-**

141 Officer's Signature **EDMUNDS, JONATHAN** 142 Badge No. **166** 143 Reviewed By **PALANCHI, MARC** Badge No. **138** 144 Case Status **CLOSED**

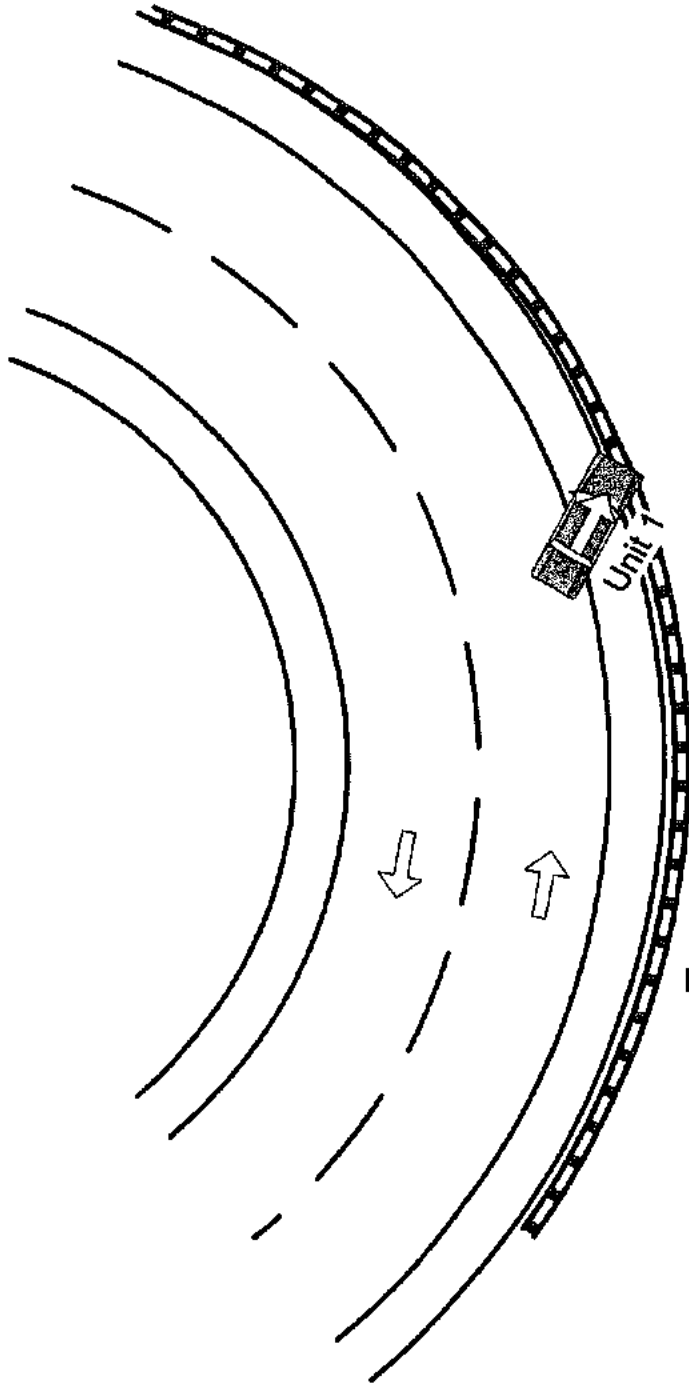
145 Occupant Information Table

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	18	F	--	--	--	09	09	08	--	DIAZ, ISHNAHIR, 208 TULSA TRL, HOPATCONG, NJ 07843-1257
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

ROXBURY TWP. POLICE
2014-00695
PTL. EDMUNDS
PAGE 2 OF 2.



NOT TO SCALE



LANDING ROAD

Page 1 of 2 Fatal **New Jersey Police Crash Investigation Report** Reportable Non-Reportable Change Report

1 Case Number: **2014-023423** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit: **35** 12 Route No: **0831** 13 Milepost: **A .10**

2 Police Dept of: **ROXBURY TWP** Code: **01** Road Name: **NORTH** Dir: **35** 17 Cross Road Name: **SHIPPENPORT RD** 18 Speed Limit: **36**

3 Station/Precinct: **100** 14 At Intersection with N E Feet S W of: **SHIPPENPORT RD** 15 Miles: **14** 16

4 Date of Crash: **10/06/14** 5 Day of Week: **MON** 6 Time (use 2400 hrs): **1803** 7 Municipality Code: **1436** 8 Total Killed: **--** 9 Total Injured: **--** 19 Ramp: To: From: **20** Route/Name: **SHIPPENPORT RD** NB EB SB WB

21 Latitude: **--** 22 Longitude: **--**

23 Veh No: **01** 24 Policy No.: **31704661** 25 Ins Code: **134** 53 Veh No: **02** 54 Policy No.: **--** 55 Ins Code: **--**

PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name: **EDNA** Initial: **M** Last Name: **MAZUR** 28 Sex: **F** 56 Driver's First Name: **HIT & RUN** Initial: **--** Last Name: **--** 59 Sex: **--**

27 Number and Street: **303 WEST END AVE** 30 Eyes: **4** 57 Number and Street: **--** 60 Eyes: **--**

28 City: **HOPATCONG** State: **NJ** Zip: **07843-1219** 58 City: **--** State: **--** Zip: **--**

31 State: **NJ** 32 Driver's License No: **M09821917487414** 33 DOB: **07/29/41** 34 Expires: **12/17** 61 State: **--** 62 Driver's License No: **--** 63 DOB: **--** 64 Expires: **--**

35 Owner's First Name: **GEORGE** Initial: **--** Last Name: **MAZUR** 65 Owner's First Name: **--** Initial: **--** Last Name: **--**

Same As Driver

36 Number and Street: **303 WEST END AVE** 66 Number and Street: **--**

37 City: **HOPATCONG** State: **NJ** Zip: **07843-1219** 67 City: **--** State: **--** Zip: **--**

38 Make: **M-B** 39 Model: **320** 40 Color: **SILVE** 41 Year: **2003** 42 Plate No.: **HZ9217** 43 State: **NJ** 68 Make: **--** 69 Model: **--** 70 Color: **--** 71 Year: **--** 72 Plate No.: **--** 73 State: **--**

44 VIN: **WDBUF66J83A108304** 45 Expires: **06/15** 74 VIN: **--** 75 Expires: **--**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending

49 Hazardous Material On Board: Spill Name or Placard No.: **--** 79 Hazardous Material On Board: Spill Name or Placard No.: **--**

50 Carrier No.: USDOT Other 80 Carrier No.: USDOT Other **978174**

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

52 Carrier name: **JERSEY BULK CARRIERS CO INC** 82 Carrier name: **JERSEY BULK CARRIERS CO INC**

135 Crash Description: **ON OCTOBER 6, 2014 AT 1800 HRS I SPOKE WITH D#1 WHO RESPONDED TO ROXBURY POLICE HEADQUARTERS. D#1 SAID SHE WAS TRAVELING NORTHBOUND ON LANDING RD, AND AS ROUTE 80 WESTBOUND EXIT 28 OFFRAMP MERGED WITH LANDING RD, SHE OBSERVED V#2, A TANKER TRUCK, TRAVELING ON THE OFFRAMP. D#1, TRAVELING IN THE RIGHT LANE, SAID V#2 DID NOT YIELD AND AS A RESULT COLLIDED WITH HER VEHICLE.**

I OBSERVED BLACK SCUFF MARKS ON PASSENGER SIDE OF V#1 FRONT FENDER AND SIDE VIEW MIRROR. I FURTHER OBSERVED A CRACKED FRONT PASSENGER SIDE HEADLIGHT LENS COVER. D#1 STATED LEFT FRONT TIRE OF V#2 MADE CONTACT WITH FRONT PASSENGER SIDE OF HER VEHICLE. THE DAMAGE I

136 Damage To Other Property: **NONE**

137 Charge: **--** Multiple Charges 138 Summons No.: **--** Oper.: **2** 139 Charge: **--** Multiple Charges 140 Summons No.: **--**

141 Officer's Signature: **FOURNIER, MICHAEL** 142 Badge No.: **193** 143 Reviewed By: **PALANCHI, MARC** Badge No.: **138** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	73	F	--	--	--	08	04	--	--	MAZUR, EDNA M, 303 WEST END AVE, HOPATCONG, NJ 07843-1219
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

New Jersey Police Crash Investigation Report Motor Vehicle Crash Description	Police Dept: ROXBURY TWP Code: 01 Station: Case No: 2014-023423
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(Refer to vehicle by number)

ALL INVOLVED	Veh Occ	Pos In/On	Eject	Phys Cond	Age	Sex	Loc Inj	Type Inj	Ref Med	E:Uip Avail	E:Uip Used	Bat: Depl	Hosp Code	Names & Addresses of Occupants - If Deceased, Date & Time of Death
	83	84	85	86	87	88	89	90	91	92	93	94	95	
F	--	--	--	--	--	--	--	--	--	--	--	--	--	
G	--	--	--	--	--	--	--	--	--	--	--	--	--	
H	--	--	--	--	--	--	--	--	--	--	--	--	--	
I	--	--	--	--	--	--	--	--	--	--	--	--	--	
J	--	--	--	--	--	--	--	--	--	--	--	--	--	

135 Crash Description

OBSERVED TO V#1 WAS CONSISTENT WITH A TIRE MAKING CONTACT WITH A VEHICLE. V#1 ALSO COULD HAVE MADE CONTACT WITH MUD FLAP ON VEHICLE.

D#1 SAID FOLLOWING THE COLLISION, V#2 DID NOT STOP AND CONTINUED NORTHBOUND ON LANDING RD. V#2 PROCEEDED TO TRAVEL UNTIL IT PULLED INTO ENRITE GAS STATION IN HOPATCONG JURISDICTION (40 HOPATCHUNG RD). D#1 CONFRONTED D#2, AND SAID D#2 WAS DISMISSIVE OF HER PRESENCE. D#1 SAID SHE CALLED HOPATCONG POLICE, AND WAS ASSISTED BY SGT WOJTACH.

ACCORDING TO D#1, SGT WOJTACH INITIALLY DID NOT OBSERVE DAMAGE TO V#1 DUE TO SUN GLARE COUPLED WITH THE MINOR DAMAGE. SGT WOJTACH ADVISED SINCE HE BELIEVED AT THE TIME THERE WAS NO COLLISION HE DID NOT RETRIEVE D#2 INFORMATION, ONLY V#2 REGISTRATION. D#1 LEFT THE SCENE AND WENT HOME, WHERE SHE CONFIRMED THERE WAS DAMAGE TO HER VEHICLE. SHE RETURNED SHORTLY AFTER AND SHOWED THE DAMAGE SHE OBSERVED TO SGT WOJTACH, WHICH HE DOCUMENTED IN AN OPERATIONS REPORT (SEE HOPATCONG PD CASE 14-11606).

I ASCERTAINED V#2 INFORMATION FROM SGT WOJTACH'S REPORT, AND MADE CONTACT ON OCTOBER 6 WITH THE COMPANY V#2 WAS REGISTERED TO, JERSEY BULK CARRIERS CO INC. I SPOKE WITH A MALE ON THE PHONE WHO IDENTIFIED HIMSELF AS BORIS WHO ALSO SAID HE WAS THE OWNER OF THE BUSINESS. I ADVISED BORIS THE REASON I WAS CALLING. BORIS IDENTIFIED HIMSELF AS THE DRIVER OF V#2 BUT REFUSED TO PROVIDE ANY FURTHER INFORMATION. HE SAID HE HAD JUST SPOKEN TO THE POLICE ABOUT THE INCIDENT AND THEY LET HIM GO. I EXPLAINED TO BORIS THE REASON THE DAMAGE WAS NOT SEEN (SUN GLARE, MINOR DAMAGE) IN HOPATCONG. HE BECAME IRATE AND PUT ME ON HOLD FOR OVER TEN MINUTES UNTIL I DISCONNECTED. I ATTEMPTED TO CALL BORIS AGAIN ABOUT TWO HOURS LATER BUT WAS UNABLE TO MAKE CONTACT. I ALSO ATTEMPTED MAKE CONTACT ON OCTOBER 7 AND OCTOBER 11 WITH SIMILAR RESULTS.

I TRAVELED TO THE ADDRESS IN WHICH THE BUSINESS WAS LISTED - 132 ROUTE 46 IN BUDD LAKE NJ 07828. THAT ADDRESS WAS A DELTA GAS STATION, BUT THERE DID NOT APPEAR TO BE ANY AREA WHERE A TRACTOR TRAILER COULD BE PARKED. THE GAS STATION ATTENDANT STATED HE DID NOT KNOW A MAN NAMED BORIS NOR DID HE KNOW ANYTHING ABOUT JERSEY BULK CARRIERS. I THEN CONTACTED MOUNT OLIVE TAX ASSESSOR IN ATTEMPT TO LOCATE MORE INFORMATION ABOUT THE OWNER OF THE COMPANY. I WAS ADVISED BY THE ASSESSOR THE OWNER OF THE BUSINESS FROM THEIR RECORDS WAS LISTED AS PETRO STOPPING CENTER, ANOTHER BUSINESS NAME. THE MT OLIVE TAX ASSESSOR DID NOT HAVE ANY OTHER INFORMATION TO PROVIDE.

ON OCTOBER 15, I WAS ABLE TO MAKE CONTACT WITH BORIS AGAIN. I EXPLAINED TO BORIS HE MAY NOT HAVE NOTICED THERE WAS A COLLISION WITH V#1 BECAUSE OF THE SIZE OF HIS VEHICLE. I ADVISED BORIS IF HE LET ME SEE V#2 I COULD SHOW WHERE THE DAMAGE WOULD BE. BORIS SAID HE WOULD COME TO ROXBURY POLICE HEADQUARTERS ON OCTOBER 16. BORIS DID NOT COME TO POLICE HEADQUARTERS ON OCTOBER 16.

WITH ASSISTANCE FROM DET NIEMYSKI OF THIS DEPARTMENT, I LOCATED ANOTHER OWNER OF JERSEY BULK CARRIERS THROUGH AN OFFLINE SEARCH (TLO). I SPOKE WITH GENE RABKIN WHO DID IDENTIFY HIMSELF AS PART OWNER OF JERSEY BULK CARRIERS. I EXPLAINED TO MR RABKIN I WAS ATTEMPTING TO MAKE CONTACT WITH BORIS ABOUT THE CRASH. MR RABKIN BECAME IRATE AND TOLD ME HE HAD ALREADY SPOKEN TO BORIS ABOUT THE CRASH AND SAID "IT WAS ONLY A FENDER" INDICATING THERE WAS CONTACT

[Signature]
Officer's Signature

MADE BETWEEN V#1 AND V#2. MR RABKIN WENT ON TO SAY HE WOULD NOT HELP ME.

ON NOVEMBER 5, WITH THE ASSISTANCE OF MORRIS COUNTY DISPATCH, I REQUESTED V#2 REGISTRATION (XK263G) BE CHECKED IN ATTEMPT TO SEE IF ANYONE WITH THE NAME BORIS WAS LISTED AS A DRIVER IN THE PAST. DISPATCH ADVISED IN 2004 D#2 WAS LISTED AS THE DRIVER IN V#2 FOR A MOTOR VEHICLE VIOLATION. D#2 AT THE TIME POSSESSED A NEW YORK DRIVER LICENSE. ON NOVEMBER 8, DISPATCH SENT A MESSAGE TO NEW YORK MOTOR VEHICLE REQUESTING A DRIVER LICENSE PICTURE OF D#2 FOR POSITIVE IDENTIFICATION BY D#1 AND SGT WOJTACH. AS OF NOVEMBER 27 THERE HAS BEEN NO RESPONSE.

ON DECEMBER 15, 2014 I RECEIVED MENTIONED NEW YORK DMV PHOTOGRAPH OF BORIS PLOTKIN, SUSPECTED DRIVER #2. I SHOWED PHOTO OF PLOTKIN TO D#1 AND SGT WOJTACH OF HOPATCONG POLICE. BOTH STATED PLOTKIN WAS NOT THE DRIVER.

BOTH D#1 AND SGT WOJTACH DESCRIBED D#2 AS A HEAVY SET MALE, DARK COMPLEXION, IN HIS FORTIES. THEY ALSO STATED D#2 SPOKE WITH WHAT THEY DESCRIBED AS A RUSSIAN ACCENT.

AT THIS TIME SINCE D#2 COULD NOT BE IDENTIFIED, SUMMONSES WILL BE ISSUED TO GENE RABKIN, WHO IDENTIFIED HIMSELF AS PART OWNER OF JERSEY BULK CARRIERS. MR RABKIN STATED TO ME ON THE PHONE HE HAD SPECIFIC KNOWLEDGE THAT V#2 WAS INVOLVED IN THE CRASH. SUMMONSES SENT VIA MAIL TO 335 PRESTON AVE, STATEN ISLAND NY 10512 (ADDRESS LISTED ON MR RABKIN'S DRIVERS LICENSE) THE FOLLOWING SUMMONSES WERE ISSUED 39:4-129 AND 39:4-130.

I ADVISED D#1 TO SAVE PHOTOGRAPHS OF DAMAGE TO HER VEHICLE FOR COURT/INSURANCE PURPOSES.

1 Case Number: **2014-001400** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit: **35** 12 Route No: **0631** 13 Milepost: **A .55**
 2 Police Dept of Code: **ROXBURY TWP 01** 250 14 At Intersection with N E Feet S W of: **HART ST** 18 Speed Limit: **25**
 3 Station/Preinct: **14** 15 Miles 16 17 Cross Road Name: **HART ST** 18 Ramp To: From: NB EB SB WE

4 Date of Crash: **01/18/14** 5 Day of Week: **SAT** 6 Time (Use 2400 hrs): **1324** 7 Municipality Code: **1438** 8 Total Killed: **0** 9 Total Injured: **01** 20 Route/Name: **01** 21 Latitude: **01** 22 Longitude: **01**

23 Veh No: **01** 24 Policy No: **9836162761061** 25 Ins Code: **129** 53 Veh No: **01** 54 Policy No: **01** 55 Ins Code: **01**

26 Driver's First Name: **CHRISTOPH** 27 Number and Street: **134 WEST END AVENUE** 28 Sex: **M** 30 Eyes: **6** 56 Driver's First Name: **CHRISTOPH** 57 Number and Street: **134 WEST END AVENUE** 58 Sex: **M** 60 Eyes: **6**

28 City: **HOPATCONG** 31 State: **NJ** 32 Driver's License No: **024381249007788** 33 DOB: **07/03/76** 34 Expires: **12/17** 61 State: **NJ** 62 Driver's License No: **024381249007788** 63 DOB: **07/03/76** 64 Expires: **12/17**

35 Owner's First Name: **CHRISTOPH** 36 Number and Street: **134 WEST END AVENUE** 37 City: **HOPATCONG** 65 Owner's First Name: **CHRISTOPH** 66 Number and Street: **134 WEST END AVENUE** 67 City: **HOPATCONG**

38 Make: **SUZ** 39 Model: **GRV** 40 Color: **BK** 41 Year: **2007** 42 Plate No: **VML56T** 43 State: **NJ** 68 Make: **SUZ** 69 Model: **GRV** 70 Color: **BK** 71 Year: **2007** 72 Plate No: **VML56T** 73 State: **NJ**

44 VIN: **JS2YB413976104101** 45 Expires: **02/16** 74 VIN: **JS2YB413976104101** 75 Expires: **02/16**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending

49 Hazardous Material On Board: Spill: Name or Placard No.: **50 Carrier No. USDOT Other** 79 Hazardous Material On Board: Spill: Name or Placard No.: **80 Carrier No. USDOT Other**

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 28,000 lbs > 28,001 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 28,000 lbs > 28,001 lbs

52 Carrier name: **135 Crash Description** 82 Carrier name: **135 Crash Description**

D#1 STATED HE WAS TRAVELING SOUTHBOUND ON LANDING RD. HE FURTHER STATED THAT AS HE BEGAN TO NEGOTIATE CURVE IN THE ROADWAY, HE BEGAN TO LOSE CONTROL ON BLACK ICE. HE SAID HE OVER CORRECTED STEERING, CAUSING V#1 TO ROTATE COUNTER-CLOCKWISE UNTIL IT STRUCK EMBANKMENT ON EAST SIDE OF ROADWAY. AFTER V#1 STRUCK EMBANKMENT IT OVERTURNED AND CAME TO REST ON ITS ROOF, FACING SOUTHEAST.

UPON OBSERVING THE SCENE, I OBSERVED CLEAR ROADWAY AT THE BEGINNING OF THE CURVE APPROXIMATELY 200 FEET NORTH OF V#1 AT REST. THERE WAS NO BLACK ICE, HOWEVER I OBSERVED LOOSE GRAVEL IN THE AREA OF SHOULDER LINE WHICH WAS DISTURBED BY V#1 TIRE MARK. IT APPEARED

136 Damage To Other Property: **137 Charge Multiple Charges** 138 Summons No: **139 Charge Multiple Charges** 140 Summons No:

141 Officer's Signature: **FOURNIER, MICHAEL** 142 Badge No: **194** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	36	M	--	--	--	09	04	--	--	DELVECCHIO, CHRISTOPH, 134 WEST END AVENUE, HOPATCONG, NJ 07843-1256
B	1	03	01	04	36	F	04	06	01	09	04	--	--	KRONYAK, KARYN, 134 WEST END AE, HOPATCONG, NJ 07843
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

(Refer to vehicle by number)

ALL INVOLVED

Veh Occ	Pos In/On	Eject	Phys Cond	Age	Sex	Loc Inj	Type Inj	Ref Med	Equip Avail	Equip Used	Bar Dept	Hosp Code	
83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
-	-	-	-	-	-	-	-	-	-	-	-	-	
-	-	-	-	-	-	-	-	-	-	-	-	-	
-	-	-	-	-	-	-	-	-	-	-	-	-	
-	-	-	-	-	-	-	-	-	-	-	-	-	
-	-	-	-	-	-	-	-	-	-	-	-	-	

135 Crash Description

AS THOUGH D#1 LOST CONTROL ON COMBINATION OF LOOSE GRAVEL/WET ROADWAY.

THERE WERE NO OTHER COLLISIONS IN THE AREA ON THIS DAY.

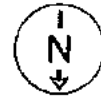
PASSENGER OF V#1 SUSTAINED INJURY TO HER NECK FROM SEATBELT. HOWEVER SHE REFUSED MEDICAL ATTENTION AT THE SCENE.



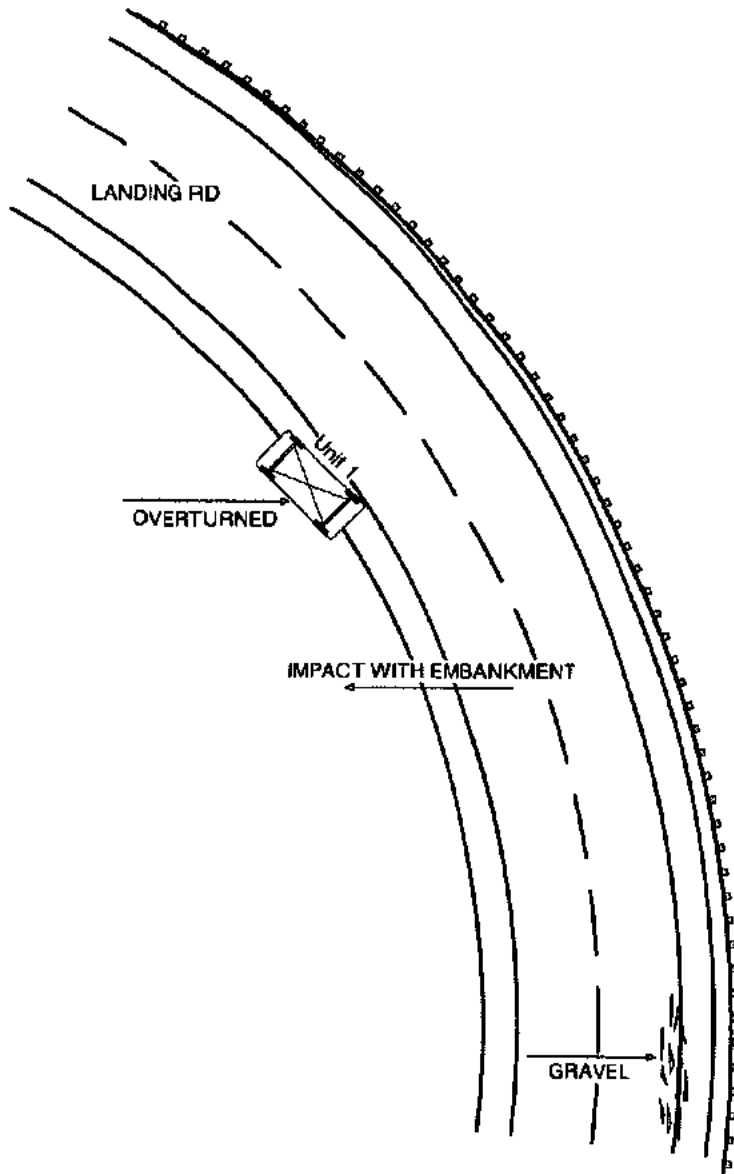
Officer's Signature

194

Badge Number



NOT TO SCALE



ROXBURY TOWNSHIP
2014-001400
PTL M. FOURNIER
PAGE 3 OF 3

1 Case Number: **2014-007601** 10 Crash Occurred On: **LANCING RD** 11 Speed Limit: **35** 12 Route No. **0931** 13 Milepost: **A 0.70**
 2 Police Dept of: **ROXBURY TWP** Code: **01** 200 Road Name: **SOUTH** 14 At Intersection with N E S W of: **HART STREET** 16
 3 Station/Precinct: **14** 15 Feet Miles 17 Cross Road Name: **HART STREET** 18 Speed Limit: **25**

4 Date of Crash: **04/06/14** 5 Day of Week: **SUN** 6 Time (use 2400 hrs): **1626** 7 Municipality Code: **1436** 8 Total Killed: **0** 9 Total Injured: **0** 19 Ramp To: NB SB WB Front: From: 20 Route/Name: NB SB WB
 21 Latitude: 22 Longitude

23 Veh No: **01** 24 Policy No.: 25 Ins Code: 53 Veh No: 54 Policy No.: 55 Ins Code:
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name: **CHRISTOPH** Initial: **J** Last Name: **JACKSON** 29 Sex: **M** 56 Driver's First Name: Initial: Last Name: 59 Sex:
 27 Number and Street: **373 HOWARD BLVD** 30 Eyes: **5** 57 Number and Street: 60 Eyes:
 28 City: **MT ARLINGTON** State: **NJ** Zip: **07856-1111** 58 City: State: Zip:

31 State: **NJ** 32 Driver's License No: **J00621247107636** 33 DOB: **07/04/83** 34 Expires: **11/17** 61 State: 62 Driver's License No: 63 DOB: 64 Expires:
 35 Owner's First Name: **CHRISTOPH** Initial: **J** Last Name: **JACKSON** 65 Owner's First Name: Initial: Last Name:
 Same As Driver: Same As Driver:

36 Number and Street: **373 HOWARD BLVD** 66 Number and Street:
 37 City: **MT ARLINGTON** State: **NJ** Zip: **07856-1111** 67 City: State: Zip:
 38 Make: **AUD** 39 Model: **A6** 40 Color: **SL** 41 Year: **2004** 42 Plate No.: **W84DST** 43 State: **NJ** 68 Make: 69 Model: 70 Color: 71 Year: 72 Plate No.: 73 State:
 44 VIN: **WAUCD84B84N101731** 45 Expires: **11/14** 74 VIN: 75 Expires:

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: 0. % Pending
 49 Hazardous Material On Board: Spill: Name of Placard No.: 79 Hazardous Material On Board: Spill: Name of Placard No.:
 50 Carrier No.: USDOT Other 80 Carrier No.: USDOT Other
 51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 52 Carrier name: 82 Carrier name:

136 Damage To Other Property: **NONE STATED OR OBSERVED**
 Oper. 137 Charge: Multiple Charges 138 Summons No.: Oper. 139 Charge: Multiple Charges 140 Summons No.:
 141 Officer's Signature: **BECK, WILLIAM** 142 Badge No.: **134** 143 Reviewed By: **MURRAY, DANIEL** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01		50	M				09	04			JACKSON, CHRISTOPH J, 373 HOWARD BLVD, MT ARLINGTON, NJ 07856-1111
B														
C														
D														
E														

New Jersey Police Crash Investigation Report

Reportable
 Non-Reportable
 Change Report

96	Page 1 of 1 <input type="checkbox"/> Fatal										New Jersey Police Crash Investigation Report																				
97	1 Case Number 2014-012842										10 Crash Occurred On: LANDING RD										11 Speed Limit 36										116a
98	2 Police Dept of ROXBURY TWP										Road Name NORTH										12 Route No. 0631										116b
99	3 Station/Precinct 01										At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E										13 Milepost .70										116c
100	4 Date of Crash mm dd yy 06/10/14										5 Day of Week TUE										6 Time (use 2400 hrs) 0737										119a
101	7 Municipality Code 1436										8 Total Killed 0										9 Total Injured 0										119b
102	23 Veh No 01										24 Policy No. HPA00001083678										25 Ins Code 411										120
103	26 Driver's First Name LAUREN										27 Number and Street 342 QUAKER CHURCH RD										28 City RANDOLPH										121
104	29 Sex F										30 Eyes 4										31 State NJ										122
105	32 Driver's License No T36014437952814										33 DOB mm dd yy 02/02/81										34 Expires mm yy 03/18										123
106	35 Owner's First Name <input type="checkbox"/> Same As LAUREN										36 Number and Street 342 QUAKER CHURCH RD APT160										37 City RANDOLPH										124
107	38 Make SUB										39 Model IMP										40 Color SL										125
108	41 Year 2012										42 Plate No. LN89G										43 State NJ										126
109	44 VIN JF1GJAA66CG028553										45 Expires 06/15										46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed										127
110	47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police										48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending										49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>										128a
111	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other										51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs										52 Carrier name										128b
112	53 Crash Description										54										55										128c
113	Investigation revealed that vehicle #1 was traveling Landing Road north bound when it struck a deer which ran into the front of the vehicle. No injuries were report and the vehicle received minor damage.																														128d
114	No diagram																														129a
115	136 Damage To Other Property NONE STATED OR OBSERVED																														129b
116	137 Charge <input type="checkbox"/> Multiple Charges										138 Summons No.										139 Charge <input type="checkbox"/> Multiple Charges										129c
117	141 Officer's Signature BECK, WILLIAM										142 Badge No. 134										143 Reviewed By PALANCHI, MARC										129d
118	144 Case Status CLOSED																														130
119	83										84										85										131
120	86										87										88										132
121	89										90										91										133
122	92										93										94										134
123	95										96										97										135
124	98										99										100										136
125	101										102										103										137
126	104										105										106										138
127	107										108										109										139
128	110										111										112										140
129	113										114										115										141
130	116										117										118										142
131	119										120										121										143
132	122										123										124										144
133	125										126										127										145
134	128										129										130										146
135	131										132										133										147
136	134										135										136										148
137	137										138										139										149
138	140										141										142										150
139	143										144										145										151
140	146										147										148										152
141	149										150										151										153
142	152										153										154										154

1 Case Number: **2014-001031** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit: **46** 12 Route No: **0631** 13 Milepost: **.06**
 2 Police Dept Code: **01** 14 At Intersection with N E of: **SOUTH** 17 Cross Road Name: **NORTH FRONTAGE RD** 19 Speed Limit: **36**
 3 Station/Precinct: **ROXBURY TWP** 15 Feet S W 16 Miles 18 NB EB

4 Date of Crash: **01/14/14** 5 Day of Week: **TUE** 6 Time (use 2400 hrs): **0838** 7 Municipality Code: **1438** 8 Total Killed: **0** 9 Total Injured: **01**
 19 Ramp: To: From: SB WB 20 Route/Name: NB EB
 21 Latitude: 22 Longitude: SB WB

23 Veh No: **01** 24 Policy No: **PAA00001004082** 25 Ins Code: **983** 53 Veh No: **01** 54 Policy No: **01** 55 Ins Code: **01**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name: **SHAWNTAE** Initial: **M** Last Name: **GOLDBERRY** 29 Sex: **F** 56 Driver's First Name: **SHAWNTAE** Initial: **M** Last Name: **GOLDBERRY** 59 Sex: **F**
 27 Number and Street: **282 CENTER ST** 30 Eyes: **2** 57 Number and Street: **282 CENTER ST** 60 Eyes: **2**
 28 City: **LANDING** State: **NJ** Zip: **07860** 58 City: **LANDING** State: **NJ** Zip: **07860**

31 State: **NJ** 32 Driver's License No: **G82237047464922** 33 DOB: **04/06/92** 34 Expires: **06/16** 61 State: **NJ** 62 Driver's License No: **G82237047464922** 63 DOB: **04/06/92** 64 Expires: **06/16**
 35 Owner's First Name: **HOWARD** Initial: **G** Last Name: **GOLDBERRY** 65 Owner's First Name: **HOWARD** Initial: **G** Last Name: **GOLDBERRY**
 Same As Driver

36 Number and Street: **282C CENTER STREET APT C** 66 Number and Street: **282C CENTER STREET APT C**
 37 City: **LANDING** State: **NJ** Zip: **07860-1031** 67 City: **LANDING** State: **NJ** Zip: **07860-1031**
 38 Make: **GMC** 39 Model: **ENV** 40 Color: **WT** 41 Year: **2004** 42 Plate No: **ZNH55T** 43 State: **NJ** 68 Make: **GMC** 69 Model: **ENV** 70 Color: **WT** 71 Year: **2004** 72 Plate No: **ZNH55T** 73 State: **NJ**
 44 VIN: **1GKDT139242362666** 45 Expires: **01/14** 74 VIN: **1GKDT139242362666** 75 Expires: **01/14**

46 Vehicle Removed To: Driven Left at Scene Towed 47 Authority: Owner Driver Police
JC AND DAUGHTER Impound Disabled Impound Disabled

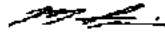

48 Alcohol/Drug Test Given: No Yes Refused 76 Alcohol/Drug Test Given: No Yes Refused
 Type: Breath Blood Urine 77 Authority: Owner Driver Police
 Results: **0** % Pending Impound Disabled Impound Disabled

49 Hazardous Material On Board: Spill 78 Hazardous Material On Board: Spill 79 Authority: Owner Driver Police
 Name or Placard No. 80 Carrier No. USDOT Other' Name or Placard No.

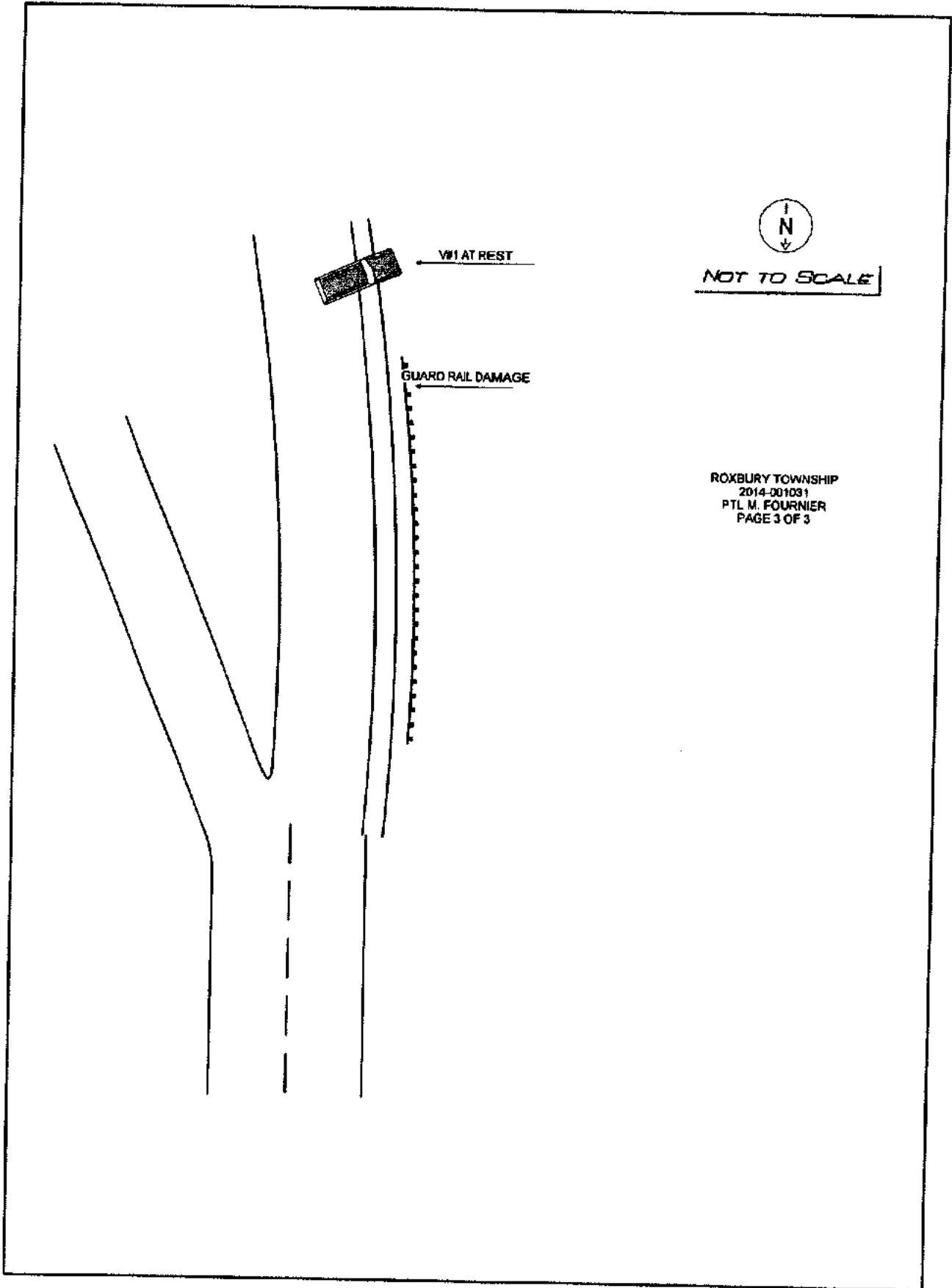
50 Carrier No. USDOT Other' 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 52 Carrier name:

135 Crash Description
D#1 STATED SHE WAS TRAVELING SOUTHBOUND ON LANDING RD. SHE FURTHER STATED THAT AS SHE PASSED ROUTE 80 EASTBOUND EXIT SHE DID NOT REMEMBER HOW SHE LOST CONTROL OF HER VEHICLE. V#1 STRUCK CURB FOLLOWED BY GUARDRAIL, CAUSING IT TO ROTATE CLOCKWISE AND STRIKE CURB AGAIN. INDEPENDENT WITNESS, TOM MASTIC, STATED HE WAS IN FRONT OF V#1 IN THE AREA OF HART ST IN SINGLE LANE OF TRAVEL. MASTIC FURTHER STATED THAT WHEN SINGLE LANE OPENED UP TO TWO LANES V#1 PROCEEDED TO PASS HIM AT A HIGH RATE OF SPEED. MASTIC SAID HE OBSERVED V#1 CHANGE LANES AT LEAST TWO TIMES, EACH TIME HE OBSERVED D#1 ALMOST LOSE CONTROL AS BACK END WOULD ALMOST GIVE WAY. HE OBSERVED V#1 IN LEFT LANE IN THE AREA OF ROUTE 80 EASTBOUND EXIT WHEN V#1 CHANGED TO RIGHT LANE AND LOST CONTROL WHERE IT STRUCK CURB, GUARDRAIL, THEN ROTATED

136 Damage To Other Property: **APPROXIMATELY 20 FEET OF GUARDRAIL**
 Oper. 1: **1** 137 Charge: Multiple Charges **39:4-97** 138 Summons No: **R-060548** Oper. 2: Multiple Charges 139 Charge: Multiple Charges 140 Summons No:

141 Officer's Signature: **FOURNIER, MICHAEL** 142 Badge No: **194** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**
 

	63	64	65	66	67	68	69	70	71	72	73	74	75	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	04	21	F	01	08	01	08	04	--	--	GOLDBERRY, SHAWNTAE M, 282 CENTER ST, LANDING, NJ 07860
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	



NOT TO SCALE

ROXBURY TOWNSHIP
2014-001031
PTL M. FOURNIER
PAGE 3 OF 3

07 1 Case Number: 2014-001098
 10 Crash Occurred On: LANDING RD
 11 Speed Limit: 45
 12 Route No.: 0631
 13 Milepost: .06
 14 Police Dept of: ROXBURY TWP
 15 Code: 01
 16 Dir: S
 17 Cross Road Name: N. FRONTAGE RD.
 18 Speed Limit: 40

08 3 Station/Precinct: 300
 09 4 Date of Crash: 01/15/14
 5 Day of Week: WED
 6 Time (use 2400 hrs): 0328
 7 Municipality Code: 1436
 8 Total Killed: 0
 9 Total Injured: 0
 19 Ramp: To: From: NB EB
 20 Route/Name: SB WB

101 23 Veh No: 01
 24 Policy No.: 28438446-2
 25 Ins Code: 134
 53 Veh No:
 54 Policy No.:
 55 Ins Code: 01
 26 Driver's First Name: JOHNNY
 Last Name: SALDARRIAGA
 29 Sex: M
 56 Driver's First Name:
 Last Name:
 59 Sex:
 27 Number and Street: 8 E CHRYSTAL ST
 30 Eyes: 2
 57 Number and Street:
 80 Eyes:
 28 City: DOVER
 State: NJ
 Zip: 07801
 58 City:
 State:
 Zip:
 31 State: NJ
 32 Driver's License No: 902634070012892
 33 DOB: 12/14/89
 34 Expires: 07/15
 61 State:
 62 Driver's License No:
 63 DOB:
 64 Expires:
 35 Owner's First Name: VIVIANA
 Initial: A
 Last Name: BARRIENTOS
 65 Owner's First Name:
 Initial:
 Last Name:
 36 Number and Street: 76 OAKWOOD VILLAGE
 66 Number and Street:
 37 City: FLANDERS
 State: NJ
 Zip: 07836-9079
 67 City:
 State:
 Zip:
 38 Make: NIS
 39 Model: ALT
 40 Color: BK
 41 Year: 2001
 42 Plate No.: J60CSW
 43 State: NJ
 68 Make:
 69 Model:
 70 Color:
 71 Year:
 72 Plate No.:
 73 State:
 44 VIN: 1N4DL01D61C184330
 45 Expires: 01/14
 74 VIN:
 75 Expires:
 46 Vehicle Removed To: Driven Left at Scene Towed
 47 Authority: Owner Driver
 76 Vehicle Removed To: Driven Left at Scene Towed
 77 Authority: Owner Driver Police

109 48 Alcohol/Drug Test Given: No Yes Refused
 Type: Breath Blood Urine
 Results: 0. % Pending
 78 Alcohol/Drug Test Given: No Yes Refused
 Type: Breath Blood Urine
 Results: 0. % Pending
 49 Hazardous Material On Board: Spill Name or Placard No.:
 50 Carrier No.: USDOT Other
 51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 52 Carrier name:
 53 Crash Description:
 136 Damage To Other Property: COUNTY ROAD DEPARTMENT GUARD RAIL
 137 Charge: Multiple Charges
 138 Summons No.:
 139 Charge: Multiple Charges
 140 Summons No.:
 141 Officer's Signature: CEREZO, THOMAS
 142 Badge No.:
 143 Reviewed By: PALANCHI, MARC
 144 Case Status: CLOSED

108 44 VIN: 1N4DL01D61C184330
 45 Expires: 01/14
 74 VIN:
 75 Expires:
 46 Vehicle Removed To: Driven Left at Scene Towed
 47 Authority: Owner Driver
 76 Vehicle Removed To: Driven Left at Scene Towed
 77 Authority: Owner Driver Police

110 48 Alcohol/Drug Test Given: No Yes Refused
 Type: Breath Blood Urine
 Results: 0. % Pending
 78 Alcohol/Drug Test Given: No Yes Refused
 Type: Breath Blood Urine
 Results: 0. % Pending
 49 Hazardous Material On Board: Spill Name or Placard No.:
 50 Carrier No.: USDOT Other
 51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs
 52 Carrier name:
 53 Crash Description:
 136 Damage To Other Property: COUNTY ROAD DEPARTMENT GUARD RAIL
 137 Charge: Multiple Charges
 138 Summons No.:
 139 Charge: Multiple Charges
 140 Summons No.:
 141 Officer's Signature: CEREZO, THOMAS
 142 Badge No.:
 143 Reviewed By: PALANCHI, MARC
 144 Case Status: CLOSED

117 Driver #1 said that he was travelling south on Landing Rd. in the left lane. D1 said that he was attempting to change into the right lane when he lost control of his vehicle and struck the guardrail face. No injuries reported. Cause of crash is icy road condition.

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	24	M	--	--	--	09	09	01	--	SALDARRIAGA, JOHNNY, 8 E CHRYSTAL ST, DOVER, NJ 07801
B	1	03	01	--	21	M	--	--	--	09	09	01	--	GIRALDO, JORGE M, 59 FIRST STREET, DOVER, NJ 07801
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

New Jersey Police Crash Investigation Report

Police Dept: COXENKILL TWP. P.D.

Code: 01436

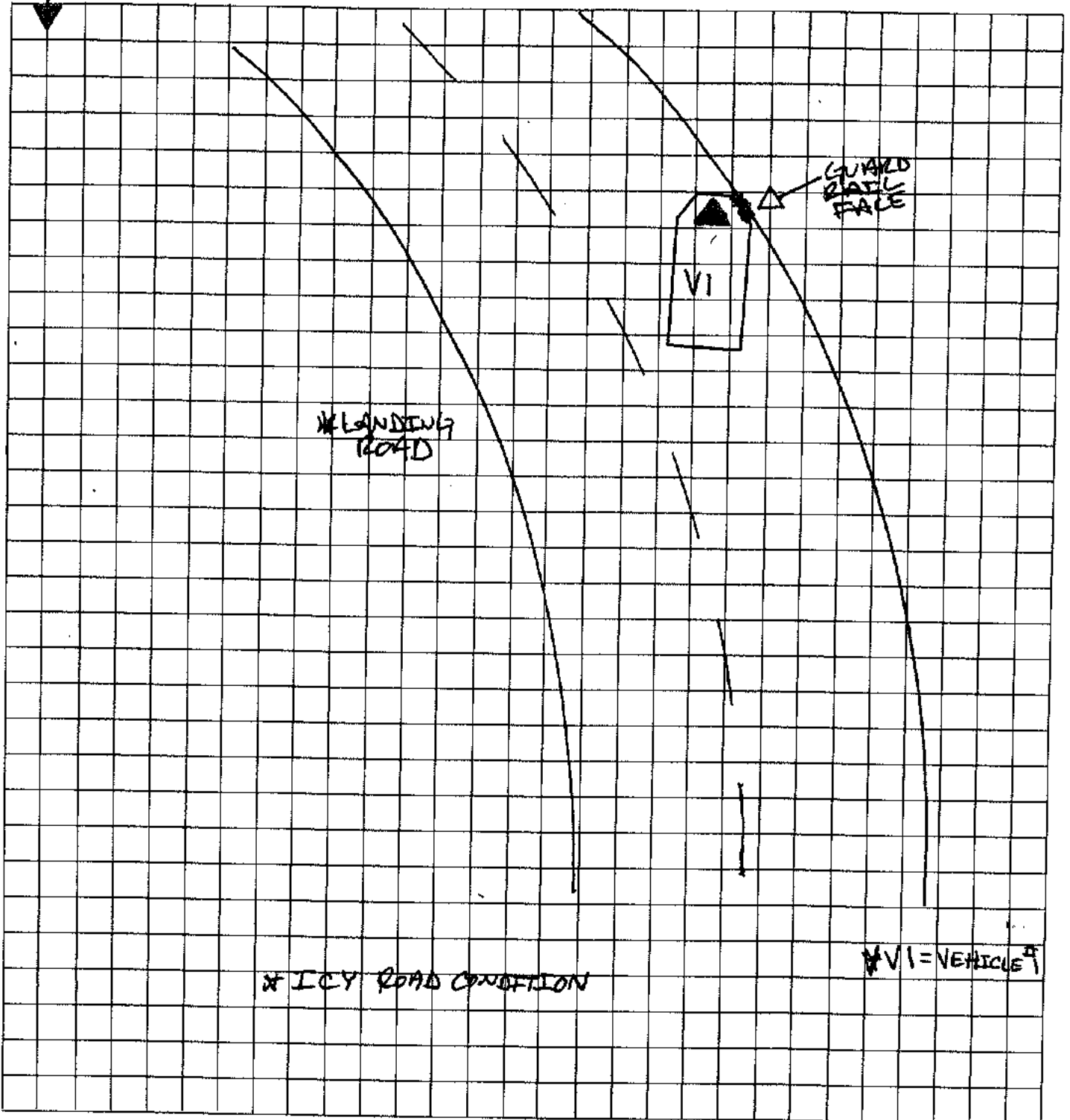
Motor Vehicle Crash Diagram

Station: _____

Case No: 2014-001098

134 Crash Diagram (NOT TO SCALE)

Indicate North



ACTY Lino
Officer's Signature

01 Case Number: **2014-006207** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit: **S 36** 0631 **A .05** 118a **26**
 02 Police Dept of: **ROXBURY TWP** Code: **01** 280 At Intersection with N E **Dir** **S** 12 Route No. **0631** **A** 13 Milepost **.05** 118b **26**
 03 Station/Precinct: **14** Feet S W of: **NORTH FRONTAGE ROAD** 18 Speed Limit **40** 119a **04**
 04 Date of Crash: **03/20/14** 5 Day of Week: **THU** 6 Time (Use 2400 hrs): **1812** 7 Municipality Code: **1438** 8 Total Killed: **0** 9 Total Injured: **0** 17 Cross Road Name: **19** To: NB EB **119b** **02**
 20 Route Name: SB WB

23 Veh No: **01** 24 Policy No: **190320580** 25 Ins Code: **21852** 53 Veh No: **02** 54 Policy No: **DXA80299383** 55 Ins Code: **043** 120 **01**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name: **DAVID** Initial: **KEVIN** Last Name: **RADCLIFFE** 29 Sex: **M** 56 Driver's First Name: **JULIAN** Initial: **M** Last Name: **ZORRILLA** 58 Sex: **M** 121 **01**
 27 Number and Street: **517 E 8TH ST.** 30 Eyes: **2** 57 Number and Street: **33 JACKSON AVE** 60 Eyes: **6** 122 **00**

28 City: **CORONA** State: **CA** Zip: **92879** 58 City: **DOVER** State: **NJ** Zip: **07801-3825** 123 **00**

31 State: **CA** 32 Driver's License No: **A3420045** 33 DOB: **09/23/66** 34 Expires: **09/14** 61 State: **NJ** 62 Driver's License No: **Z66504207403816** 63 DOB: **03/19/81** 64 Expires: **03/16** 124 **01**

35 Owner's First Name: **DAVID** Initial: **KEVIN** Last Name: **RADCLIFFE** 65 Owner's First Name: **MARKAS FINANCIAL SERVICES INC** Initial: **MARKAS FINANCIAL SERVICES INC** Last Name: **MARKAS FINANCIAL SERVICES INC** 125 **02**
 36 Number and Street: **517 E 8TH ST** 66 Number and Street: **167 STEPHENS PARK RD** 126 **-**

37 City: **CORONA** State: **CA** Zip: **92879** 67 City: **HACKETTSTOWN** State: **NJ** Zip: **07840** 127 **08**

38 Make: **NISS** 39 Model: **TRUCK** 40 Color: **SILVER** 41 Year: **2001** 42 Plate No.: **3Y86034** 43 State: **CA** 68 Make: **MACK** 69 Model: **GU7** 70 Color: **GREEN** 71 Year: **2014** 72 Plate No.: **AP807U** 73 State: **NJ** 128a **06**
 44 VIN: **1NBND1184KC404936** 45 Expires: **10/15** 74 VIN: **1M2AX07C2EM010664** 75 Expires: **09/14** 128b **46**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police 128c **28**

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 128d **-**
 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 128e **26**

49 Hazardous Material On Board: Spill: Name or Placard No.: **79 Hazardous Material On Board: Spill: Name or Placard No.:** 129b **-**

50 Carrier No.: USDOT Other: **80 Carrier No.: USDOT Other: 2343046** 129d **-**

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs **81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs** 130 **11**

52 Carrier name: **52 Carrier name: VALIENT CONTRACTING** 131 **11**

135 Crash Description: **V1 was traveling south on Landing Road when V2 exited North Frontage Road. V2 was a dump truck with Valient Contracting and proceeded onto Landing Road. V2 had to take a wide turn onto Landing Road and occupied both lanes of the roadway. D1 stated he was cut off by V2 and was forced into the left lane of Landing Road to avoid collision. D1 further advised V2 then swerved into his lane causing him to strike the guardrail along with the rear tire of the dump truck.** 132 **08**

D2 stated he stopped at the stop sign and did not observe any traffic traveling south on Landing Road. He then pulled out and struck the side of V1.

D2 was at fault for failing to yield to an oncoming vehicle, no injuries occurred during the crash.

136 Damage To Other Property: **NONE** 133 **08**

Oper: **N/A** 137 Charge: **N/A** Multiple Charges 138 Summons No.: **N/A** Oper: **N/A** 139 Charge: **39:4-97** Multiple Charges 140 Summons No.: **1436-R-061921**

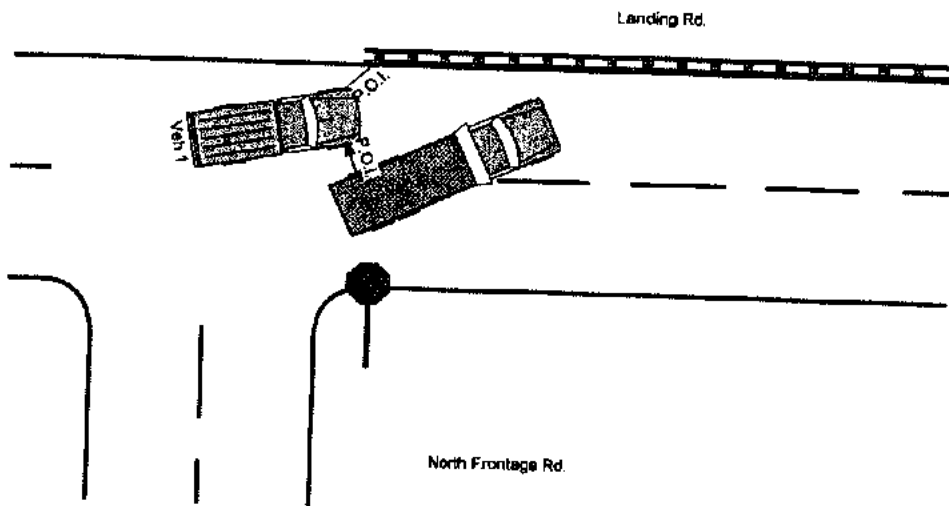
141 Officer's Signature: **GAFFEY, SEAN** 142 Badge No.: **215** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	-	-	68	M	-	-	-	04	04	-	-	RADCLIFFE, DAVID KEVIN, 517 E 8TH ST., CORONA, CA 92879
B	1	03	-	-	49	F	-	-	-	04	04	-	-	RADCLIFFE, NANCY ELLEN, PO BOX 1813, CORONA, CA 92879
C	2	01	-	-	33	M	-	-	-	09	04	-	-	ZORRILLA, JULIAN M, 33 JACKSON AVE, DOVER, NJ 07801-3825
D	-	-	-	-	-	-	-	-	-	-	-	-	-	
E	-	-	-	-	-	-	-	-	-	-	-	-	-	

Case Number: 2014-006207
Ptl. S. Gelfey #215
Roxbury Township Police Department
Page 2 of 2



NOT TO SCALE



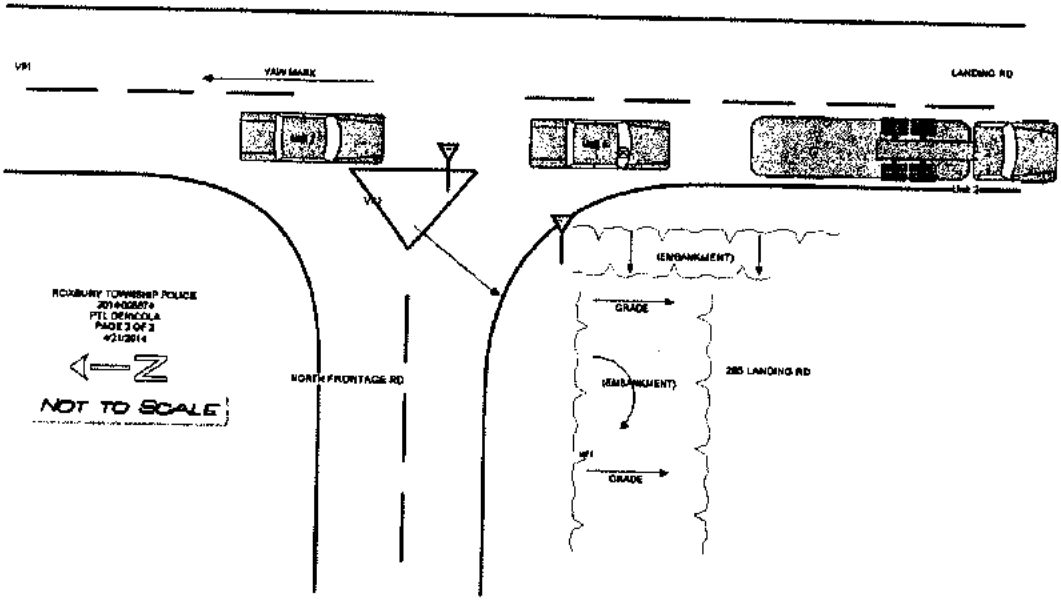
New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

03	Page 1 of 1 <input type="checkbox"/> Fatal	New Jersey Police Crash Investigation Report										<input type="checkbox"/> Reportable	<input checked="" type="checkbox"/> Non-Reportable	<input type="checkbox"/> Change Report
01	1 Case Number 2014-008874		10 Crash Occurred On: LANDING RD		11 Speed Limit SOUTH 35		12 Route No. 0631		13 Milepost A .02		118a		26	
01	2 Police Dept of ROXBURY TWP		Code 01		500		Road Name SOUTH		Dir SOUTH		12 Route No.		13 Milepost	
01	3 Station/Precinct		14		<input type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E		<input checked="" type="checkbox"/> Feet		<input checked="" type="checkbox"/> S <input type="checkbox"/> W		of:		18 Speed Limit	
05	4 Date of Crash mm dd yy 04/21/14		5 Day of Week MON		8 Time (use 2400 hrs) 1148		7 Municipality Code 1438		8 Total Killed 00		9 Total Injured 00		19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB	
01	23 Veh No 01		24 Policy No. PAA00002815271		25 Ins Code 963		53 Veh No 02		54 Policy No. SELF INSURED		55 Ins Code		119b	
01	26 Driver's First Name ALBERT		Initial H		Last Name JOSEPHS		29 Sex M		56 Driver's First Name JOSE		Initial M		Last Name MARRERO	
01	27 Number and Street 280 LACKAWANNA DRIVE		30 Eyes 2		57 Number and Street 292 EAST 6TH ST		60 Eyes 2		21 Latitude		22 Longitude		120	
01	28 City ANDOVER		State NJ		Zip 07821		58 City CLIFTON		State NJ		Zip 07011-1740		121	
02	31 State NJ		32 Driver's License No J66970176801382		33 DOB mm dd yy 01/16/38		34 Expires mm yy 06/14		61 State NJ		62 Driver's License No M06644107408552		63 DOB mm dd yy 08/23/66	
01	35 Owner's First Name <input type="checkbox"/> Same As PAULINE		Initial J		Last Name JOSEPHS		65 Owner's First Name <input type="checkbox"/> Same As		Initial T		Last Name TRANSPORTATION DEPT OF NJ		59 Sex M	
01	36 Number and Street 280 LACKAWANNA DR		37 City BYRAM		State NJ		Zip 07821-4113		66 Number and Street PO BOX 600 BLDG1		67 City TRENTON		State NJ	
04	38 Make TOY		39 Model HIG		40 Color GN		41 Year 2001		42 Plate No. UDU34F		43 State NJ		68 Make INT	
01	44 VIN JTEGF21A910023323		45 Expires 11/14		74 VIN 1HSGLGUR6LH281404		75 Expires 06/16		69 Model TRUCK		70 Color WHITE		71 Year 1990	
01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		47 Authority <input checked="" type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input checked="" type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		72 Plate No. TD4306		73 State NJ		122	
01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0 % <input type="checkbox"/> Pending		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0 % <input type="checkbox"/> Pending		123	
01	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight		124	
01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		51 Commercial Vehicle Weight		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight		82 Carrier name		82 Carrier name		125	
01	52 Carrier name		135 Crash Description		82 Carrier name		82 Carrier name		82 Carrier name		82 Carrier name		126	
01	136 Damage To Other Property NONE		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.		141 Officer's Signature DENICOLA, THOMAS <i>Thomas Denicola</i>		142 Badge No. 207	
01	141 Officer's Signature		142 Badge No.		143 Reviewed by PALANCHI, MARC <i>Marc Palanchi</i>		144 Case Status CLOSED		145		146		147	

D1 STATES HE WAS TRAVELING SOUTH ON LANDING RD WHEN A STONE KICKED UP FROM V2 CAUSING HIS WINDSHIELD TO CRACK. D2 STATED HE DROPPED OFF EQUIPMENT AT THE LANDING RD MEDIAN AND PULLED ONTO LANDING RD TRAVELING SOUTH AND DIDN'T REALIZE A STONE KICKED UP. V2 IS A NJ STATE DOT TRUCK WHICH WAS TOWING A STATE DOT TRAILER (NJ REG#TD10766). THE STATE DOT TRUCK AND TRAILER ARE SELF INSURED THROUGH THE STATE OF NJ.

83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death	
A	1	01	01	--	76	M	--	01	09	04	--	--	JOSEPHS, ALBERT H, 280 LACKAWANNA DRIVE, ANDOVER, NJ 07821	
B	2	01	01	--	67	M	--	01	04	04	--	--	MARRERO, JOSE M, 292 EAST 6TH ST, CLIFTON, NJ 07011-1740	
C	--	--	--	--	--	--	--	--	--	--	--	--		
D	--	--	--	--	--	--	--	--	--	--	--	--		
E	--	--	--	--	--	--	--	--	--	--	--	--		



ROXBURY TOWNSHIP POLICE
 2014022674
 PFL DEMICOLA
 PAGE 2 OF 3
 4/21/2014

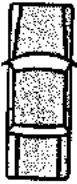
← Z
 NOT TO SCALE

06	05	1 Case Number 2014-000423										118a
	01	10 Crash Occurred On: LANDING RD. SOUTH 45 0631 A .04										25
	01	2 Police Dept of ROXBURY TWP 01										118b
	01	3 Station/Precinct 200										-
	06	11 Speed Limit 50										118c
	02	4 Date of Crash 01/07/14										119a
	02	5 Day of Week TUE										02
	02	6 Time (use 2400 hrs) 1042										119b
	02	7 Municipality Code 1436										-
	02	8 Total Killed -										120
	02	9 Total Injured -										01
	02	23 Veh No 01 24 Policy No. 170478837 25 Ins Code 315										121
	02	26 Driver's First Name LEIGH A 29 Sex M										01
	02	27 Number and Street 14 FARRAGUT PLACE 30 Eyes 2										122
	02	28 City MORRISTOWN State NJ Zip 07960-6213										123
	02	31 State NJ 32 Driver's License No B06664636101942 33 DOB 01/31/94 34 Expires 02/15										124
	02	35 Owner's First Name MARTIN W Last Name BARRETT										125
	02	36 Driver's First Name RICHARD M Last Name SPRAGUE 58 Sex M										07
	02	37 City MORRISTOWN State NJ Zip 07960-6213										126
	02	38 Make HYU 39 Model VEL 40 Color RD 41 Year 2012 42 Plate No. J36BVZ 43 State NJ										09
	02	44 VIN KMHTC6AD8CU048346 45 Expires 02/16										127
	02	46 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input checked="" type="checkbox"/> Towed										11
	02	47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Impound <input type="checkbox"/> Disabled <input type="checkbox"/> Police										128a
	02	48 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused										26
	02	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No. <input type="checkbox"/>										128b
	02	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other <input type="checkbox"/>										128c
	02	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs										128d
	02	52 Carrier name <input type="checkbox"/>										130
	02	135 Crash Description										131
	02	136 Damage To Other Property NONE										08
	02	137 Charge 39:4-97 <input type="checkbox"/> Multiple Charges										132
	02	138 Summons No. R-060016 Oper. -										12
	02	139 Charge <input type="checkbox"/> Multiple Charges										12
	02	140 Summons No. -										12
	02	141 Officer's Signature EDMUNDS, JONATHAN										12
	02	142 Badge No. 166										12
	02	143 Reviewed By PALANCHI, MARC										12
	02	144 Case Status OPEN										12

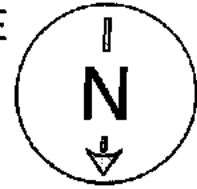
Driver one stated that he exited Rt. 80 east via exit 28 and was yielding to traffic at Landing Rd. south when vehicle two, which was directly behind him, struck his vehicle from behind. Driver two stated that he exited Rt. 80 east via exit 28 and was looking over his shoulder at the oncoming traffic and didn't realize that vehicle one stopped for traffic at which time he struck the rear of vehicle one. Driver two was at fault for the accident because he was driving with out due caution End of report.

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	-	19	M	-	-	-	09	04	-	-	BARRETT, LEIGH A, 14 FARRAGUT PLACE, MORRISTOWN, NJ 07960-6213
B	2	01	01	-	31	M	-	-	-	09	04	-	-	SPRAGUE, RICHARD M, 390 BEAUMONT AVE, TUCKERTON, NJ 08087-2508
C	-	-	-	-	-	-	-	-	-	-	-	-	-	
D	-	-	-	-	-	-	-	-	-	-	-	-	-	
E	-	-	-	-	-	-	-	-	-	-	-	-	-	

LANDING RD.



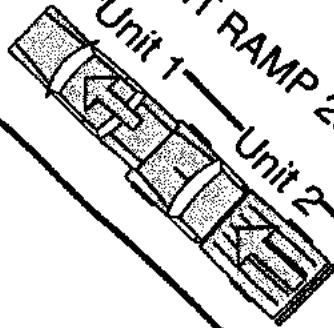
ROXBURY TWP. POLICE
2014-000423
PTL. EDMUNDS
PAGE 2 OF 2.



NOT TO SCALE



EXIT RAMP 28



1 Case Number: **2014-003097** 10 Crash Occurred On: **LANDING RD SOUTH 45 0631 A .04** 11 Speed Limit: **25**

2 Police Dept of: **ROXBURY TWP 01** Code: **01** Road Name: **LANDING RD** Dr: **SOUTH** 12 Route No.: **45** Suffix: **A** 13 Milepost: **.04**

3 Station/Precinct: **01** 14 At Intersection with: N E S W of: **ROUTE 46** 18 Speed Limit: **50**

15 Feet Miles 16 Feet Miles 17 Cross Road Name: **ROUTE 46**

4 Date of Crash: **02/11/14** 5 Day of Week: **TUE** 6 Time (use 2400 hrs): **0923** 7 Municipality Code: **1436** 8 Total Killed: **0** 9 Total Injured: **01**

19 To: NB EB SB WB WB

20 Route/Name: **01** 21 Latitude: **01** 22 Longitude: **01**

23 Veh No: **01** 24 Policy No.: **4076089714** 25 Ins Code: **148** 53 Veh No: **02** 54 Policy No.: **ACPA000927037** 56 Ins Code: **172**

PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name: **CEANNA** Initial: **M** Last Name: **CHMURA** 29 Sex: **F** 56 Driver's First Name: **LINDA** Initial: **M** Last Name: **DIPAULO** 59 Sex: **F**

27 Number and Street: **33 MITCHELL ROAD** 30 Eyes: **4** 57 Number and Street: **27 ROLLING RAPIDS COURT** 60 Eyes: **2**

28 City: **HACKETTSTOWN** State: **NJ** Zip: **07840-2511** 58 City: **HACKETTSTOWN** State: **NJ** Zip: **07840-1736**

31 State: **NJ** 32 Driver's License No: **C35631167469814** 33 DOB: **08/14/81** 34 Expires: **10/16** 61 State: **NJ** 62 Driver's License No: **D45864737457712** 63 DOB: **05/14** 64 Expires: **05/14**

35 Owner's First Name: **CEANNA** Initial: **M** Last Name: **CHMURA** 65 Owner's First Name: **JOSEPH** Initial: **A** Last Name: **DIPAULO**

36 Number and Street: **33 MITCHELL ROAD** 66 Number and Street: **27 ROLLING RAPIDS COURT**

37 City: **HACKETTSTOWN** State: **NJ** Zip: **07840-2511** 67 City: **HACKETTSTOWN** State: **NJ** Zip: **07840-1736**

38 Make: **FOR** 39 Model: **ESC** 40 Color: **BL** 41 Year: **2002** 42 Plate No.: **R82CDS** 43 State: **NJ** 68 Make: **HON** 69 Model: **PL** 70 Color: **BLUE** 71 Year: **2009** 72 Plate No.: **ZTW48N** 73 State: **NJ**

44 VIN: **1FMYU04122KA20426** 45 Expires: **06/14** 74 VIN: **5FMYF38819B026388** 75 Expires: **06/14**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled

47 Authority: Owner Driver Police

76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled

77 Authority: Owner Driver Police

48 Alcohol/Drug Test Given: No Yes Refused

Type: Breath Blood Urine

Results: **0** % Pending

49 Hazardous Material On Board: Spill: Name or Placard No.:

50 Carrier No.: USDOT Other

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

52 Carrier name:

78 Alcohol/Drug Test Given: No Yes Refused

Type: Breath Blood Urine

Results: **0** % Pending

79 Hazardous Material On Board: Spill: Name or Placard No.:

80 Carrier No.: USDOT Other

81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs

82 Carrier name:

135 Crash Description

D#1 STATED THAT SHE WAS TRAVELING ON ROUTE 80 EB OFF RAMP ONTO LANDING RD, AND UNKNOWN VEHICLE STOPPED ABRUPTLY IN FRONT OF HER TO WHICH SHE WAS ABLE TO STOP. D#1 FURTHER STATED THAT AS SHE WAS COMING TO A STOP HER VEHICLE WAS STRUCK FROM BEHIND BY V#2. D#2 STATED SHE WAS TRAVELING BEHIND V#1 AND SHE DID OBSERVE IT COME A STOP, HOWEVER SHE WAS UNABLE TO STOP TO AVOID COLLISION. D#1 COMPLAINED OF NECK PAIN BUT REFUSED MEDICAL ATTENTION AT THE SCENE.

136 Damage To Other Property: **NONE**

137 Charge: Multiple Charges 138 Summons No.: **02** Oper. **39:4-97** 139 Charge: Multiple Charges 140 Summons No.: **R-060666**

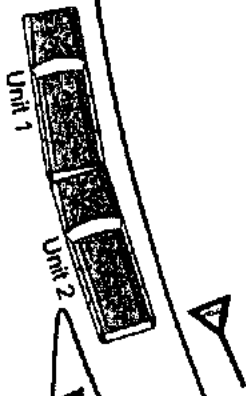
141 Officer's Signature: **FOURNIER, MICHAEL** 142 Badge No.: **194** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	04	32	F	06	08	01	09	04	--	--	CHMURA, CEANNA M. 33 MITCHELL ROAD, HACKETTSTOWN, NJ 07840-2511
B	1	09	01	--	1	F	--	--	--	06	06	--	--	CHMURA, CANDACE, 33 MITCHELL AVE, HACKETTSTOWN, NJ 07840
C	2	01	01	--	--	F	--	--	--	09	09	01	--	DIPAULO, LINDA M. 27 ROLLING RAPIDS COURT, HACKETTSTOWN, NJ 07840-1736
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	



NOT TO SCALE

LANDING RD



ROUTE 80 EB OFF RAMP

ROXBURY TWP
2014-003097
PTL M. FOURNIER
PAGE 2 OF 2

1 Case Number: **2014-003255** 10 Crash Occurred On: **LANDING RD SOUTH 35 0631 B .09** 118a **01**
 2 Police Dept Code: **ROXBURY TWP 01** 12 Route No. **12** Suffix **B** 13 Milepost **.09** 118b **49**
 3 Station/Precinct: **3** 14 At Intersection with N E of: **RT.46** 18 Speed Limit **50** 119a **--**
 Feet S W 16 17 Cross Road Name NB EB 119b **--**
 Miles 15 19 To: SB WB 20 Route/Name NB WB 119c **--**

4 Date of Crash: **02/13/14** 5 Day of Week: **THU** 6 Time (use 2400 hrs): **1226** 7 Municipality Code: **1436** 8 Total Killed: **-** 9 Total Injured: **-** 119d **--**
 21 Latitude: **-** 22 Longitude: **-** 120 **01**

23 Veh No: **01** 24 Policy No: **F-468306-9** 25 Ins Code: **426** 53 Veh No: **-** 54 Policy No: **-** 55 Ins Code: **-** 121 **--**
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN

26 Driver's First Name Initial Last Name: **PAUL J SMITH** 29 Sex: **M** 56 Driver's First Name Initial Last Name: **- - -** 58 Sex: **-** 122 **--**
 27 Number and Street: **2 ONEIDA AVENUE** 30 Eyes: **4** 57 Number and Street: **- - -** 80 Eyes: **-** 123 **--**

28 City: **LANDING** State: **NJ** Zip: **07860-1318** 58 City: **-** State: **-** Zip: **-** 124 **01**

31 State: **NJ** 32 Driver's License No: **967786187103664** 33 DOB: **03/23/86** 34 Expires: **06/16** 61 State: **-** 62 Driver's License No: **-** 63 DOB: **-** 64 Expires: **-** 125 **--**

35 Owner's First Name Initial Last Name: Same As **PAUL J SMITH** 65 Owner's First Name Initial Last Name: Same As **- - -** 126 **04**
 Driver: **2 ONEIDA AVENUE** 66 Number and Street: **- - -**

37 City: **LANDING** State: **NJ** Zip: **07860-1318** 67 City: **-** State: **-** Zip: **-** 127 **--**

38 Make: **TOY** 39 Model: **CAM** 40 Color: **GY** 41 Year: **2011** 42 Plate No.: **LXV98A** 43 State: **NJ** 68 Make: **-** 69 Model: **-** 70 Color: **-** 71 Year: **-** 72 Plate No.: **-** 73 State: **-** 128a **26**

44 VIN: **4T1B03E02BU624988** 45 Expires: **10/14** 74 VIN: **-** 75 Expires: **-** 128b **69**

46 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 47 Authority: Owner Driver Police 76 Vehicle Removed To: Driven Left at Scene Towed Impound Disabled 77 Authority: Owner Driver Police 128c **--**

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0. %** Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0. %** Pending 128d **--**

49 Hazardous Material On Board: Spill: Name or Placard No.: **-** 79 Hazardous Material On Board: Spill: Name or Placard No.: **-** 129a **--**

50 Carrier No.: USDOT Other: **-** 80 Carrier No.: USDOT Other: **-** 129b **--**

51 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight: < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 129c **--**

52 Carrier name: **-** 82 Carrier name: **-** 130 **12**

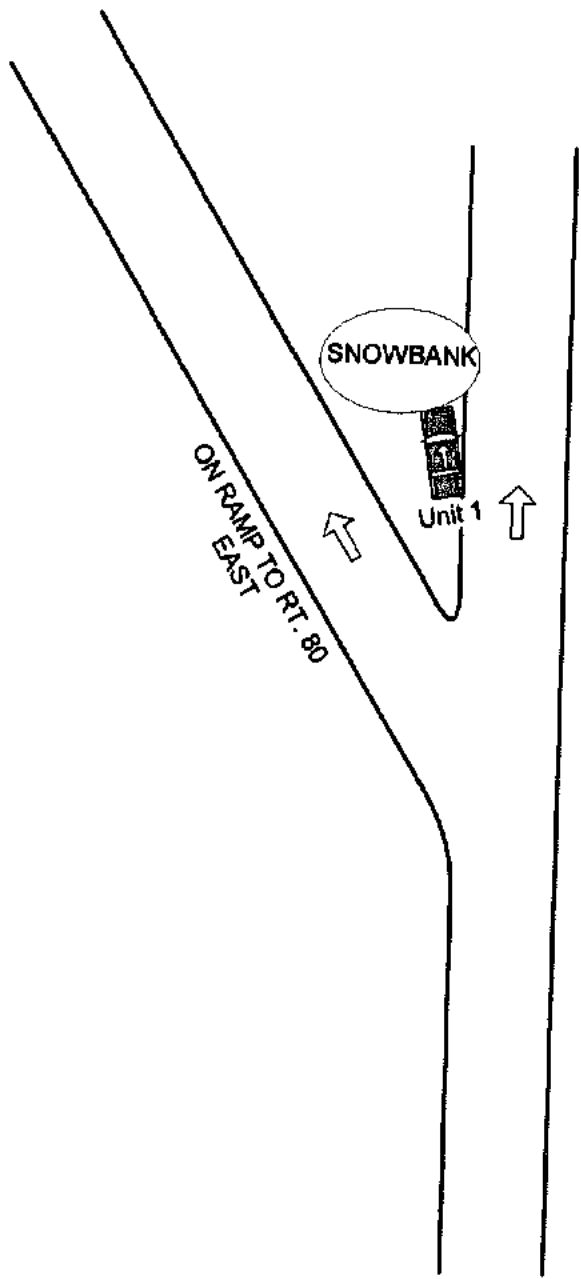
135 Crash Description: **Driver one stated that he was traveling south on landing Rd. toward Succasunna when his vehicle ran over some snow which caused his vehicle to veer from the roadway and strike a snow bank. Ron and Sons towing responded and pulled the vehicle from the snowbank. Driver one was at fault for the accident because he was operating his vehicle at an unsafe speed for the road condition.** 132 **--**

136 Damage To Other Property: **NONE** 133 **--**

137 Charge: Multiple Charges 138 Summons No.: **-** Oper.: **-** 139 Charge: Multiple Charges 140 Summons No.: **-**

141 Officer's Signature: **EDMUNDS, JONATHAN** 142 Badge No.: **166** 143 Reviewed By: **PALANCHI, MARC** 144 Case Status: **CLOSED**

	63	64	65	66	67	68	69	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	57	M	--	--	--	09	04	--	--	SMITH, PAUL 2 ONEIDA AVENUE, LANDING, NJ 07860-1318
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	



NOT TO SCALE

ROXBURY TWP. POLICE
2014-03255
PTL. EDMUNDS
PAGE 2 OF 2

1 Case Number **2014-003162** 11 Speed Limit **35** 118a **25**
 2 Police Dept of **ROXBURY TWP** Code **01** 30 Crash Occurred On: **LANDING RD.** SOUTH **0631** A **04** 118b **-**
 3 Station/Precinct **14** 400 At Intersection with N E S W of **RT. 46** 12 Route No **18** 13 Milepost **60** 119a **25**
 4 Date of Crash **02/12/14** 5 Day of Week **WED** 6 Time (use 2400 hrs) **0858** 7 Municipality Code **1438** 8 Total Killed **-** 9 Total Injured **-** 10 To From **20** Route/Name **RT. 46** 17 Cross Road Name **RT. 46** 18 NB EB SB WB 119b **-**
 21 Latitude **-** 22 Longitude **-** 119c **-**

23 Veh No **01** 24 Policy No. **5574208340** 25 Ins Code **946** 53 Veh No **02** 54 Policy No. **940929-202J** 55 Ins Code **019** 120 **-**
 PARKED PEDESTRIAN PEDALCYCLIST DRESP TO EMERGENCY HIT & RUN PARKED PEDESTRIAN PEDALCYCLIST DRESP TO EMERGENCY HIT & RUN 121 **01**

26 Driver's First Name Initial Last Name **CARMEN A PADILLA** 28 Sex **F** 58 Driver's First Name Initial Last Name **MARY R HUSELTON** 59 Sex **F** 122 **-**
 27 Number and Street **43 EAGLE ROCK VLG APT. 6B** 30 Eyes **2** 57 Number and Street **100 E SHAWNEE TRAIL** 60 Eyes **4** 123 **-**
 28 City **BUDD LAKE** State **NJ** Zip **07828-3343** 58 City **WHARTON** State **NJ** Zip **07886-2920** 124 **08**

31 State **NJ** 32 Driver's License No **P01021116157723** 33 DOB **07/12/72** 34 Expires **06/16** 61 State **NJ** 62 Driver's License No **H94326277963694** 63 DOB **03/07/69** 64 Expires **06/16** 125 **08**
 35 Owner's First Name Initial Last Name **CARMEN A PADILLA** 65 Owner's First Name Initial Last Name **MARY R HUSELTON** 126 **09**
 Same As Driver Driver Same As Driver Driver

36 Number and Street **43 EAGLE ROCK VLG APT 6B** 66 Number and Street **100 E SHAWNEE TRAIL** 127 **11**
 37 City **BUDD LAKE** State **NJ** Zip **07828-3343** 67 City **WHARTON** State **NJ** Zip **07886-2920** 128a **28**
 38 Make **FOR** 39 Model **ESC** 40 Color **GREEN** 41 Year **1997** 42 Plate No. **Z64DTT** 43 State **NJ** 68 Make **NIS** 69 Model **ALT** 70 Color **BR** 71 Year **2013** 72 Plate No. **ZUT30C** 73 State **NJ** 128b **-**
 44 VIN **1FALP13P7CW181440** 45 Expires **12/14** 74 VIN **1N4AL3AP6DN437720** 75 Expires **06/14** 128c **-**

46 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 47 Authority Owner Driver Police 76 Vehicle Removed To Driven Left at Scene Towed Impound Disabled 77 Authority Owner Driver Police 128d **-**

48 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 78 Alcohol/Drug Test Given: No Yes Refused Type: Breath Blood Urine Results: **0** % Pending 129a **28**

49 Hazardous Material On Board Spill Name or Placard No. **-** 79 Hazardous Material On Board Spill Name or Placard No. **-** 129b **-**
 50 Carrier No. USDOT Other **-** 80 Carrier No. USDOT Other **-** 129c **-**
 51 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 81 Commercial Vehicle Weight < 10,000 lbs 10,001 - 26,000 lbs > 26,001 lbs 130 **06**
 52 Carrier name **-** 82 Carrier name **-** 131 **06**

135 Crash Description 132 **06**

Driver one reported that she was traveling on Rt. 80 east and exited the highway via exit 28 and was yielding to the traffic on Landing Rd. when she was struck from behind by vehicle two. Driver two reported that she was directly behind vehicle one and stopped because vehicle one had stopped. She was then struck from behind by vehicle three which caused her vehicle to hit the rear of vehicle one. Driver three reported he was directly behind vehicle two and thought vehicle two was going to proceed. He then accelerated and struck the rear of vehicle two. Driver three was at fault for the accident. End of report.

136 Damage To Other Property **-** 137 Charge Multiple Charges 138 Summons No. **-** 139 Charge Multiple Charges 140 Summons No. **-**

141 Officer's Signature **EDMUNDS, JONATHAN** 142 Badge No. **166** 143 Reviewed By **PALANCHI, MARC** 144 Case Status **CLOSED**
[Signature] *[Signature]*

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	41	F	--	--	--	09	04	--	--	PADILLA, CARMEN A, 43 EAGLE ROCK VLG APT. 6B, BUDD LAKE, NJ: 07828-3343
B	1	06	01	--	37	F	--	--	--	04	04	--	--	ROAS-ASENI, D, CLAUDIA, 43 EAGLE ROCK VILLAGE 6B, BUDD LAKE, NJ: 07828
C	2	01	01	--	54	F	--	--	--	09	04	--	--	HUSELTON, MARY R, 100 E SHAWNEE TRAIL, WHARTON, NJ: 07886-2920
D	3	01	01	--	20	M	--	--	--	09	04	--	--	SHERMAN, KEVIN N, 14 ROLLING HILLS DR, BUDD LAKE, NJ: 07828-2638
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

Main form containing fields for Case Number (2014-003162), Police Dept (ROXBURY TWP), Station/Precinct, Date of Crash (02/12/14), Driver Information (KEVIN N SHERMAN), Vehicle Information (MAZ 3), and various checkboxes for vehicle status and tests.

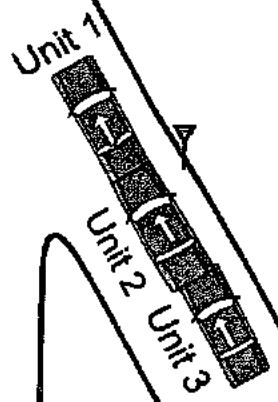
138 Damage To Other Property

Oper. 137 Charge Multiple Charges 138 Summons No. Oper. 139 Charge Multiple Charges 140 Summons No.

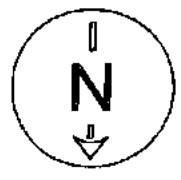
141 Officer's Signature EDMUNDS, JONATHAN 142 Badge No. 166 143 Reviewed By PALANCHI, MARC 144 Case Status CLOSED

Table with columns 83-95 and 'Names & Addresses of Occupants - If Deceased, Date & Time of Death'. Rows A-E are currently empty.

LANDING
RD.



EXIT RAMP 28



NOT TO SCALE

ROXBURY TWP. POLICE
2014-03162
PTL. EDMUNDS
PAGE 2 OF 2.

New Jersey Police Crash Investigation Report

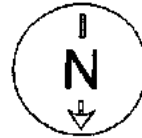
Reportable
 Non-Reportable
 Change Report

Page 1 of 1 <input type="checkbox"/> Fatal		New Jersey Police Crash Investigation Report										<input checked="" type="checkbox"/> Reportable <input type="checkbox"/> Non-Reportable <input type="checkbox"/> Change Report			
1 Case Number 2014-005500		10 Crash Occurred On: LANDING RD										11 Speed Limit 35		118a 02	
2 Police Dept of ROXBURY TWP		Code 01		Road Name SOUTH		Dir 35		12 Route No. 0631		Suffix B		13 Milepost .04		118b --	
3 Station/Princt		400		<input type="checkbox"/> At Intersection with <input checked="" type="checkbox"/> Feet <input type="checkbox"/> Miles		<input checked="" type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> S <input type="checkbox"/> W		of:		17 Cross Road Name RT 46		18 Speed Limit 50		118c --	
4 Date of Crash mm dd yy 03/12/14		5 Day of Week		6 Time (use 2400 hrs) 0924		7 Municipality Code 1436		8 Total Killed		9 Total Injured		19 Ramp <input type="checkbox"/> To: <input type="checkbox"/> From:		118d --	
23 Veh No 01		24 Policy No. 60577203-7		25 Ins Code 134		53 Veh No 02		54 Policy No. Y1N8103D173166		55 Ins Code 080		21 Latitude		120 01	
26 Driver's First Name MICHAEL		Initial J		Last Name MURRAY		29 Sex M		56 Driver's First Name ROBERT		Initial R		Last Name HOWARD		59 Sex M	
27 Number and Street 21 OAKWOOD VILLAGE APT 12		30 Eyes 4		57 Number and Street 2698 COLESVILLE RD		60 Eyes 8		58 City HARPURVILLE		State NY		Zip 13787		121 01	
31 State NJ		32 Driver's License No M94026447111604		33 DOB mm dd yy 11/18/60		34 Expires mm yy 08/18		61 State NY		62 Driver's License No 238629922		63 DOB mm dd yy 04/01/88		64 Expires mm yy 04/18	
35 Owner's First Name <input type="checkbox"/> Same As MICHAEL		Initial J		Last Name MURRAY		65 Owner's First Name <input type="checkbox"/> Same As RYDER TRUCK RENTAL		66 Number and Street 329 JEFFERSON RD		67 City ROCHESTER		State NY		Zip 14623	
36 Number and Street 21 OAKWOOD VILLAGE APT 12		37 City FLANDERS		State NJ		Zip 07836-9022		68 Make FRHT		69 Model TRUCK		70 Color WHITE		71 Year 2009	
38 Make MAZ		39 Model MGI		40 Color BL		41 Year 2010		42 Plate No. F74AHU		43 State NJ		72 Plate No. 10089PB		73 State NY	
44 VIN 1YVHZ8CH7A6M60988		45 Expires 12/14		46 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input checked="" type="checkbox"/> Towed		47 Authority <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		48 Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		74 VIN 1FUJA6CK99DAM4369		75 Expires 04/14		76 Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police	
48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		79 Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		80 Authority <input type="checkbox"/> Impound <input type="checkbox"/> Disabled	
49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name of Placard No.		50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name WILLOW RUN FOODS		83 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		84 Carrier name		85 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	
51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		52 Carrier name		135 Damage To Other Property NONE		136 Charge <input type="checkbox"/> Multiple Charges		137 Summons No.		138 Charge <input type="checkbox"/> Multiple Charges		139 Summons No.		140 Case Status CLOSED	
141 Officer's Signature EDMUNDS, JONATHAN		142 Badge No. 166		143 Reviewed By MURRAY, DANIEL		144 Case Status CLOSED		145 Charge <input type="checkbox"/> Multiple Charges		146 Summons No.		147 Charge <input type="checkbox"/> Multiple Charges		148 Summons No.	

Driver one stated in effect that he exited Rt. 80 east via exit 28 and upon approaching the merge with Landing Rd. two cars directly in front of him were stopped. After the two cars proceeded he merged onto Landing Rd. where he was struck from behind by vehicle two which was traveling on Landing Rd. Driver two stated in effect that he was traveling south on Landing Rd. when vehicle one pulled out in front of him. He attempted to stop but was unable to and ultimately struck the rear of vehicle one. Driver one was at fault for the accident because he failed to yield the right of way to vehicle two. End of report.

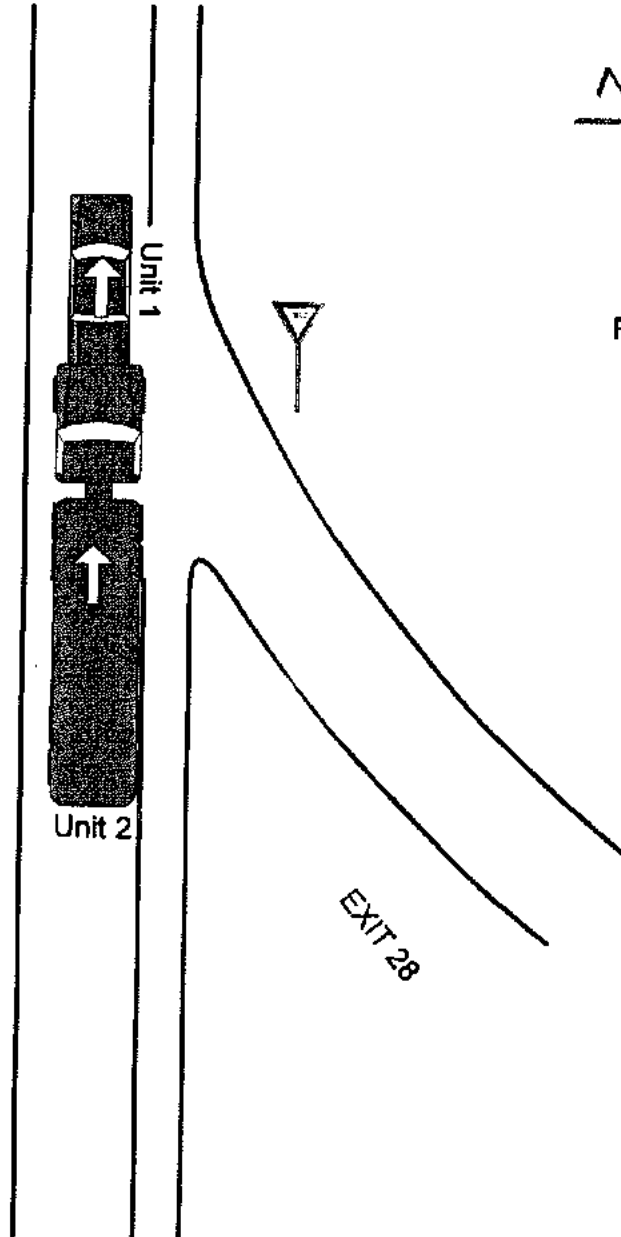
	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death	
A	1	01	01	--	83	M	--	--	--	09	04	--	--	MURRAY, MICHAEL J. 21 OAKWOOD VILLAGE APT 12, FLANDERS, NJ 07836-9022	
B	2	01	01	--	25	M	--	--	--	09	04	--	--	HOWARD, ROBERT R. 2698 COLESVILLE RD, HARPURVILLE, NY 13787	
C	--	--	--	--	--	--	--	--	--	--	--	--	--		
D	--	--	--	--	--	--	--	--	--	--	--	--	--		
E	--	--	--	--	--	--	--	--	--	--	--	--	--		

NOT TO SCALE



ROXBURY TWP. POLICE
2014-05500
PTL. J. EDMUNDS
PAGE 2 OF 2.

LANDING ROAD



New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

03	Page 1 of 1 <input type="checkbox"/> Fatal										
01	1 Case Number 2014-008388			10 Crash Occurred On: LANDING RD			11 Speed Limit 45			118a 26	
01	2 Police Dept of ROXBURY TWP			Road Name SOUTH			12 Route No. 0631			118b 04	
01	3 Station/Princt 01			At Intersection with <input checked="" type="checkbox"/> N <input type="checkbox"/> E			13 Milepost 04			118c 04	
05	4 Date of Crash 03/22/14			5 Day of Week SAT			14 <input type="checkbox"/> Feet <input type="checkbox"/> S <input type="checkbox"/> W			119a 09	
01	6 Time (use 2400 hrs) 1410			7 Municipality Code 1438			15 <input type="checkbox"/> Miles <input type="checkbox"/> 10			119b 02	
02	23 Veh No 01			24 Policy No. 65-PHH880106-250062			25 Ins Code 242			120 01	
01	26 Driver's First Name DIANE			29 Sex F			53 Veh No 02			121 01	
01	27 Number and Street 341 MATTISON RESERVOIR AVE			30 Eyes 4			54 Policy No. 028475835C71016			122 01	
01	28 City BRANCHVILLE			31 State NJ			55 Veh No 02			123 01	
02	32 Driver's License No B47191677589844			33 DOB 09/24/54			56 Driver's First Name ANGELINA			124 01	
01	34 Expires 12/16			35 DOB 10/01/88			57 Number and Street 19 INDIAN LN			125 01	
01	36 Owner's First Name RONALD			37 City BRANCHVILLE			58 City HACKETTSTOWN			126 01	
02	38 Number and Street 341 MATTISON RES. AVE			39 State NJ			59 Sex F			127 01	
01	39 Model ACC			40 Color BK			60 Driver's License No W36120437460862			128 01	
01	41 Year 2013			42 Plate No. YRR48H			61 State NJ			129 01	
01	43 State NJ			44 VIN 1HGCR2E60DA098448			62 Driver's License No W36120437460862			130 01	
01	45 Expires 02/15			46 VIN 1G8AL66F97Z172999			63 DOB 10/01/88			131 01	
01	47 Authority <input checked="" type="checkbox"/> Driver			48 Vehicle Removed To <input checked="" type="checkbox"/> Driven			64 Expires 05/16			132 01	
01	49 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused			50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other			65 Owner's First Name VINCENT			133 01	
01	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs			52 Carrier name			66 Driver's License No W36120437460862			134 01	
01	53 Commercial Vehicle Weight <input type="checkbox"/> 10,001 - 26,000 lbs			53 DOB 10/01/88			67 Authority <input checked="" type="checkbox"/> Driver			135 01	
01	54 Commercial Vehicle Weight <input type="checkbox"/> > 26,001 lbs			54 Expires 02/15			68 Impound <input type="checkbox"/> Impound			136 01	
03	135 Crash Description										137 06

Investigation revealed that vehicle #1 was yielding off the Route 80 ramp onto Landing Road when it was struck from behind by vehicle #2. Driver #2 stated she was also trying to yield and did not see vehicle #1 slow to a stop as she was also looking back to yield to traffic on Landing Road. Driver #2 was at fault in the accident.

Driver #1 stated she had a headache from her head bouncing against her headrest. She refused to seek medical attention but wanted same documented. Both vehicles received minor damage.

136 Damage To Other Property NONE STATED OR OBSERVED			
137 Charge 39:4-97	<input type="checkbox"/> Multiple Charges	138 Summons No. R061724	139 Charge <input type="checkbox"/> Multiple Charges
141 Officer's Signature BECK, WILLIAM		142 Badge No. 134	143 Reviewed By PALANCHI, MARC
		144 Case Status CLOSED	145 Summons No.

										Names & Addresses of Occupants - If Deceased, Date & Time of Death			
A	1	01	01	-	58	F	01	08	01	08	04	-	BITTIGER, DIANE N, 341 MATTISON RESERVOIR AVE, BRANCHVILLE, NJ 07826-4117
B	1	03	01	-	72	M	-	-	-	09	04	-	BITTIGER, RONALD F, 341 MATTISON RES. AVE, BRANCHVILLE, NJ 07826
C	2	01	01	-	27	F	-	-	-	09	04	-	WHITE, ANGELINA M, 19 INDIAN LN, HACKETTSTOWN, NJ
D	-	-	-	-	-	-	-	-	-	-	-	-	
E	-	-	-	-	-	-	-	-	-	-	-	-	

4 of 2

New Jersey Police Crash Investigation Report

Police Dept: Roxbury PD

Code: 1436

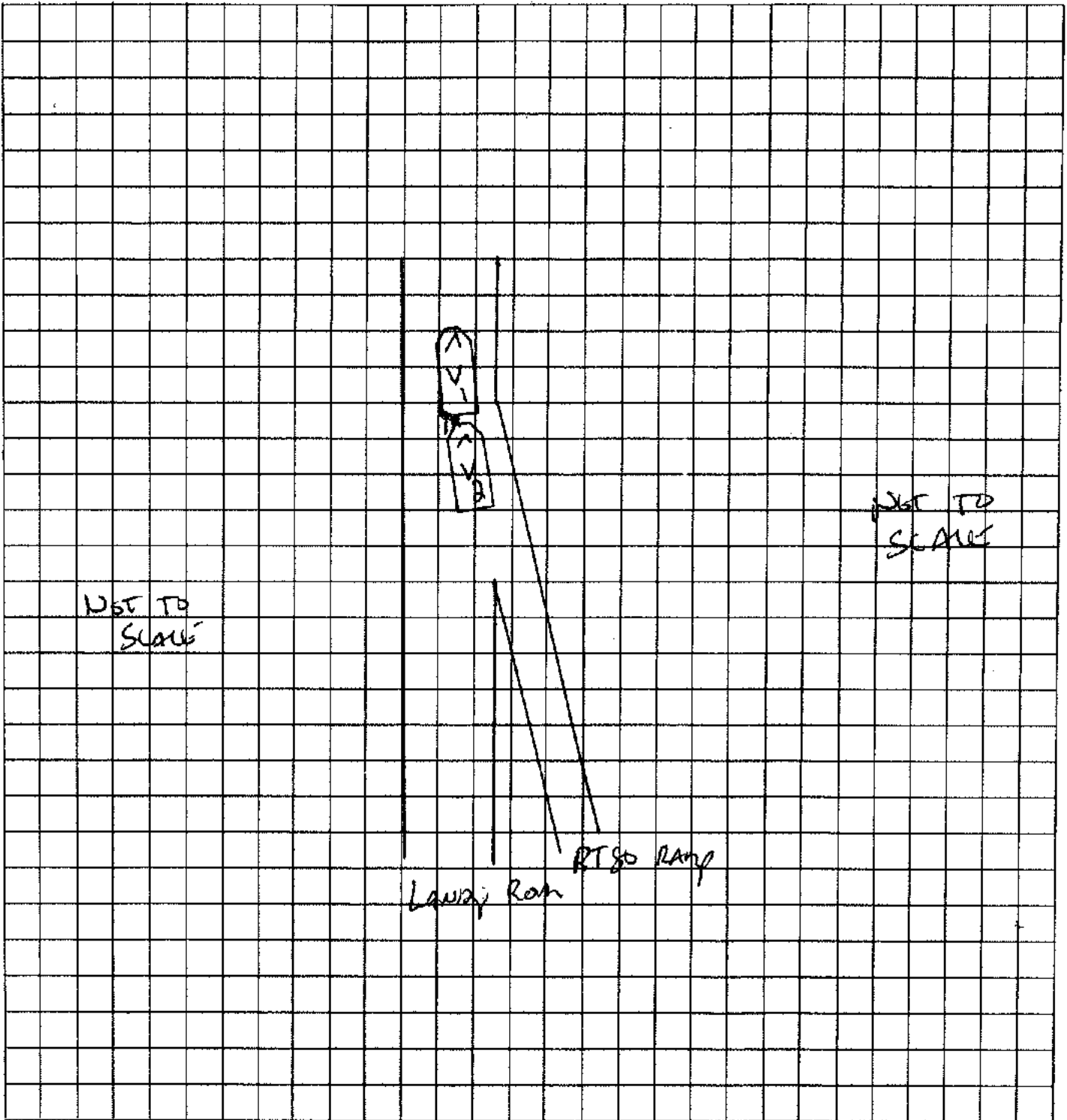
Motor Vehicle Crash Diagram

Station: _____

Case No: 2014-6388

134 Crash Diagram (NOT TO SCALE)

Indicate North



[Handwritten Signature]

Officer's Signature

134

Badge Number

New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

99 Page 1 of 1 <input type="checkbox"/> Fatal	10 Crash Occurred On: 1-80 EAST OFF RAMP										11 Speed Limit 36		12 Route No. 0080		13 Milepost C .04		118a 26
97 01 1 Case Number 2014-008442	2 Police Dept of ROXBURY TWP	Code 01	14 <input checked="" type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> S <input type="checkbox"/> W	15 <input type="checkbox"/> Feet <input type="checkbox"/> Miles	16 <input type="checkbox"/> 18	17 Cross Road Name LANDING RD	18 Speed Limit 36	19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB	20 <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB	21 Latitude	22 Longitude	118b 26					
98 01 3 Station/Precinct	4 Date of Crash 04/16/14	5 Day of Week WED	6 Time (use 2400 hrs) 0916	7 Municipality Code 1438	8 Total Killed 0	9 Total Injured 0	19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB	20 <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB	21 Latitude	22 Longitude	119a 02						
100 01	23 Veh No 01	24 Policy No. PANJ-006094433	25 Ins Code 071	53 Veh No 02	54 Policy No. Q13821189U71041	55 Ins Code 355	119b 02										
101 02	<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN	<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> PEDALCYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN	120 01														
102 01	26 Driver's First Name LAUREN	Initial L	Last Name DEBONIS	29 Sex F	56 Driver's First Name ADRIANA	Initial A	Last Name MORALES-GREEN	59 Sex F	121 01								
103 01	27 Number and Street 14 LOZIER ROAD	30 Eyes 2	57 Number and Street 60 BROCK LANE	60 Eyes 4	122 00												
104 2	28 City BUDD LAKE	State NJ	Zip 07828-1108	58 City HACKETTSTOWN	State NJ	Zip 07840-5677	123 00										
105 01	31 State NJ	32 Driver's License No D20834430052862	33 DOB 02/28/88	34 Expires 05/17	61 State NJ	62 Driver's License No M66540118268964	63 DOB 08/26/95	64 Expires 07/17	124 12								
106 --	35 Owner's First Name LAUREN	Initial L	Last Name DEBONIS	65 Owner's First Name LUIS	Initial L	Last Name MORALES	125 01										
107 --	36 Number and Street 14 LOZIER ROAD	66 Number and Street 60 BROCK LANE	126 09														
108 01	37 City BUDD LAKE	State NJ	Zip 07828-1108	67 City HACKETTSTOWN	State NJ	Zip 07840-5677	127 09										
109 01	38 Make NIS	39 Model ALT	40 Color BG	41 Year 1996	42 Plate No. ZPV35L	43 State NJ	68 Make FIA	69 Model 500	70 Color WT	71 Year 2012	72 Plate No. S19DHP	73 State NJ	128a 26				
110 01	44 VIN 1N4BU31D5TC152784	45 Expires 03/14	74 VIN 3C3CFFAR0CT124910	75 Expires 06/15	128b --												
111 --	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police	76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	77 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police	128c --												
112 --	48 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused	Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine	Results: 0. % <input type="checkbox"/> Pending	78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused	Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine	Results: 0. % <input type="checkbox"/> Pending	128d --										
113 --	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	Name or Placard No.	79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	Name or Placard No.	129a 26												
114 --	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	129b --														
115 --	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	129c --														
116 02	52 Carrier name	82 Carrier name	130 06														
117 02	135 Crash Description	132 12															
133 12	Driver of veh.1 advised she got off Rt 80 EB and was waiting to merge onto Landing road, when veh.2 struck her vehicle from behind. Veh.2 caused a small dent and paint scratched on the rear bumper. Driver 1 advised she was not injured and did not have any complaint of pain. Driver of veh.2 stated she got off RT 80 EB and slowed as she approached the merge to Landing rd. As she was approaching Landing rd, she observed Veh. 1 waiting to merge. Driver 2 said she looked over her shoulder at traffic to see if it was clear and when she looked forward to proceed, she struck veh.1 in the rear, because she thought veh.1 had merged already. Veh.2 had some paint scratched on the front bumper. Driver of veh.2 advised she was not injured and did not have any complaint of pain. Both vehicles were driven from the scene.	133 12															
136 Damage To Other Property NONE	137 Charge <input type="checkbox"/> Multiple Charges	138 Summons No. -	139 Charge <input type="checkbox"/> Multiple Charges	140 Summons No. -													
141 Officer's Signature FEENEY, BRIAN	142 Badge No. 169	143 Reviewed By MURRAY, DANIEL	144 Case Status CLOSED														

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	28	F	--	--	--	08	04	--	--	DEBONIS, LAUREN, 14 LOZIER ROAD, BUDD LAKE, NJ 07828-1108
B	2	01	01	--	18	F	--	--	--	08	04	--	--	MORALES-GREEN, ADRIANA S. 60 BROCK LANE, HACKETTSTOWN, NJ 07840-5677
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

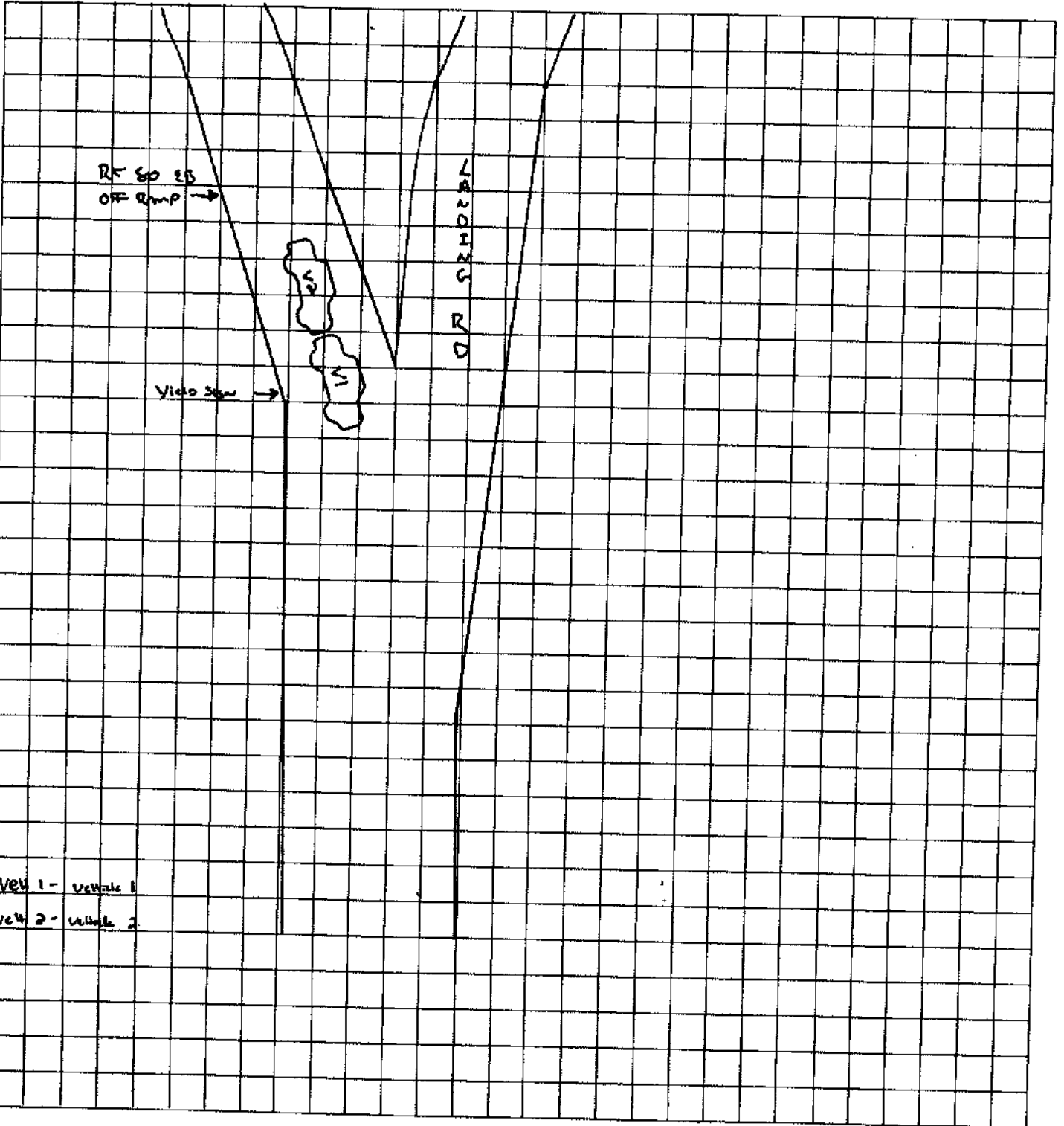
New Jersey Police Crash Investigation Report

Police Dept: Roxbury Twp Police Dept Code:

Motor Vehicle Crash Diagram

Station: Case No: 2014-00142

134 Crash Diagram (NOT TO SCALE)



veh 1 - vehicle 1
veh 2 - vehicle 2

[Handwritten Signature]

Officer's Signature

168

New Jersey Police Crash Investigation Report

Police Dept: ROXBURY TWP

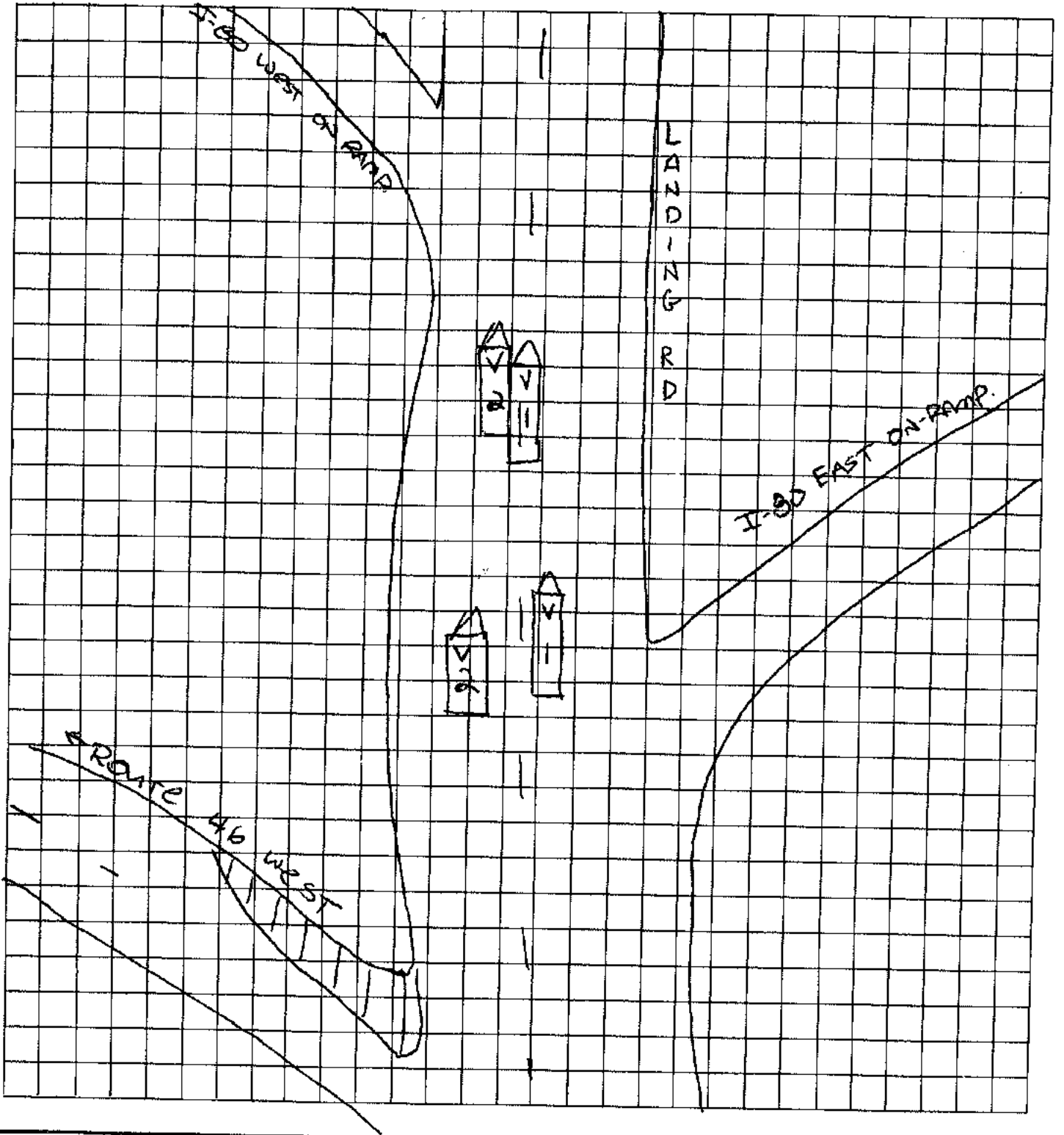
Code: 01

Motor Vehicle Crash Diagram

Station:

Case No: 14-010462

134 Crash Diagram (NOT TO SCALE)



PRE

207

07 1 Case Number **2014-001572** 10 Crash Occurred On: **LANDING RD** 11 Speed Limit **35** 12a **118a**
 08 2 Police Dept of **ROXBURY TWP** Code **01** 250 At Intersection with N E **NORT** **35** **0631** **A** 12b **26**
 09 3 Station/Precinct **260** Feet S W of: **SHIPPENPORT ROAD** 13 Milepost **35** 118b
 14 Miles 16 17 Cross Road Name 18 Speed Limit **35** 119a
 15 19 To: NB EB 119b
 20 From: 20 Route/Name SB WB 119c

100 4 Date of Crash **01/21/14** 5 Day of Week **TUE** 6 Time (use 2400 hrs) **1029** 7 Municipality Code **1436** 8 Total Killed **0** 9 Total Injured **0** 21 Latitude 22 Longitude 119d
 101 23 Veh No **01** 24 Policy No. **DT8100C90552AT1L13** 25 Ins Code **341** 53 Veh No **02** 54 Policy No. 55 Ins Code 120
 PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN PARKED PEDESTRIAN PEDALCYCLIST RESP TO EMERGENCY HIT & RUN 121

102 26 Driver's First Name Initial Last Name **MIGUEL A VARGAS** 29 Sex **M** 56 Driver's First Name Initial Last Name 59 Sex 121
 27 Number and Street **428 E 22ND ST APT1** 30 Eyes **2** 57 Number and Street 60 Eyes 122
 103 28 City **PATERSON** State **NJ** Zip **07614-2320** 58 City State Zip 123

104 31 State **NJ** 32 Driver's License No **V06815476109872** 33 DOB **09/17/87** 34 Expires **08/15** 61 State 62 Driver's License No 63 DOB 64 Expires 124
 35 Owner's First Name Initial Last Name **ADVANCED FIBER WORKS INC** 65 Owner's First Name Initial Last Name 125
 Same As Driver Same As Driver 126

106 36 Number and Street **700 ROUTE 46 WEST** 66 Number and Street 128
 37 City **CLIFTON** State **NJ** Zip **07013** 67 City State Zip 127
 107 38 Make **TOY** 39 Model **TAC** 40 Color **WT** 41 Year **2009** 42 Plate No. **XS878U** 43 State **NJ** 68 Make 69 Model 70 Color 71 Year 72 Plate No. 73 State 128a
 44 VIN **5TENX22N990654837** 45 Expires **08/14** 74 VIN 75 Expires 128b

109 46 Vehicle Removed To Driven Left at Scene Towed 47 Authority Owner Driver 76 Vehicle Removed To Driven Left at Scene Towed 77 Authority Owner Driver 128c
 Impound Disabled Impound Disabled Police Police 128d

110 48 Alcohol/Drug Test Given: No Yes Refused 78 Alcohol/Drug Test Given: No Yes Refused 128d
 111 Type: Breath Blood Urine 79 Type: Breath Blood Urine 128e
 Results: 0. % Pending Results: 0. % Pending 128f

112 49 Hazardous Material On Board Spill Name or Placard No. 79 Hazardous Material On Board Spill Name or Placard No. 129a
 113 50 Carrier No. USDOT Other 80 Carrier No. USDOT Other 129b
 114 51 Commercial Vehicle Weight < 10,000 lbs 81 Commercial Vehicle Weight < 10,000 lbs 129c
 10,001 - 26,000 lbs 10,001 - 26,000 lbs 129d
 > 26,001 lbs > 26,001 lbs 130

115 52 Carrier name 82 Carrier name 131
 116 135 Crash Description: **Driver #1 stated he was traveling Landing Road north bound when a oncoming vehicle made a left turn in front of him. Vehicle #2 left the area and Driver #1 was unable to get any information on vehicle #2. Driver #1 was not familiar with the area so was not sure of the exact location. I checked all the private driveways in the area and I was unable to locate any vehicles with damage.** 132
 117 **No Diagram** 133

138 Damage To Other Property **NONE STATED OR OBSERVED**

137 Charge Multiple Charges 138 Summons No. Oper. 139 Charge Multiple Charges 140 Summons No.

141 Officer's Signature **BECK, WILLIAM** 142 Badge No. **134** 143 Reviewed By **PALANCHI, MARC** 144 Case Status **CLOSED**
 138

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death
A	1	01	01	--	26	M	--	--	--	09	04	--	--	VARGAS, MIGUEL A, 428 E 22ND ST APT1, PATERSON, NJ: 07614-2320
B	--	--	--	--	--	--	--	--	--	--	--	--	--	
C	--	--	--	--	--	--	--	--	--	--	--	--	--	
D	--	--	--	--	--	--	--	--	--	--	--	--	--	
E	--	--	--	--	--	--	--	--	--	--	--	--	--	

New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

05	Page 1 of 2 <input type="checkbox"/> Fatal		New Jersey Police Crash Investigation Report										<input checked="" type="checkbox"/> Reportable <input type="checkbox"/> Non-Reportable <input type="checkbox"/> Change Report												
01	1 Case Number 2014-008833		10 Crash Occurred On: LANDING RD										11 Speed Limit N 35 0631 A 0.60			118a 04									
01	2 Police Dept of ROXBURY TWP		Code 01		Road Name SHIPPENPORT RD		Dir 12		Route No. 35		Suffix 18		Milepost 35		118b -										
01	3 Station/Precinct -		260 14		<input type="checkbox"/> At Intersection with <input checked="" type="checkbox"/> Feet		<input checked="" type="checkbox"/> N <input type="checkbox"/> E <input type="checkbox"/> S <input type="checkbox"/> W		15		17 Cross Road Name		19 <input type="checkbox"/> To: <input type="checkbox"/> NB <input type="checkbox"/> EB <input type="checkbox"/> From: <input type="checkbox"/> SB <input type="checkbox"/> WB		119a 26										
04	4 Date of Crash mm dd yy 04/20/14		5 Day of Week SUN		6 Time (use 2400 hrs) 1810		7 Municipality Code 1438		8 Total Killed -		9 Total Injured -		21 Latitude		22 Longitude	119b -									
02	23 Veh No 01		24 Policy No. AOU-238-660925-75 3 2				25 Ins Code 090		53 Veh No 02		54 Policy No. 2935806				55 Ins Code 23841		120 01								
01	26 Driver's First Name MARY				Initial T				Last Name CINEWICZ				29 Sex F		56 Driver's First Name EUCABETH				Initial K		Last Name MBOGA		59 Sex F		121 01
01	27 Number and Street 36 SWEDE MINE ROAD				30 Eyes 6				57 Number and Street 274 STEVENS AVENUE FL2				60 Eyes 1				122 -								
01	28 City ROCKAWAY BORO				State NJ				Zip 07866-3750				58 City JERSEY CITY				State NJ				Zip 07305-1314		123 -		
02	31 State NJ		32 Driver's License No C44985278360508				33 DOB mm dd yy 10/21/60		34 Expires mm yy 07/16		61 State NJ		62 Driver's License No M10572437261831				63 DOB mm dd yy 01/10/83		64 Expires mm yy 09/16		124 03				
07	35 Owner's First Name <input checked="" type="checkbox"/> Same As MARY				Initial T				Last Name CINEWICZ				65 Owner's First Name <input type="checkbox"/> Same As REM INC				Initial RE				Last Name INC		125 01		
01	36 Number and Street 36 SWEDE MINE ROAD				66 Number and Street 80 COTTONTAIL LANE				126 11																
01	37 City ROCKAWAY BORO				State NJ				Zip 07866-3750				67 City SOMERSET				State NJ				Zip 08873		127 04		
01	38 Make JEE		39 Model LRB		40 Color RD		41 Year 2007		42 Plate No. ZYU36U		43 State NJ		68 Make FOR		69 Model ECO		70 Color WT		71 Year 2010		72 Plate No. W78BK		73 State NJ		128a 26
01	44 VIN 1J8GL68K17W576457				45 Expires 06/14		74 VIN 1FTNE1EWXADA17361				75 Expires 11/14		128b -												
01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed				47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed				77 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		128c -												
01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending				78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending				128d -																
01	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.				79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/> Name or Placard No.				129a 26																
01	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other				129b -																
01	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs				129c -																
01	52 Carrier name				82 Carrier name				129d -																
01	135 Crash Description <p>Driver veh #1 stated she was making a left out of the driveway of the "Ice Factory" on Landing Rd.. As she made her left she thought she had enough room to make her turn ahead of traffic traveling north on Landing Rd.. As she made her turn the driver side front corner of her vehicle collided with the passenger side front corner of veh #2. Driver veh #2 stated she was traveling north on Landing Rd when veh #1 attempted to make a left onto Landing Rd south, directly in front of her. Unable to avoid, the passenger side front corner of her vehicle collided with the driver side front corner of veh #1. No injuries were reported or observed. As a precaution, the handicapped passengers of veh #2 were transported to the hospital for a check up. Driver veh #1 at fault for the crash.</p>																								132 01

136 Damage To Other Property		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.	
141 Officer's Signature MURRAY, DANIEL				142 Badge No. 159		143 Reviewed By PALANCHI, MARC		144 Case Status CLOSED	

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death	
A	1	01	01	-	63	F	-	-	-	09	04	-	-	CINEWICZ, MARY T, 36 SWEDE MINE ROAD, ROCKAWAY BORO, NJ 07866-3750	
B	1	03	01	-	80	F	-	-	-	09	04	-	-	CINEWICZ, SABINA, 2 HERON DR, APT 218, HACKETTSTOWN, NJ	
C	1	06	01	-	67	F	-	-	-	09	04	-	-	BAIN, ANNE, 172 ALEXANDER AVENUE, NUTLEY, NJ	
D	2	01	01	-	31	F	-	-	-	09	04	-	-	MBOGA, EUCABETH K, 274 STEVENS AVENUE FL2, JERSEY CITY, NJ 07305-1314	
E	2	05	01	00	46	F	-	-	02	04	04	-	8403	POWERS, MARIE, 230 MT ARLINGTON BLVD, LANDING, NJ 07850	

<p>New Jersey Police Crash Investigation Report</p> <p>Motor Vehicle Crash Description</p>	<p>Police Dept: ROXBURY TWP Code: 01</p> <p>Station: -- Case No: 2014-008833</p>
--	---

(Refer to vehicle by number)

ALL INVOLVED	Veh Occ	Pos Inj/Ch	Eject	Phys Cond	Age	Sex	Loc Inj	Type Inj	Ref Med	Equip Avail	Equip Used	Bar Dep	Hosp Code	Names & Addresses of Occupants - If Deceased, Date & Time of Death
	83	84	85	86	87	88	89	90	91	92	93	94	95	
F	2	03	01	--	53	F	--	--	--	09	04	--		ADEBATO, SADE, 11 MILDRED AVE, STANHOPE, NJ
G	2	08	01	00	62	F	--	--	02	04	04	--	6403	NIENJADLY, LAURIE, 230 MT ARLINGTON BLVD, MT ARLINGTON, NJ
H	2	10	01	00	49	F	--	--	02	04	04	--	6403	DOLSON, LISA, 230 MT ARLINGTON BLVD, MT ARLINGTON, NJ
I	--	--	--	--	--	--	--	--	--	--	--	--	--	
J	--	--	--	--	--	--	--	--	--	--	--	--	--	

135 Crash Description

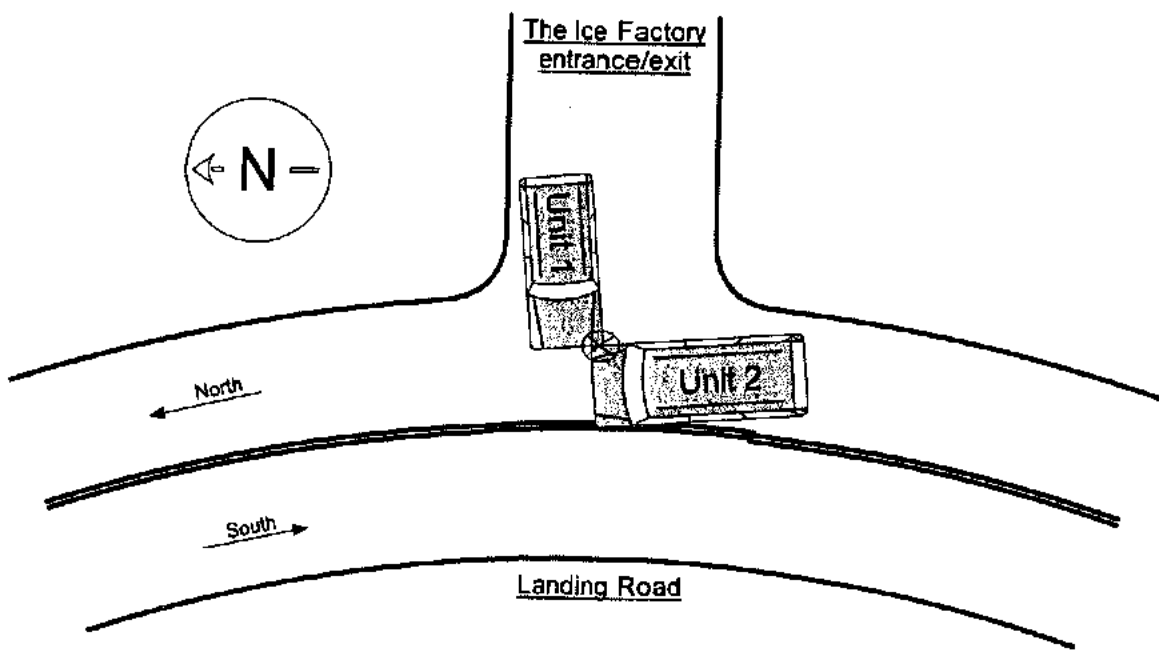


Officer's Signature

159

Badge Number

Case No.: 2014-008833
Agency: Roxbury Twp.
Officer: Sgt. D. Murray #159



NOT TO SCALE

New Jersey Police Crash Investigation Report

Page 1 of 1 Fatal

Reportable Non-Reportable Change Report

1 Case Number 2014-008411	10 Crash Occurred On: LANDING RD	11 Speed Limit 35	12 Route No. 0631	13 Milepost A 1.0	118a 26
2 Police Dept of ROXBURY TWP	Code 01	Road Name NORTH	Dir 35	16 Speed Limit 35	118b --
3 Station/Precinct	360 14	At Intersection with <input checked="" type="checkbox"/> Feet <input type="checkbox"/> Miles	N of SHIPPENPORT ROAD	17 Cross Road Name	118c --
4 Date of Crash mm dd yy 04/15/14	5 Day of Week TUE	6 Time (use 2400 hrs) 1757	7 Municipality Code 1436	8 Total Killed	118e 02
9 Total Injured	19 Ramp	20 Route/Name	21 Latitude	22 Longitude	118f --
23 Veh No 01	24 Policy No. 9912233721051	25 Ins Code 129	53 Veh No 02	54 Policy No. ACPA001116638	119e 01
26 Driver's First Name ZACHERY		27 Number and Street 21 BYRAM BAY ROAD	28 City HOPATCONG	29 Sex M	120 01
30 Eyes 6		31 State NJ	32 Driver's License No. R9467918303948	33 DOB mm dd yy 03/03/84	121 --
34 Expires mm yy 03/17		35 Owner's First Name ZACHERY	36 Number and Street 21 BYRAM BAY ROAD	37 City HOPATCONG	122 --
38 State NJ		39 Model 240	40 Color RED	41 Year 1995	123 --
42 Plate No. V66CUE		43 State NJ	44 VIN JN1AS44D1S015953	45 Expires mm yy 03/16	124 08
46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed		47 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	125 07
50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	52 Carrier name	53 Crash Description	126 09
54 State NJ		55 Owner's First Name DONNA	56 Driver's First Name DONNA	57 Number and Street 114 DARTMOUTH TRL	127 11
58 City HOPATCONG		59 Model RAV	60 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	61 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	128a 26
62 Driver's License No. R21631747462682		63 DOB mm dd yy 12/18/68	64 Expires mm yy 03/17	65 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending	128b --
66 Number and Street 114 DARTMOUTH TRL		67 City HOPATCONG	68 Make TOY	69 Model RAV	128c --
69 State NJ		70 Color GY	71 Year 2013	72 Plate No. Z82DTK	128d --
73 State NJ		74 VIN 2T3BFREV40W122930	75 Expires mm yy 12/16	76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	129a 26
77 Authority <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police		78 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>			129b --
79 State NJ		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other			129c --
81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs		82 Carrier name			129d --
83 State NJ		84 Case Status CLOSED			130 06

136 Damage To Other Property
NONE

137 Charge Multiple Charges 138 Summons No. 139 Charge Multiple Charges 140 Summons No.

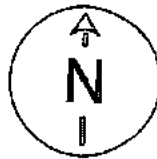
141 Officer's Signature
EDMUNDS, JONATHAN

142 Badge No. **166** 143 Reviewed By **PALANCHI, MARC** 144 Case Status **CLOSED**

145 Names & Addresses of Occupants - If Deceased, Date & Time of Death

A	1	01	01	--	30	M	--	--	--	09	04	--	--	RUTLEDGE, ZACHERY T, 21 BYRAM BAY ROAD, HOPATCONG, NJ 07843-1621
B	2	01	01	--	55	F	--	--	--	09	04	--	--	REED, DONNA M, 114 DARTMOUTH TRL, HOPATCONG, NJ 07843-1153
C	--	--	--	--	--	--	--	--	--	--	--	--	--	--
D	--	--	--	--	--	--	--	--	--	--	--	--	--	--
E	--	--	--	--	--	--	--	--	--	--	--	--	--	--

LANDING RD

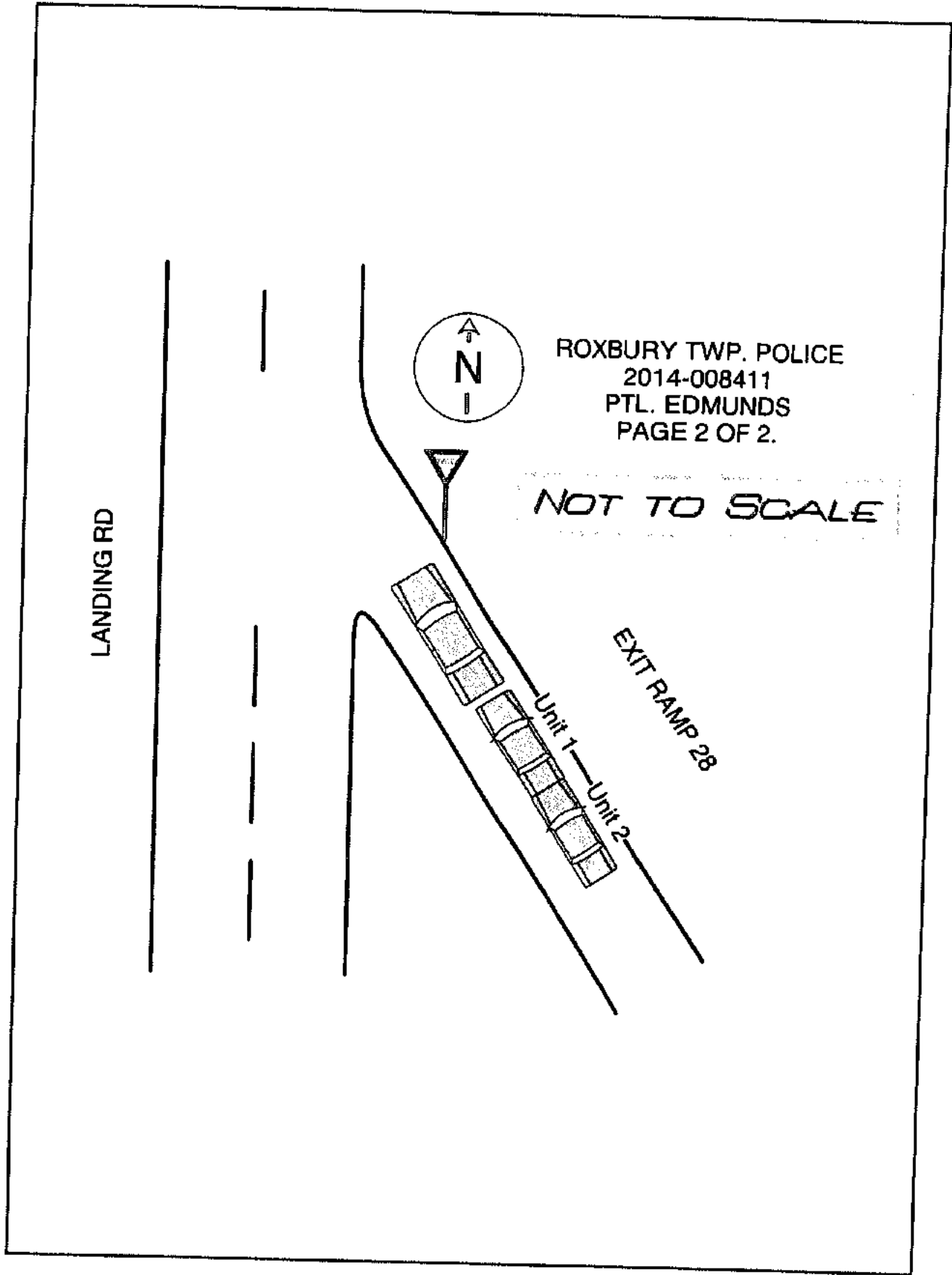


ROXBURY TWP. POLICE
2014-008411
PTL. EDMUNDS
PAGE 2 OF 2.

NOT TO SCALE

EXIT RAMP 28

Unit 1
Unit 2



New Jersey Police Crash Investigation Report

Reportable Non-Reportable Change Report

05	Page 1 of 2 <input type="checkbox"/> Fatal		New Jersey Police Crash Investigation Report										<input checked="" type="checkbox"/> Reportable <input type="checkbox"/> Non-Reportable <input type="checkbox"/> Change Report																			
01	1 Case Number 2014-012589		10 Crash Occurred On: LANDING ROAD				11 Speed Limit NORTH 35 50				118a 25																					
01	2 Police Dept of ROXBURY TWP		Code 01		Road Name SHIPPENPORT ROAD				Dir NORTH		12 Route No. Suffix 50		13 Milepost 36		118b --																	
01	3 Station/Precinct 1436		14		<input checked="" type="checkbox"/> At Intersection with		<input type="checkbox"/> N <input type="checkbox"/> E		<input type="checkbox"/> S <input type="checkbox"/> W		17 Cross Road Name		19		119a 10																	
07	4 Date of Crash 06/06/14		5 Day of Week FRI		6 Time 1302		7 Municipality 1436		8 Total Killed 0		9 Total Injured 0		20 Route/Name		119b 02																	
01	23 Veh No 01		24 Policy No. 809611640 1/18		25 Ins Code 012		53 Veh No 02		54 Policy No. MWTB300487		55 Ins Code 263		21 Latitude		22 Longitude		120 01															
01	<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> BICYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN		<input type="checkbox"/> PARKED <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> BICYCLIST <input type="checkbox"/> RESP TO EMERGENCY <input type="checkbox"/> HIT & RUN		26 Driver's First Name SHARON		Initial A		Last Name REGETZ		28 Sex F		56 Driver's First Name MAYNARD		Initial M		Last Name CONGDON		58 Sex M		121 01											
01	27 Number and Street 4 OAKWOOD VILLAGE APT 4		30 Eyes 4		57 Number and Street 11738 KELLY HILL ROAD		60 Eyes 60		28 City FLANDERS		State NJ		Zip 07836-8013		58 City PINE CITY		State NY		Zip 14871		122 --											
01	31 State NJ		32 Driver's License No. R22197036156714		33 DOB 06/13/71		34 Expires 06/16		61 State NY		62 Driver's License No. 767274041		63 DOB 11/20/52		64 Expires 11/17		123 --		124 08		125 13											
08	35 Owner's First Name <input type="checkbox"/> Same As SHARON		Initial A		Last Name REGETZ		65 Owner's First Name <input type="checkbox"/> Same As		Initial TRANSPORT LIMITED		Last Name TRANSPORT LIMITED		66 Number and Street 76 OAKWOOD VILLAGE APT 4		68 Number and Street 385 BOSTWICK ROAD		67 City FLANDERS		State NJ		Zip 07836-8075		67 City PHELPS		State NY		Zip 14532		126 --			
02	36 Make JEE		39 Model COM		40 Color WT		41 Year 2011		42 Plate No. D58EFZ		43 State NJ		68 Make VOLV		69 Model TRUCK		70 Color 2014		71 Year 2014		72 Plate No. 36303PC		73 State NY		127 --		128a 26					
04	44 VIN 1JANF1FB3BD169163		45 Expires 06/16		74 VIN 4VANC9EH2EN148666		75 Expires 03/15		46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		76 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed <input type="checkbox"/> Impound <input type="checkbox"/> Disabled		77 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police		128b --		128c --		128d --		129a 26		129b --		129c --		129d --			
01	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		78 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused		Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine		Results: 0. % <input type="checkbox"/> Pending		79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>		Name or Placard No.		80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other		81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input checked="" type="checkbox"/> > 26,000 lbs		82 Carrier name		130 12		131 12			
01	50 Carrier No. <input checked="" type="checkbox"/> USDOT <input type="checkbox"/> Other		286626		51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input checked="" type="checkbox"/> > 26,000 lbs		52 Carrier name		135 Crash Description		136 Damage To Other Property		137 Charge <input type="checkbox"/> Multiple Charges		138 Summons No.		139 Charge <input type="checkbox"/> Multiple Charges		140 Summons No.		141 Officer's Signature MARCIANO, ANTHONY P <i>PT. Anthony Marciano</i>		142 Badge No. 217		143 Reviewed By PALANCHI, MARC <i>Marc Palanchi</i>		144 Case Status CLOSED		132 06		133 06	

Vehicle 2 was travelling North on Landing Road from Route 80 West. The driver passed the U-Turn exit for Shippenport Road. At that time, he began illegally backing up in order to get back into position to get off on Shippenport Road. Vehicle 1 stopped behind vehicle 2 when the driver observed vehicle 2 backing up. The driver of vehicle 2 did not see vehicle 1 stopped behind him. Vehicle 2 struck vehicle 1 in the front bumper causing damage to the front end. Vehicle 2 did not have any damage.

The driver of vehicle 2 did not realize he hit vehicle 1 and got off on Shippenport Road. The driver then proceeded to make the U-Turn and began traveling East on Route 46. A witness to the accident pulled up next to Vehicle 2 and told him to pull over. Vehicle 2 then made the U-Turn at Route 46 and Drake Road. Vehicle 2 then pulled over on Route 46

	83	84	85	86	87	88	89	90	91	92	93	94	95	Names & Addresses of Occupants - If Deceased, Date & Time of Death	
A	1	01	01	--	42	F	--	--	--	09	04	--	--	REGETZ, SHARON A, 4 OAKWOOD VILLAGE APT 4, FLANDERS, NJ 07836-8013	
B	2	01	01	--	61	M	--	--	--	09	04	--	--	CONGDON, MAYNARD, 11738 KELLY HILL ROAD, PINE CITY, NY 14871	
C	--	--	--	--	--	--	--	--	--	--	--	--	--		
D	--	--	--	--	--	--	--	--	--	--	--	--	--		
E	--	--	--	--	--	--	--	--	--	--	--	--	--		

New Jersey Police Crash Investigation Report

Police Dept: **ROXBURY TWP**

Code: **01**

Motor Vehicle Crash Description

Station: **1436**

Case No: **2014-012589**

(Refer to vehicle by number)

ALL INVOLVED	Veh Occ	Pos In/On	Eject	Phys Cond	Age	Sex	Loc Inj	Type Inj	Ref Med	Equip Avail	Equip Used	Bsq Depl	Hosp Code	Names & Addresses of Occupants - If Deceased, Date & Time of Death
	83	84	85	86	87	88	88	90	91	92	93	94	95	
F	--	--	--	--	--	--	--	--	--	--	--	--	--	
G	--	--	--	--	--	--	--	--	--	--	--	--	--	
H	--	--	--	--	--	--	--	--	--	--	--	--	--	
I	--	--	--	--	--	--	--	--	--	--	--	--	--	
J	--	--	--	--	--	--	--	--	--	--	--	--	--	

135 Crash Description

west in the area of Town Toyota, and contacted the Roxbury Police Department.

Upon speaking to the driver he advised he did not know he struck vehicle 1 while backing up. The witness to the accident was able to record the entire event due to traveling with a video recording system in his vehicle. (See flash drive in Roxbury Evidence Room)

Driver 2 is a fault due to illegal backing and driver inattention.

PTL. Anthony Mancini

Officer's Signature

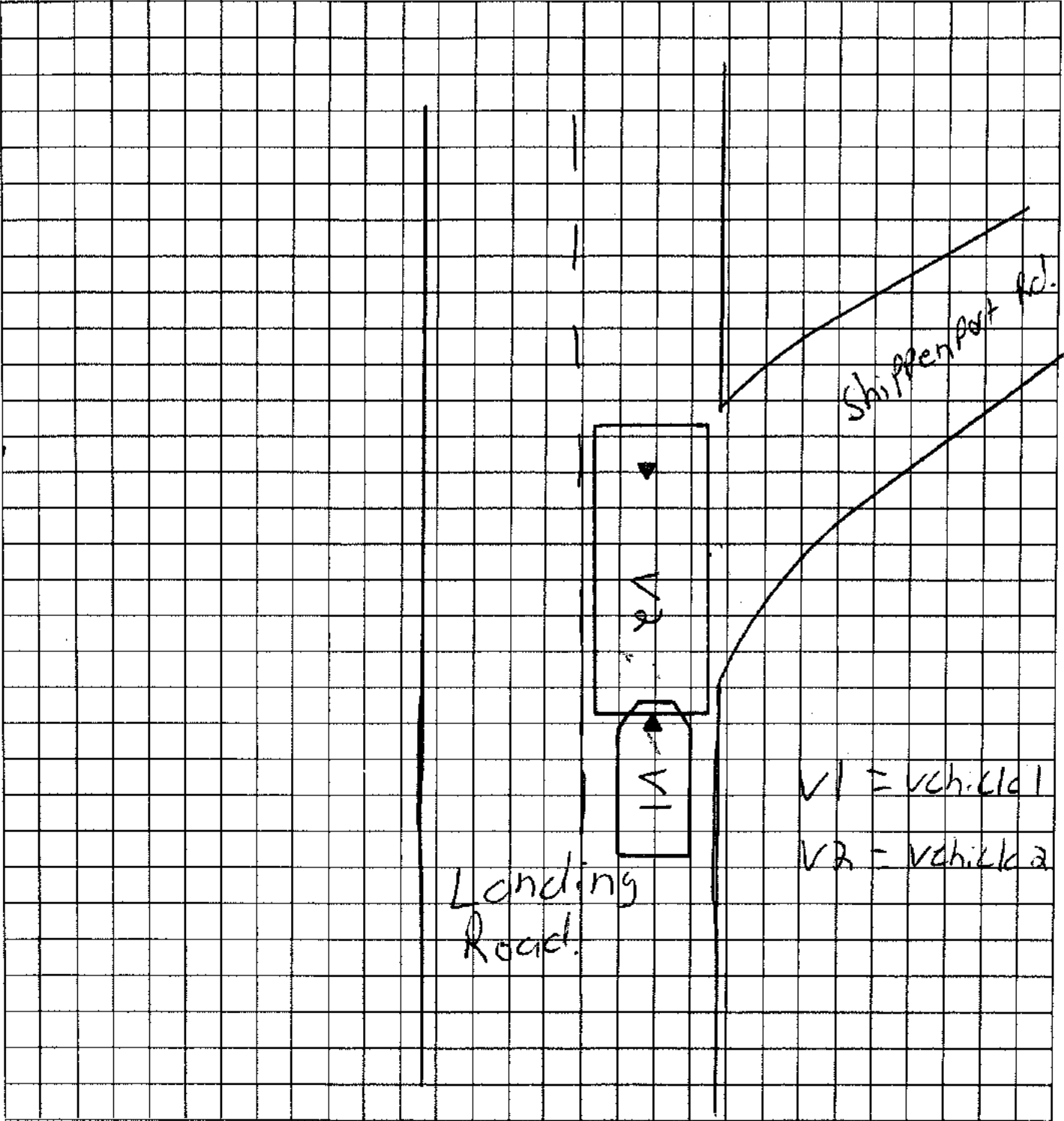
217

Badge Number

134 Crash Diagram (NOT TO SCALE)



Indicate North



PTI. Anthony Amore

Officer's Signature

217

Badge Number

96	01	Page 1 of 1 <input type="checkbox"/> Fatal	New Jersey Police Crash Investigation Report	<input type="checkbox"/> Reportable	<input checked="" type="checkbox"/> Non-Reportable	<input type="checkbox"/> Change Report	
97	01	1 Case Number 2014-010468	10 Crash Occurred On: LANDING RD	11 Speed Limit 60		119a 25	
98	01	2 Police Dept of Code ROXBURY TWP 01	Road Name 600	N 60 0631 A .08		119b --	
99	06	3 Station/Precinct 14	<input type="checkbox"/> At Intersection with <input type="checkbox"/> N <input type="checkbox"/> E <input checked="" type="checkbox"/> Feet <input checked="" type="checkbox"/> S <input type="checkbox"/> W <input type="checkbox"/> Miles <input type="checkbox"/> 16	of: SHIPPENPORT RD		119c 02	
100	04	4 Date of Crash mm dd yy 06/10/14	5 Day of Week SAT	6 Time (use 2400 hrs) 1823	7 Municipality Code 1436	8 Total Killed --	
101	02	23 Veh No 01	24 Policy No. F016277-6	25 Ins Code 426	53 Veh No	54 Policy No.	
102	01	26 Driver's First Name MICHAEL	Initial L	Last Name KELLY	29 Sex M	56 Driver's First Name	
103	01	27 Number and Street 402 JUST CT	30 Eyes 2	57 Number and Street	58 City	59 Sex	
104	01	28 City CEDAR KNOLLS	State NJ	Zip 07927	61 State	62 Driver's License No	
105	11	31 State NJ	32 Driver's License No K24076447301962	33 DOB mm dd yy 01/28/95	34 Expires mm yy 08/16	63 DOB mm dd yy	
106	--	35 Owner's First Name <input type="checkbox"/> Same As CHRISTINA	Initial S	Last Name KELLY	65 Owner's First Name	66 Driver's License No	
107	--	36 Number and Street 402 JUSTIN COURT BLDG 4	37 City CEDAR KNOLLS	State NJ	Zip 07927-0437	67 City	
108	01	38 Make HOND	39 Model CRV	40 Color BLUE	41 Year 1997	42 Plate No. L67DRK	
109	04	43 State NJ	44 VIN JHLRD1841VC038144	45 Expires 10/14	74 VIN	75 Expires	
110	01	46 Vehicle Removed To <input checked="" type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	47 Authority <input type="checkbox"/> Owner <input checked="" type="checkbox"/> Driver <input type="checkbox"/> Police	76 Vehicle Removed To <input type="checkbox"/> Driven <input type="checkbox"/> Left at Scene <input type="checkbox"/> Towed	77 Authority <input type="checkbox"/> Owner <input type="checkbox"/> Driver <input type="checkbox"/> Police	128b --	
111	00	48 Alcohol/Drug Test Given: <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending	78 Alcohol/Drug Test Given: <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> Refused Type: <input type="checkbox"/> Breath <input type="checkbox"/> Blood <input type="checkbox"/> Urine Results: 0. % <input type="checkbox"/> Pending		128c --	128d --	
112	--	49 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	Name or Placard No.	79 Hazardous Material On Board <input type="checkbox"/> Spill <input type="checkbox"/>	Name or Placard No.	128e 99	
113	--	50 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	80 Carrier No. <input type="checkbox"/> USDOT <input type="checkbox"/> Other	128f --	128g --	128h --	
114	--	51 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	81 Commercial Vehicle Weight <input type="checkbox"/> < 10,000 lbs <input type="checkbox"/> 10,001 - 26,000 lbs <input type="checkbox"/> > 26,001 lbs	128i --	128j --	128k 11	
115	01	52 Carrier name	82 Carrier name	128l 11	128m 11	128n 11	
116	01	135 Crash Description Driver #1 said he was travelling north on Landing Road in the left lane. D1 said that a black jeep wrangler was travelling north in the right lane slightly ahead of his vehicle. D1 said that the wrangler changed lanes almost striking his vehicle. D1 said he was forced into the guardrail which damaged his tire. Witnesses: Jim & Renee Miners 40 Reservoir Ave Dover, NJ 07801 said that a black jeep wrangler was in the right lane heading north. They said the jeep almost struck V1 forcing it into the guardrail. No registration was obtained for the jeep. No injuries reported. Cause of crash is unsafe lane change for the operator of the jeep wrangler.					132 00
117	01	136 Damage To Other Property					133 00
Oper. 1		137 Charge <input type="checkbox"/> Multiple Charges	138 Summons No.	Oper. --		139 Charge <input type="checkbox"/> Multiple Charges	
141 Officer's Signature HENRIKSEN, ERICK		142 Badge No. 188	143 Reviewed By PALANCHI, MARC		Badge No. 138	144 Case Status OPEN	
A	83	84	85	86	87	88	
B	1	01	01	--	19	M	
C	--	--	--	--	--	--	
D	--	--	--	--	--	--	
E	--	--	--	--	--	--	
Names & Addresses of Occupants - If Deceased, Date & Time of Death							
KELLY, MICHAEL L, 402 JUST CT, BLDG 4, CEDAR KNOLLS, NJ 07927							

APPENDIX – F

CORRESPONDENCE WITH INTERESTED PARTIES

Township of Roxbury

1715 Route 46
Ledgewood, New Jersey 07852

www.roxburynj.us

fax 973-448-2111

Historic Advisory Committee

INFORMATION 973-448-2000
CONSTRUCTION/BLDG 973-448-2009
COURT 973-448-2034
ENGINEER 973-448-2018
FIRE OFFICIAL 973-448-2012
HEALTH 973-448-2028
MANAGER 973-448-2002
MAYOR & COUNCIL 973-448-2001
POLICE 973-448-2100
PLANNING/ZONING 973-448-2008

PUBLIC WORKS 973-448-2089
RECREATION 973-448-2015
TAX ASSESSOR 973-448-2021
TAX COLLECTOR 973-448-2022
TECHNOLOGY 973-448-2099
TOWNSHIP CLERK 973-448-2001
TREASURER 973-448-2006
WASTEWATER TREATMENT PLANT 973-448-5360
WATER PLANT 973-398-2818

June 26, 2006

S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th floor
New York, NY 10005

Dear Sajid:

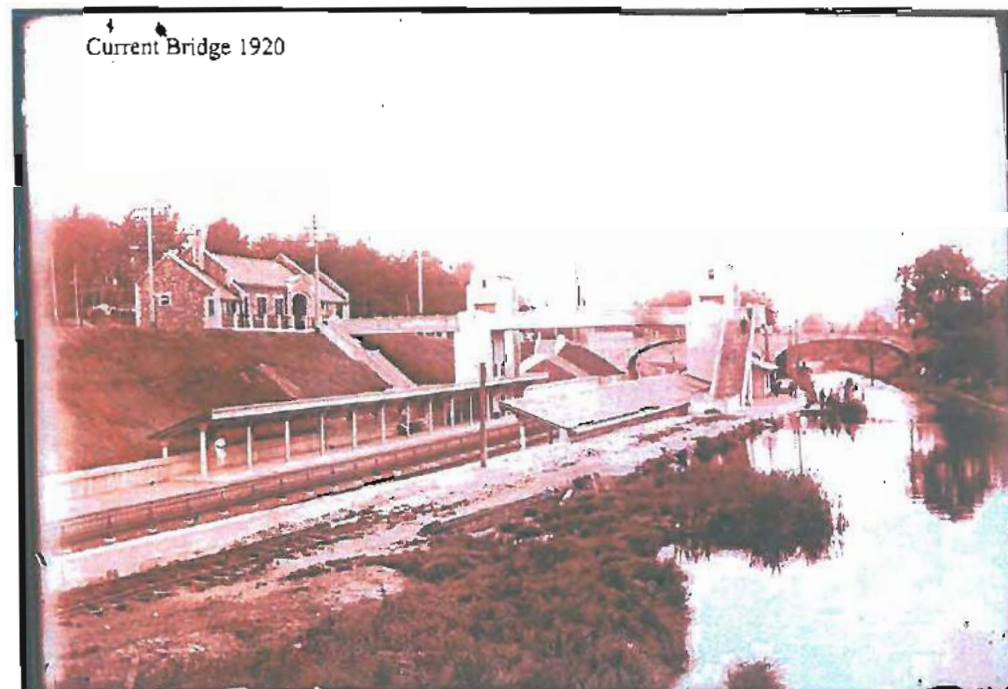
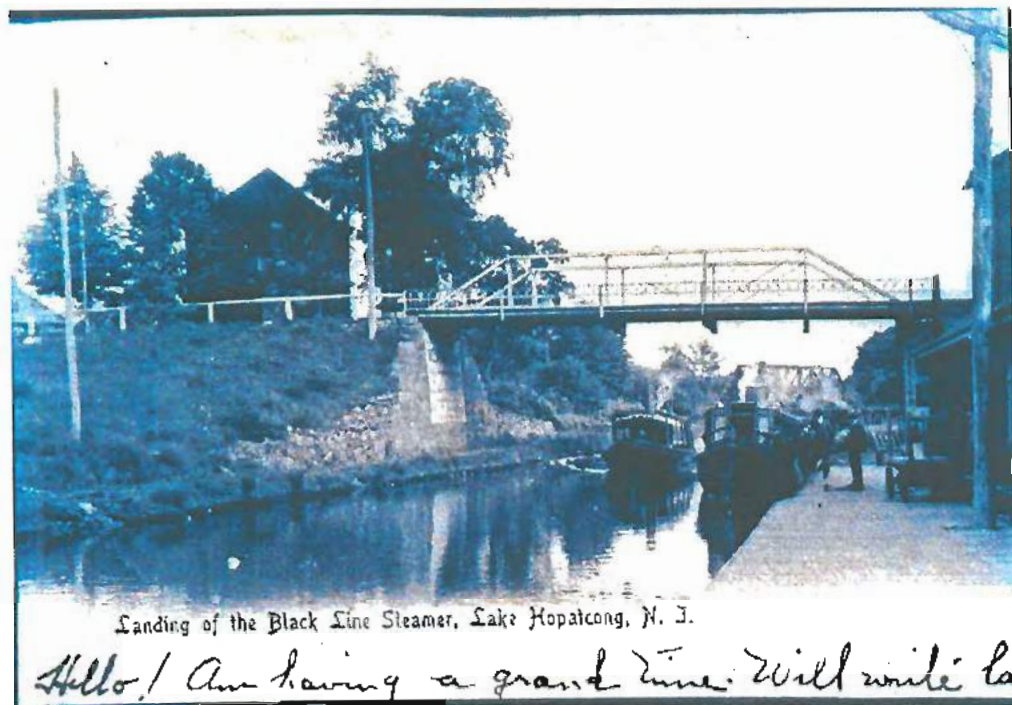
We are pleased to provide information on the historic preservation issues that replacement of this bridge (CR 631) 1400-073 over the Morris Canal and NJ Transit Morristown Line might impact. Enclosed you will find a sheet of photos of the old bridge and the current bridge as seen in different time frames. The photos are a visual reference to the historic nature of the location of the two bridges and what may be found during the replacement process, especially the new supports.

It is important to note that the arrangement of the Morris Canal adjacent to the railroad at the same level and separated by a platform for easy transfer is unique. The early 1900 (upper left) view shows the steel truss section of the bridge over the Morris Canal and the lower left photo shows the railroad platform with the other section of the bridge. Also note that there was a tower from the platform to the bridge so that passengers could obtain livery or steamship service to their hotels on Lake Hopatcong. Passengers could also obtain steamship service to their hotel by crossing the platform and taking the boat up the feeder canal to Lake Hopatcong, as indicated in the upper left photo. The upper right photo shows the current bridge with the old bridge behind still in tact. The lower right photo shows the Morris Canal and the railroad side by side with the Landing Railroad Station to the left as well as the concrete towers that provided access to the east and westbound rails. Currently the land under the arch that the Morris Canal went through is filled in and paved over to provide Park and Ride parking for the railroad.

This review is to orient you and those who will be doing the 106 review where the Morris Canal was and what may be found relating to the historic resources of the canal and railroad when construction begins. If you have any questions please feel free to call me at 973-584-7903 so that I may be of service.

Sincerely,


Richard D. Cramond, Chairman



CANAL SOCIETY OF NEW JERSEY

P.O. Box 737, Morristown, NJ 07963-0737

Answering Machine: (908) 722-9556

July 18, 2006

S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th Floor
New York, NY 10005

Dear Mr. Iqbal:

Thank you for the opportunity to provide the Canal Society of New Jersey's input regarding the proposed replacement of Landing Road (CR631) Bridge over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. We understand that this project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. We also understand from your letter dated June 14, 2006 that your firm has prepared a cultural resources survey for the project area. At this time we have not seen this survey and therefore are unable to comment on its completeness and accuracy. We would appreciate receiving a copy of the survey at your earliest convenience.

We are familiar with the project area. It is the Canal Society's opinion that the Landing Road Bridge is an important contributing structure to the Morris Canal Historic District, which is listed on both the National and New Jersey Registers of Historic Places. The loss of this historic bridge would be a serious negative impact to the Morris Canal Historic District. We therefore recommend that the bridge be repaired and reused on its original site.

The Landing Road Bridge was built in 1907, during the last decades of the Morris Canal's operation. It is one of the last extant road bridges in New Jersey built to span the historic Morris Canal. Of the hundreds of road bridges that once crossed the canal, nearly all have been removed, mostly by the State of New Jersey as part of the closing and dismantling of the Morris Canal in the mid and late 1920s. Therefore the rare surviving bridges are extremely important contributing resources to the Morris Canal Historic District. The Landing Road Bridge fits in this category.

In addition, the Landing Road Bridge is located at an especially important site within the overall Morris Canal Historic District. It was at this site that an extensive interchange of passengers occurred between the Delaware, Lackawanna & Western Railroad and the Morris Canal. This interchange of pedestrian traffic was part of the once prominent

tourist trade which flourished on Lake Hopatcong during the late nineteenth and early twentieth centuries. Tourists traveled via the railroad from various parts of New Jersey. Upon reaching Landing they would disembark and then board steam launches that plied the Morris Canal and its Feeder. The Feeder led into Lake Hopatcong and enabled passengers to be carried to their lake resort hotels and camps. There is no other comparable site where such an extensive passenger interchange occurred between a railroad and the Morris Canal along its overall length. It is therefore a unique site within the Morris Canal Historic District. The Landing Road Bridge is an integral component of this unique site since its two arched spans define and separate the railroad corridor from the adjacent site of the Morris Canal's channel. In addition, the supporting footings of the bridge's center pier and end abutment toward Lakeside Boulevard would appear to be integrally connected with the historic fabric of the Morris Canal's prism, towpath, supporting embankments, laid-up stonework and potential remnants of the passenger interchange platform. It is anticipated that elements of all of this historic fabric from the canal would survive as archeological remains beneath the current blacktop-paved commuter parking lot covering the canal site. This parking lot serves New Jersey Transit passengers. Any construction to remove the existing bridge and to build a new bridge would have the strong potential to result in serious negative impacts to the Morris Canal's historic construction elements.

In addition, the Landing Road Bridge is a contemporary structure with the Delaware, Lackawanna & Western Railroad's nearby Cutoff, which was an engineering marvel of its time. The Lackawanna Cutoff and its associated structures would certainly be eligible for listing on the National Register of Historic Places. If in fact the Landing Road Bridge can be associated with the Lackawanna Cutoff's construction, the bridge would also appear to be eligible for listing on the National Register due to its railroad association.

The Landing Road Bridge is also of historical significance due its early twentieth century use by the Morris County Traction Company's trolley line. The bridge was used by the trolley line to access nearby Bertrand Island and its amusement park.

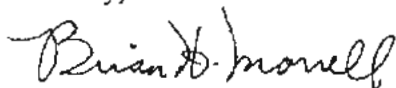
The Landing Road Bridge has added significance as a prominent landmark within the developing Morris Canal Greenway. The Canal Society of New Jersey has established the Morris Canal Greenway project to protect and preserve the historic remains of the Morris Canal as its site extends across the entire state.

The overall plan for the Morris Canal Greenway is to create a public greenway, linked with a trail where feasible, along the entire route of the Morris Canal. The Greenway's purpose is to preserve the surviving historic remains of the Morris Canal and its associated natural environment, to interpret canal sites to the public, and to offer recreational opportunities. The greenway concept combines historic preservation; environmental protection; open space preservation; recreational development; and economic development, relating to heritage tourism. The Greenway effort has many participating partners including the New Jersey Division of Parks & Forestry, the New Jersey Green Acres Program, Morris Land Conservancy and the Canal Society of New Jersey. At present, the Canal Society has received three matching grants from the New Jersey Green Acres Program to preserve portions of the Morris Canal and has mapped the

Morris County portion of the canal site and the associated canal greenway. Interest continues to grow regarding the Morris Canal and the creation and preservation of a preserved corridor along its alignment. The Landing area and its associated Morris Canal sites, including the Landing Road Bridge, have been identified as extremely significant components of the Morris Canal Greenway.

Once again the Canal Society strongly recommends that the Landing Road Bridge and the surrounding Morris Canal site be preserved intact as part of the Morris Canal Historic District and the Morris Canal Greenway. Thank you for the opportunity to comment on the proposed project.

Sincerely,



Brian H. Morrell
President

cc. Senator Anthony Bucco
Assemblyman Richard Merkt
Assemblyman Michael Patrick Carroll
Dorothy Guzzo, NJ State Historic Preservation Office
NJ Highlands Coalition
Highlands Council
Marion Harris, Morris County Trust for Historic Preservation
Morris County Board of Chosen Freeholders
Margaret Schultz, Morris County Heritage Commission
Richard Cramond, Roxbury Township Historic Advisory Committee
Martin Kane, Lake Hopatcong Historical Museum

Township of Roxbury
1715 Route 46
Ledgewood, New Jersey 07852

HISTORIC ADVISORY COMMITTEE

INFORMATION 448-2008
CONSTRUCTION/BLDG 448-2009
COURT 448-2034
ENGINEER 448-2018
FIRE OFFICIAL 448-2012
HEALTH 448-2028
MANAGER 448-2003
MAYOR & COUNCIL 448-2001
POLICE 448-2100

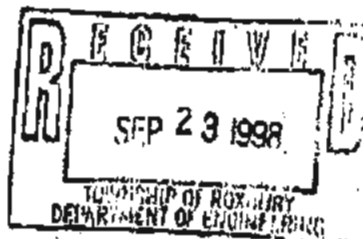
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PUBLIC WORKS 448-2009
RECREATION 448-2015
TAX ASSESSOR 448-2021
TAX COLLECTOR 448-2022
TOWNSHIP CLERK 448-2001
TREASURER 448-2005
WASTEWATER TREATMENT PLANT 584-6360
ZONING OFFICER 448-2012

Date: 09/23/98
To: Hank Huelsebusch, Twsp. Engineer
From: Richard D. Cramond, Chrm.
Subject: Landing Road Railroad Bridge
Replacement Study

Your memo to Gary Webb, Township Manager, dated August 24, 1998 has been reviewed by the HAC. We concur with the findings of the referenced study and the recommendations that you have made. Since the core samples of the bridge indicate a condition that would necessitate it's replacement rather than reconstruction and widening, we agree that the best alignment is still Alignment #7.

As you recall in our conversation about the current status of the bridge the HAC agrees to the replacement of the bridge so long as the architectural character of the new bridge maintains the current double span look. We think it is important to maintain the historic importance of the Morris Canal and the railroad being parallel at this site. We therefore request that when the County and the NJDOT have plans available for review that the HAC be involved in the final acceptance process for the architectural design. Our initial request would be to reproduce the current architectural design but we will consider alternative proposals of compatible character.

Rich
Richard D. Cramond, Chrm.



Pass-It® Fax Note	7671	Date	9-24	Page	1
To	Sandy Thapar	From	Hank Huelsebusch	Co.	Roxbury
Co/Dept	M.C. Engineering	Phone #	448-2018	Co.	Roxbury
Phone #		Phone #	448-2018	Co.	Roxbury
Fax #	539-3141	Fax #		Co.	Roxbury

PARSONS

100 Broadway • New York, New York 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

June 14, 2006

Richard Crammond, Chair
Roxbury Township Historic Advisory Committee
1715 Route 46
Ledgewood, NJ 07852

Dear Mr. Crammond:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. As such we have prepared a cultural resources survey for the project area.

Federal regulation requires public participation in the review process. We have received resolution of support from the Township of Roxbury for the replacement of this bridge. We are seeking any information you may have pertaining to historic preservation issues within the area.

Any comments you feel may be relevant should be communicated to the undersigned within 15 days of the receipt of this letter.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area.

Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,



S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th floor
New York, NY 10005



PARSONS

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June 14, 2006

Ruth Ann Sealy, Local Historian
210 Emmans Road
Flanders, NJ 07836

Dear Ms. Sealy:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

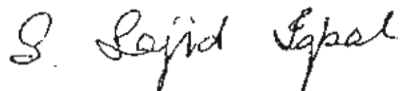
Federal regulation requires public participation in the review process. We have received resolution of support from the Township of Roxbury for the replacement of this bridge. We are seeking any information you may have pertaining to historic preservation issues within the area.

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Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,



S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th floor
New York, NY 10005



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June 14, 2006

Robert Barth
Canal Society of New Jersey
P.O. Box 737
Morristown, NJ 07963

Dear Mr. Barth:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.


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Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area.

Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,



S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th floor
New York, NY 10005



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100 Broadway • New York, New York 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

June 14, 2006

County College of Morris
Local History Program
C310
214 Center Grove
Randolph, NJ 07860

Dear Sir/Madam:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

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Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,



S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th floor
New York, NY 10005



PARSONS

100 Broadway • New York, New York 10005 • (212) 266-8300 • Fax: (212) 571-6825 • www.parsons.com

June 14, 2006

Brian Morrell
Morris Canal Preservation Society
P.O. Box 737
Morristown, NJ 07963-0737

Dear Mr. Morrell:

We are writing to solicit your input concerning the proposed replacement of Landing Road (CR 631) Bridge No. 1400-073 over the Morris Canal and NJ Transit Morristown Line in the Township of Roxbury, Morris County. The project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on Historic resources. As such we have prepared a cultural resources survey for the project area.

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Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area.

Thank you for your cooperation in this matter. If you have any questions, please contact me at (212) 266-8536.

Sincerely,



S. Sajid Iqbal
Project Manager
PARSONS
100 Broadway, 20th floor
New York, NY 10005



ARCH², Inc.
395 Main Street, Suite 8
Metuchen, NJ 08840-1806

Nancy L. Zerbe, President

Jeanne H. Watson, Director
Morris County Historical Society
68 Morris Avenue
Morristown, NJ 07960-0170

Dear Ms. Watson:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

Federal regulations require public participation in the review process. We are reaching out to you as someone knowledgeable about the Township of Roxbury. We are seeking any information you may have pertinent to historic preservation issues within the area.

Any comments you feel may be relevant should be communicated as soon as possible to Ms. Stacy E. Spies of ARCH², Inc. at 395 Main Street, Metuchen, New Jersey, 08840.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area. We thank you in advance for your assistance. Please call 732-906-8203 with any questions.

Thank You,

Nancy L. Zerbe

NLZ/cbb

ARCH², Inc.
395 Main Street, Suite 8
Metuchen, NJ 08840-1806

Nancy L. Zerbe, President

August 31, 2001

Mrs. Terry Schlatter, President
Morris County Historical Society
18 Jeffrie Trail
Whippany, NJ 07891

Dear Mrs. Schlatter:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area. We thank you in advance for your assistance. Please call 732-906-8203 with any questions.

Thank You,

Nancy L. Zerbe

NLZ/cbb

ARCH², Inc.
395 Main Street, Suite 8
Metuchen, NJ 08840-1806

Nancy I. Zerbe, President

August 31, 2001

Morris County Heritage Commission
P.O. Box 900
Morristown, NJ 07963

To Whom It May Concern:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Any comments you feel may be relevant should be communicated as soon as possible to Ms. Stacy E. Spies of ARCH², Inc. at 395 Main Street, Metuchen, New Jersey, 08840.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area. We thank you in advance for your assistance. Please call 732-906-8203 with any questions.

Thank You,

Nancy L. Zerbe

NLZ/cbb

ARCH², Inc.
395 Main Street, Suite 8
Metuchen, NJ 08840-1806

Nancy L. Zerbe, President

August 31, 2001

Roxbury Township Historical Society
PO Box 18
Succasunna, NJ 07876

To Whom It May Concern:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

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Any comments you feel may be relevant should be communicated as soon as possible to Ms. Stacy E. Spies of ARCH², Inc. at 395 Main Street, Metuchen, New Jersey, 08840.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area. We thank you in advance for your assistance. Please call 732-906-8203 with any questions.

Thank You,

Nancy L. Zerbe

NLZ/cbb

ARCH², Inc.
395 Main Street, Suite 8
Metuchen, NJ 08840-1806

Nancy L. Zerbe, President

September 19, 2001

Marion Harris, Chair
Morris County Trust for Historic Preservation
14 Oak Street
Morristown, NJ 07960-5240

Dear Ms. Harris:

We are writing to solicit your input concerning the proposed replacement or rehabilitation of Bridge No. 1400-073 on County Route 631 (Landing Road), over the Morris Canal and the New Jersey Transit Railroad, Morristown Line, Township of Roxbury, Morris County, New Jersey. This project is being reviewed under Section 106 of the National Historic Preservation Act for its impacts on historic resources. Therefore, we are preparing a cultural resources survey for the project area.

Federal regulations require public participation in the review process. We are reaching out to you as someone knowledgeable about the Township of Roxbury. We are seeking any information you may have pertinent to historic preservation issues within the area.

Any comments you feel may be relevant should be communicated as soon as possible to Ms. Stacy E. Spies of ARCH², Inc. at 395 Main Street, Metuchen, New Jersey, 08840.

Your input is important and will help the New Jersey Historic Preservation Office in identifying and protecting historic resources within the project area. We thank you in advance for your assistance. Please call 732-906-8203 with any questions.

Thank You,

Nancy L. Zerbe

NLZ/ss

July 21, 2006

643599

Mr. Surinder Thapar, P.E., P.P.
Assistant County Engineer
County of Morris
Department of Public Works
Administration and Record Building
Morristown, New Jersey 07963-0900

Attention: Ms. Roslyn Khurdan

Subject: **Landing Road Bridge over NJ Transit and Morris Canal**
Roxbury Township/Morris County
Local Scoping Program
Historic Bridge Alternative Analysis Report

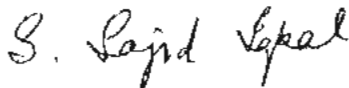
Dear Mr. Thapar:

Enclosed please find five (5) copies of the updated Historic Bridge Alternative Analysis Report for the subject project. This updated report is submitted as per request of NJTPA and for onward submission to the NJSHPO for their review and concurrence.

If you have any questions or need additional copies please contact me.

Thank you.

Very truly yours,



S. Sajid Iqbal, P.E.
Project Manager

Enclosures



ROXBURY TOWNSHIP HISTORICAL ADVISORY COMMITTEE

1715 Route 46, Ledgewood, NJ 07852

November 5, 2015

VIA REGULAR MAIL AND EMAIL

Ms. Roslyn Khurdan
Morris County Engineering
P. O. Box 900
Morristown, NJ 07930

RE: Morris County, Roxbury Township
Landing Road Bridge Improvements (Morris County Bridge 1400073)
Landing Road over NJ Transit Montclair-Boonton Line and the Morris Canal
HPO Project No. 05-0435-5

Dear Ms. Khurdan:

The Historic Advisory Committee of Roxbury would naturally like to preserve the existing historic two-span concrete bridge dating to 1911, which is the only existing bridge that spanned the Morris Canal when the Canal was in operation. However, the Committee recognizes that due to the volume of traffic, and the age and condition of the existing bridge, a complete replacement of the bridge, with a slightly altered approach, is the only viable alternative. Therefore, we accept the Replacement Alternative (Alternative 7-C).

The Committee is, however, extremely concerned about the current configuration shown in the design we received with regard to pedestrian access safety and use. There are currently Township plans to provide a walkway from the recently constructed park and athletic fields on Landing Road to the bridge. The existing bridge, although beautiful in its original design, functioned well because of a walkway and step structure associated with the Landing Train Station that provided safe pedestrian access to the Lake, Morris Canal and Landing area. Since these features were later removed, and likely could not be replaced with the new handicap accessibility regulations, we are left with a bridge and intersection that is not currently safe for pedestrians, nor for its traffic volume and traffic flow.

The walkway across the bridge, on its northern side, does not provide safe access to the other side of the bridge for those pedestrians. Therefore the new bridge would have to include a pedestrian walkway on the southern side as well to accommodate those pedestrians. There is a historical precedent for this second walkway, as shortly after construction of the bridge, a second walkway was added on the southern side, spanning the Canal, and providing access from the lake via walkway and stairway down to the train/canal boat platform. *(The footings of the eastern end of this pedestrian bridge remain visible next to the main bridge abutment. We recognize that the demolition of the existing concrete bridge will also result in the loss of these footings.)*

We also feel that the pedestrian sidewalk along the Lake Hopatcong side retaining wall is too narrow and unsafe and does not provide any interaction with Lake Hopatcong. The historical pedestrian access from the train station and canal to Lake Hopatcong should be emphasized in any new design. Therefore, a minimum 10 foot wide pedestrian walkway should be provided. At a minimum, it should include safety bollards or features to separate the vehicular traffic and pedestrians and some features to view the lake should also be included. This could be in the form of openings in the wall as wide as 20 feet with an observation area for pedestrians to view the lake; possibly with stair access to the lakeside below.

In summary of this preliminary review, we recommend:

- 1) safe and signaled pedestrian access on the south side of the bridge from the train station to the lake,
- 2) a safe and wide pedestrian area between the realigned roadway and the lake,
- 3) provide interaction with the lake through open observation areas and access.

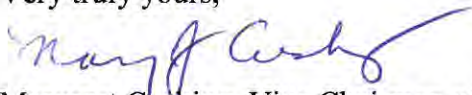
This area of Landing was once a beautiful gateway to Lake Hopatcong with trains and canal boats bringing tourists to various destination points on the Lake via boat, carriage and trolley. Now the once thriving area of Landing has deteriorated with many vacant business properties. We would like to see the replacement bridge designed with a street scape that includes attractive historic elements of the area during its heyday. Additionally, with the existing historical Landing Train Station being restored, the new bridge should retain its original style so as complement, rather than detract from, the Station's appearance.

In the future design of the bridge and the intersection improvements, we feel strongly that the County Engineering Department consider:

- A) a double arch bridge to match the historical significance and uniqueness of a bridge serving the Morris Canal and train service simultaneously,
- B) the historical surfaces of the bridge (historical photo attached),
- C) the railing design,
- D) the need of a fence and style of fence separating the roadway and pedestrian sidewalk from the lake.

We look forward to working together with the engineers and incorporating these historical elements in this important reconstruction.

Very truly yours,



Margaret Cushing, Vice Chairman of the Roxbury Historic Advisory Committee

CC: Daniel D. Saunders, Deputy State Historic Preservation Officer
Amy Rhead, Clerk, Roxbury Township
Joseph Macasek, Canal Society of New Jersey
Richard Porter, RBA Group
Peg Shultz, Morris County Heritage Commission
Marty Kane, Lake Hopatcong Foundation via email
Sean Ream, Bureau of Environmental Program Resources

THE VIADUCT AT LANDING HOPATCONG, N. J.





Township of Roxbury

1715 Route 46
Ledgewood, NJ 07852
www.roxburynj.us

Information
973-448-2000

Building &
Construction
973-448-2009

Court
973-448-2034

Engineer
973-448-2018

Finance
973-448-2006

Fire Official
973-448-2012

Health
973-448-2028

Manager
973-448-2002

Mayor and
Council
973-448-2001

Police
973-448-2100

Planning
and Zoning
973-448-2008

Public Works
973-448-2069

Recreation
973-448-2015

Tax Assessor
973-448-2021

Tax Collector
and Utilities
973-448-2022

Technology
973-448-2099

Township Clerk
973-448-2001

Sewer Plant
973-584-5360

Water Plant
973-398-2818

November 10, 2015

VIA EMAIL AND REGULAR MAIL

Roslyn Khurdan, Assistant Engineer
Morris County Engineering
P.O. Box 900
Morristown, NJ 07930

RE: Morris County, Roxbury Township, HPO Project No. 05-0435-5
Landing Road Bridge Improvements (Morris County Bridge 1400073)

Dear Ms. Khurdan:

The Landing Gateway Committee was established by Council Resolution to beautify the Landing area in the immediate area of the existing Landing Bridge. We received a copy of the State Historic Preservation Office's (SHPO) recommendation to Sean Ream of the Bureau of Environment Program Resources regarding the bridge replacement alternatives and would like to comment.

The 37,079 (as of 2011) cars that cross the bridge daily deserve to have a safe structure, intersection and an attractive entry to our community and our biggest amenity, Lake Hopatcong. Presently, the upper portion of the bridge is not historic, safe or attractive and is a hazard to pedestrians.

The correct design for a replacement bridge and intersection is key to the economic rejuvenation of Roxbury. The once thriving area of Landing has deteriorated with many vacant business properties. Recent projects hold potential for revitalizing the area (the new Landing Park and renovation of the existing historical train station). A design for the bridge and intersection that incorporates train/trail/lake/bike/pedestrian access would not only recreate the historic character of Landing but also create a tourist and business destination.

We support alternative 7C, the realignment and replacement of the bridge utilizing the double arch design. Our Committee would like input on the design of the bridge including sidewalks, pedestrian crossings, materials, lighting, bollards, street plantings, parking, fencing and Lake access.

We respect the SHPO's position of trying to preserve the old bridge but our community needs a safe, functional, attractive bridge and intersection as soon as possible.

Respectfully submitted,
The Landing Gateway Committee

Christine Hardy Chairman

2011 "America's Promise Alliance 100 Best Communities for Young People"



CC: Daniel D. Saunders, Deputy State Historic Preservation Officer
Sean Ream, Bureau of Environmental Program Resources
Peg Shultz, Morris County Heritage Commission
Richard Porter, RBA Group
Roxbury Historic Advisory Committee via email
Amy Rhead, Clerk, Roxbury Township
Joseph Macasek, Canal Society via email
Lake Hopatcong Foundation via email



LANDING ROAD BRIDGE OVER NJ TRANSIT AND MORRIS CANAL

Public Information Center

August 12, 2015

6:00 – 8:00 PM, Roxbury Township Municipal Building
1715 Route 46, Ledgewood, NJ

MEETING NOTES

FROM: Denice daCunha, Supervising Engineer
RBA Project Number J4804.01

ATTENDEES:

Project Representatives:

Morris County Department of Planning & Public Works:

Christopher Vitz, P.E., County Engineer

Roslyn Khurdan, P.E., Assistant County Engineer

RBA Group: Richard L. Porter, Director of Cultural Resources

Denice daCunha P.E. Supervising Engineer

Roxbury Township / Members of the Public: see attached Sign-In Sheet

Purpose

The purpose of this information center was to inform locals of the preferred alignment of the replacement of the Landing Road Bridge over NJ Transit and Morris Canal. The project includes the construction of a wider new bridge at its existing location.

Several presentation boards were utilized including: existing bridge condition photos, historic pictures/plans of bridge, previously considered alternatives, Alternative 7C with recent request for additional sidewalk and property owner aerial with Block and Lot (within 200 feet). The meeting format intent was informal where attendees could view boards at their convenience and ask questions. Most attendees arrived by 6:15 and remained after viewing boards and asking questions. Morris County gave a brief presentation of the project and offered a more formal question/answer opportunity.

Roslyn Khurdan introduced the project team representatives and gave a history of the project, noting numerous alternatives considered and were compared respective of environmental impacts, ROW, costs, etc. She discussed that both the No Build and Rehabilitation options were also considered and dismissed. She described the preferred alternative, state funding, anticipated schedule and estimated costs. Richard Porter explained the historic significance of the Landing Road Bridge as a contributing element within the Morris Canal Historic District, which is listed on the NJ Register of Historic Places and on the National Register of Historic Places. The NJ Register of Historic Places Act requires that any undertaking that will “encroach upon, damage

or destroy” a resource listed on the NJ register be subject to review. This review commences with the submission of an Application for Project Authorization to the NJ Historic Preservation Office, with a determination of Encroachment or No Encroachment. Since the structure is being replaced, it is anticipated that this project is an Encroachment thus further review by the NJ Historic Sites Council will be required. The process concludes with final action by the Commissioner of the DEP based on recommendations provided by the NJ Historic Preservation Office and the NJ Historic Sites Council.

The following are questions/comments observed by this memo author asked before, during and after the “formal” question and answer discussion:

1. What is the status of the Mount Arlington Avenue water main and is it part of this project?

Response: That project is a Roxbury Township project and not part of the bridge replacement improvements. The Roxbury Township Manger, Christopher Rath, discussed the status with concerned residents separately from the bridge replacement discussions.

2. I learned about this meeting from the local paper and some of my neighbors knew about it but what about people that live outside of the immediate area? How come they do not get notified?

Response: It is standard protocol on most projects to alert property owners with 200 feet and provide a public notice. It is not customary to reach out to other communities. Prior to construction, signing will alert the travelling public that construction will be starting.

3. Appreciate that this project is finally going through. The area is in need of attention. What is the projected schedule?

Response: Design 2016
Construction 2019, with a duration estimated at 18 months

4. Detours: the following questions were asked:

- Concern for traveling public and local business that traffic remain open during construction. Will a detour be required and if so, will Shippenport Road be used and paved since most likely this roadway will be used as a detour route.

Response: The project is in the concept phase and the intent at this time is to stage construction so that one lane in each direction is maintained across the bridge. There may be the need for a temporary traffic signal during the staged construction for safety reasons but is the intent is to minimize traffic impacts. If a detour were planned, an evaluation of the detour route would be conducted. At this time a detour is not planned nor improvements to Shippenport Road included as part of the County project.

- Will an alternate route be suggested to motorists traveling through the area even if there is not a detour in an effort to lessen impacts? Concern is for local residents.

Reponses: A detailed maintenance of traffic evaluation will be conducted during design including if the need is warranted. It is noted that business owners voiced that they do not want traffic detoured for fear of business losses.

5. The Roxbury Planning Board Chairman requested that provisions be made on the new structure for a future sewer line.
Response: This can be evaluated/considered/coordinated during the next design phase, Preliminary Engineering.
6. Was the current alignment similar to the one previously approved by Township resolution?
Response: Yes, the alignment is similar to the 2003 however Roxbury has requested additional sidewalk. A new Resolution of Support is anticipated.
7. Will local police be used to manage traffic during construction?
Response: On Morris County projects, the County uses local police and neighboring towns or Sheriff's office if needed. These details get worked out much later in design.
8. Will there be future meetings with the public?
Response: Yes, another PIC would occur most likely in 2017 and a website will be setup for access about design, construction.
9. Will other improvements be made such as replacing the chain link fence on the Lake Hopatcong "sea wall" and other decorative improvements?
Response: The Roxbury Township Manager, Chris Raths, noted that a committee with the purpose for a streetscape vision has been created and the intent is to have a "gateway" in addition to decorative cross walks and lighting. The committee will coordinate with the County.
10. What will happen with train services access, parking during construction and will there be additional improvements to station that are much needed?
Response: NJ Transit is a key stakeholder in the project as bridge crosses tracks/property. They will be involved in design including clearance, parking, aces during construction etc. These are all deign issues that will be worked out. Any improvements to the station are not proposed as part of the project and would be a transit issue.
11. Resident and Lake Hopatcong Foundation (LHF) representative Donna Macalle-Holly wanted to thank the County for being great partners with the community. She was looking at this project as a positive opportunity for enhancements.
12. Will the "Lake Hopatcong Foundation building" (Lake Hopatcong Train Station) be impacted?
Response: Not the building, but there are impacts to the parking lot.
13. ROW Impacts. What will happen to existing businesses and properties shown as being completely impacted?

Response: As part of the ROW process, negotiations will take place. The ROW will be refined during design and impacted property owners will be contacted.

- A member of the audience spoke out that he was an attorney who handles these types of issues and noted that Morris County does an effective job at such negotiations. He added that several additional properties in addition to the 3 major impacts with partial takes will also have impacts.

Response: The alternative shown is conceptual and ROW will be refined during design.

14. A gentleman who introduced himself as a member of the Historic Society stated he likes the bridge the way it is.

Response: It was explain the rehabilitation of the bridge was investigated and the option eliminated since not practical. The goal of the replacement is to provide a new bridge with the appearance of the existing bridge.

15. Will the bridge deck be build off site in an effort to expedite construction delays at the site?

Response: During design options to expedite construction duration will be investigated including the use of precast bridge elements

16. Where does the four lane section (two lanes per direction) section end towards the park and will this just create another bottle neck? Will Landing Road be widened throughout?

Response: The four lane section ends in the vicinity of the Foundation building. Two lanes in each direction across the bridge will help facilitate the operation of the traffic signal at Mount Arlington Boulevard. Providing four lanes on the bridge will also facilitate future traffic and since bridge is being replaced makes sense for the life of bridge. There are no other current plans to widen Landing Road.

17. Existing sidewalks are very narrow. Can other areas also be considered for new sidewalk such as by the Wave service station?

Response: Sidewalks will be improved to ADA compliant conditions. Additional sidewalk requests can be considered during design. In order to accommodate sidewalks in some areas, Right of Way may be required.

18. Will the Design and Construction be bid separately? Is funding allocated?

Response: Yes, Design and Construction will be bid separately. Project is now state funded and money has been allocated. Estimated construction cost is \$6.1M and \$1M for ROW. ROW cost will be evaluated and funding adjusted if needed.

19. Canal Street is in need of paving? Is this part of project?

Response: Canal Street will be altered by bridge demolition and/or construction so improvements would be part of this project.

20. Will the posted speed limit be the same as existing after construction is completed?

Response: Yes, that is the intent.

21. Can the traffic islands have plantings? Do they need curbs? There are local volunteers that provide plantings now and additional areas would look nicer.

Response: These requested can be investigated during design and coordinated with Roxbury Township. Curbing is required around islands.

22. Can the size and shape of the island by Kings Highway be modified? It seems very large and would impact access to businesses.

Response: The graphics are conceptual and the size will be refined during design. The crossing distance for pedestrians is a safety improvement and it appears the island size can be reduced.

23. Are there meeting handouts of the presentation boards? How can we get updated plans?

Responses: The County will provide pdfs of the preferred alternative to the Township. During design a website will be created and maintained.

It is noted that several attendees thanked the team representatives for the opportunity to learn more about the project and provide their input. The meeting ended at 8PM after all members of the public left.

These notes are the author's recollections of the meeting and represent a complete and accurate record of the decisions and agreements made. Amendments to this record shall be made in writing to the author.

**PUBLIC INFORMATION CENTER
(WEDNESDAY AUGUST 12, 2015 AT 6:00 PM)**

FOR

**REPLACEMENT OF MORRIS COUNTY BRIDGE NO. 1400-073 LANDING
ROAD OVER NJ TRANSIT AND MORRIS CANAL IN THE TOWNSHIP OF
ROXBURY, MORRIS COUNTY**

SIGN-IN SHEET

NAME & ADDRESS

(Phone Number)

Wahmeta Bears, 333 Borton St, Landing ⁹⁷³ 398-9375

Barbara Bieleva 169 Mt. Arlington Blvd.

Denice da Cunha RBA Group 973-946-5024

Maria & David Matorana (973) 895-5584

100 Kingsland Rd - Landing

Raymond Billia 169 Mt. Arlington Blvd

Christine Houtz 576 Doll Rd. Landing, 973-770-3723

John Yingling 209 Lakeside Blvd Hopatcong ⁹⁷³ 398-9354

BRAD TREACY 146 LANDING ROAD LANDING N.J. 973-398-4753

JOE GATES 132 LANDING ROAD, LANDING, NJ (973) 398-4860

Chris Rafks 1715 Rt 46 ~~Landing~~ Ledgewood NJ 07876

Maria Sudol 30 Cowin St. Kenil 07847

ART SCANTLEBURY 112 Kings Hwy

THOM COATES 31 ONEIDA AVE.

**PUBLIC INFORMATION CENTER
(WEDNESDAY AUGUST 12, 2015 AT 6:00 PM)**

FOR

REPLACEMENT OF MORRIS COUNTY BRIDGE NO. 1400-073 LANDING
ROAD OVER NJ TRANSIT AND MORRIS CANAL IN THE TOWNSHIP OF
ROXBURY, MORRIS COUNTY

SIGN-IN SHEET

NAME & ADDRESS (Phone Number)

Donna Macolte Helly, LHF	37 Robin Park Rd L.H.	973-663-2500
James Coates	1 IRROUOIS Ave Landing, NJ	
Shawn Potillo	54 Laurie RD Landing 34 KINGSCAMP RD	201-230-6672
R GILFILLAN	LANDING	973-398-6780
TOM & JUNE VILIANTE		973-418-6869
117 LAKESIDE BLVD,		
Jeff + Mark Feldman	101 Kings Highway	973-770-9710
PAUL CORRENTE		
47 ONEIDA Ave Landing		973-229-5628
TERRY McHugh	TERRY'S AUTO 124 Landings Rd	973-945-5633
Ralph C. NAPP		
29 FORD RD. LANDING		(973)-398-5624
David Klein	10 DEER LAKE Succasunna NJ	(973) 584-4971
Mark Weitzel	15 Deer Lake Succasunna, NJ 07826	973-584-9597
Charles Bantz		201-841-0552

APPENDIX – G

REHABILITATION DESIGN STUDY

Bill 643599

STORCH ENGINEERS

220 RIDGEDALE AVENUE, P.O. BOX 267
FLORHAM PARK, NEW JERSEY 07932
201-822-2600 FAX 201-822-3904

pl's Refson

LANDING ROAD BRIDGE REHABILITATION STUDY

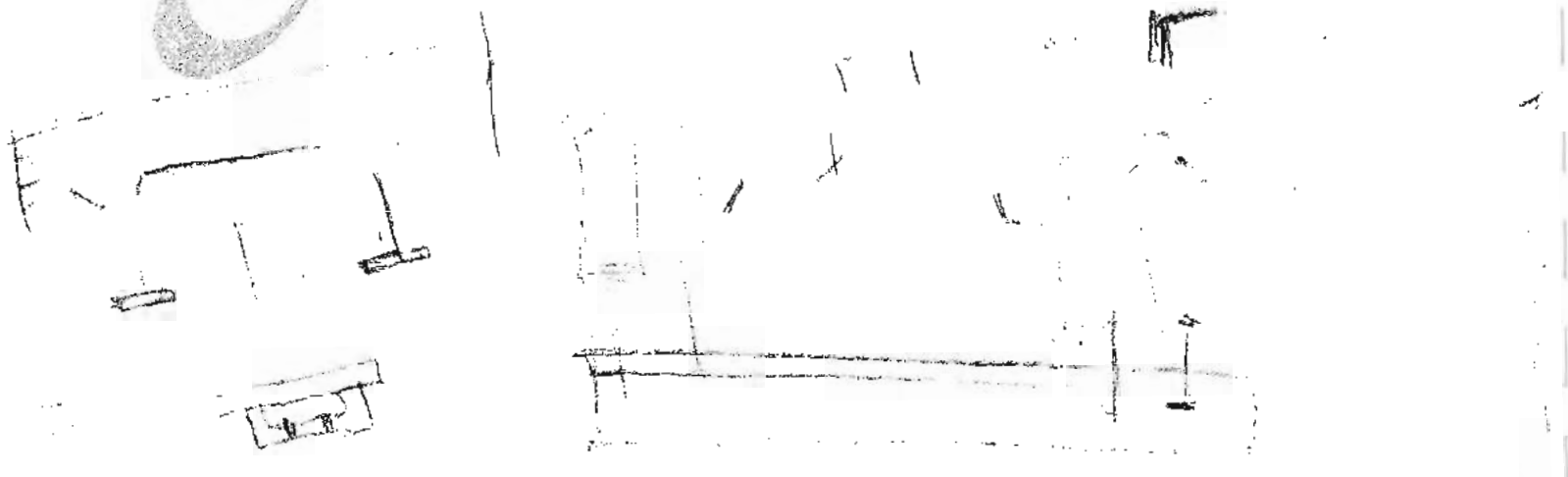
Landing Road Bridge over New Jersey Transit Railroad
Township of Roxbury, Morris County, New Jersey

Prepared for:

County of Morris
Department of Public Works
P.O. Box 900
Morristown, NJ 07963-0900

May 1997

draft



BOSTON
MASSACHUSETTS

FLORHAM PARK
NEW JERSEY

MANCHESTER
NEW HAMPSHIRE

HICKSVILLE
NEW YORK

CROMWELL
CONNECTICUT

NEW YORK
CITY

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5.0 COST ESTIMATE 11

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- Appendix B - Photographs
- Appendix C - Coring Location Plan
- Appendix D - Core Test Results
- Appendix E - Traffic Analyses
- Appendix F - Drawings and Sketches
- Appendix G - Cost Estimate

1.0 EXECUTIVE SUMMARY

The County of Morris plans to replace or rehabilitate the existing County Bridge No. 73 on Landing Road over the New Jersey Transit Railroad and Morris Canal in Roxbury Township.

This study has been undertaken to examine the existing physical conditions of the structure and make recommendations with respect to replacement or rehabilitation. It will take into consideration the existing and projected traffic volumes carried by the structure and their impacts on the intersection (and its approaches) directly adjacent to the bridge.

This report summarizes the results of Storch Engineers' investigation on behalf of the County of Morris, Department of Public Works to recommend replacement or rehabilitation schemes for the Landing Road Bridge.

The results of this study have found that replacement of the existing structure is necessary due to the deteriorated condition of the arches and spandrel walls. It has also been determined that the structure should be widened to allow for an additional lane in the northbound direction. This will improve the level of service for the intersection and Landing Road in the northbound approach to the intersection from unacceptable levels of service to a level of service "B". Improvements to the intersection will provide additional capacity to the intersection and its approaches.

The cross section of the widened structure will consist of three traffic lanes (11 feet wide each), outside shoulders (2 feet wide each), a five foot wide sidewalk, and one foot wide parapets and/or bridge rails. The out-to-out of the new structure would be 44 feet.

The improvements to the intersection would include minor alignment changes and modifications to the traffic signal timing.

2.0 INTRODUCTION

The County of Morris plans to replace or rehabilitate the existing County Bridge No. 73 on Landing Road over the New Jersey Transit Railroad and Morris Canal in Roxbury Township.

Landing Road, also known as Morris County Route 631, is located in Roxbury Township. It extends from Interstate Route 80 in the northwest approximately 0.65 miles to the southern edge of Lake Hopatcong. Landing Road then travels over the Old Delaware, Lackawanna and Western Railway (currently known as New Jersey Transit) via Landing Road Bridge (County Bridge No. 73) and intersects with Lakeside Boulevard (County Route 602) and Shippenport Road. Please refer to the Site Location Map and U.S.G.S. Map contained in Appendix A.

The existing bridge, constructed in the beginning of the 20th century, is approximately 136 feet long. The superstructure consists of twin reinforced concrete arches and a reinforced concrete deck with an asphalt overlay having a curb-to-curb width of 29.6 feet and an overall width of 38.8 feet. The substructure includes concrete abutments, which are at a 90-degree angle with the bridge deck, and wingwalls.

In the 1970's and 1980's, the County constructed major improvements to Lakeside Boulevard from the County line to the intersection of Landing Road. In addition, substantial improvements to Mount Arlington Boulevard and to Howard Boulevard were made using Federal funding, State Aid Road System grants, and County resources. A study to realign Landing Road to the west of its present location in the late 1970's and early 1980's met its demise through lack of sufficient Federal funding, environmental problems and lack of community support.

A bridge evaluation study report prepared by Maitra Associates, Inc., in September 1990 gave the bridge an overall condition rating of fair. This report also stated that the bridge meets minimum tolerable limits to be left in place as is although eleven improvements/repairs to the bridge were recommended in order to preserve the bridge's structural integrity and extend its useful life.

The County of Morris determined the need to investigate alternatives for replacing or rehabilitating the existing County Bridge No. 73 or to construct a new roadway alignment and a new bridge within the local vicinity of the existing bridge.

Storch Engineers, on behalf of the County of Morris, Department of Public Works, was assigned this responsibility and in the process, developed eight (8) improvement alternatives. In addition, two (2) other alignments studied in 1985 by Elam and Popoff, P.A. were included in the evaluation.

The evaluation assessed the ten alternatives with respect to various criteria including, but not limited to, bridge impacts, intersection impacts, environmental impacts, utility impacts, right-of-way impacts, and economic impacts. Based on the results of that study, a recommendation was made to construct a new bridge. The new structure would be an extension of Lakeside Boulevard joining Landing Road south of the existing bridge. The existing bridge would be rehabilitated or have necessary repairs made and remain in service to provide local access to Landing Road and King's Highway. The cost of this alternative was estimated at \$7.6 million.

The amount of time to acquire the funding, right-of-way, and third party agency permits for this alternative was estimated to take ten to twelve years. This, along with the current condition of the existing bridge, has prompted the County of Morris to move forward with this study to determine replacement/rehabilitation/repair alternatives for the existing structure.

3.0 EXISTING CONDITIONS

3.1 HISTORY

The Landing Road Bridge provides one of the few major ingresses and egresses for residents on both the east and west shores of Lake Hopatcong. Lakeside Boulevard provides access to Landing Road Bridge from the north and to Mount Arlington Boulevard from the south. Howard Boulevard provides the only other major access to Interstate Route 80 from the southern end of Lake Hopatcong.

In the 1970's and 1980's, the County constructed major improvements to Lakeside Boulevard from the County line to the intersection of Landing Road. In addition, substantial improvements to Mount Arlington Boulevard and to Howard Boulevard were made using Federal funding, State Aid Road System grants, and County resources. A study to realign Landing Road to the west of its present location in the late 1970's and early 1980's met its demise through lack of sufficient Federal funding, environmental problems and lack of community support.

3.2 ROADWAY

The Landing Road Bridge is served by two forked intersections at each of its west and east approaches. The bridge connects Landing Road to the southwest and King's Highway to the northwest with Mount Arlington Boulevard to the southeast and Lakeside Boulevard to the northeast. The approaching roadways are relatively flat with the east approach serviced by a three-phase traffic signal.

As one approaches the bridge from Interstate Route 80 in Roxbury via Landing Road, a 620-foot radius curve to the left is encountered followed by a short tangent roadway alignment leading to a 173-foot radius curve to the right just before the juncture of the bridge and King's Highway. The average width of Landing Road is 25 feet, carrying one lane of traffic in each direction, and there is a posted speed limit of 30 m.p.h.

The King's Highway approach contains a 760-foot radius curve to the right just before the Landing Road intersection. The average width of King's Highway is 23 feet and there is a posted speed limit of 25 m.p.h.

Approaching the bridge from the east, a signalized forked intersection is encountered which includes Lakeside Boulevard to the north, Mount Arlington Boulevard to the southeast, and Landing Road to the southwest.

Approaching the bridge from Lake Hopatcong, a 740-foot radius curve to the right is encountered along Lakeside Boulevard. Beyond the curve, a short tangent alignment is followed by a sharp right turn onto Landing Road at the signalized intersection is at the east bridge approach.

Lakeside Boulevard in this region is a two lane approach to the bridge. The right lane serves as an exclusive right onto the bridge and the left lane serves as an exclusive left onto Mount Arlington Boulevard.

Turning left off of Lakeside Boulevard, via a 175-foot radius curve, the road becomes Mount Arlington Boulevard and heads toward Shippenport Road and Mount Arlington.

Approaching the bridge from Mount Arlington via Mount Arlington Boulevard, a 160-foot radius curve to the right is encountered prior to the left turn onto the east bridge approach at the signalized intersection.

Landing Road is a four lane roadway with a posted speed limit of 35 m.p.h. while Mount Arlington Boulevard has an average width of about 25 feet and a posted speed limit of 25 m.p.h.

3.3 BRIDGE

The existing bridge, constructed in the early 20th century, passes over the New Jersey Transit Railroad and a railroad station parking area over the filled in Morris Canal. An archaeological and historical survey report prepared by Herbert C. Kraft in August 1979, judged the bridge to be eligible for the National Register of Historic Places.

The structure is approximately 136 feet long. The superstructure consists of twin reinforced concrete arches and a reinforced concrete deck with an asphalt overlay having a curb-to-curb width of 29.6 feet and an overall width of 38.8 feet. It appears that the undersides of the arches have been treated with shotcrete as a maintenance measure. The substructure includes concrete abutments, which are at a 90-degree angle with the bridge deck, and wingwalls. No weight limit is posted on the bridge.

A bridge evaluation study report prepared by Maitra Associates, Inc. in September 1990 gave the bridge an overall condition rating of fair with severe spalling and cracking on the undersides of the arches. This report also stated that the bridge meets minimum tolerable limits to be left in place as is, although eleven improvements involving safety upgrades and repairs to the bridge were recommended in order to preserve the bridge's structural integrity and extend its useful life.

Based on our observations during more recent site visits and concrete core samples, the existing bridge is in severely deteriorated condition. The gunite is cracked throughout the underside of the arches and is spalled in many areas. The spandrel walls and outside edges of the arches are severely cracked and spalled with exposed rusted reinforcing steel. It has been reported that pieces of gunite and arch concrete routinely fall from the underside of the arches when large trucks cross the bridge. Evidence of the fallen debris has been noted on the ground under both spans, especially under the southern spandrel wall. Please refer to various site photographs contained in Appendix B.

The Morris County Engineer's Office requested that Storch Engineers acquire cores of the arch and substructure concrete and have analyses performed to determine the feasibility and extent of repair and/or rehabilitation necessary to extend the bridge's serviceable life.

The test results and observations of the core samples and the following previous inspection material have been reviewed and incorporated into the evaluations and recommendations made in this report:

- * "Bridge Evaluation Survey Report" dated September 1990 by Maitra Associates, Inc.
- * "Cost Feasibility Study" dated July 1991 by A.G. Lichtenstein & Associates.
- * "Bridge Re-evaluation Survey Report" dated October 1994 by Bettigole, Andrews & Clark, Inc.

In June 1991, four core samples were taken at the mid-span of each arch for A.G. Lichtenstein & Associates. Five of the eight cores crumbled during the coring and extraction operations. The remaining three cores were tested for compression strength and one of the cores failed before a load was registered on the compression machine. The other two cores achieved compression strengths of 4,000 and 6,000 psi. Chloride tests were also performed on all of the cores and revealed very high levels of chloride ion in the concrete, indicating an adequate environment to support electrochemical corrosion of the embedded reinforcing steel.

*if taken from underside, I agree 5
to demolish if underside
retainer are evident,*

These cores were taken at the tops of the arches where the depth of cover over the arches is minimal and it would be expected that the highest levels of chloride ion would be found. The additional cores proposed by Storch Engineers were located closer to the substructures to determine if similar conditions exist throughout the arches.

Due to the increasing thickness of the arches near the substructures and the increasing depth of cover over the arches, full depth cores could not be taken and partial depth cores from the top could not be extracted. Therefore, it was decided to take the cores from the underside of the arches. However, due to the railroad located under the west span, ~~six partial depth cores from the underside of the arch were taken in the east span.~~ It is assumed that the condition of the west span will be similar to the conditions found in the east span.

* One core was taken in each of the abutments and the pier and cores were attempted in the spandrel walls and wingwall faces. The locations of these cores are shown on the Coring Location Plan in Appendix C.

The cores were visually examined and tested for compressive strength, chloride content, air content, and freeze-thaw durability as indicated in the coring test results in Appendix D.

Core No. 7 was eliminated ^{three} when it was realized that the cores could not be taken from the west span. Core Numbers 8, 9, and 10 were attempted, but could not be taken because sound concrete for the core drill anchor bolts could not be found.

* The general conditions of the arch cores were described by Materials Testing, Inc. as fair to poor. The aggregate to paste bond is fair to poor, the grading of the fine and coarse aggregate is fair and one sample had many vertical cracks throughout the sample.

* Several of the cores broke into several pieces after extraction. The remaining intact portions were tested for compressive strength with results ranging from 2,700 to 5,150 psi. * Two of the cores had chloride levels below the accepted threshold of two lb/cy and the levels of the remaining four cores exceeded the threshold. The highest level was indicated at 7.56 lb/cy.

* After 15 cycles of the 25 cycle freeze/thaw test, the cement paste in all of the tested cores had completely separated from the coarse aggregate and broke up into small pieces. This is indicative of very poor durability of the existing concrete.

3.4 TRAFFIC

Traffic data was collected for the intersections at each end of the Landing Road Bridge as well as the adjacent intersection of Shippenport Road and Mount Arlington Boulevard. Intersection counts were taken on Thursday, July 28, 1994, by Storch Engineers at the following three (3) locations:

- Site 1 - The intersection of Landing Road and King's Highway at the western end of the Landing Road Bridge.
- Site 2 - The intersection of Landing Road, Mount Arlington Boulevard and Lakeside Boulevard at the eastern end of the Landing Road Bridge.
- Site 3 - The intersection of Shippenport Road and Mount Arlington Boulevard.

The intersection counts, which describe the movements of vehicles through the intersections, were taken during the a.m. (6:00 to 9:00) and p.m. (3:00 to 6:00) peak periods. These counts show the a.m. and p.m. hourly counts for each location at 15 minute intervals. The intersection peak hours and their corresponding volumes for the morning and afternoon counts were determined. A total of 3,499 vehicles used the bridge during the peak hours. The p.m. peak hour count of 1,906 vehicles was greater than the a.m. peak hour count of 1,593 vehicles by about 20%.

Analyses of the level of service (LOS) for the existing signalized and unsignalized intersections were performed using the traffic count data from the peak hour counts. The results of these analyses are included in Appendix E. The existing LOS for Sites 1 and 2, as described above, was found to be at an unacceptable level. Intersections or approaches are considered unacceptable when the LOS is E or below.

The approaches are described as follows:

Northbound Approach - Landing Road approaching the intersection with Lakeside Boulevard and Mount Arlington Boulevard. This approach consists of one lane until just before reaching the intersection where a second lane is developed.

Southbound Approach - Lakeside Boulevard approaching the intersection with Landing Road and Mount Arlington Boulevard. This approach consists of two lanes.

Westbound Approach - Mount Arlington Boulevard approaching the intersection with Landing Road and Lakeside Boulevard. This approach consists of a single lane.

With the current lane configuration and 1994 p.m. peak volumes, the northbound approach fails. This also causes the intersection to fail. By adding a second lane to the northbound approach, it improves to a LOS "B" using the 1994 volumes. For the purposes of this report, we will call this the "Amended Configuration". The amended configuration also improves the intersection to a LOS "B". Using the projected 2004 and 2014 traffic volumes, the widened northbound approach LOS would be "C" and "F", respectively. A summary of the analyses is contained below.

	LEVEL-OF-SERVICE (LOS)							
	Westbound Approach		Southbound Approach		Northbound Approach		Intersection	
	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak
Existing Configuration w/1994 Volumes	N/A	C	N/A	B	N/A	*	N/A	*
Amended Configuration w/1994 Volumes	C	C	C	B	A	B	B	B
Amended Configuration w/2004 Volumes	C	C	*	C	D	D	*	C
Amended Configuration w/2014 Volumes	C	D	*	*	D	*	*	*

N/A No analysis was performed*
 LOS "F" or worse (failure)

As the above summary indicates, the southbound approach (and hence the intersection) exhibits failure for the 2004 and 2014 volumes. It should be noted that the failing LOS of this approach is not caused by the addition of the second lane to the northbound approach, but an increase in traffic volume.

4.0 CONCLUSIONS AND RECOMMENDATIONS

4.1 STRUCTURAL

Based on the previous evaluations, the observed condition of the concrete and the results of the core testing program, it is our opinion that the existing concrete arches are in poor condition and should be replaced.

Repair of the structure was considered and is determined to be impractical for the following reasons:

1. The limits of repair would be difficult to determine. Considering that the cores at the tops of the arches crumbled and that other cores broke apart after extraction, it cannot be guaranteed that concrete adjacent to areas being removed would remain intact during removal operations. The estimated removal quantities may not be accurate and could result in excessive costs to the County.
2. Since the middle portions of each arch, the spandrel walls, and the outside edges of the arches must be replaced, extensive temporary shoring of the portions of the arches to remain would be required.
3. It is possible that the remaining portions of the arches would continue to deteriorate and would not have the same life expectancy as the repaired areas.

The following replacement alternatives were considered:

1. Replace the existing structure with cast-in-place concrete arches. This option will maintain the existing appearance of the structure, but is expected to be more costly than the other alternatives due to the formwork and labor costs.
2. Replace with a precast concrete arch. Several arch types were investigated and it was found that the existing arch profile cannot be matched by the pre-cast concrete arches available. Also, the available arch profiles would reduce vertical clearance over the railroad tracks to below acceptable levels.

3. Replace with adjacent prestressed concrete deck units with a concrete facade to match the original spandrels. The depth of the deck units required is greater than the depth of the arches at the midspan. This would require altering the arch shape.
4. Replace with continuous composite steel girders with a concrete facade to match the original spandrels. This option could maintain the appearance of the existing arches in elevation, but would not match the appearance from underneath. Preliminary discussions with the State Historic Preservation Office have indicated that this option would not be acceptable.

It is recommended that the existing Landing Road Bridge be completely replaced with cast in place concrete arches matching the original elevation and architectural treatment of the existing bridge.

4.2 TRAFFIC

In order to address the existing LOS failures of the northbound approach and the intersection of Landing Road with Lakeside Boulevard and Mount Arlington Boulevard we recommend the following:

1. Add an additional lane across the bridge in the northbound direction. Based on AASHTO standards, the minimum recommended cross section across the bridge would be three-11 foot wide lanes, 2 foot wide outside shoulders, a 5 foot wide sidewalk along the southbound side, and 1 foot wide parapets or railings. The minimum out-to-out of the bridge would be 44 feet. The existing structure is approximately 38.8 feet out-to-out.
2. Modify the existing signal timing to increase the Landing Road capacity and shorten the vehicle queues on Landing Road during the p.m. peak hours. This can be accomplished by setting the Mount Arlington Boulevard approach to a very low cycle length of between 8 and 10 seconds.
3. Make alignment improvements to the intersection of Landing Road with Lakeside Boulevard to improve traffic flow and hence the capacity through the intersection.

It should be noted that we are not recommending an additional southbound lane across the bridge because the additional width necessary would have a significant impact on existing buildings adjacent to the structure, the intersection, and along Lakeside Boulevard.

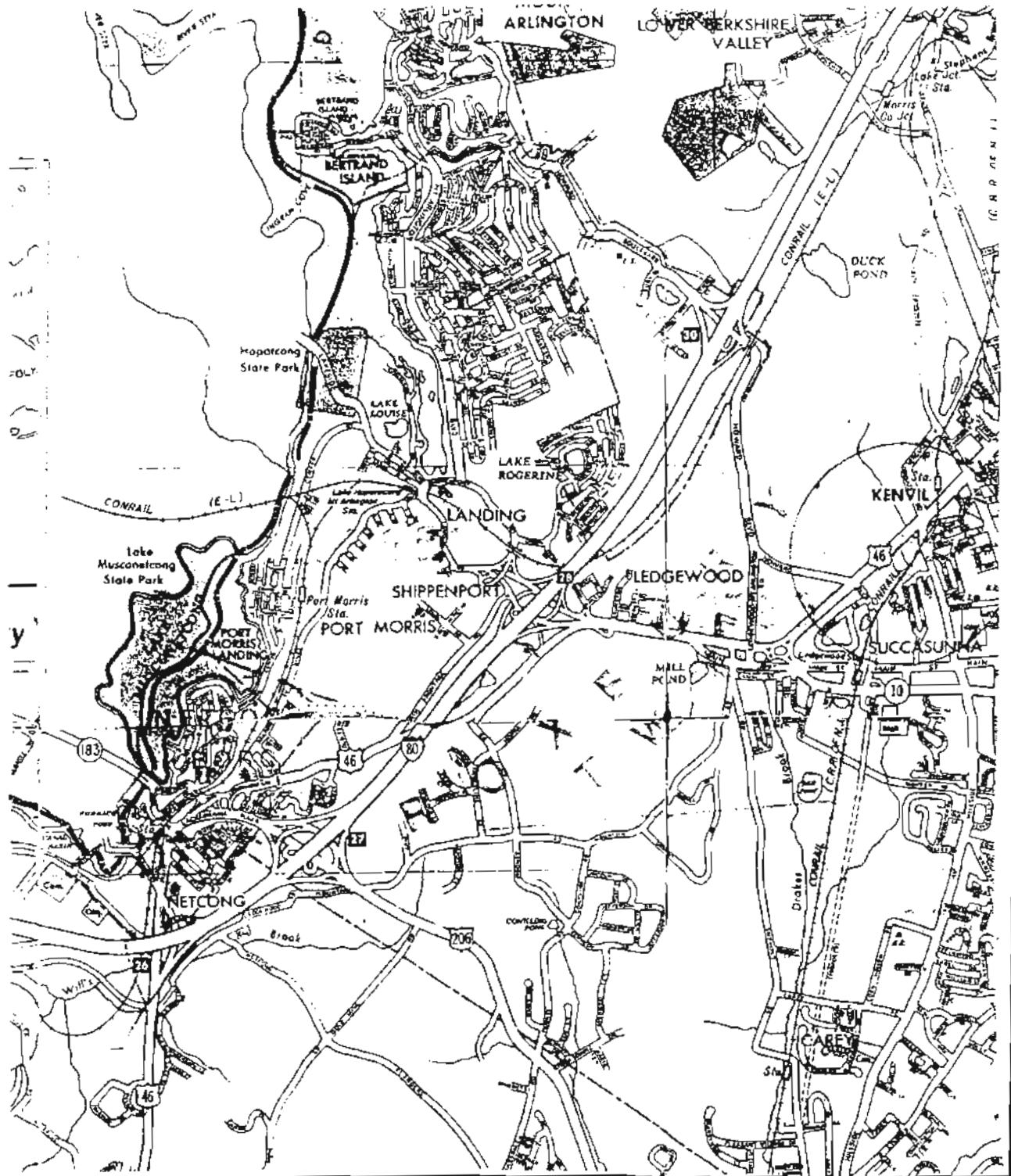
5.0 COST ESTIMATE

The recommended improvements noted in Section 4.0 above are similar to those specified for Alignment No. 7 of a previous report prepared by Storch Engineers entitled "Landing Road Bridge Replacement Study". This report was submitted to the County of Morris in March 1996. The cost for Alignment No. 7 was estimated at \$3.45 million.

It should be noted that Alignment No. 7 proposed widening the existing structure to four lanes and an out-to-out width of 63 feet. Our current recommendation is to widen the existing structure to only three lanes and an out-to-out width of 44 feet. Additionally, Alignment No. 7 did not take into consideration the cost of replacing the existing structure, but merely repairing it in conjunction with the widening. Therefore, we have revised the previous cost estimate.

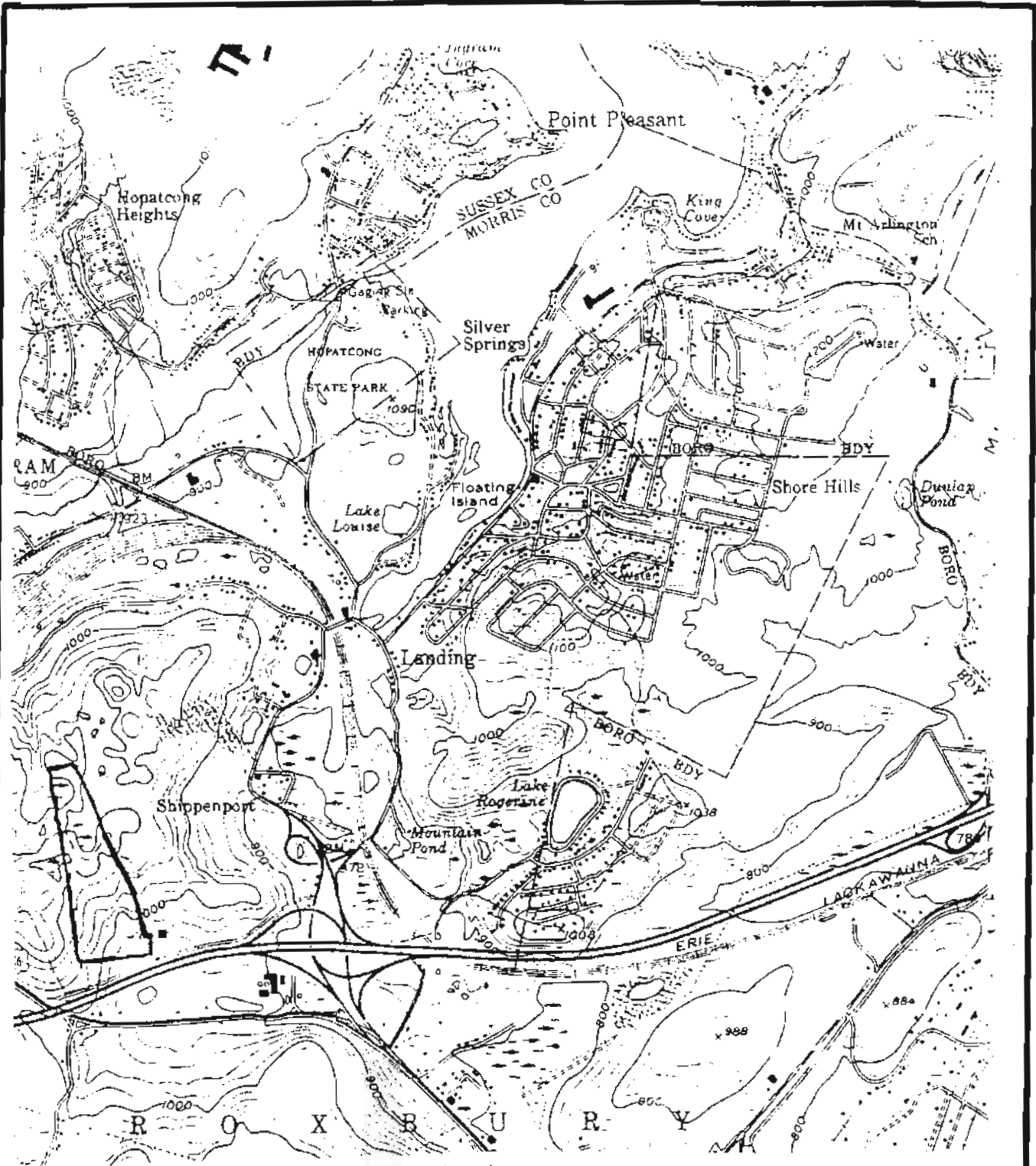
The estimated cost to replace and widen the existing bridge and make improvements to the intersection is \$3.7 million. Please refer to Appendix G for a more detailed breakdown of the cost estimate.

APPENDIX A



SOURCE:
HAGSTROM MAP
MORRIS COUNTY

LANDING ROAD BRIDGE REPLACEMENT STUDY	
ROXBURY TWP., MORRIS COUNTY, N.J.	
SITE LOCATION MAP	PROJECT NO. 2626
STORCH ENGINEERS 220 RIDGEDALE AVENUE FLORHAM PARK, NEW JERSEY 07932	SCALE: N.T.S.
	DATE: 3/22/94
	DRAWING NO. 1



SOURCE:
USGS STANHOPE
QUADRANGLE

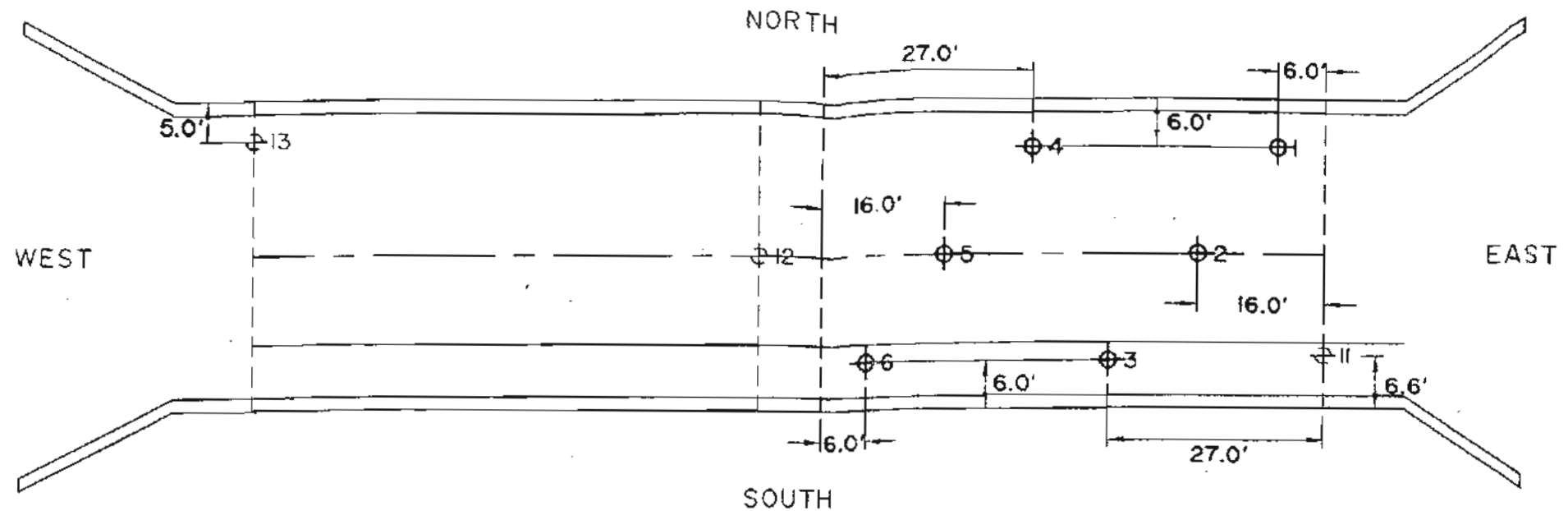
LANDING ROAD BRIDGE REPLACEMENT STUDY ROXBURY TWP., MORRIS COUNTY, N.J.	
U.S.G.S. MAP	PROJECT NO. 2626
STORCH ENGINEERS 220 RIDGEDALE AVENUE FLORHAM PARK, NEW JERSEY 07932	SCALE: N.T.S. DATE: 3/22/94
	DRAWING NO. 13

105373

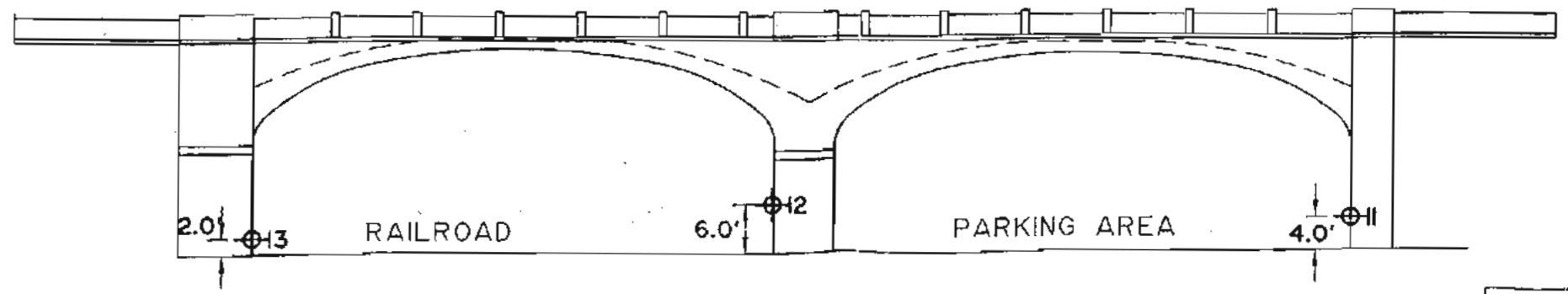
APPENDIX B



APPENDIX C



PLAN VIEW



SOUTH FASCIA ELEVATION

SCALE: 1"=20'

LANDING ROAD BRIDGE	
STUDY	
ROXBURY TOWNSHIP	MORRIS COUNTY, N.J.
CORING LOCATION PLAN	
PROJECT NO. 01-2626	SCALE: AS INDICATED
STORCH ENGINEERS	220 RIDGEDALE AVENUE FLORHAM PARK, N.J. 07932 12011 822-2600
JAMES A. BAUMGARTNER PROFESSIONAL ENGINEER N.J. LIC. NO. 32937	DRAWN BY: CPH CHECKED BY: AD DATE: 10/16/96 DWG. NO. 1 OF 1
DATE: 1/16/97	

APPENDIX D



MATERIALS TESTING INC. 732

NEW JERSEY DIVISION
180 MILL ROAD, EDISON, NEW JERSEY 08817 • (908) 248-1777 • FAX (908) 248-7979

RECEIVED

DEC 10 1996

STORCH ENGINEERS

CLIENT: STORCH ENGINEERING
220 RIDGEDALE AVENUE
P.O. BOX 267
FLORHAM PARK, N.J. 07932

REPORT: MTSE-002
DATE: 11/01/96

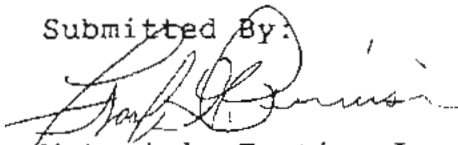
PROJECT: LANDING ROAD BRIDGE
TEST: Freeze Thaw
METHOD: ASTM C666

CERTIFICATE OF ANALYSIS

On October 15, 1996 Materials Testing Inc. started a 25 freeze thaw in accordance with ASTM C666. On three cores numbered 3; 4 & 12. A visual examination was performed after five cycles. The cores appeared to have a loose aggregate paste bond. After 10 cycles the cores showed signs of deterioration. At 15 cycles the cores were in a state of total separation of cement paste from course aggregate. The cement paste broke up into small pieces.

Please see photo for visual.

Submitted By:


Materials Testing Inc.



MATERIALS TESTING INC.

NEW JERSEY DIVISION
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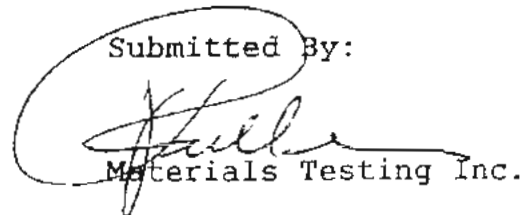
CLIENT: STORCH ENGINEERING
220 RIDGEDALE AVENUE
P.O. BOX 267
FLORHAM PARK, N.J. 07932

REPORT: MTSE-001
DATE: 10/15/96

PROJECT: LANDING ROAD BRIDGE
TEST: Chloride Content
METHOD: ASTM C1218

<u>CORE #1</u>	<u>CHLORIDE CONTENT</u> <u>LBS. PER CUBIC YARD</u>
#01	6.35
#02	6.07
#03	<.81
#04	7.56
#05	1.80
#06A	2.69
#12	6.09

Submitted By:


Materials Testing Inc.



MATERIALS TESTING INC.

NEW JERSEY DIVISION
180 MILL ROAD • EDISON, NEW JERSEY 08817 • (908) 248-3777 • FAX (908) 248-7979

Storch Engineering

Sample #1A & 1B

Core Length 22" (cores in 3 pieces)

Sample #1 showed stone fracture from the side of the core. The sample also showed a poor paste aggregate bond. The maximum coarse aggregate size was 1" trap-rock. Also included a soft aggregate, with high porosity. The grading on these materials were fair. The fine aggregate was crushed sand with some grains angular in shape over all conditions of sample fair.

Sample #2

Core Length 8" (core in 2 pieces)

Sample #2 coarse aggregate size was a maximum of 1" trap rock. The grading on the aggregate was fair. The fine aggregate was a natural sand with some grains angular in shape. The aggregate to paste bond was fair. The overall condition of the sample was found to be fair.

Sample #3

Core Length 12 1/2" (core in 2 pieces)

Sample #3 showed maximum aggregate size of 1 1/2". The granite stone graded was not uniform. The fine sand was natural sand with some grains angular in shape. 1 piece of 3/4" square rebar was noted. Also several other smaller air void less than 1/4" in size. Aggregate to paste bond was fair. The overall condition of core was found to be fair.

Sample #5

Core Length 12 1/2"

Coarse aggregate size 1" maximum size trap rock with a fairly graded and uniform. Fine aggregate crushed natural sand that was fairly graded and uniform. Sample had many vertical cracks through the entire core. The bond between stone and paste was poor. No rebar was present in core the overall condition of sample was poor to fair.

NEW YORK CITY • LONG ISLAND • EDISON, NJ • NEWINGTON, CT • DOVER, DE

"PUT US TO THE TEST"



Storch Engineering

MATERIALS TESTING INC.

NEW JERSEY DIVISION
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Sample #6A

Core Length 17 1/2" (core in 3 pieces)

Coarse aggregate size 1" maximum size trap rock with a fairly graded and uniform. Throughout core. The fine aggregate is crushed igneous sand that was uniformly graded. The sample appeared to be in good overall conditions.

Sample #4

Core Length 13 1/2"

Coarse aggregate size of 1" maximum size crushed granite was fairly graded and uniform. Fine aggregate natural sand with some angular grains and uniformly graded paste to aggregate bond was poor. The overall condition of cores was fair.

Sample #6B

Core Length 17 1/2" (core in 3 pieces)

Coarse aggregate size 1" maximum size trap rock which was fair graded as well as the fine aggregate the core appeared to have a fair aggregate paste bond. The overall condition of core was fair.

Sample #11

Core in 2 pieces

Core #11 had 2 type of concrete one which appears dark gray in color with no coarse aggregate. The maximum aggregate size of 1/4"/ This section of core has noticeable air voids with its overall condition as being fair. The second type of concrete appeared to have 1" maximum size granite rock. The fine aggregate was natural sand with some angular grains. This section of core condition was fair.



MATERIALS TESTING INC.

NEW JERSEY DIVISION
 180 MILL ROAD • EDISON, NEW JERSEY 08817 • (908) 248-3777 • FAX (908) 248-7979

CLIENT: STORCH ENGINEERING

PROJECT: Landing Road Bridge

Date: 10/08/96
 Lab No: ENC-001

TEST REQUIRED:

Identification	#01	#02	#03	#04	#05	#06a	#06b
Length of Core Submitted:	22"	8"	12 1/2"	13 1/2"	12 1/2"	17 1/2"	17 1/2"
Length of Core Prepared:	a 6.825 b 7.400	5.050	7.370	5.640	4.580	5.960	5.760
Diameter:	3.7"	3.7"	3.7"	3.7"	3.7"	3.7"	3.7"
Area	11.34	11.34	11.34	11.34	11.34	11.34	11.34
Ratio H to D:	1.84 2.00	1.36	1.99	1.52	1.24	1.61	1.56
Correction Factor:	.987 1.00	.942	.998	.962	.929	.929	.965
Crushing Load Lbs.:	a 55,000 b 53,000	62,000	48,000	46,000	63,000	53,500	40,000
Corrected Load Lbs.:	a 4.850 4.720	5.470	4.230	4.060	2.910	4.720	3.530
Corrected P.S.I.	4780 4720	5150	4220	3910	2700	4570	3410
Cinder of Base Measurement:							

Location:



MATERIALS TESTING INC.

NEW JERSEY DIVISION
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Page #2
 Report #ENC-001

Identification	#11	#12	#13				
Length of Core Submitted:	10 1/2"	12"	12"				
Length of Core Prepared:	6.720	7.450	7.420				
Diameter:	3.7"	3.7"	3.7"				
Area	11.34	11.34	11.34				
Ratio H to D:	1.82	2.0	2.0				
Correction Factor:	.986	1.00	1.00				
Crushing Load Lbs.:	46,500	44,000	35,000				
Corrected Load Lbs.:	4100	3880	3090				
Corrected P.S.I.	4040	3880	3090				
Cinder of Base Measurement:							

Location:

APPENDIX E

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDEPM.HC9
 Area Type: Other 5-28-96 PM PEAK
 Comment: EXISTING CONDITIONS (1994 DATA)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1		1	<		1	1
Volumes				57		304		1280	61		294	602
PHF or PK15				0.90		0.90		0.90	0.90		0.90	0.90
Lane Width				12.0		12.0		15.0			12.0	12.0
Grade						0			0			0
% Heavy Veh				2		2		2	2		2	2
Parking				(Y/N) N				(Y/N) N			(Y/N) N	
Bus Stops						0			0			0
Con. Peds			0			0			0			0
Ped Button				(Y/N) Y	12.3 s		(Y/N) Y	8.5 s		(Y/N) Y	4.0	
Arr Type				3		3		3		3	3	
RTOR Vols						0			0			0
Lost Time				3.00		3.00		3.00	3.00		3.00	3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left			
Thru					Thru	*		
Right					Right	*		
Peds					Peds			
WB Left	*				SB Left		*	
Thru					Thru	*	*	
Right		*			Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right		*	
Green	8.0A				Green	48.0A	22.0A	
Yellow/AR	4.0				Yellow/AR	4.0	4.0	
Cycle Length:	90 secs	Phase combination order: #1 #5 #6						

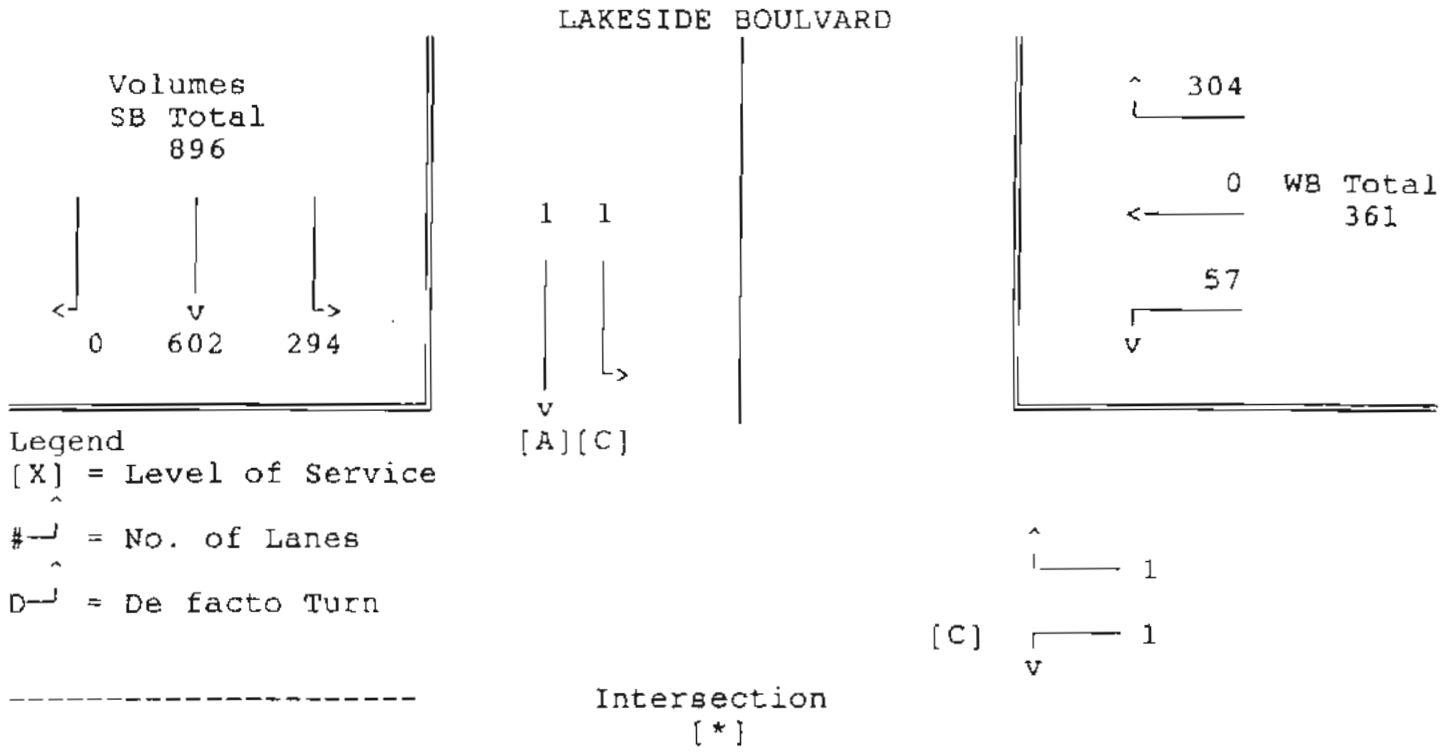
Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
WB L	177	1770	0.356	0.100	25.0	C		16.2	C
R	616	1583	0.549	0.389	14.6	B			
NB TR	996	1830	1.495	0.544	*	*		*	*
SB L	452	1770	0.723	0.256	23.6	C		8.7	B
T	1552	1863	0.431	0.833	1.4	A			

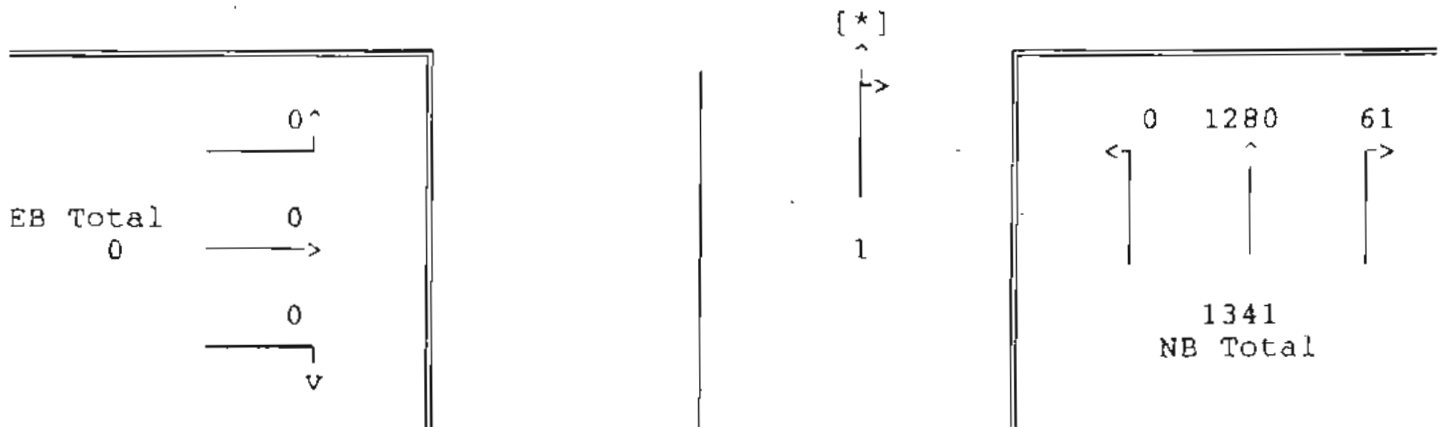
Intersection Delay = * (sec/veh) Intersection LOS = *
 (g/C)*(V/c) is greater than one. Calculation of DI is infeasible.

INTERSECTION DIAGRAM

Intersection: LANDING RD/MT ARLI'G and LAKESIDE BOULEVARD
 Time period: PM PEAK



LANDING RD/MT ARLI'G



BRIDGE

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDEAM.HC9
 Area Type: Other 5-28-96 AM PEAK
 Comment: EXISTING CONDITIONS (1994 DATA)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1		2	<		1	1
Volumes				100		137		211	137		177	1253
PHF or PK15				0.90		0.90		0.90	0.90		0.90	0.90
Lane Width				12.0		12.0		12.0			12.0	12.0
Grade						0			0			0
% Heavy Veh				2		2		2	2		2	2
Parking				(Y/N) N				(Y/N) N			(Y/N) N	
Bus Stops						0			0			0
Con. Peds			0			0			0			0
Ped Button				(Y/N) Y	14.5 s		(Y/N) Y	8.5 s		(Y/N) Y	4.0	
Arr Type				3		3		3			3	3
RTOR Vols						0			0			0
Lost Time				3.00		3.00		3.00	3.00		3.00	3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left								
Thru						*		
Right						*		
Peds								
WB Left	*							
Thru						*	*	
Right		*						
Peds								
NB Right								
SB Right								*
Green	12.0A				54.0A	12.0A		
Yellow/AR	4.0				4.0	4.0		
Cycle Length:	90 secs							
Phase combination order:	#1	#5	#6					

Intersection Performance Summary

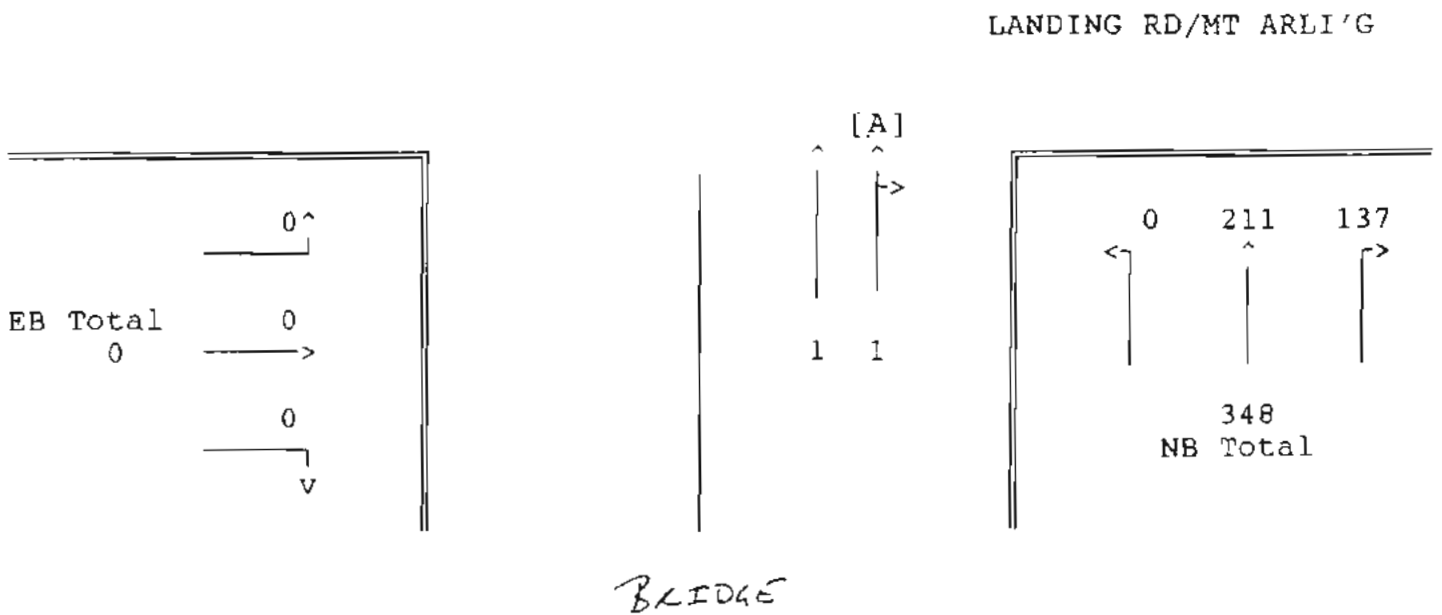
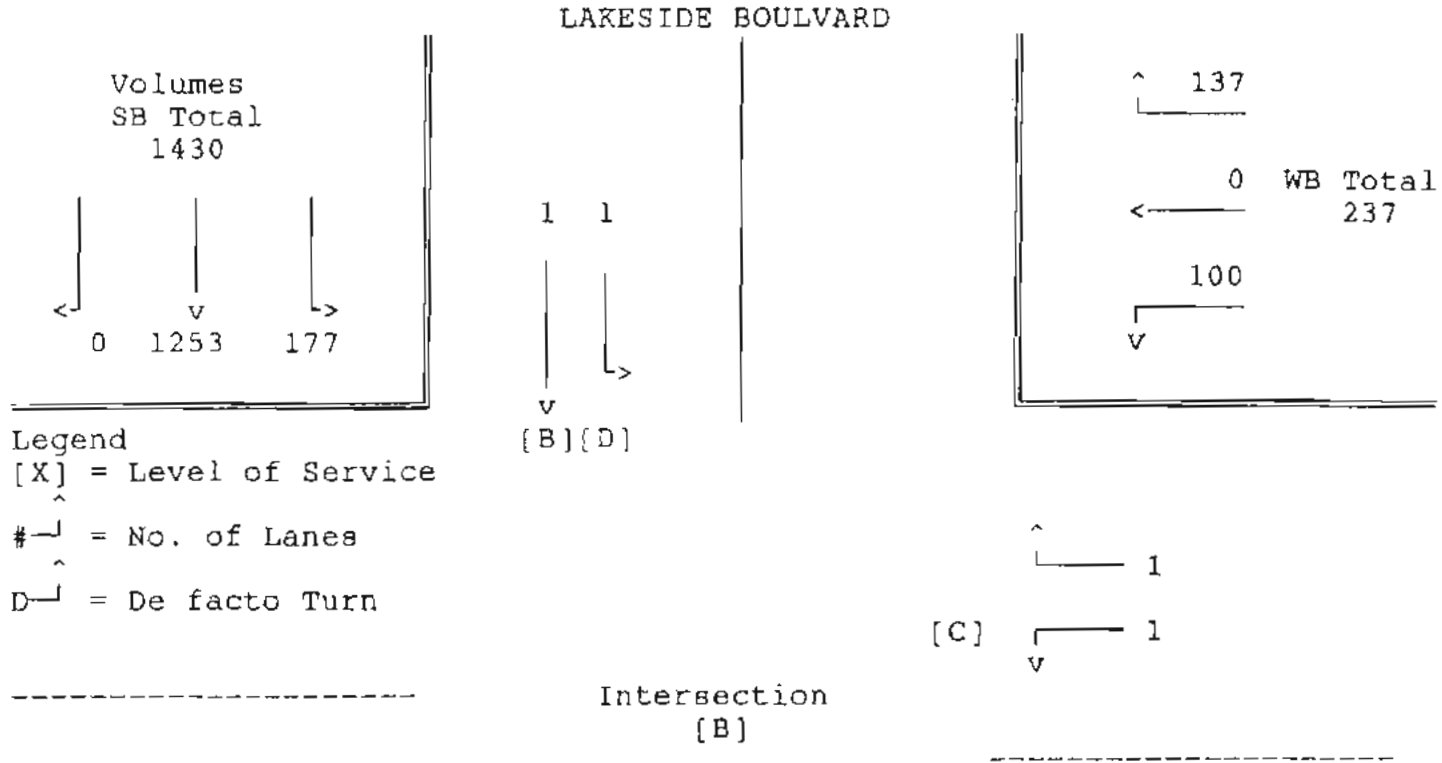
	Lane	Group:	Adj Sat			g/C	Delay	LOS	Approach:		
			Mvmts	Cap	Flow				Ratio	Ratio	Delay
WB	L		256		1770	0.434	0.144	23.5	C	18.5	C
	R		510		1583	0.298	0.322	14.9	B		
NB	TR		2142		3505	0.189	0.611	5.0	A	5.0	A
SB	L		256		1770	0.771	0.144	33.0	D	17.0	C
	T		1470		1863	0.947	0.789	14.7	B		

Intersection Delay = 15.0 sec/veh Intersection LOS = B

Lost Time/Cycle, L = 6.0 sec Critical v/c(x) = 0.868

INTERSECTION DIAGRAM

Intersection: LANDING RD/MT ARLI'G and LAKESIDE BOULEVARD
 Time period: AM PEAK



Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDEPM.HC9
 Area Type: Other 5-28-96 PM PEAK
 Comment: EXISTING CONDITIONS (1994 DATA)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1	2	<		1		1
Volumes				57		304	1280		61	294		602
PHF or PK15				0.90		0.90	0.90		0.90	0.90		0.90
Lane Width				12.0		12.0	12.0			12.0		12.0
Grade						0			0			0
% Heavy Veh				2		2	2		2	2		2
Parking				(Y/N)	N		(Y/N)	N		(Y/N)	N	
Bus Stops						0			0			0
Con. Peds			0			0			0			0
Ped Button				(Y/N)	Y	14.5 s	(Y/N)	Y	8.5 s	(Y/N)	Y	4.0
Arr Type				3		3	3		3	3		3
RTOR Vols						0			0			0
Lost Time				3.00		3.00	3.00		3.00	3.00		3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left			
Thru					Thru	*		
Right					Right	*		
Peds					Peds			
WB Left		*			SB Left		*	
Thru					Thru	*	*	
Right		*			Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right		*	
Green		8.0A			Green	48.0A	22.0A	
Yellow/AR		4.0			Yellow/AR	4.0	4.0	
Cycle Length:	90 secs Phase combination order: #1 #5 #6							

Intersection Performance Summary

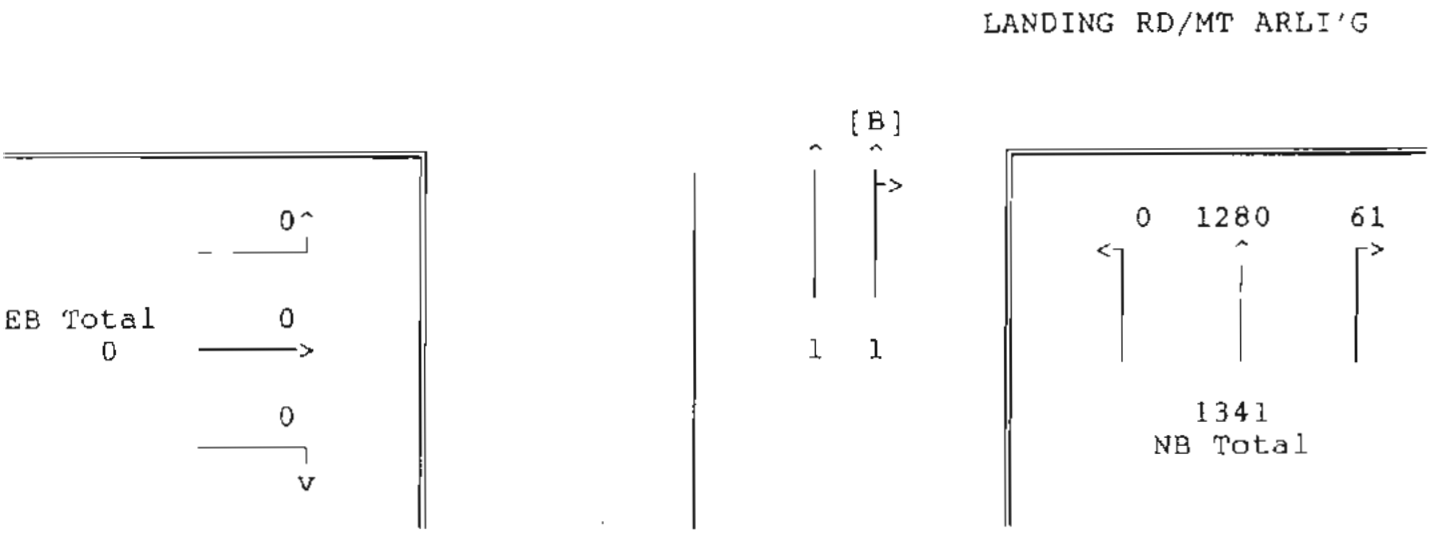
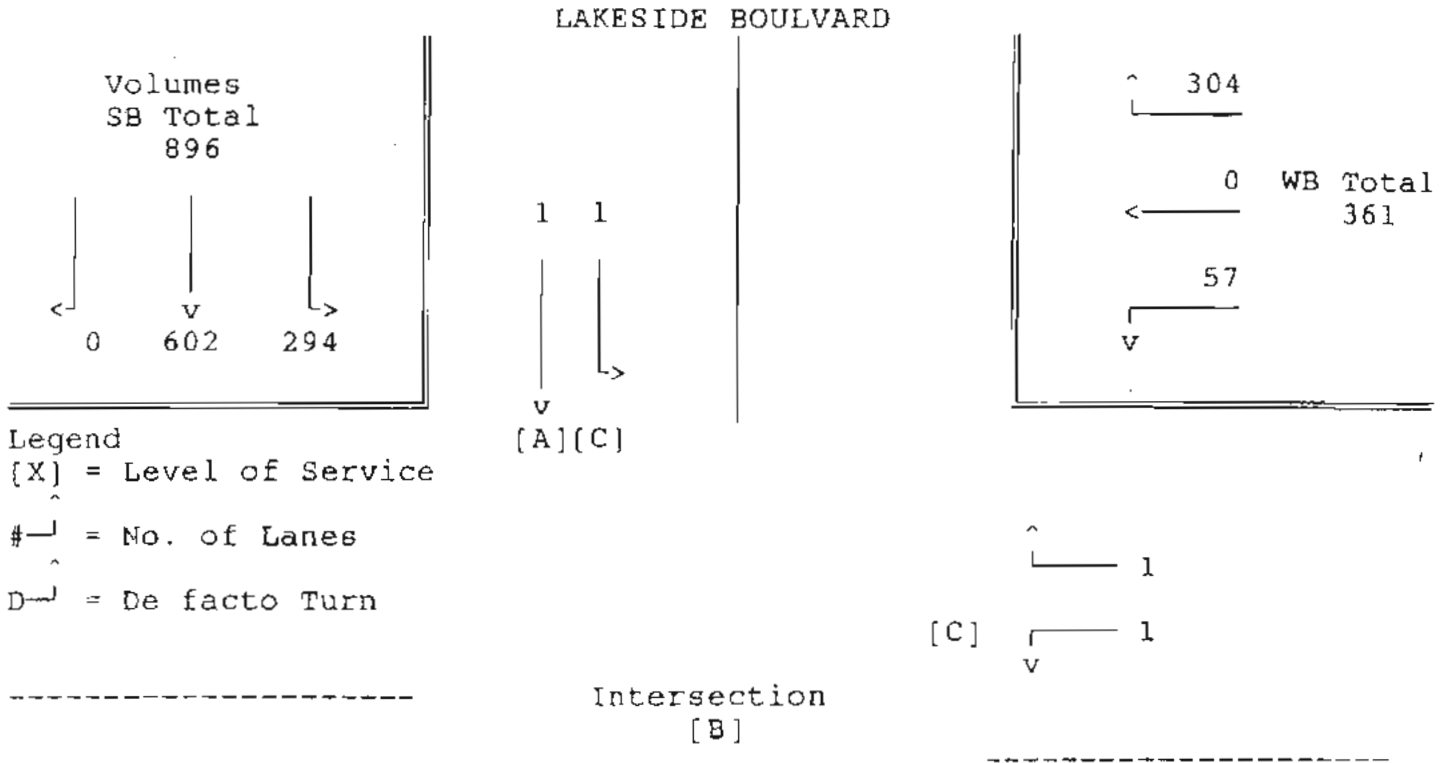
	Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:		
								Mvmte	Cap	Flow
WB	L		177	1770	0.356	0.100	25.0	C	16.2	C
	R		616	1583	0.549	0.389	14.6	B		
NB	TR		2014	3700	0.776	0.544	11.8	B	11.8	B
SB	L		452	1770	0.723	0.256	23.6	C	8.7	B
	T		1552	1863	0.431	0.833	1.4	A		

Intersection Delay = 11.4 sec/veh Intersection LOS = B

Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.714

INTERSECTION DIAGRAM

Intersection: LANDING RD/MT ARLI'G and LAKESIDE BOULVARD
 Time period: PM PEAK



BRIDGE

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDAM04.HC9
 Area Type: Other 5-28-96 AM PEAK
 Comment: 2004 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1	2	<		1	1	
Volumes				128		175	270	175		227	1604	
PHF or PK15				0.90		0.90	0.90	0.90		0.90	0.90	
Lane Width				12.0		12.0	12.0			12.0	12.0	
Grade						0					0	
% Heavy Veh				2		2	2	2		2	2	
Parking				(Y/N) N			(Y/N) N			(Y/N) N		
Bus Stops						0				0		0
Con. Peds			0			0				0		0
Ped Button				(Y/N) Y	14.5 s		(Y/N) Y	8.5 s		(Y/N) Y	4.0	
Arr Type				3		3	3			3	3	
RTOR Vols						0				0		0
Lost Time				3.00		3.00	3.00	3.00		3.00	3.00	
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left			
Thru					Thru	*		
Right					Right	*		
Peds					Peds			
WB Left		*			SB Left		*	
Thru					Thru	*	*	
Right		*			Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right		*	
Green	10.0A				Green	18.0A	60.0A	
Yellow/AR	4.0				Yellow/AR	4.0	4.0	
Cycle Length: 100 secs Phase combination order: #1 #5 #6								

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
WB	L	195	1770	0.729	0.110	36.5	D	16.8	C
	R	1187	1583	0.163	0.750	2.3	A		
NB	TR	666	3506	0.779	0.190	29.0	D	29.0	D
SB	L	1080	1770	0.233	0.610	5.7	B	*	*
	T	1546	1863	1.152	0.830	*	*		

Intersection Delay = * (sec/veh) Intersection LOS = *
 (g/C)*(V/c) is greater than one. Calculation of D1 is infeasible.

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDPM04.HC9
 Area Type: Other 5-28-96 PM PEAK
 Comment: 2004 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1	2	<		1		1
Volumes				73		389	1638	78		376		771
PHF or PK15				0.90		0.90	0.90	0.90		0.90		0.90
Lane Width				12.0		12.0	12.0			12.0		12.0
Grade						0						0
% Heavy Veh				2		2	2	2		2		2
Parking				(Y/N)	N		(Y/N)	N		(Y/N)		N
Bus Stops						0				0		0
Con. Peds			0			0				0		0
Ped Button				(Y/N)	Y	14.5 s	(Y/N)	Y	8.5 s	(Y/N)	Y	4.0
Arr Type				3		3		3		3		3
RTOR Vols						0				0		0
Lost Time				3.00		3.00	3.00	3.00		3.00		3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left			
Thru					Thru	*		
Right					Right	*		
Peds					Peds			
WB Left		*			SB Left		*	
Thru					Thru	*	*	
Right		*			Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right		*	
Green		10.0A			Green	54.0A	24.0A	
Yellow/AR		4.0			Yellow/AR	4.0	4.0	
Cycle Length: 100 secs Phase combination order: #1 #5 #6								

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
WB	L	195	1770	0.416	0.110	27.7	D	20.3	C
	R	617	1583	0.700	0.390	19.0	C		
NB	TR	2034	3698	0.984	0.550	26.6	D	26.6	D
SB	L	442	1770	0.945	0.250	45.0	E	16.2	C
	T	1546	1863	0.554	0.830	2.1	A		

Intersection Delay = 22.2 sec/veh Intersection LOS = C
 Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.905

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDAM14.HC9
 Area Type: Other 5-28-96 AM PEAK
 Comment: 2014 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1		2	<		1	1
Volumes				164		225		346	225		290	2053
PHF or PK15				0.90		0.90		0.90	0.90		0.90	0.90
Lane Width				12.0		12.0		12.0			12.0	12.0
Grade						0			0			0
% Heavy Veh				2		2		2	2		2	2
Parking				(Y/N) N				(Y/N) N			(Y/N) N	
Bus Stops						0			0			0
Con. Peds			0			0			0			0
Ped Button				(Y/N) Y	14.5	6		(Y/N) Y	8.5	6	(Y/N) Y	4.0
Arr Type				3		3		3			3	3
RTOR Vols						0			0			0
Lost Time				3.00		3.00		3.00	3.00		3.00	3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left			
Thru					Thru	*		
Right					Right	*		
Peds					Peds			
WB Left	*				SB Left		*	
Thru					Thru	*	*	
Right		*			Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right		*	
Green	16.0A				Green	27.0A	65.0A	
Yellow/AR	4.0				Yellow/AR	4.0	4.0	

Cycle Length: 120 secs Phase combination order: #1 #5 #6

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
WB L	251	1770	0.726	0.142	38.6	D	18.4	C	
R	1134	1583	0.220	0.717	3.7	A			
NB TR	819	3508	0.814	0.233	32.5	D	32.5	D	
SB L	974	1770	0.331	0.550	9.7	B	*	*	
T	1506	1863	1.515	0.808	*	*			

Intersection Delay = * (sec/veh) Intersection LOS = *
 (g/C)*(V/c) is greater than one. Calculation of D1 is infeasible.

Streets: (E-W) LANDING RD/MT ARLI'G (N-S) LAKESIDE BOULEVARD
 Analyst: ST File Name: LANDPM14.HC9
 Area Type: Other 5-28-96 PM PEAK
 Comment: 2014 CONDITIONS (1994 DATA expanded @ 2.5% compounded rate)

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes				1		1	2	<		1		1
Volumes				93		498	2097	100		482		987
PHF or PK15				0.90		0.90	0.90	0.90		0.90		0.90
Lane Width				12.0		12.0	12.0			12.0		12.0
Grade						0				0		0
% Heavy Veh				2		2	2	2		2		2
Parking				(Y/N)	N		(Y/N)	N		(Y/N)	N	
Bus Stops						0				0		0
Con. Peds			0			0				0		0
Ped Button				(Y/N)	Y	14.5 s	(Y/N)	Y	8.5 s	(Y/N)	Y	4.0
Arr Type				3		3	3			3		3
RTOR Vols						0				0		0
Lost Time				3.00		3.00	3.00	3.00		3.00		3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left			
Thru					Thru	*		
Right					Right	*		
Peds					Peds			
WB Left		*			SB Left		*	
Thru					Thru	*	*	
Right		*			Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right		*	
Green		14.0A			Green	66.0A	28.0A	
Yellow/AR		4.0			Yellow/AR	4.0	4.0	

Cycle Length: 120 secs Phase combination order: #1 #5 #6

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
WB	L	221	1770	0.466	0.125	32.7	D	32.8	D
	R	620	1583	0.892	0.392	32.8	D		
NB	TR	2065	3698	1.241	0.558	*	*	*	*
SB	L	428	1770	1.253	0.242	*	*	*	*
	T	1537	1863	0.714	0.825	4.0	A		

Intersection Delay = * (sec/veh) Intersection LOS = *
 (g/C)*(V/c) is greater than one. Calculation of D1 is infeasible.

APPENDIX F

APPENDIX G

Clearing	100,000
Earthwork	10,000
Pavement	30,000
Culverts	0
Bridges	2,300,000
Drainage	10,000
Incidental Items	10,000
Landscape	2,500
Noise Abatement	0
General Items	20,000

PROJECT SUBTOTAL = 2,482,500

	% of Proj. Subtotal	x	Proj. Subtotal	=
Lighting, Traffic Stripes Signs and Delineators	3			74,475
Maintenance of Traffic	x 1.5			37,238
Training	1			24,825
Mobilization & Project Schedule	11			273,075
Construction Layout	x 0			

DURATION OF CONSTRUCTION IN YEARS 2

PROJECT TOTAL = 2,892,113

CONTINGENCIES

$$\text{Project Total} \times (1 + \overset{3\%}{C}) = 2,978,876$$
 C = Contingencies from table on Page Nos. 2 & 3
 P.T.W.C. = Project Total with Contingencies

2-14

1995

Classification No. 2 - RECONSTRUCTION, WIDENING & DUALIZATION

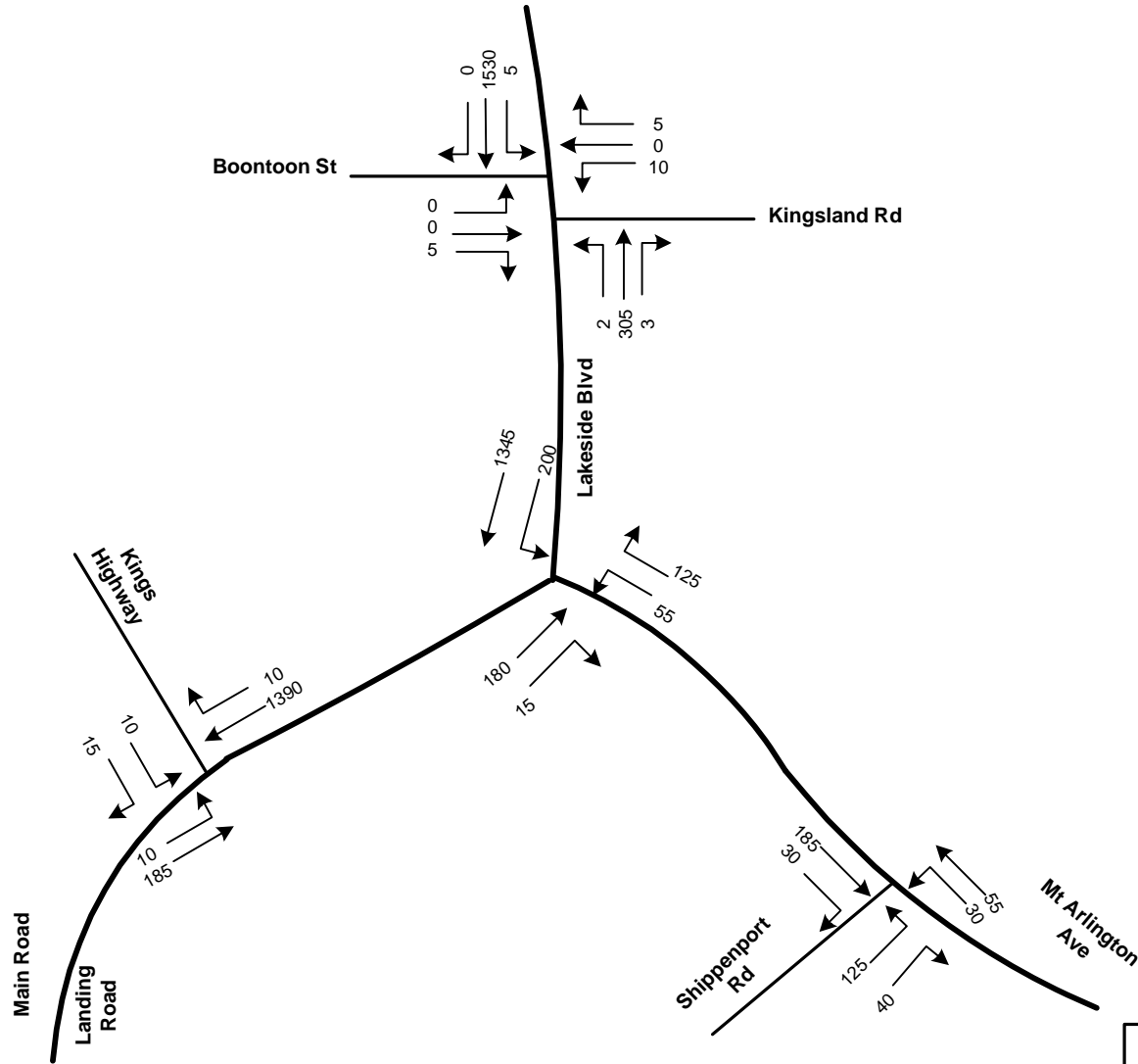
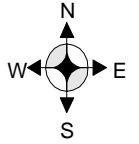
ESCALATION

$$\text{P.T.W.C.} \times \frac{1 + [0.01(Y+1)(Y-2)]}{1} = 3,098,031$$
 Y = Number of Years until midpoint of construction. If midpoint is less than 2 years no escalation is required. $\leftarrow y=3$
 Construction Cost for Preliminary Estimate

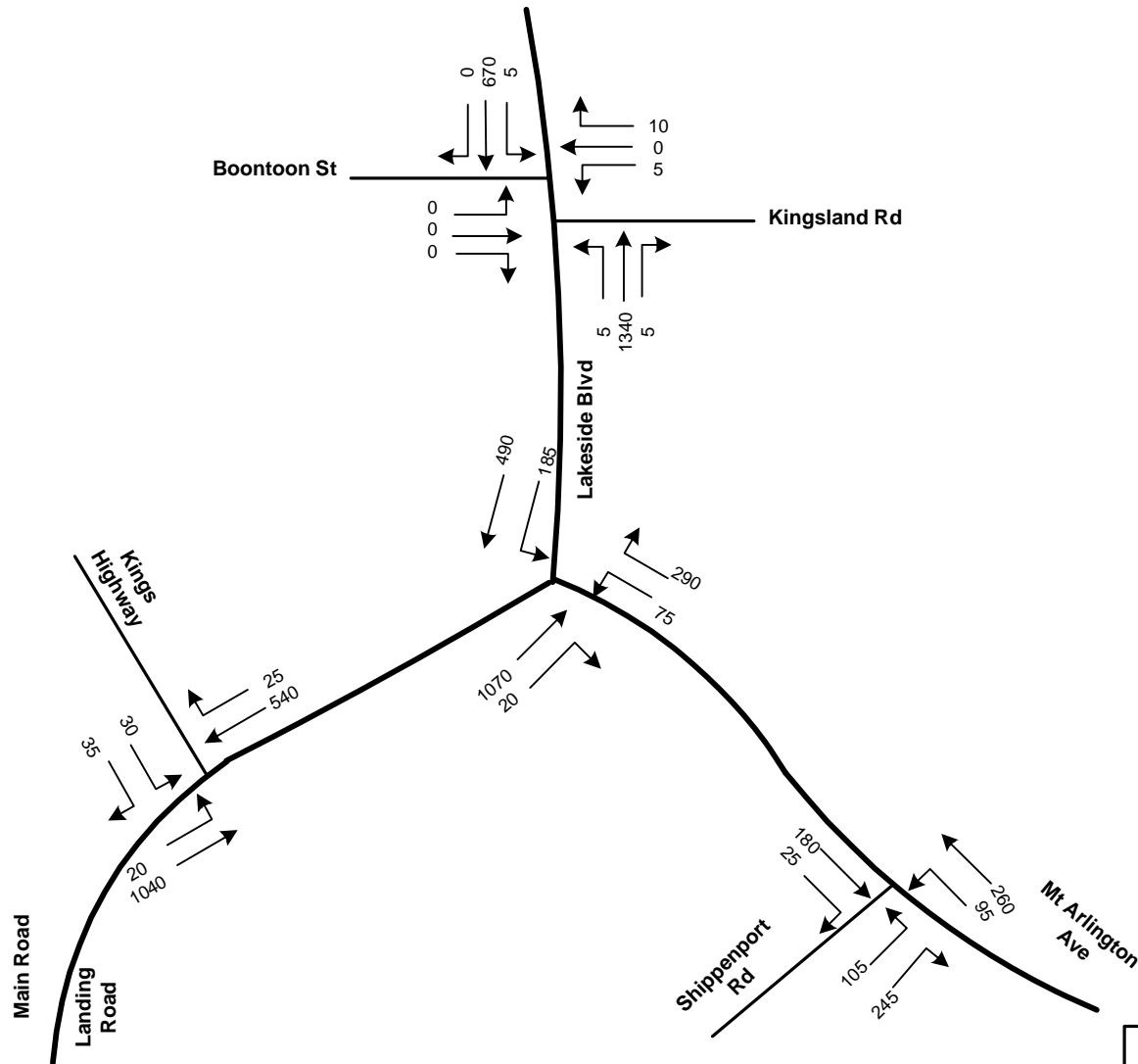
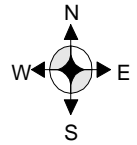
UTILITY RELOCATION COST

APPENDIX – H

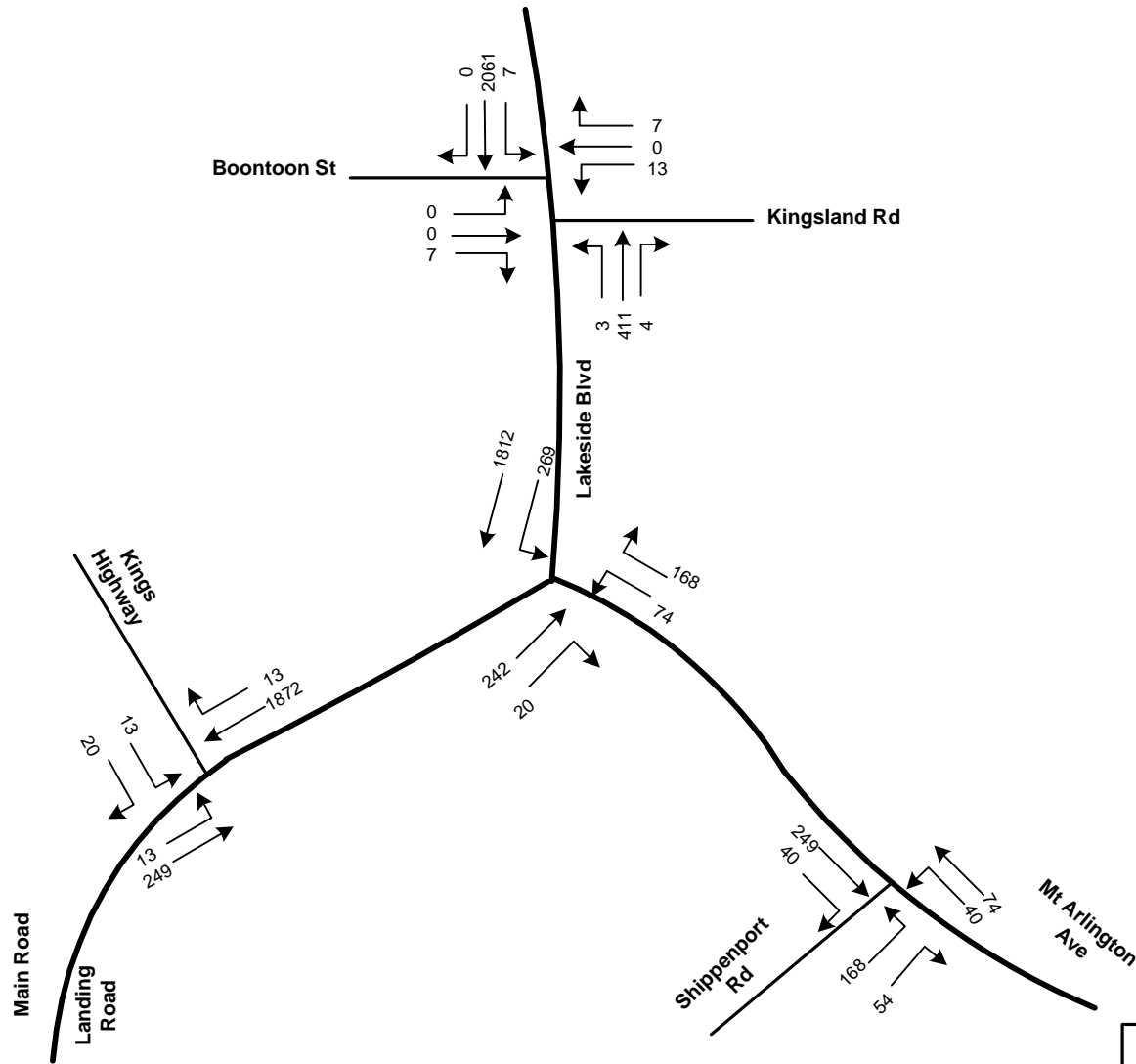
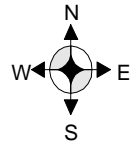
TRAFFIC STUDY 2011



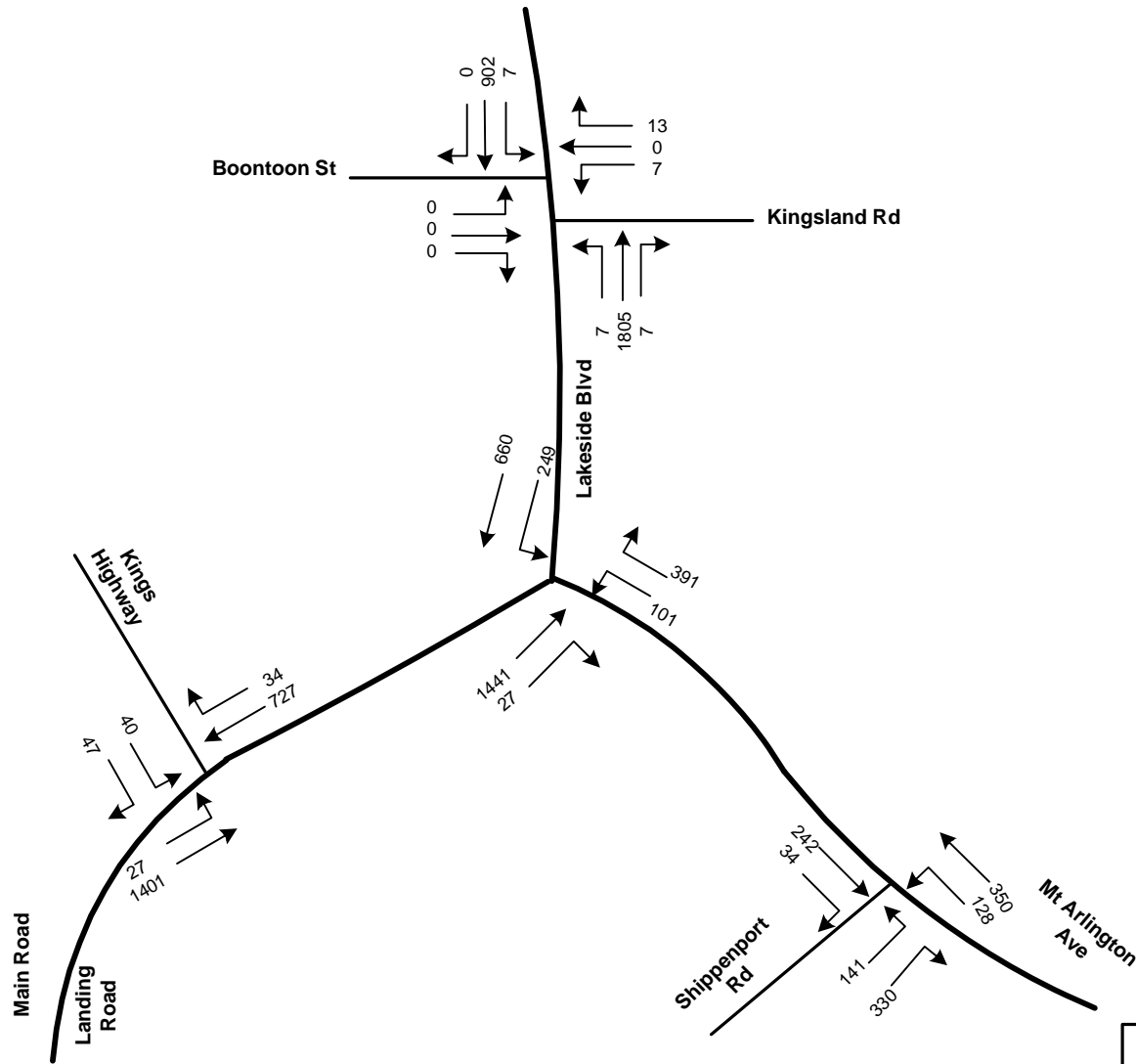
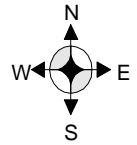
Flow Map for Existing Traffic Volume AM Peak Hour 7:00AM-8:00AM
Year 2011
Landing Road Bidge, Landing, NJ
Prepared By: Associated Consulting, Inc
Not to Scale



Flow Map for Existing Traffic Volume PM Peak Hour 5:00PM-6:00AM
Year 2011
Landing Road Bidge, Landing, NJ
Prepared By: Associated Consulting, Inc
Not to Scale



Flow Map for Future Traffic Volume AM Peak Hour 7:00AM-8:00AM
Year 2031
Landing Road Bidge, Landing, NJ
Prepared By: Associated Consulting, Inc
Not to Scale



Flow Map for Future Traffic Volume PM Peak Hour 5:00PM-6:00AM
Year 2031
Landing Road Bidge, Landing, NJ
Prepared By: Associated Consulting, Inc
Not to Scale

AM Peak Hour Volume Summary
Existing : Year 2011
Reconstruction of Landing Road Bridge, New Jersey

Volume													
S. No	Intersection	NB			SB			EB			WB		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	10	185			1390	10	10		15			
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.		180	15	200	1345					55		125
3	Mt. Arlington Blvd and Shippenport Rd.	125		40					185	30	30	55	
4	Lakeside Blvd. and Kingsland Rd/Canal St.	2	305	3	5	1530	0	0	0	5	10	0	5

PM Peak Hour Volume Summary
Existing : Year 2011
Reconstruction of Landing Road Bridge, New Jersey

Volume													
S. No	Intersection	NB			SB			EB			WB		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	20	1040			540	25	30		35			
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.		1070	20	185	490					75		290
3	Mt. Arlington Blvd and Shippenport Rd.	105		245					180	25	95	260	
4	Lakeside Blvd. and Kingsland Rd/Canal St.	5	1340	5	5	670	0	0	0	0	5	0	10

AM Peak Hour Volume Summary
(No Build : Year 2031)
Reconstruction of Landing Road Bridge, New Jersey

Volume													
S. No	Intersection	NB			SB			EB			WB		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	13	249	0	0	1872	13	13	0	20	0	0	0
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.	0	242	20	269	1812	0	0	0	0	74	0	168
3	Mt. Arlington Blvd and Shippenport Rd.	168	0	54	0	0	0	0	249	40	40	74	0
4	Lakeside Blvd. and Kingsland Rd/Canal St.	3	411	4	7	2061	0	0	0	7	13	0	7

PM Peak Hour Volume Summary
(No Build : Year 2031)
Reconstruction of Landing Road Bridge, New Jersey

Volume													
S. No	Intersection	NB			SB			EB			WB		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
1	Landing Rd/Main St. and Kings Highway	27	1401	0	0	727	34	40	0	47	0	0	0
2	Landing Rd Bridge/Lakeside Blvd. and Mt. Arlington Blvd.	0	1441	27	249	660	0	0	0	0	101	0	391
3	Mt. Arlington Blvd and Shippenport Rd.	141	0	330	0	0	0	0	242	34	128	350	0
4	Lakeside Blvd. and Kingsland Rd/Canal St.	7	1805	7	7	902	0	0	0	0	7	0	13

APPENDIX I

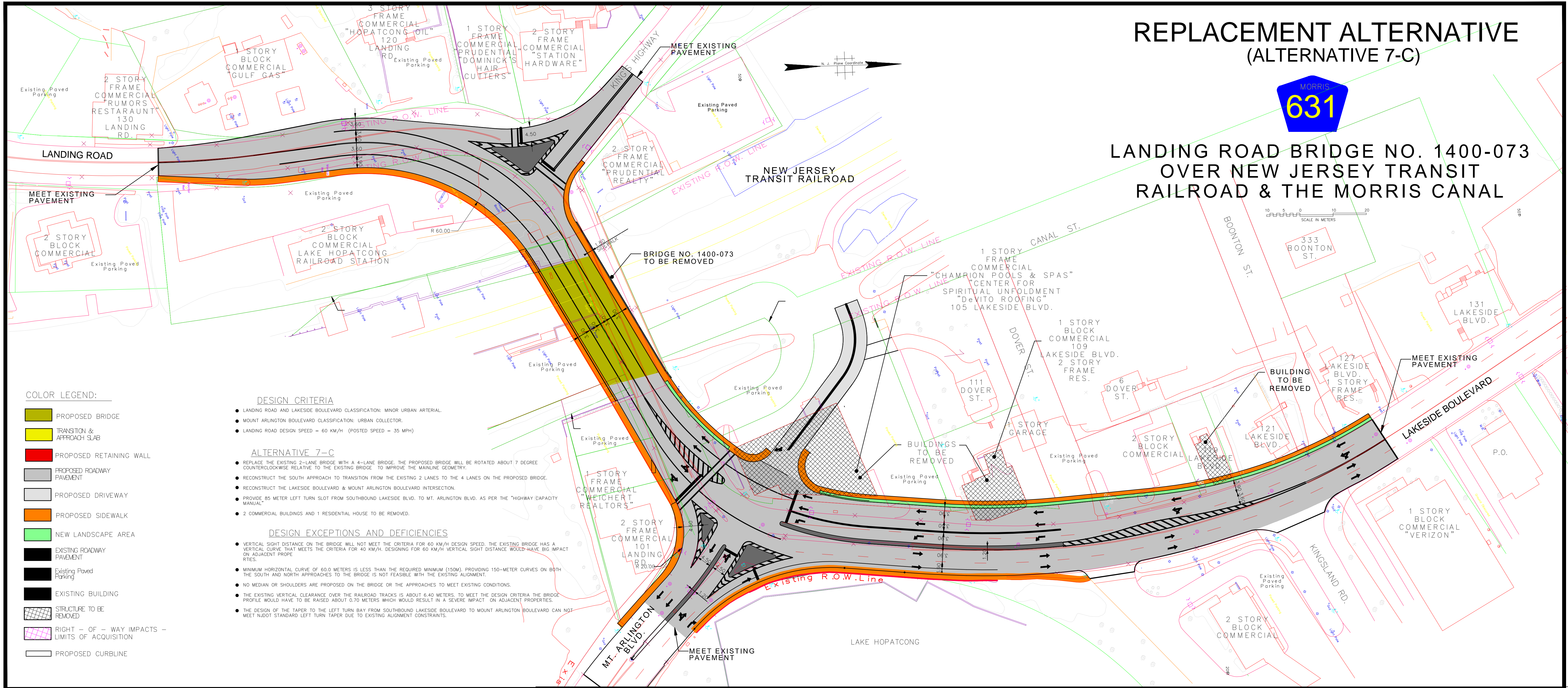
SCHEMATIC OF ALTERNATE 7C WITH SIDEWALKS ON BOTH SIDES OF BRIDGE

REPLACEMENT ALTERNATIVE (ALTERNATIVE 7-C)



LANDING ROAD BRIDGE NO. 1400-073 OVER NEW JERSEY TRANSIT RAILROAD & THE MORRIS CANAL

SCALE IN METERS



- COLOR LEGEND:**
- PROPOSED BRIDGE
 - TRANSITION & APPROACH SLAB
 - PROPOSED RETAINING WALL
 - PROPOSED ROADWAY PAVEMENT
 - PROPOSED DRIVEWAY
 - PROPOSED SIDEWALK
 - NEW LANDSCAPE AREA
 - EXISTING ROADWAY PAVEMENT
 - EXISTING PAVED PARKING
 - EXISTING BUILDING
 - STRUCTURE TO BE REMOVED
 - RIGHT-OF-WAY IMPACTS - LIMITS OF ACQUISITION
 - PROPOSED CURBLINE

DESIGN CRITERIA

- LANDING ROAD AND LAKESIDE BOULEVARD CLASSIFICATION: MINOR URBAN ARTERIAL.
- MOUNT ARLINGTON BOULEVARD CLASSIFICATION: URBAN COLLECTOR.
- LANDING ROAD DESIGN SPEED = 60 KM/H (POSTED SPEED = 35 MPH)

ALTERNATIVE 7-C

- REPLACE THE EXISTING 2-LANE BRIDGE WITH A 4-LANE BRIDGE. THE PROPOSED BRIDGE WILL BE ROTATED ABOUT 7 DEGREE COUNTERCLOCKWISE RELATIVE TO THE EXISTING BRIDGE TO IMPROVE THE MAINLINE GEOMETRY.
- RECONSTRUCT THE SOUTH APPROACH TO TRANSITION FROM THE EXISTING 2 LANES TO THE 4 LANES ON THE PROPOSED BRIDGE.
- RECONSTRUCT THE LAKESIDE BOULEVARD & MOUNT ARLINGTON BOULEVARD INTERSECTION.
- PROVIDE 85 METER LEFT TURN SLOT FROM SOUTHBOUND LAKESIDE BLVD. TO MT. ARLINGTON BLVD. AS PER THE "HIGHWAY CAPACITY MANUAL"
- 2 COMMERCIAL BUILDINGS AND 1 RESIDENTIAL HOUSE TO BE REMOVED.

DESIGN EXCEPTIONS AND DEFICIENCIES

- VERTICAL SIGHT DISTANCE ON THE BRIDGE WILL NOT MEET THE CRITERIA FOR 60 KM/H DESIGN SPEED. THE EXISTING BRIDGE HAS A VERTICAL CURVE THAT MEETS THE CRITERIA FOR 40 KM/H. DESIGNING FOR 60 KM/H VERTICAL SIGHT DISTANCE WOULD HAVE BIG IMPACT ON ADJACENT PROPERTIES.
- MINIMUM HORIZONTAL CURVE OF 60.0 METERS IS LESS THAN THE REQUIRED MINIMUM (150M). PROVIDING 150-METER CURVES ON BOTH THE SOUTH AND NORTH APPROACHES TO THE BRIDGE IS NOT FEASIBLE WITH THE EXISTING ALIGNMENT.
- NO MEDIAN OR SHOULDERS ARE PROPOSED ON THE BRIDGE OR THE APPROACHES TO MEET EXISTING CONDITIONS.
- THE EXISTING VERTICAL CLEARANCE OVER THE RAILROAD TRACKS IS ABOUT 6.40 METERS. TO MEET THE DESIGN CRITERIA THE BRIDGE PROFILE WOULD HAVE TO BE RAISED ABOUT 0.70 METERS WHICH WOULD RESULT IN A SEVERE IMPACT ON ADJACENT PROPERTIES.
- THE DESIGN OF THE TAPER TO THE LEFT TURN BAY FROM SOUTHBOUND LAKESIDE BOULEVARD TO MOUNT ARLINGTON BOULEVARD CAN NOT MEET NJDOT STANDARD LEFT TURN TAPER DUE TO EXISTING ALIGNMENT CONSTRAINTS.